



AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY AGENDA

Riverside County Administrative Center
4080 Lemon Street, 1st Floor Board Chambers
Riverside, California

Thursday 9:30 A.M., September 12, 2019

CHAIR

Steve Manos
Lake Elsinore

VICE CHAIR

Russell Betts
Desert Hot Springs

COMMISSIONERS

Arthur Butler
Riverside

John Lyon
Riverside

Steven Stewart
Palm Springs

Richard Stewart
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Gary Youmans
Temecula

STAFF

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Simon A. Housman

John Guerin
Paul Rull
Barbara Santos

County Administrative Center
4080 Lemon St, 14th Floor
Riverside, CA 92501
(951) 955-5132

www.rcaluc.org

NOTE: If you wish to speak, please complete a "SPEAKER IDENTIFICATION FORM" and give it to the Secretary. The purpose of the public hearing is to allow interested parties to express their concerns. Comments shall be limited to 5 minutes and to matters relevant to the item under consideration. Please do not repeat information already given. If you have no additional information, but wish to be on record, simply give your name and address and state that you agree with the previous speaker(s). Also please be aware that the indicated staff recommendation shown below may differ from that presented to the Commission during the public hearing.

Non-exempt materials related to an item on this agenda submitted to the Airport Land Use Commission or its staff after distribution of the agenda packet are available for public inspection in the Airport Land Use Commission's office located at 4080 Lemon Street, 14th Floor, Riverside, CA 92501 during normal business hours.

Live Streaming of the meeting will be available during the meeting on our website at www.rcaluc.org.

In compliance with the Americans with Disabilities Act, if any accommodations are needed, please contact Barbara Santos at (951) 955-5132 or E-mail at basantos@rivco.org. Request should be made at least 48 hours or as soon as possible prior to the scheduled meeting.

1.0 INTRODUCTIONS

1.1 CALL TO ORDER

1.2 SALUTE TO FLAG

1.3 ROLL CALL

2.0 PUBLIC HEARING: CONTINUED ITEMS

None

3.0 PUBLIC HEARING: NEW ITEMS

MARCH AIR RESERVE BASE

- 3.1 ZAP1376MA19 – Pacific Communities Builder, Inc. (Representative: Ronald Freeman) – County of Riverside Case Nos. CZ1800012 (Change of Zone), TTM37358 (Tentative Tract Map). A proposal to divide 45.6 gross acres located northerly of Mapes Road, westerly of Antelope Road, and easterly of Sherman Road into 154 single family residential lots, and change the zoning of the site from Rural Residential (R-R) and One-Family Dwelling (R-1) to One-Family Dwelling (R-1) (Airport Compatibility Zones C2 and D of the March Air Reserve Base/Inland Port Airport Influence Area). Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rivco.org

Staff Recommendation: CONSISTENT

MARCH AIR RESERVE BASE

3.2 ZAP1371MA19 – Sares-Regis Group/SRG Perris, LP (Representative: EPD Solutions)
– County of Riverside Case No. PPT190011 (Plot Plan). A proposal to construct two industrial manufacturing buildings with mezzanines totaling 710,736 square feet on a combined total 35.76 acres located northerly of Oleander Avenue, westerly of Decker Road, southerly of Nandina Avenue, and easterly of Day Street. The applicant also proposes rooftop solar panels totaling 60,000 square feet (30,000 square feet on each building) (Airport Compatibility Zone C2 High Terrain Zone of the March Air Reserve Base/Inland Port Airport Influence Area). Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rivco.org

Staff Recommendation: CONSISTENT

4.0 **ADMINISTRATIVE ITEMS**

- 4.1 Director's Approvals
- 4.2 Strategic Vision Chart

5.0 **APPROVAL OF MINUTES**

August 8, 2019

6.0 **ORAL COMMUNICATION ON ANY MATTER NOT ON THE AGENDA**

7.0 **COMMISSIONER'S COMMENTS**

**COUNTY OF RIVERSIDE
AIRPORT LAND USE COMMISSION**

STAFF REPORT

AGENDA ITEM: 3.1

HEARING DATE: September 12, 2019

CASE NUMBER: ZAP1376MA19 – Pacific Communities Builder, Inc.
(Representative: Ronald Freeman)

APPROVING JURISDICTION: County of Riverside

JURISDICTION CASE NO: CZ1800012 (Change of Zone), TTM37358 (Tentative Tract Map)

MAJOR ISSUES: The project is located partially within the 60-65 CNEL contour range from aircraft noise as identified in the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan. The project's proposed single family residences would be affected by aircraft generated noise. However, pursuant to Policy 2.3 (a) and (b), 65 dB CNEL exterior noise levels are considered normally acceptable for new residential land uses provided that interior noise levels are attenuated to a maximum of 40 dB CNEL. Therefore, recommended conditions require that noise attenuation measures be incorporated into the design of the single family residences to such extent as may be required to ensure that interior noise levels from aircraft operations are at or below 40 CNEL.

RECOMMENDATION: Staff recommends that the Commission find the proposed Change of Zone CONSISTENT with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, and find the proposed Tentative Tract Map CONSISTENT, subject to the conditions included herein.

PROJECT DESCRIPTION: The applicant proposes to divide 45.6 gross acres into 154 single family residential lots, and change the zoning of the site from Rural Residential (R-R) and One-Family Dwelling (R-1) to One-Family Dwelling (R-1).

PROJECT LOCATION: The site is located northerly of Mapes Road, westerly of Antelope Road, and easterly of Sherman Road, approximately 42,600 feet southeasterly of Runway 14-32 at March Air Reserve Base, and approximately 11,100 feet easterly of Runway 15-33 at Perris Valley Airport.

LAND USE PLAN: 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan

a. Airport Influence Area: March Air Reserve Base

b. Land Use Policy: Zones C2 and D

c. Noise Levels: Partially within the 60-65 CNEL contour from aircraft

BACKGROUND:

Residential Density: Pursuant to the 2014 March ALUCP, the project site is located within Compatibility Zones C2 and D. Zone C2 restricts residential density to a maximum of 6.0 dwelling units per acre, and Zone D does not restrict residential density at all. Approximately 38.62 acres of the site are located within Zone C2, and 7.65 acres are located within Zone D.

The proposed project of 154 single family residential lots on 45.6 gross acres would result in a density of 3.3 dwelling units per acre, which is consistent with the residential density criteria referenced above. If we calculate the project's density based on each compatibility zone, Zone C2 (137 lots on 38.62 acres) would have a density of 3.5 dwelling units per acre, and Zone D (17 lots on 7.65 acres) would have a density of 2.2 dwelling units per acre, both of which are consistent with the above density criteria.

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited or discouraged in Compatibility Zones C2 and D.

Noise: The March ALUCP depicts the site as being partially within the 60-65 CNEL contour range from aircraft noise. (The plan also identifies that 65 dB CNEL is considered normally acceptable for new residential land uses). The single family residences would be affected by aircraft generated noise. However, standard construction is normally considered to provide for a 15 dB reduction from exterior noise levels. A condition is recommended to incorporate noise attenuation measures into the design of the single family residences to such extent as may be required to ensure that interior noise levels from aircraft operations are at or below 40 CNEL.

Based on the 2018 Air Installation Compatible Use Zones Study (AICUZ), the site is located outside the 60 CNEL contour. (The March ALUCP has not yet been officially amended to reflect the 2018 AICUZ, so the noise contours included in the 2014 March ALUCP, which were based in part on the 2005 AICUZ, remain applicable.)

Part 77: Although the project is located within the March Air Reserve Base/Inland Port AIA, the actual nearest runway is Runway 15-33 at Perris Valley Airport. The southerly terminus of this runway is located approximately 11,100 feet from the project site and has an elevation of 1,413 feet above mean sea level (AMSL). Therefore, Federal Aviation Administration (FAA) review would be required for any structures with a top of roof elevation exceeding 1,524 feet AMSL. The project's site elevation is 1,453 feet AMSL and proposes a maximum building height of 26 feet, for a top point elevation of 1,479 feet AMSL. Therefore, review by the FAA Obstruction Evaluation Service (FAA OES) is not required.

Open Area: None of the Compatibility Zones for the March Air Reserve Base/Inland Port ALUCP require open area specifically.

Change of Zone: The proposed Change of Zone (from R-R and R-1 to R-1) increases the potential density of the site. However, the proposed project's overall density within the R-1 zone is consistent with the maximum density criteria within Compatibility Zone C2.

CONDITIONS:

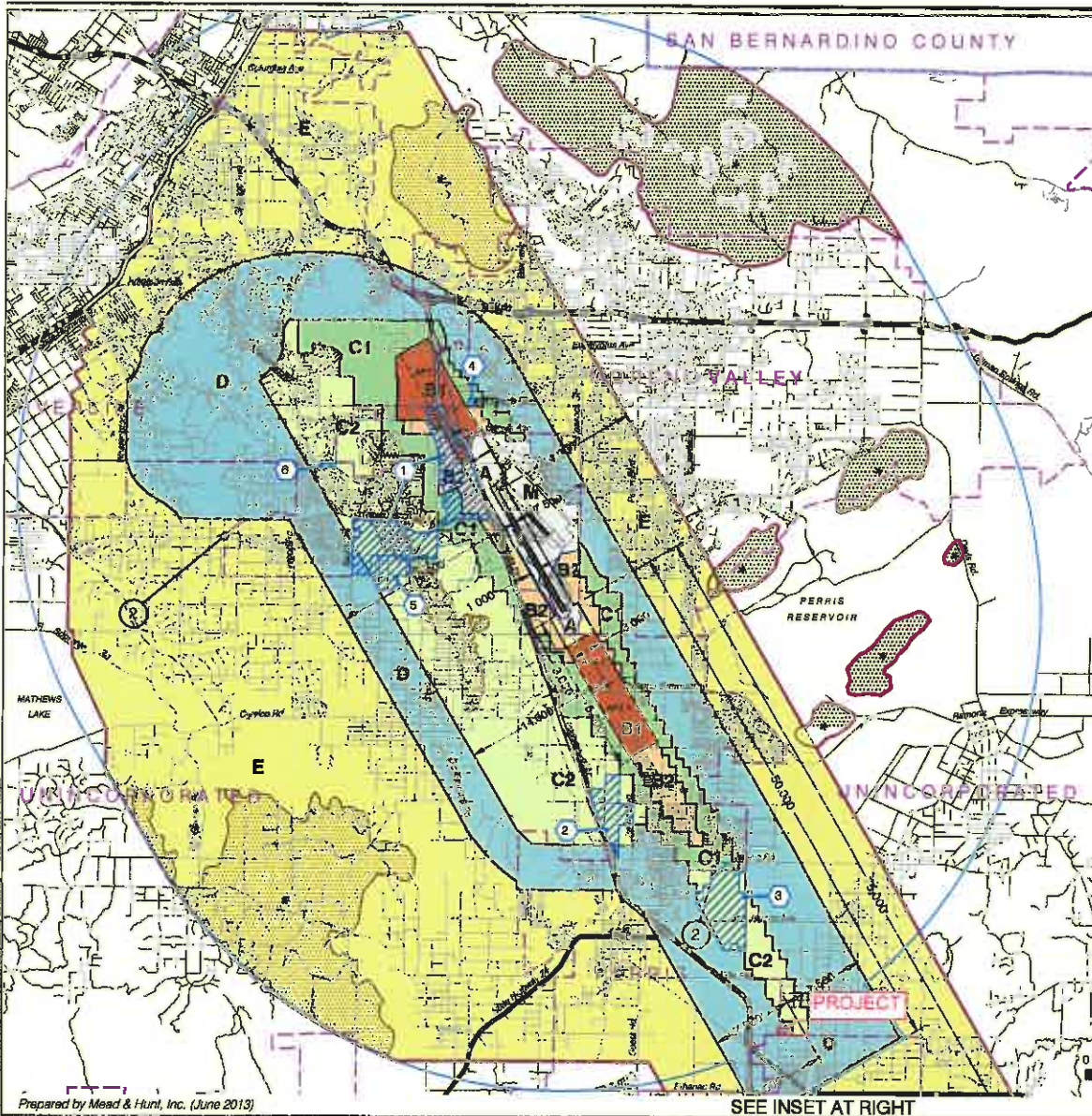
1. Any outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site, in accordance with Note A on Table 4 of the Harvest Valley/Winchester Area Plan:
 - (a) Any use or activity which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use or activity which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use or activity which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area.
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
3. The following uses/activities are specifically prohibited at this location: trash transfer stations that are open on one or more sides; recycling centers containing putrescible wastes; construction and demolition debris facilities; wastewater management facilities; incinerators; noise-sensitive outdoor nonresidential uses; and hazards to flight. Children's schools are discouraged.
4. The following uses/activities are not included in the proposed project, but, if they were to be proposed through a subsequent use permit or plot plan, would require subsequent Airport Land Use Commission review:

Day care centers; churches, temples, or other uses primarily for religious worship.

5. The attached notice shall be given to all prospective purchasers of the proposed lots and tenants of any dwellings thereon, and shall be recorded as a deed notice prior to or in conjunction with recordation of the final map. In the event that the Office of Riverside County Assessor-Clerk-Recorder declines to record said notice, the text of the notice shall be included on the Environmental Constraint Sheet (ECS) of the final parcel map, if an ECS is otherwise required.
6. The proposed detention basins on the site (including water quality management basins) shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention basins that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.
7. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.
8. Noise attenuation measures shall be incorporated into the design of the single family residences, to the extent such measures are necessary to ensure that interior noise levels from aircraft operations are at or below 40 CNEL.

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



LEGEND

Compatibility Zones

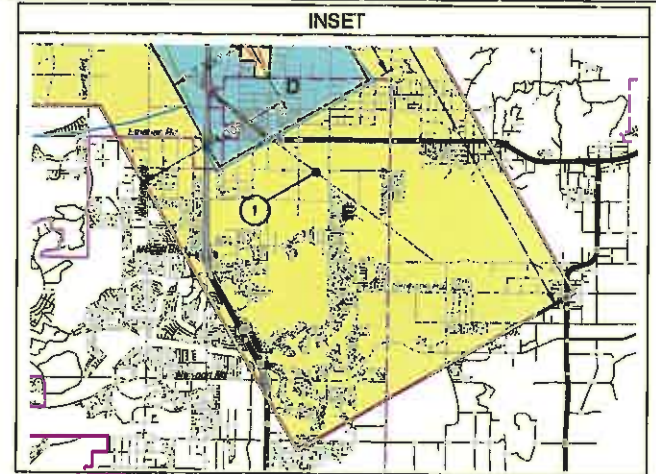
- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- Site-Specific Exceptions (existing local agency commitments to development projects)

- ① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.

- ① March JPA: March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA: Ben Clark Training Center
- ⑥ Riverside: Ridge Crest Subdivision



Riverside County
Airport Land Use Commission

March Air Reserve Base / Inland Port Airport
Land Use Compatibility Plan

(Adopted November 13, 2014)

Map MA-1

Compatibility Map
March Air Reserve Base / Inland Port Airport

Note:
All dimensions are measured from runway ends and centerlines.

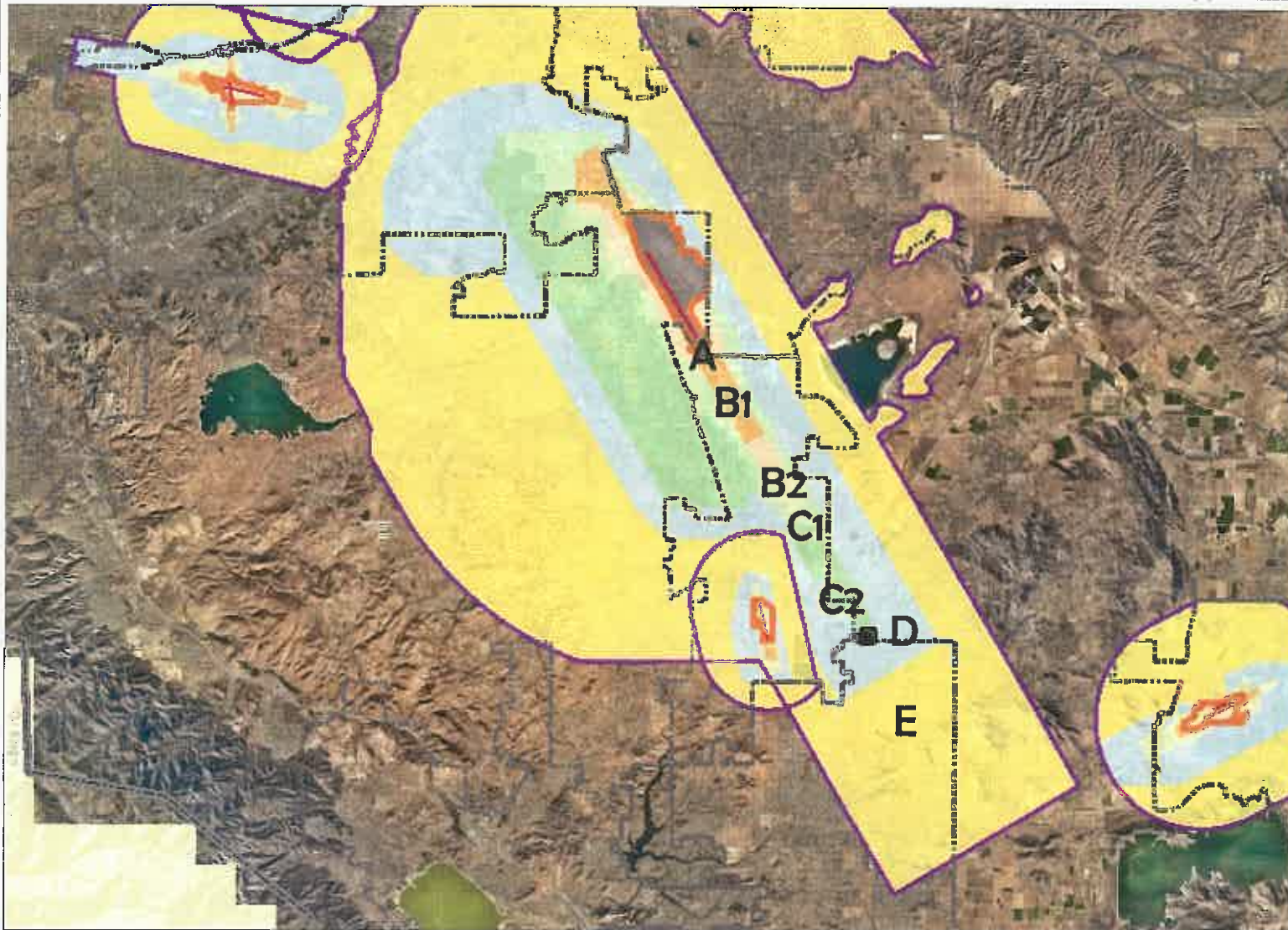


Base map source: County of Riverside 2013

Prepared by Mead & Hunt, Inc. (June 2013)

SEE INSET AT RIGHT

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

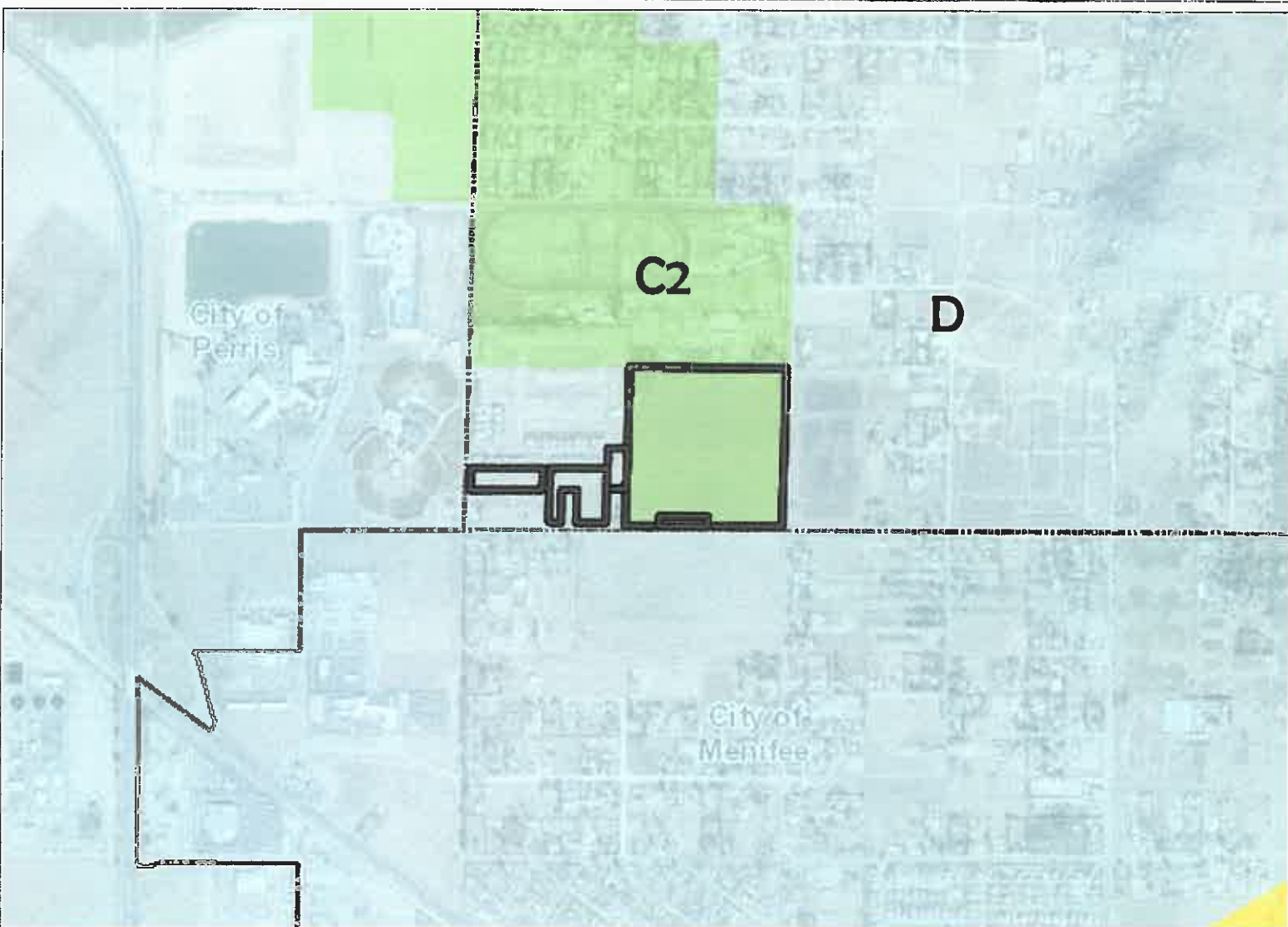


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Notes



Map My County Map



- Legend**
- Runways
 - ✈ Airports
 - Airport Influence Areas
 - Airport Compatibility Zones**

OTHER COMPATIBILITY ZONE

 - A
 - A-EXC1
 - B1
 - B1-APZ I
 - B1-APZ I-EXC1
 - B1-APZ II
 - B1-APZ II-EXC1
 - B1-EXC1
 - B2
 - B2-EXC1
 - C
 - C1
 - C1-EXC1
 - C1-EXC3
 - C1-EXC4
 - C1-HIGHT
 - C2
 - C2-EXC1
 - C2-EXC2
 - C2-EXC3
 - C2-EXC5
 - C2-EXC6

0 1 516 3,032 Feet

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Notes

Map My County Map



Legend

-  City Areas
-  World Street Map



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Notes

Map My County Map



Legend

- Blueline Streams
- City Areas
- World Street Map



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0 3 6,064 Feet
032

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Notes

Map My County Map



Legend

-  Blueline Streams
-  City Areas
-  World Street Map



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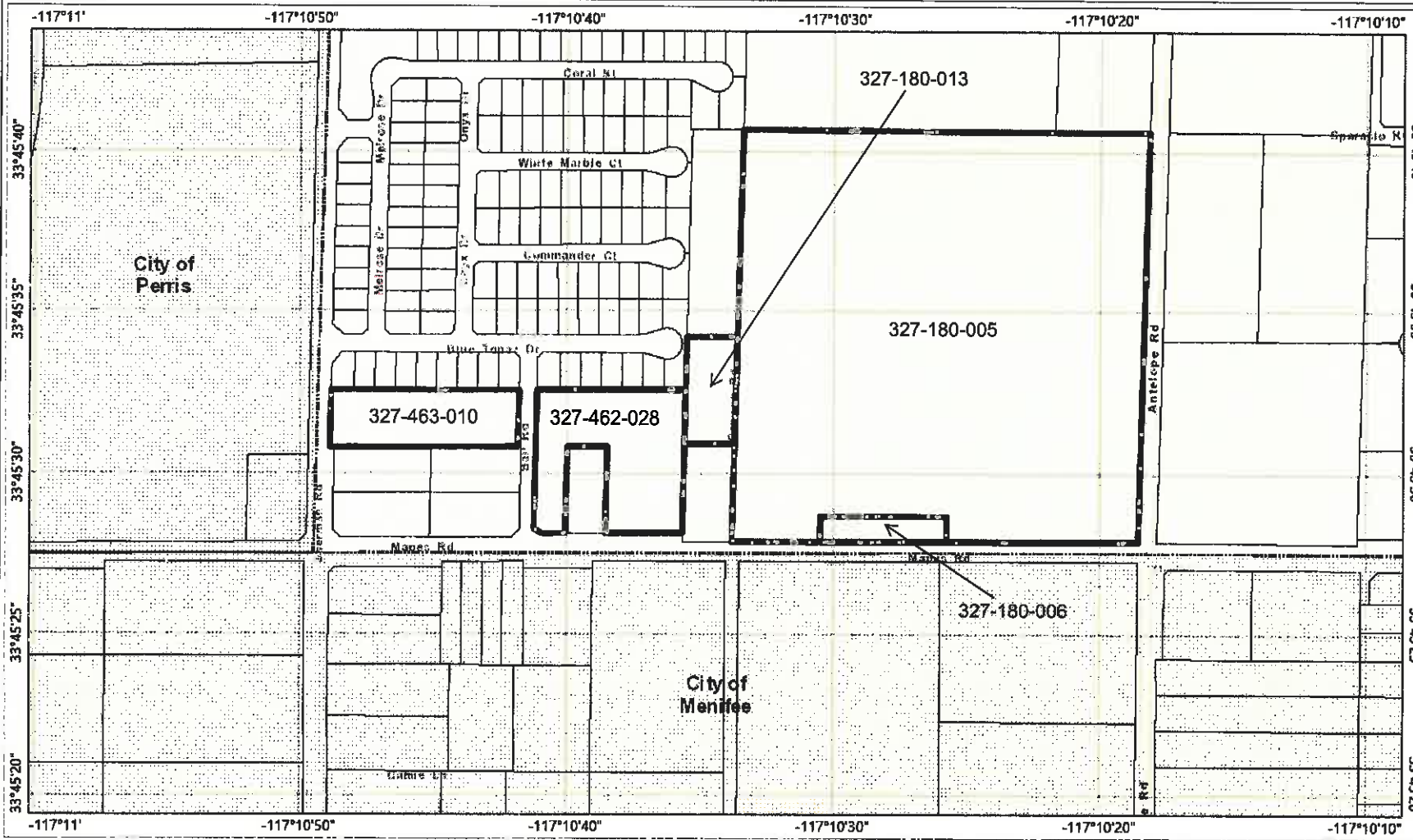


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Notes

TTM 37358



- Legend**
- Parcels
 - Blue Line Streams
 - City Areas
 - World Street Map

Existing Zoning
R-R & R-1

Proposed Zoning
R-1

Project Site
5 Parcels

Notes



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ASSESSOR'S PARCEL NO.:

THOMAS BROTHERS MAP PAGES:

LEGAL DESCRIPTION:

DEVELOPER/OWNER:

ENGINEER:

UTILITY NOTIFICATION LIST:

EXISTING LAND USE:

ADJACENT LAND USE:

ADJACENT ZONING:

PROPOSED ZONING:

EXISTING ZONING:

SCHOOL DISTRICT:

ACREAGE:

FEMA ZONE:

BASIS OF BEARINGS:

BENCHMARK:

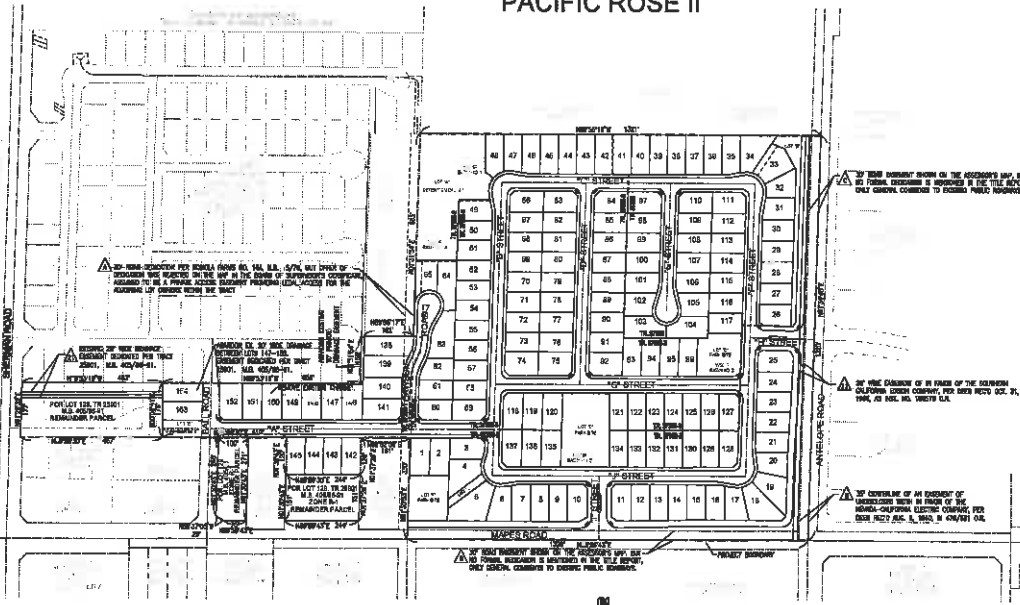
NOTES:

LEGEND:

FEMA ZONE:

EASEMENTS:

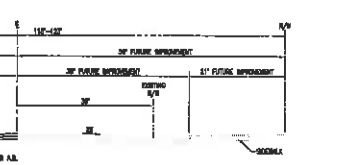
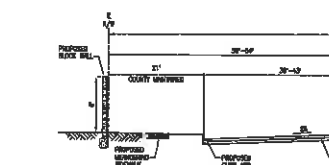
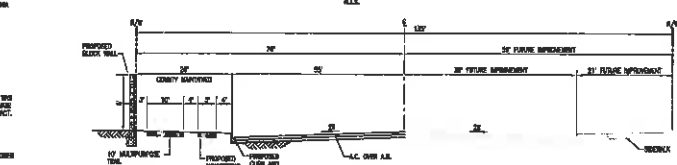
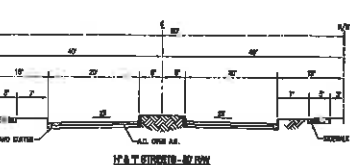
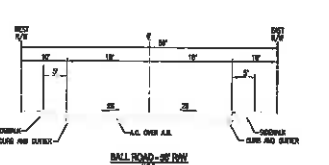
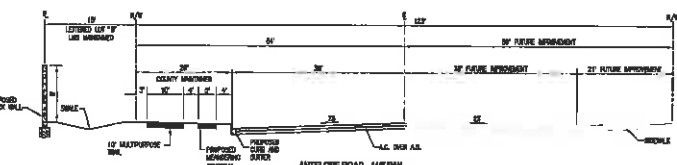
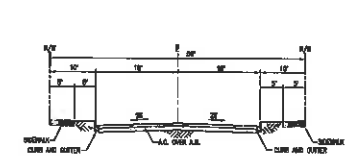
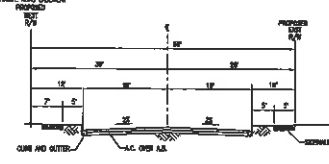
COUNTY OF RIVERSIDE
TENTATIVE TRACT MAP NO. 37358
PACIFIC ROSE II



TRACT PHASING table with columns for PHASE and LOT NUMBERS.

NET LOT ACREAGE table with columns for LOT NO., AREA, and ACREAGE.

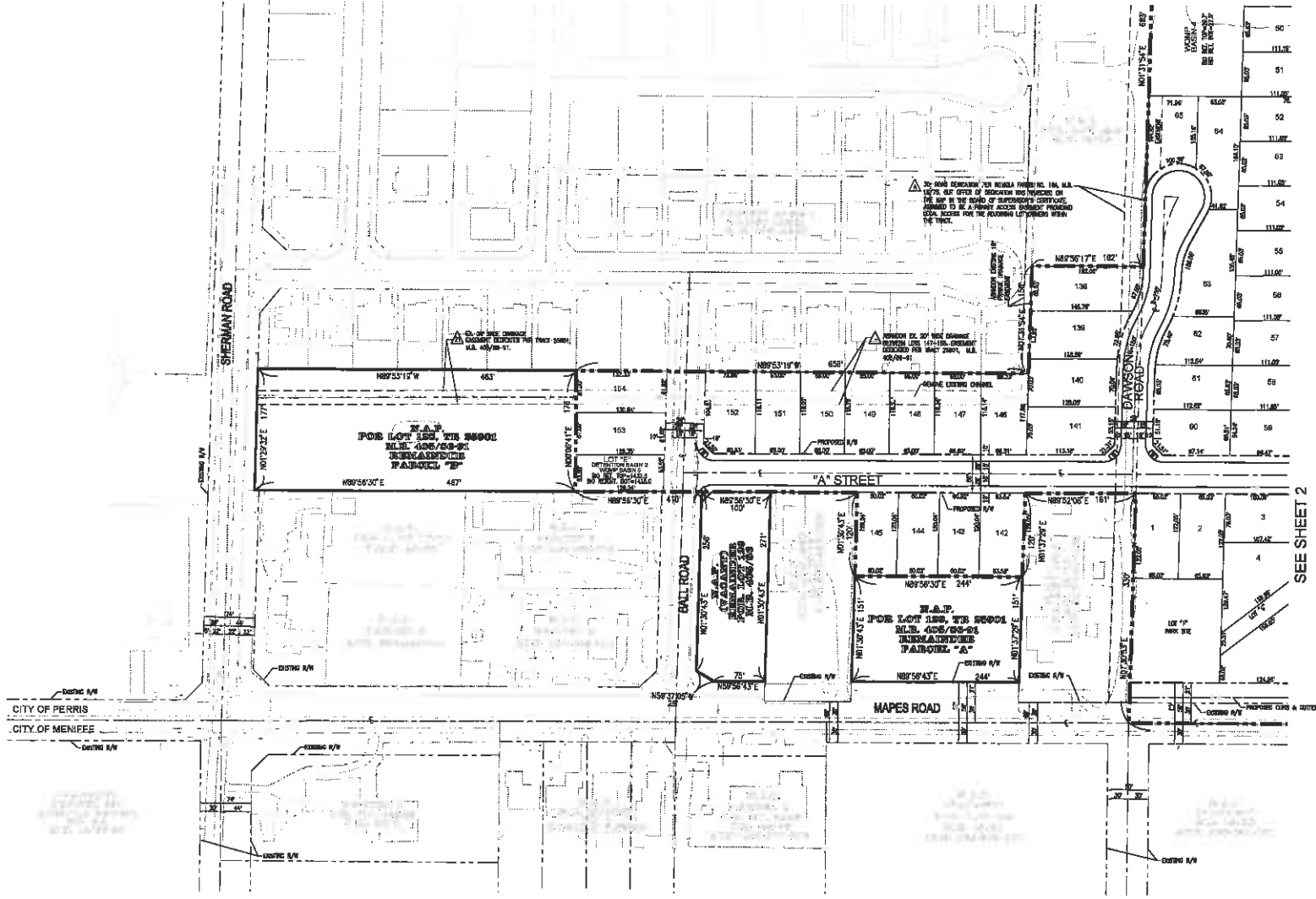
LETTERED LOTS table with columns for PURPOSE, SQUARE FOOTAGE, and LOT NO.



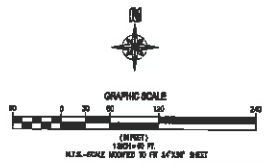
Project information including title 'TENTATIVE TRACT MAP NO. 37358', project name 'PACIFIC ROSE II', and contact details for Pacific Communities Builder, Inc.

PLAT DATE: July 02, 2019

Project No. 20201 Pacific Rose II\DWG\TENTATIVE TRACT MAP\TTL_01-37358.dwg



SEE SHEET 2



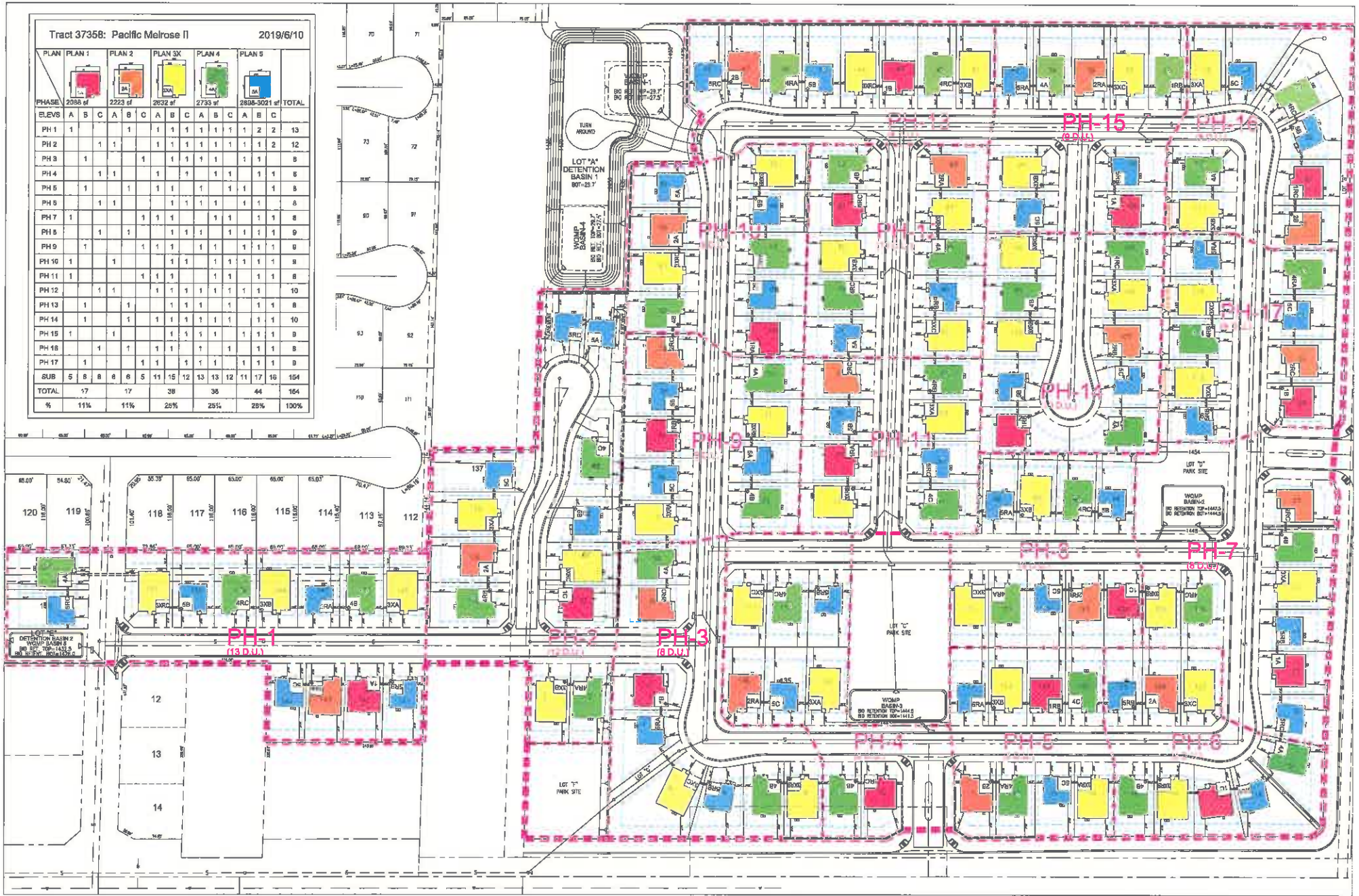
SHEET NO. 3
 PROJECT: PACIFIC ROSE II
 SHEET TITLE: TENTATIVE TRACT MAP NO. 37358
 COUNTY OF: INGLEWOOD, CALIFORNIA
 CITY OF: PERRIS
 CITY OF: MENIFEE
 DATE: May, 2019
 SCALE: AS SHOWN
 DRAWN: C.V.
 CHECKED: R.S.A.
 PROJECT BY: ALLIARD ENGINEERING
 12515 LINDSEY BLVD., SUITE 100
 PERRIS, CA 92404
 PHONE: 951-261-1111
 FAX: 951-261-1112
 WWW: WWW.AE-CA.COM
 PROJECT FOR: PACIFIC COMMUNITIES BUILDER, INC.
 1000 DIXIE STREET, SUITE 300
 MENIFEE, CA 92550
 CONTACT: (951) 261-2222



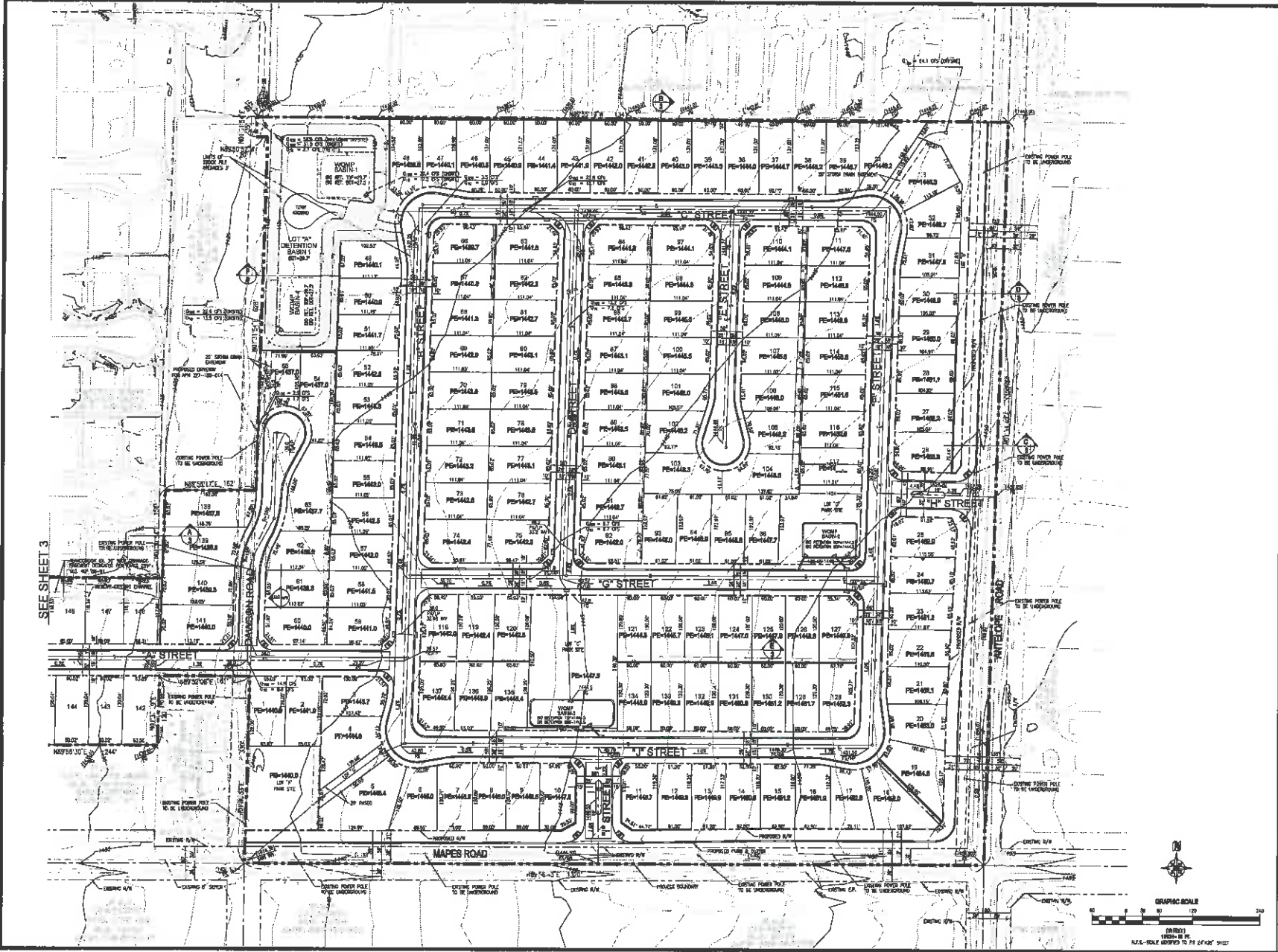
Source: Pacific Communities, Block 23031 (Acres: 8.66) (APNs: 0100010000) (TRACT MAP: 010-010000)

Tract 37358: Pacific Melrose II 2019/6/10

PLAN	PLAN 1	PLAN 2	PLAN 3X	PLAN 4	PLAN 5	TOTAL
PHASE	2086 sf	2223 sf	2632 sf	2733 sf	2808-3021 sf	
ELEVS	A B C	A B C	A B C	A B C	A B C	
PH 1	1		1	1	1	2 2 13
PH 2		1	1	1	1	1 2 12
PH 3	1		1	1	1	1 1 8
PH 4	1	1	1	1	1	1 1 8
PH 5	1		1	1	1	1 1 8
PH 7	1		1	1	1	1 1 8
PH 8		1	1	1	1	1 1 9
PH 9	1		1	1	1	1 1 8
PH 10	1	1	1	1	1	1 1 8
PH 11	1		1	1	1	1 1 8
PH 12		1	1	1	1	1 1 10
PH 13	1	1	1	1	1	1 1 8
PH 14	1	1	1	1	1	1 1 10
PH 15	1	1	1	1	1	1 1 8
PH 16	1	1	1	1	1	1 1 8
PH 17	1		1	1	1	1 1 8
SUB	5	8	8	5	11	15 12 13 13 12 11 17 16 154
TOTAL	17	17	38	36	44	164
%	11%	11%	25%	25%	28%	100%



SEE SHEET 3



SHEET TITLE: CONCEPTUAL GRADING PLAN
 PROJECT: TENTATIVE TRACT MAP NO. 37359
 CITY: PACIFIC ROSE II
 COUNTY: OF HAVESSEE, CALIFORNIA

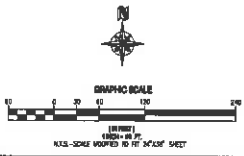
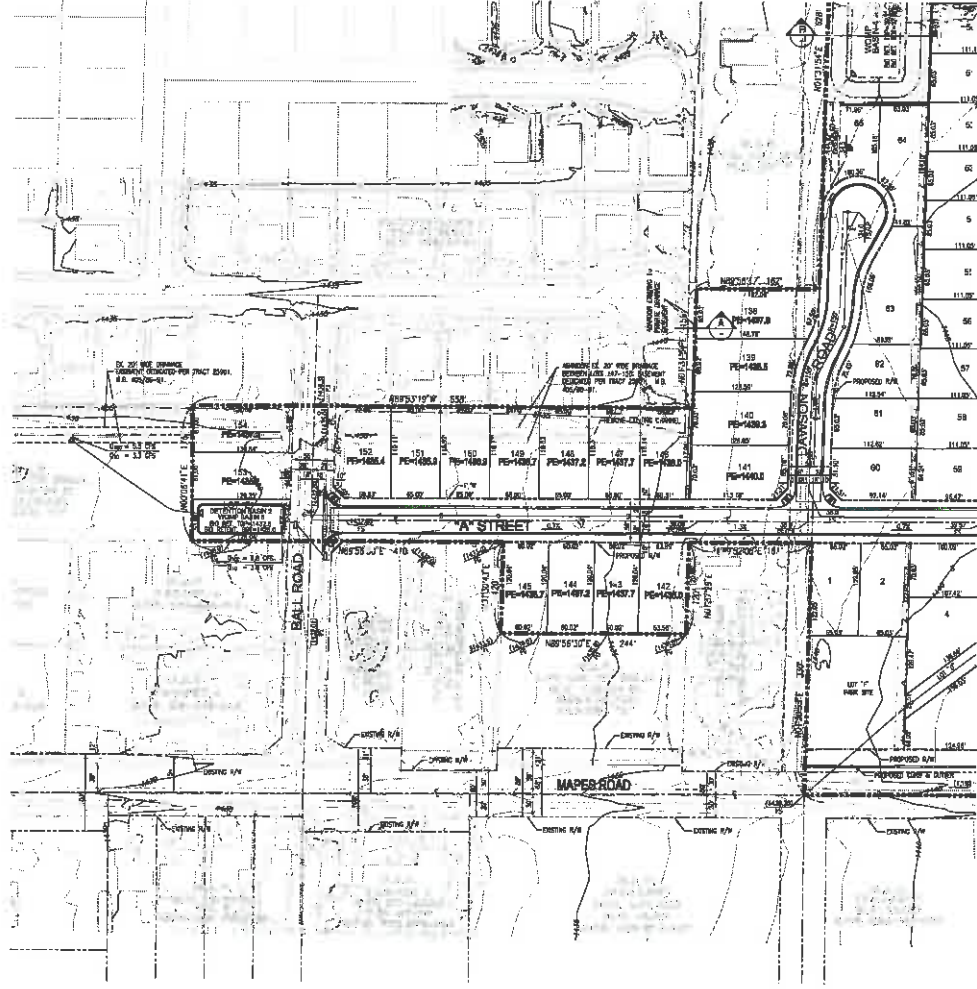
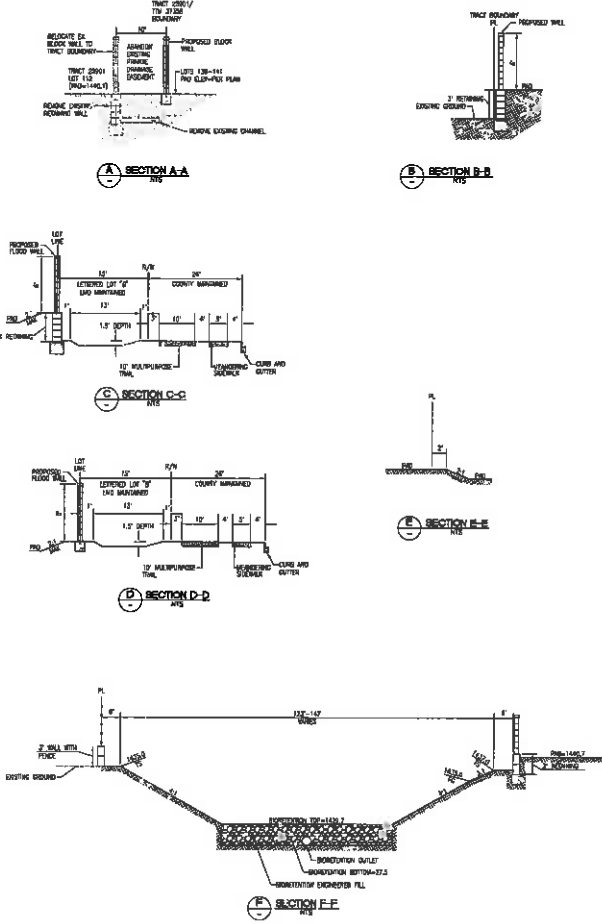
SHEET NO: 2 OF 3

Prepared by: ALLARD ENGINEERING
 1000 JAMES STREET, SUITE 200
 PACIFIC ROSE, CA 92651
 PHONE: (714) 942-1111
 FAX: (714) 942-1112
 WWW: WWW.ALLARDENGINEERING.COM

Prepared for: PACIFIC COMMUNITIES BUILDER, INC.
 1000 JAMES STREET, SUITE 200
 PACIFIC ROSE, CA 92651
 PHONE: (714) 942-1111

DATE: July, 2015
 DRAWN BY: AS SHOWN
 CHECKED BY: C.J.
 RECORDED: R.E.A.

ALLARD ENGINEERING



SEE SHEET 2

Project No. 18-001
 PACIFIC COMMUNITIES BUILDER, INC.
 10000 PACIFIC COMMUNITIES BLVD., SUITE 100
 RIVERSIDE, CALIFORNIA 92504
 OFFICE: (951) 662-6008

Project By: ALLARD ENGINEERING
 10000 PACIFIC COMMUNITIES BLVD., SUITE 100
 RIVERSIDE, CALIFORNIA 92504
 OFFICE: (951) 662-6008

DATE: July 2018
 SCALE: AS SHOWN
 SHEET: 4 OF 4
 PROJECT: PACIFIC ROSE II
 COUNTY: RIVERSIDE, CALIFORNIA

SHEET TITLE: CONCEPTUAL GRADING PLAN
 SHEET NO: 4
 PROJECT NO: 37358

Reference: L.P. 18-001, Communities Builder 18-001/1/001, from RIVERSIDE/UNIVERSITY/CONCEPT GRADING/CONCEPT GRADING, 03-2018.dwg

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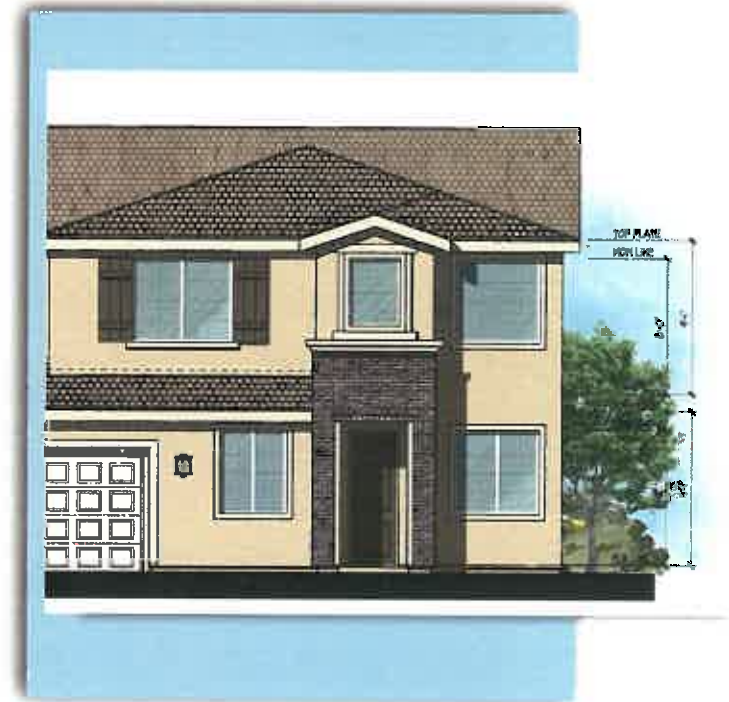
Street Scene

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Subdivision Development Plan

Pacific Melrose

Single-Family Detached

Romoland, California

Pacific Communities Builder, Inc.

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Index Sheet



OPT. LRG. SHOWER
AT MASTER BATH

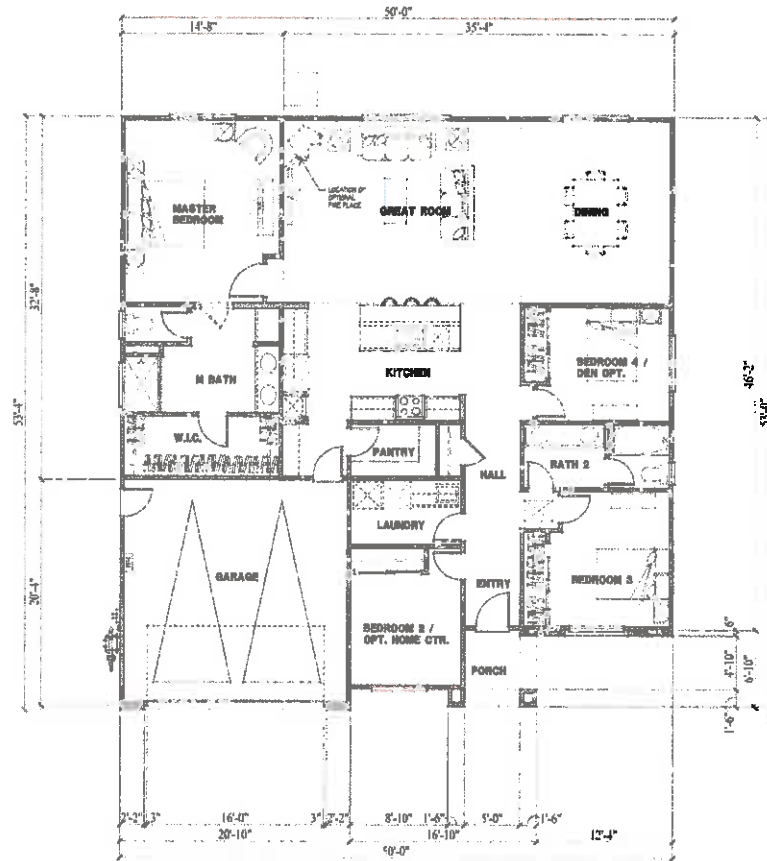


OPTION DEN
REPLACES BEDROOM 4



MUDROOM OPT.
REPLACES BEDROOM 2

Floor Options

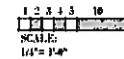


First Floor

Plan 1

Pacific Melrose
Single-Family Detached
Romoland, California
Pacific Communities Builder, Inc.

PLAN 1	SQUARE FOOTAGE
FLOOR PLAN	2922 SQ. FT.
GARAGE	224 SQ. FT.
<small>SEE DIMENSIONS ON THIS PLAN - DIMENSIONS GIVEN TO FACE UNLESS NOTED</small>	



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1.1

Subdivision Development Plan



A - Spanish - Scheme 1

SPANISH COLONIAL DESIGN ENHANCEMENTS

1. Smooth finish stucco is most common; sometimes the smooth finish is over an irregular subsurface.
2. Low pitched side gable or cross-gabled roof; sometimes hipped.
3. Front porches.
4. Arches above principal windows and doors are common.
5. Roofline is integral to the style's character, multiple intersecting roof elements that complement massing.

CRAFTSMAN DESIGN ENHANCEMENTS

1. Light lace to smooth stucco, clapboard or siding w/brick or wainscoting.
2. Roof pitches vary from 4:12 to 8:12. Shallow Pitches are most common.
3. Decorative beams, braces, struts and corbels under gables.
4. Roof rafter tails exposed at eaves.
5. Porch columns or piers with sloping (battered) sides.
6. Windows are vertically proportioned and may be placed together in groups of three or more.



B - Craftsman - Color Scheme 4

TUSCAN DESIGN ENHANCEMENTS

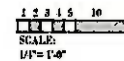
1. Fine steel-sawtooth or smooth stucco textured facade w/stone or brick.
2. Clay roof tiles sometimes stacked randomly.
3. Rough-sawn fascia boards or rough-sawn rafter tails.
4. Stopped roofed porches or bracketed shed roofs over entry points.
5. Rustic wood shutters hinged at top headers or anchored at sides of jamb.



C - Tuscan - Color Scheme 7

Plan 1

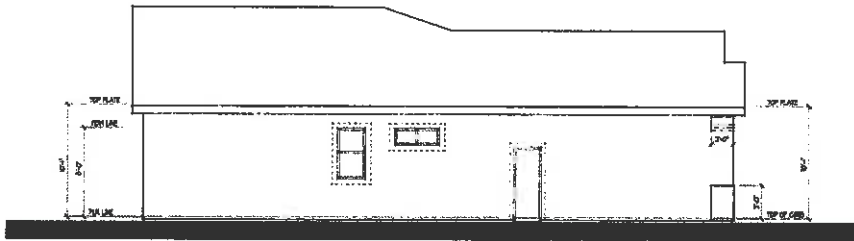
Pacific Melrose
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Romoland, California
Pacific Communities Builder, Inc.



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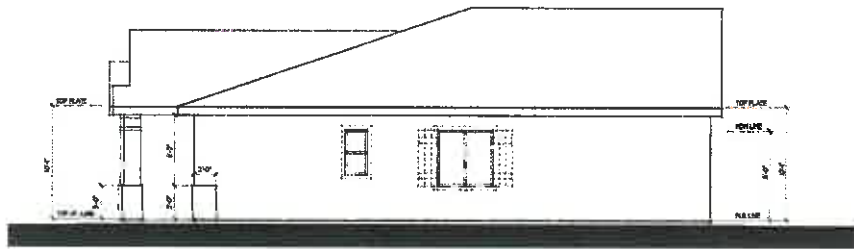
CONTRACT PARTNERSHIP CONSULTING, INC.
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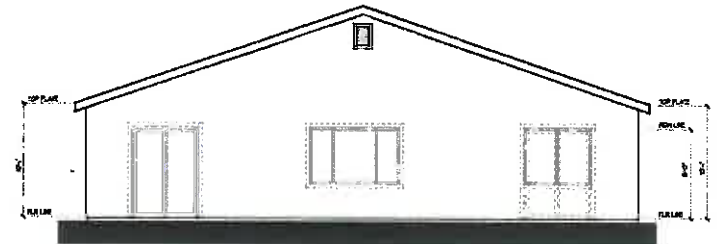
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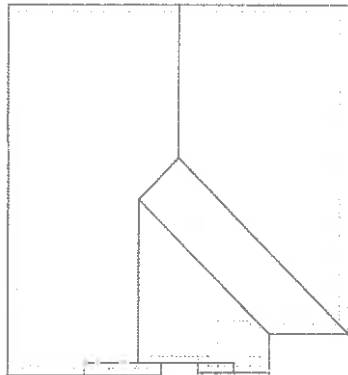
Front Elevation



Right Elevation



Rear Elevation



Roof Plan

Subdivision Development Plan

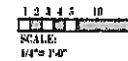
Plan 1A - Spanish
(Color Scheme 1)

Pacific Melrose

Single-Family Detached

Romoland, California

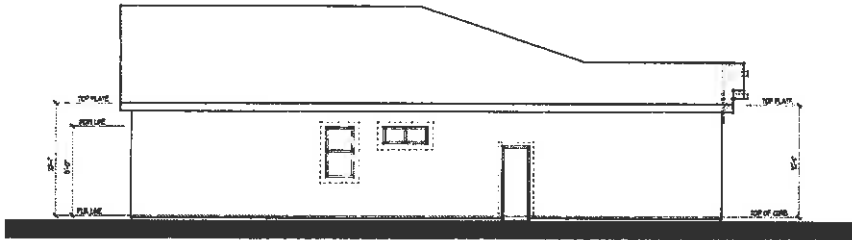
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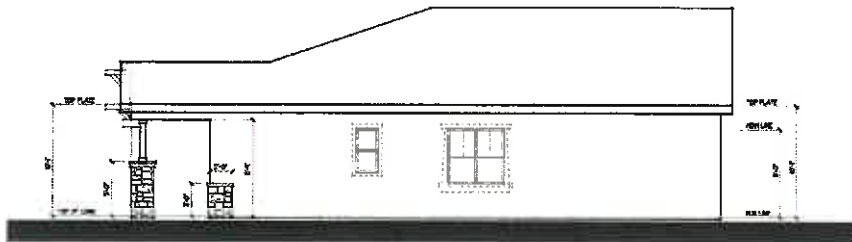
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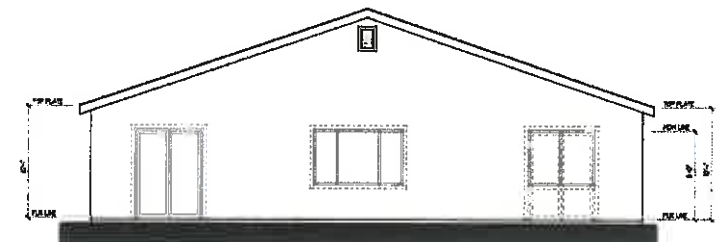
Left Elevation



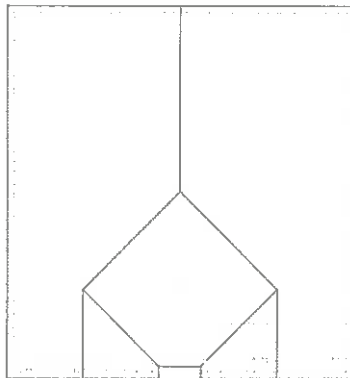
Front Elevation



Right Elevation



Rear Elevation



Roof Plan

Subdivision Development Plan

Plan 1B - Craftsman
(Color Scheme 4)

Pacific Melrose

Single-Family Detached

Romoland, California

Pacific Communities Builder, Inc.



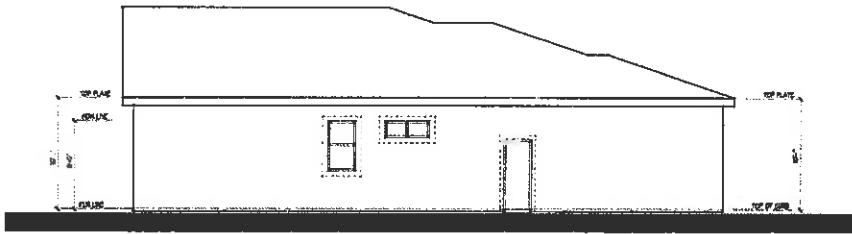
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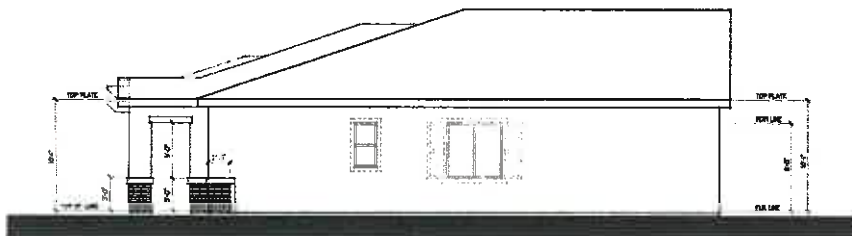
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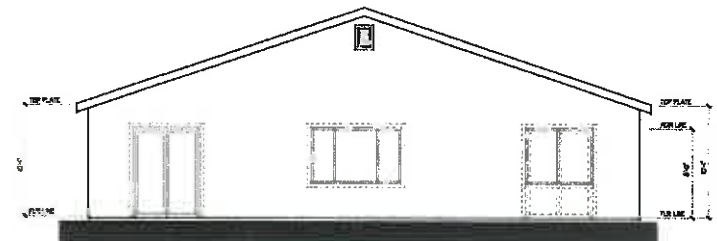
Left Elevation



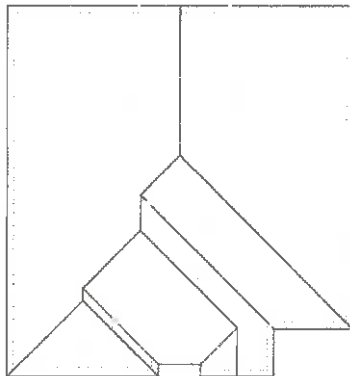
Front Elevation



Right Elevation



Rear Elevation



Roof Plan

Subdivision Development Plan

Plan 1C-Tuscan

(Color Scheme 7)

Pacific Melrose

Single-Family Detached

Romoland, California

Pacific Communities Builder, Inc.



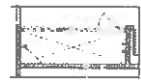
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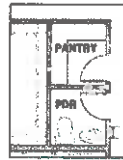
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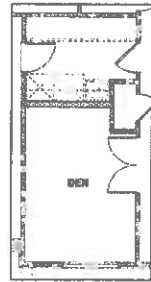
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OPT. SHOWER
AT MASTER BATH



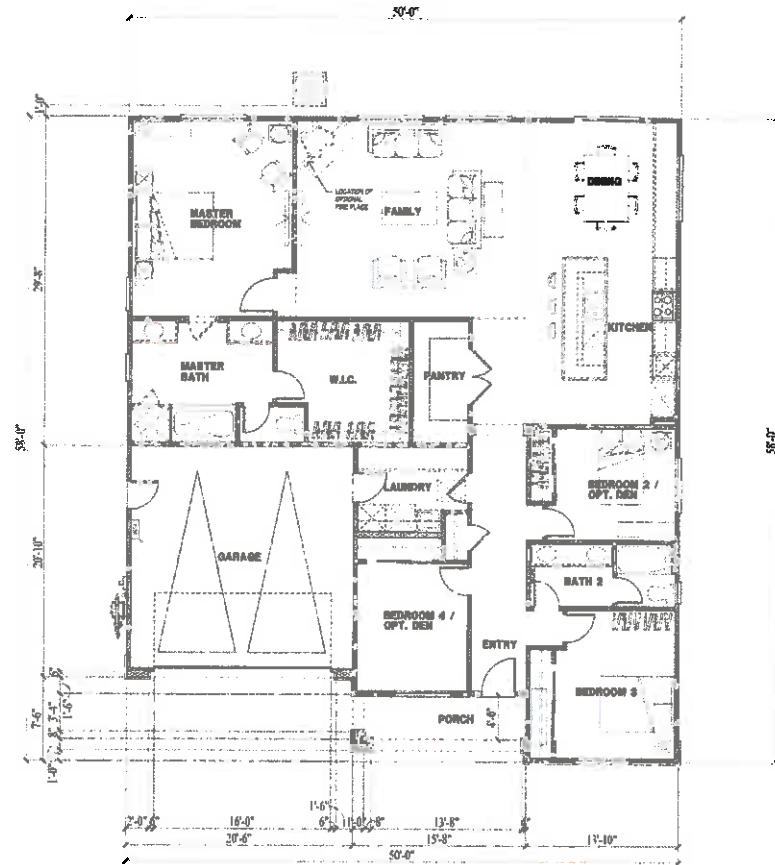
OPT. POWDER
AT PANTRY



OPT. DEN
REPLACE BEDROOM 4



OPT. DEN
REPLACE BEDROOM 2



Subdivision Development Plan

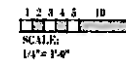
Floor Options

First Floor

Plan 2

Pacific Melrose
Single-Family Detached
Romoland, California
Pacific Communities Builder, Inc.

PLAN 2	SQUARE FOOTAGE
FLOOR PLAN	2228 SQ FT
GARAGE	408 SQ FT



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2.1



A - Spanish - Scheme 2

SPANISH COLONIAL DESIGN ENHANCEMENTS

1. Smooth finish stucco is most common; sometimes the smooth finish is over an irregular suburface.
2. Low pitched side gable or cross-gabled roof; sometimes hip roof.
3. Front porches.
4. Arches above principal windows and doors are common.
5. Roofline is integral to the style's character, multiple intersecting roof elements that complement massing.

CRAFTSMAN DESIGN ENHANCEMENTS

1. Light lace to smooth stucco, clapboard or siding w/brick or wainscoting.
2. Roof pitches vary from 4:12 to 8:12. Shallow pitches are most common.
3. Decorative beams, braces, struts and corbels under gables.
4. Roof rafter tails exposed at eaves.
5. Porch columns or piers with sloping (battered) sides.
6. Windows are vertically proportioned and may be placed together in groups of three or more.



B - Craftsman - Color Scheme 5

TUSCAN DESIGN ENHANCEMENTS

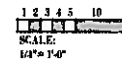
1. Fine roof-sawed or smooth stucco textured facade w/stone or brick.
2. Clay roof tiles; sometimes stacked randomly.
3. Rough-sawn fascia boards or rough-sawn rafter tails.
4. Sloped roofed porches or bracketed shed roofs over entry points.
5. Rustic wood shutters hinged at top headers or anchored at sides of jambs.



C - Tuscan - Color Scheme 8

Plan 2

Pacific Melrose
Single-Family Detached
Romoland, California
Pacific Communities Builder, Inc.



Subdivision Development Plan

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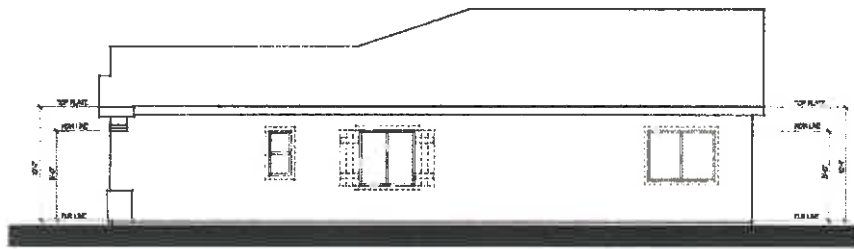
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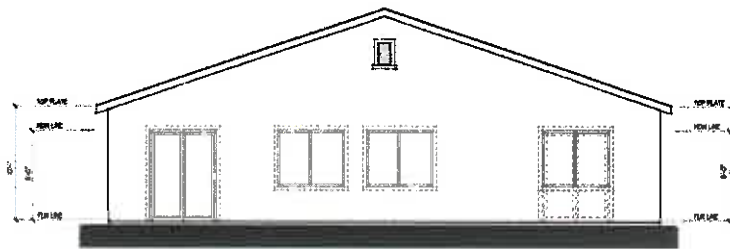
Left Elevation



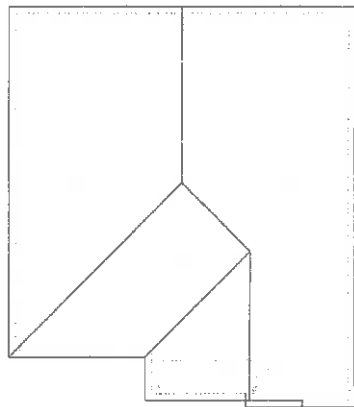
Front Elevation



Right Elevation



Rear Elevation



Roof Plan

Subdivision Development Plan

Plan 2A - Spanish

(Color Scheme 2)

Pacific Melrose

Single-Family Detached

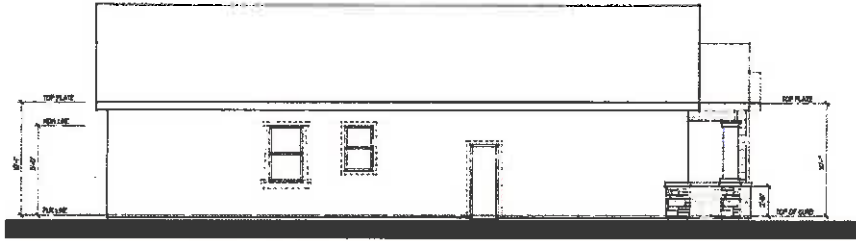
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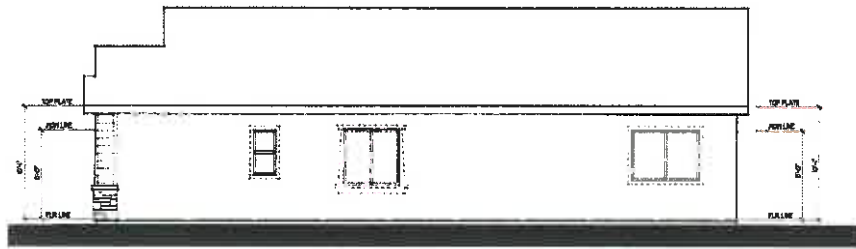
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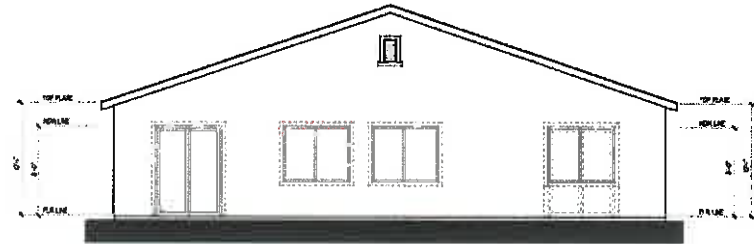
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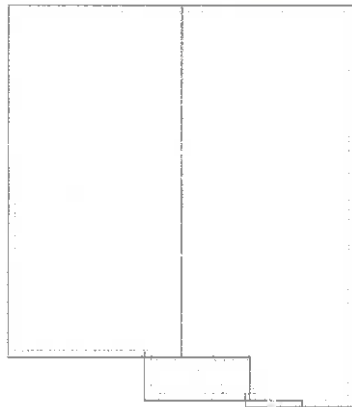
Front Elevation



Right Elevation



Rear Elevation



Roof Plan

Plan 2B - Craftsman

(Color Scheme 5)

Pacific Melrose

Single-Family Detached

Romoland, California

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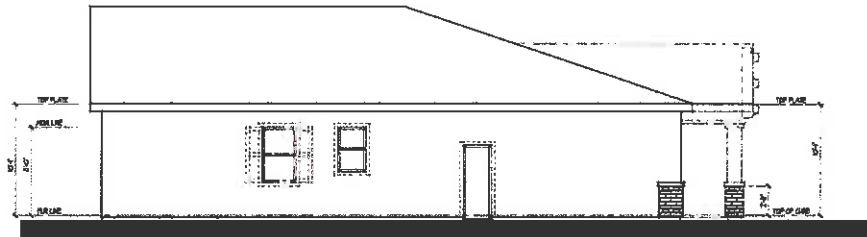
Subdivision Development Plan

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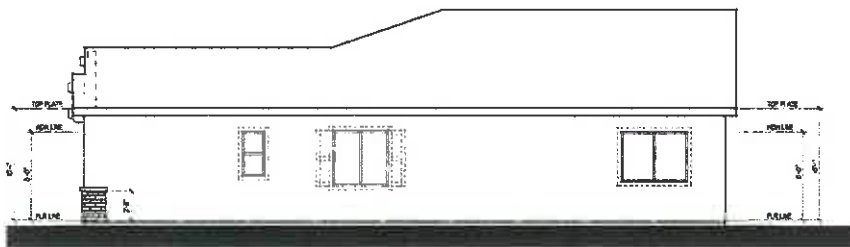
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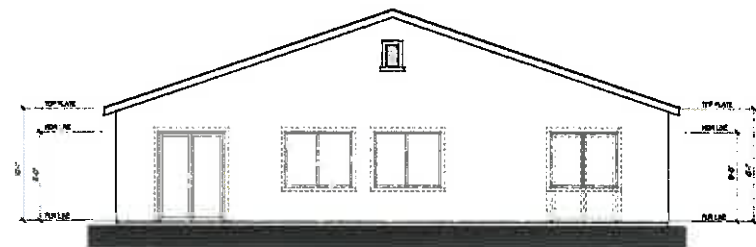
Left Elevation



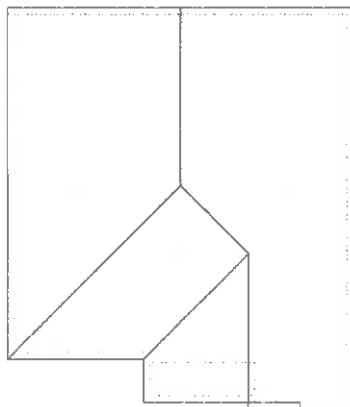
Front Elevation



Right Elevation



Rear Elevation



Roof Plan

Subdivision Development Plan

Plan 2C - Tuscan

(Color Scheme 8)

Pacific Melrose

Single-Family Detached

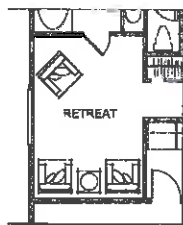
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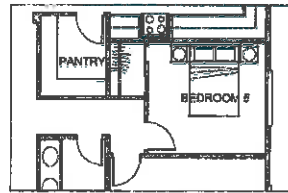
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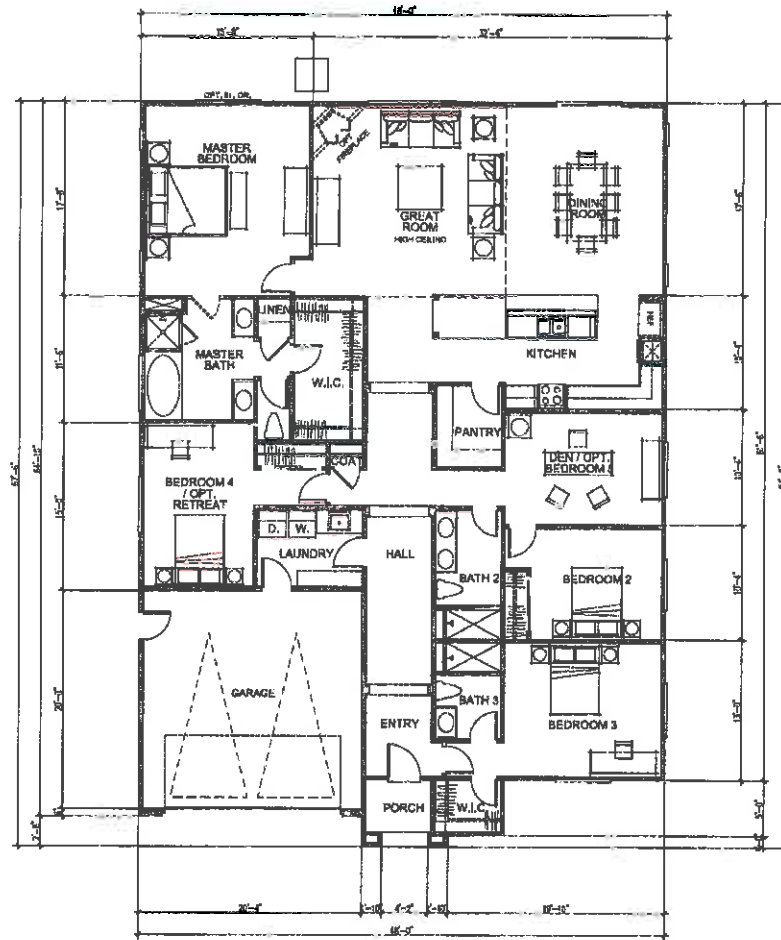
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RETREAT OPTION
REPLACES BEDROOM 4



BEDROOM 5 OPTION
REPLACES DEN



First Floor

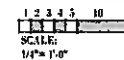
Floor Options

Subdivision Development Plan

Plan 3X

Pacific Melrose
Single-Family Detached
Romoland, California
Pacific Communities Builder, Inc.

PLAN 3X	SQUARE FOOTAGE
FLOOR PLAN	2652 sq ft
GARAGE	414 sq ft
PORCH	20 sq ft



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3X.1



A - Spanish - Color Scheme 3

SPANISH COLONIAL DESIGN ENHANCEMENTS

1. Smooth finish stucco is most common; sometimes the smooth finish is over an irregular subsurface.
2. Low pitched side gable or cross-gabled roof; sometimes hip roof.
3. Front porches.
4. Arches above principal windows and doors are common.
5. Roofline is integral to the style's character, multiple intersecting roof elements that complement massing.

CRAFTSMAN DESIGN ENHANCEMENTS

1. Light lace to smooth stucco, clapboard or siding w/brick or wainscoting.
2. Roof pitches vary from 4:12 to 8:12. Shallow pitches are most common.
3. Decorative beams, braces, struts and corbels under gables.
4. Roof rafter tails exposed at eaves.
5. Notch columns or piers with sloping (battered) sides.
6. Windows are vertically proportioned and may be placed together in groups of three or more.



B - Craftsman - Color Scheme 6



C - Tuscan - Scheme 9

TUSCAN DESIGN ENHANCEMENTS

1. Fine steel-rolled or smooth stucco textured facade w/stone or brick.
2. Clay roof tiles; sometimes stacked randomly.
3. Rough-sawn fascia boards or rough-sawn rafter tails.
4. Stepped roofed porches or bracketed shed roofs over entry points.
5. Rustic wood shutters hinged at top headers or anchored at sides of jambs.

Subdivision Development Plan

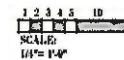
Plan 3X

Pacific Melrose

Single-Family Detached

Romoland, California

Pacific Communities Builder, Inc.

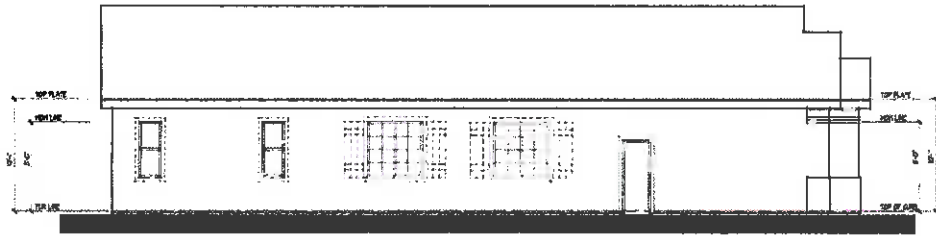


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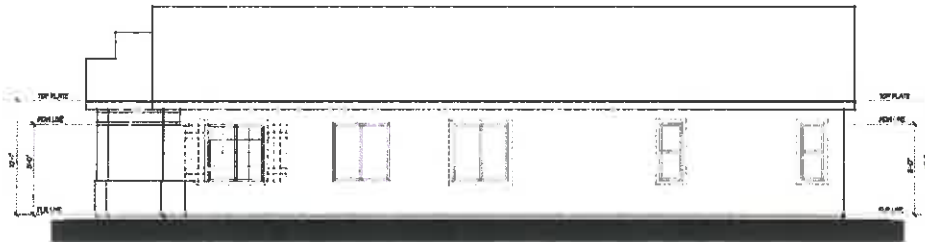
JAN / 9 / 2019
3X.2



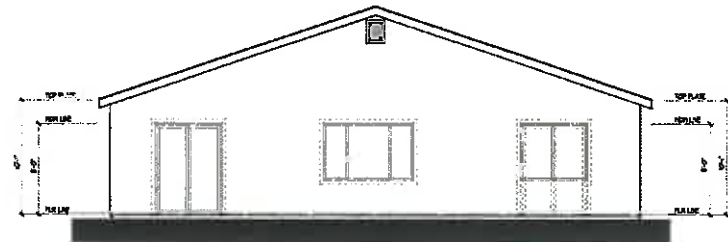
Left Elevation



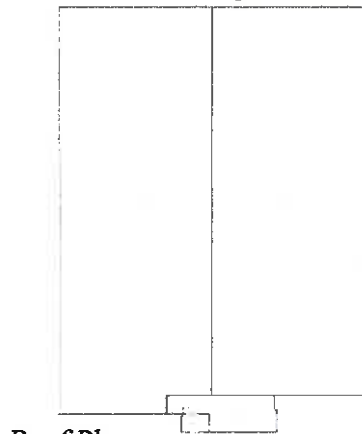
Front Elevation



Right Elevation



Rear Elevation



Roof Plan

Subdivision Development Plan

Plan 3XA - Spanish
(Color Scheme 3)

Pacific Melrose

Single-Family Detached

Romoland, California

Pacific Communities Builder, Inc.

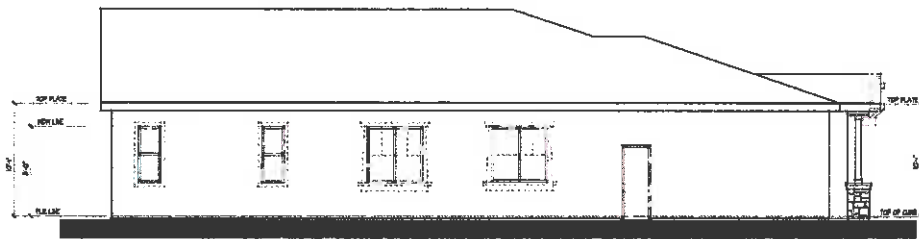


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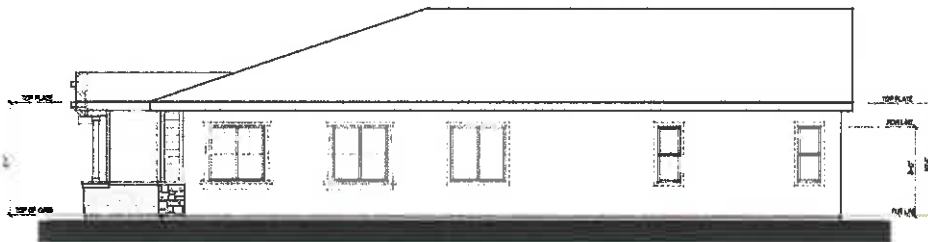
JAN / 9 / 2019
3X.3



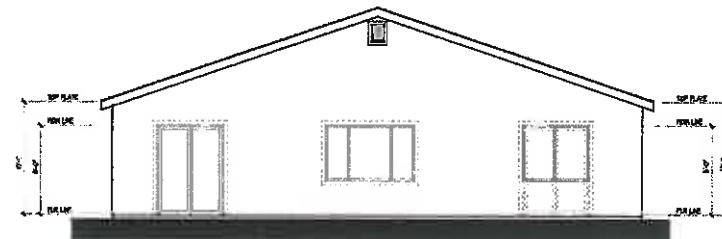
Left Elevation



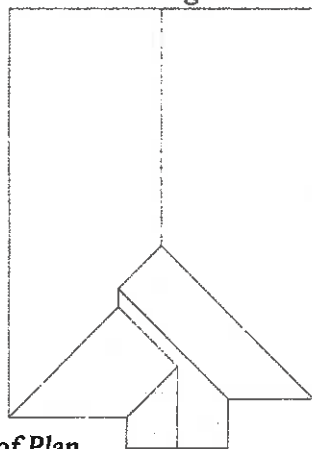
Front Elevation



Right Elevation



Rear Elevation



Roof Plan

Plan 3XB - Craftsman

(Color Scheme 6)

Pacific Melrose

Single-Family Detached

Romoland, California

Pacific Communities Builder, Inc.



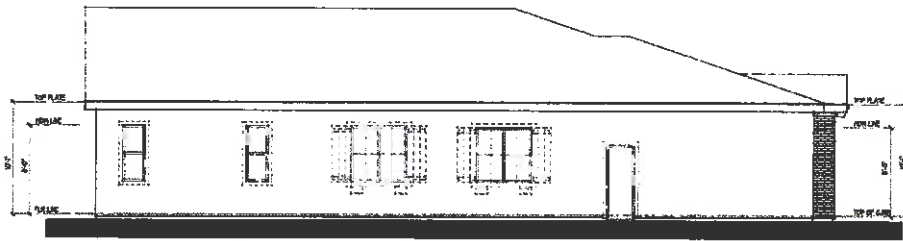
Subdivision Development Plan

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 architecture & planning

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 NEWPORT BEACH, CA 92660
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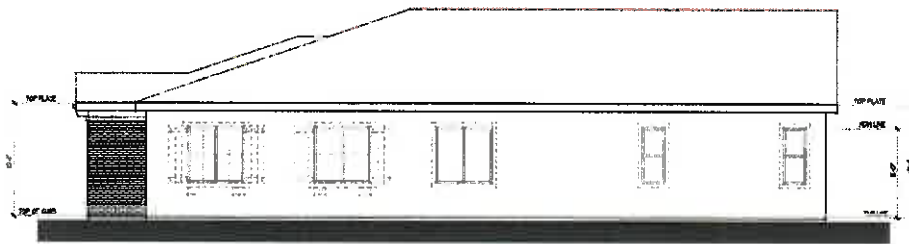
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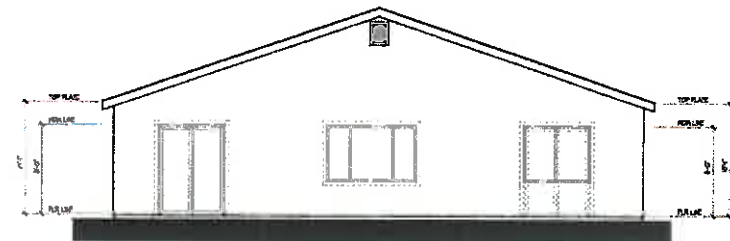
Left Elevation



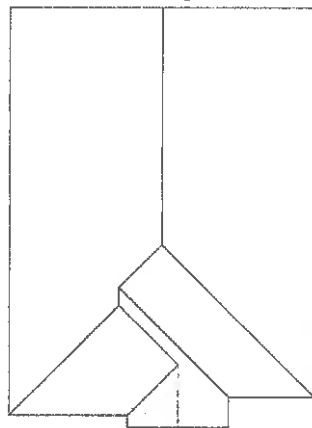
Front Elevation



Right Elevation



Rear Elevation



Roof Plan

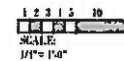
Plan 3XC - Tuscan
(Color Scheme 9)

Pacific Melrose

Single-Family Detached

Romoland, California

Pacific Communities Builder, Inc.



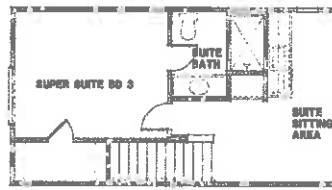
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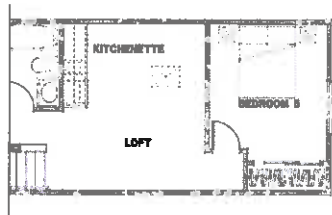
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JAN / 9 / 2019
3X.5

Subdivision Development Plan

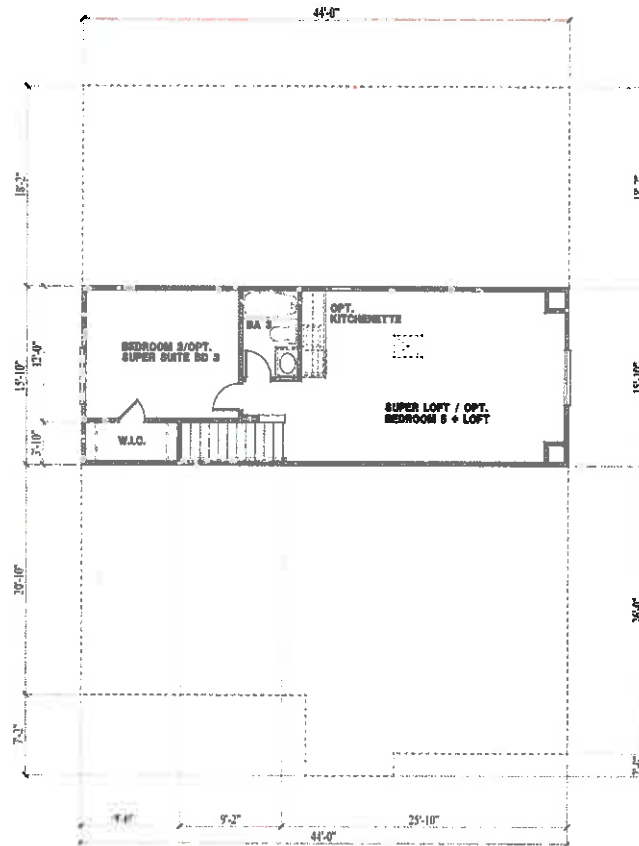


SUPER SUITE OPT.
REPLACES BEDROOM 3



BEDROOM 6 + LOFT OPTIONS
REPLACES SUPER LOFT

Floor Options



Second Floor

Plan 4

Pacific Melrose
Single-Family Detached
Romoland, California
Pacific Communities Builder, Inc.



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15053 12-21-2016
4.2



A - Spanish - Color Scheme 1

SPANISH COLONIAL DESIGN ENHANCEMENTS

1. Smooth finish stucco is most common; sometimes the smooth finish is over an irregular substrate.
2. Low pitched side gable or cross-gabled roof; sometimes hipped.
3. Front porches.
4. Arches above principal windows and doors are common.
5. Roofline is integral to the style's character, multiple intersecting roof elements that complement masonry.

CRAFTSMAN DESIGN ENHANCEMENTS

1. Light lace to smooth stucco, clapboard or siding w/brick or wainscoting.
2. Roof pitches vary from 4:12 to 8:12. Shallow pitches are most common.
3. Decorative beams, braces, struts and corbels under gables.
4. Roof rafter tails exposed at eaves.
5. Porch columns or piers with sloping (battered) sides.
6. Windows are vertically proportioned and may be placed together in groups of three or more.



B - Craftsman - Color Scheme 4



C - Tuscan - Scheme 7

TUSCAN DESIGN ENHANCEMENTS

1. Fine steel-toweled or smooth stucco textured facade w/stone or brick.
2. Clay roof tiles; sometimes stacked randomly.
3. Rough-sawn fascia boards or rough-sawn rafter tails.
4. Sloped roofed porches or bracketed shed roofs over entry points.
5. Rustic wood shutters hinged at top head or anchored at sides of jambs.

Plan 4

Pacific Melrose
Single-Family Detached
Romoland, California
Pacific Communities Builder, Inc.



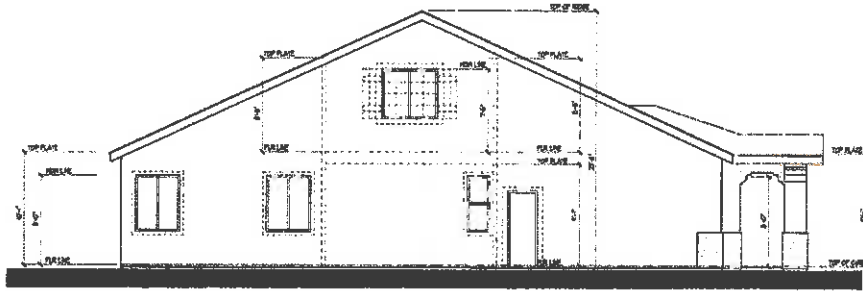
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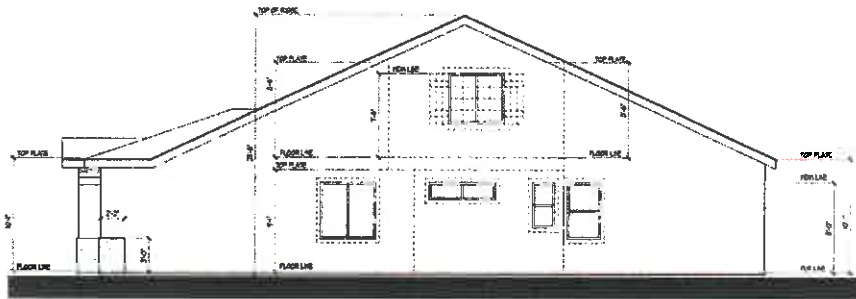
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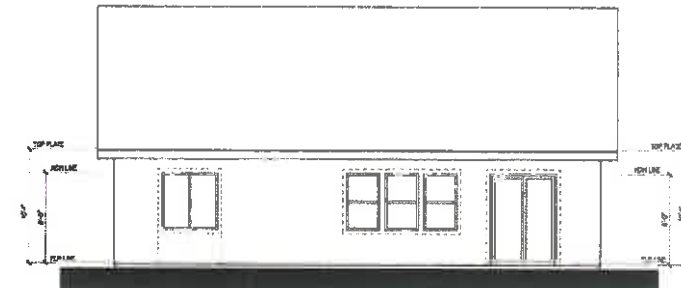
Left Elevation



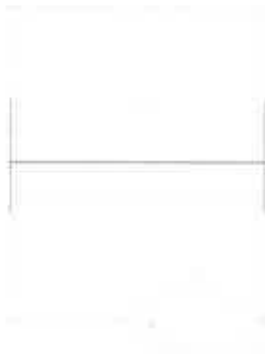
Front Elevation



Right Elevation



Rear Elevation



Roof Plan

Plan 4A - Spanish
(Color Scheme 1)

Pacific Melrose

Single-Family Detached

Romoland, California

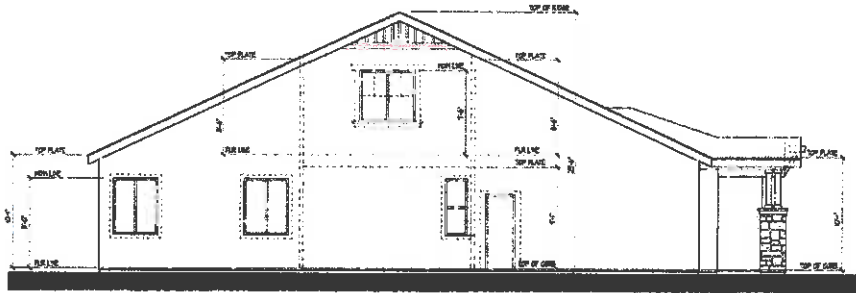
Pacific Communities Builder, Inc.



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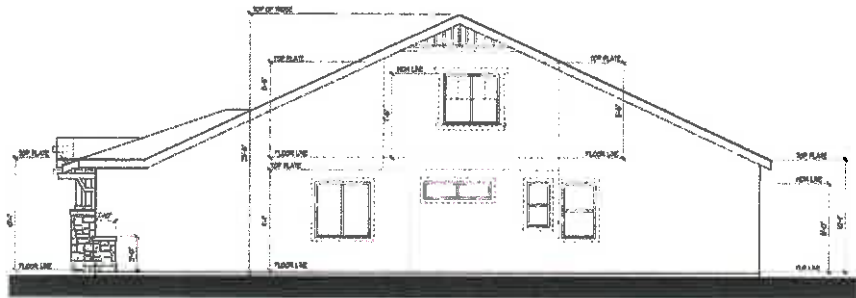
15053 12-21-2016



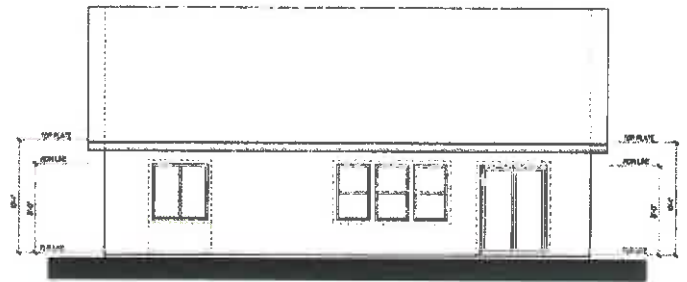
Left Elevation



Front Elevation



Right Elevation



Rear Elevation



Roof Plan

Plan 4B - Craftsman

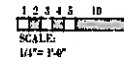
(Color Scheme 4)

Pacific Melrose

Single-Family Detached

Romoland, California

Pacific Communities Builder, Inc.



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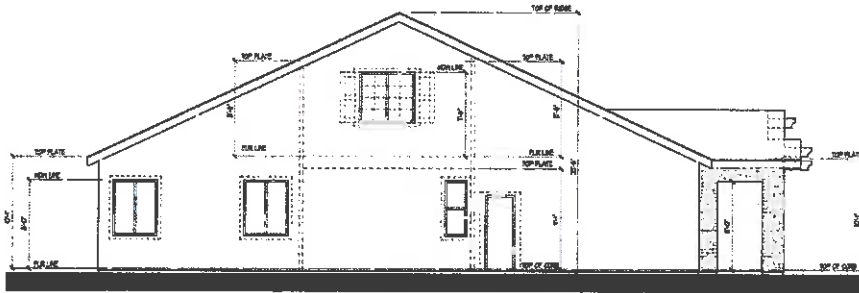
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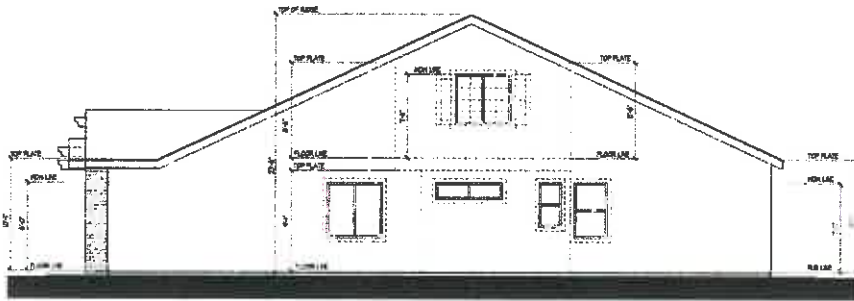
Subdivision Development Plan



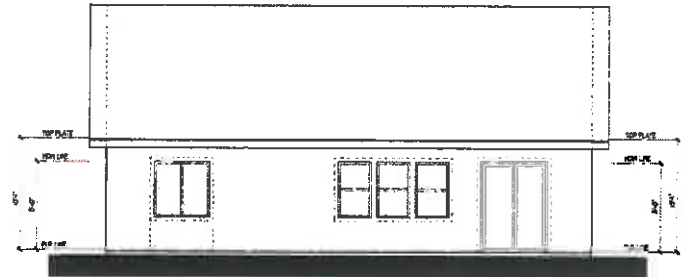
Left Elevation



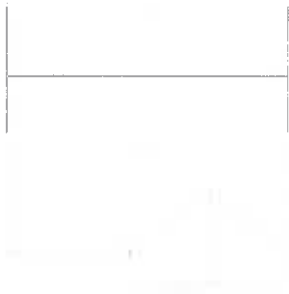
Front Elevation



Right Elevation



Rear Elevation



Roof Plan

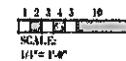
Plan 4C - Tuscan
(Color Scheme 7)

Pacific Melrose

Single-Family Detached

Romoland, California

Pacific Communities Builder, Inc.



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PROJECT NO. PACCOMBUILDER/2016/04/04
15053 12-21-2016

4.6

Subdivision Development Plan



A - Spanish - Color Scheme 3

SPANISH COLONIAL DESIGN ENHANCEMENTS

1. Smooth finish stucco is most common; sometimes the smooth finish is over an irregular suburface.
2. Low pitched side gable or cross-gabled roof; sometimes hipped.
3. Front porches.
4. Arches above principal windows and doors are common.
5. Roofline is integral to the style's character, multiple intersecting roof elements that complement massing.

CRAFTSMAN DESIGN ENHANCEMENTS

1. Light lace to smooth stucco, clapboard or siding w/brick or wainscoting.
2. Roof pitches vary from 4:12 to 8:12. Shallow pitches are most common.
3. Decorative beams, braces, struts and corbels under gables.
4. Roof rafter tails exposed at eaves.
5. Porch columns or piers with sloping (battered) sides.
6. Windows are vertically proportioned and may be placed together in groups of three or more.



B - Craftsman - Color Scheme 5

TUSCAN DESIGN ENHANCEMENTS

1. Fine steel-soweled or smooth stucco textured facade w/stone or brick.
2. Clay roof tiles; sometimes stacked randomly.
3. Rough-sawn fascia boards or rough-sawn rafter tails.
4. Sloped roofed porches or bracketed shed roofs over entry points.
5. Rustic wood shutters hinged at top headers or anchored at sides of jambs.

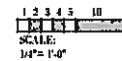


C- Tuscan - Scheme 8

Subdivision Development Plan

Plan 5

Pacific Melrose
Single-Family Detached
Romoland, California
Pacific Communities Builder, Inc.

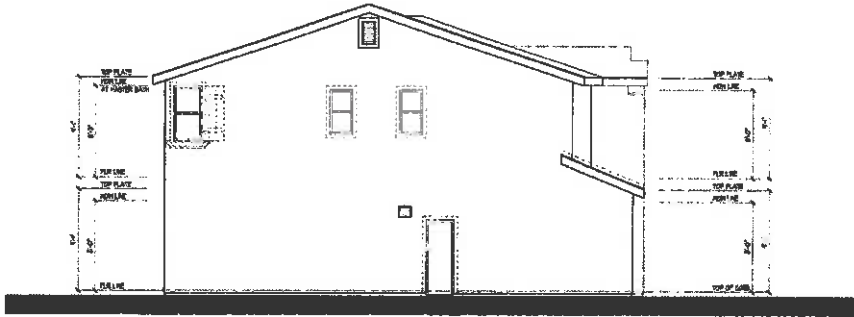


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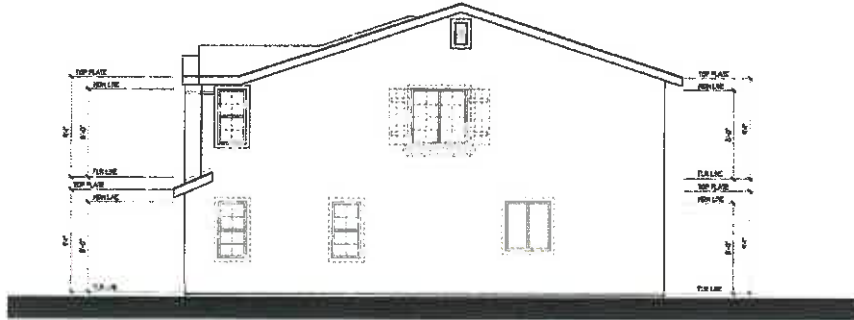
15053 12-21-2016
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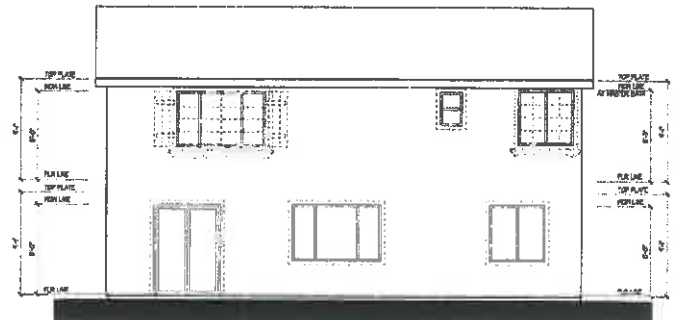
Left Elevation



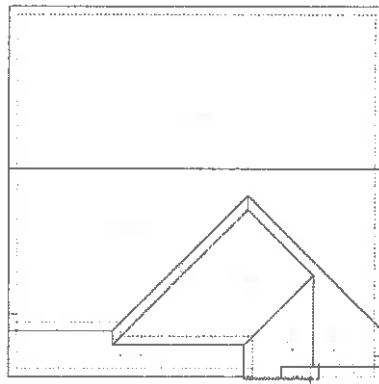
Front Elevation



Right Elevation

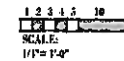


Rear Elevation



Roof Plan

Plan 5A - Spanish
 (Color Scheme 3)
Pacific Melrose
 Single-Family Detached
 Romoland, California
Pacific Communities Builder, Inc.

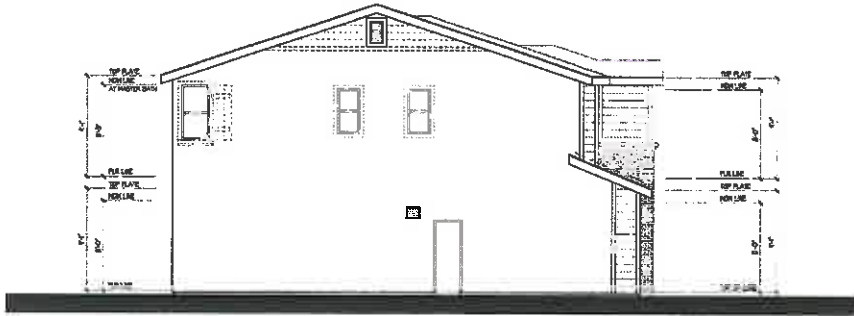


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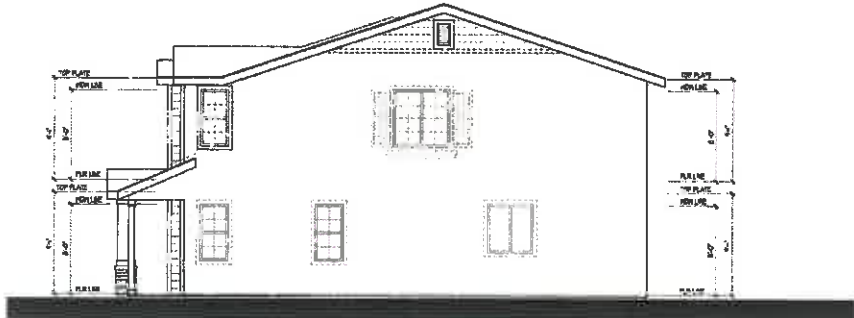
15053 12-21-2016



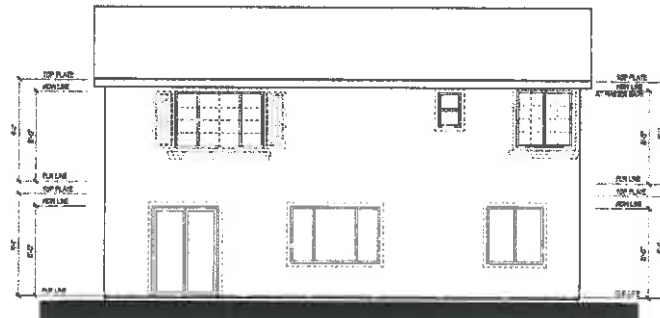
Left Elevation



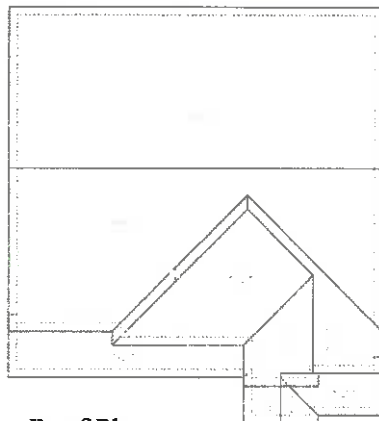
Front Elevation



Right Elevation



Rear Elevation



Roof Plan

Plan 5B - Craftsman

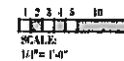
(Color Scheme 5)

Pacific Melrose

Single-Family Detached

Romoland, California

Pacific Communities Builder, Inc.

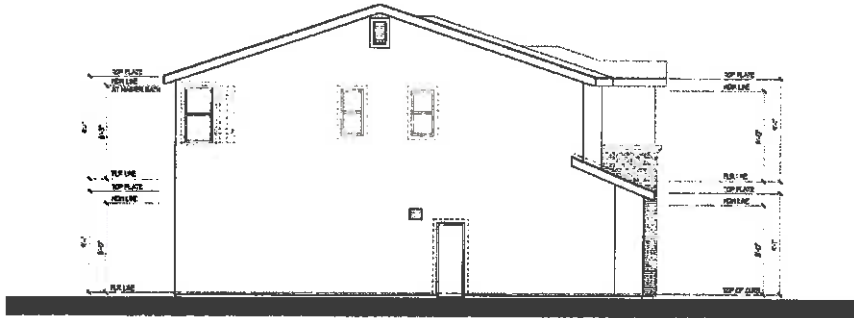


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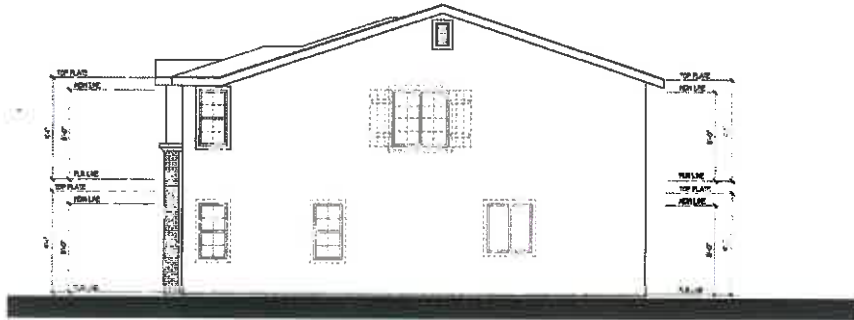
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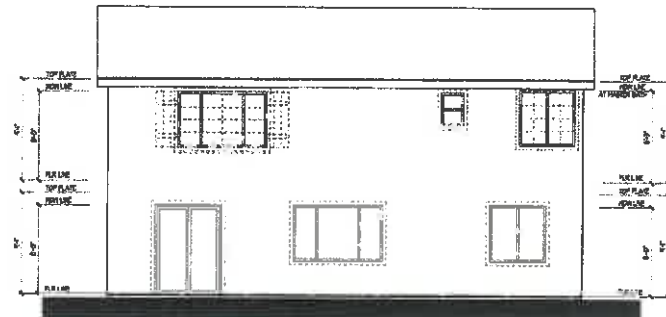
Left Elevation



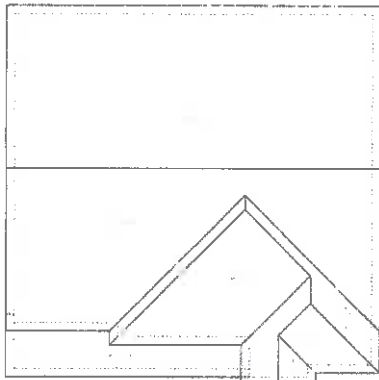
Front Elevation



Right Elevation



Rear Elevation



Roof Plan

Plan 5C - Tuscan

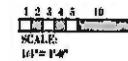
(Color Scheme 8)

Pacific Melrose

Single-Family Detached

Romoland, California

Pacific Communities Builder, Inc.



Subdivision Development Plan

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5.5



Plan 1A

Plan 3B

Plan 2C

Plan 4B

Plan 5B

Pacific Melrose
Single-Family Detached
Romoland, California
Pacific Communities Builder, Inc.

NOTICE OF PUBLIC HEARING RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the application described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. For more information please contact **ALUC Planner Paul Rull at (951) 955-6893**. The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan.

The County of Riverside Planning Department will hold hearings on this item and should be contacted on non-ALUC issues. For more information please contact County of Riverside Planner Mr. David Alvarez at (951) 955-5719.

The proposed project application may be viewed and written comments may be submitted at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Thursday from 8:00 a.m. to 5:00 p.m., except Monday September 2 (Labor Day), and by prescheduled appointment on Friday, September 6, from 9:00 a.m. to 5:00 p.m.

PLACE OF HEARING: Riverside County Administration Center
4080 Lemon Street, 1st Floor Board Chambers
Riverside California

DATE OF HEARING: September 12, 2019

TIME OF HEARING: 9:30 A.M.

CASE DESCRIPTION:

ZAP1376MA19 – Pacific Communities Builder, Inc. (Representative: Ronald Freeman) – County of Riverside Case Nos. CZ1800012 (Change of Zone), TTM37358 (Tentative Tract Map). A proposal to divide 45.6 gross acres located northerly of Mapes Road, westerly of Antelope Road, and easterly of Sherman Road into 154 single family residential lots, and change the zoning of the site from Rural Residential (R-R) and One-Family Dwelling (R-1) to One-Family Dwelling (R-1) (Airport Compatibility Zones C2 and D of the March Air Reserve Base/Inland Port Airport Influence Area).



RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

P.M.
9/12/19
MADON
2019
C2 + D

APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC CASE NUMBER: ZAP1376MA19 DATE SUBMITTED: 7/15/19

APPLICANT / REPRESENTATIVE / PROPERTY OWNER CONTACT INFORMATION

Applicant Pacific Communities Builder, Inc. Phone Number 949-660-8988
Mailing Address 1000 Dove Street Suite 300 Email Ronald@pcbinc.com
Newport Beach, CA 92660

Representative Ronald Freeman Phone Number 949-577-3946
Mailing Address 1000 Dove Street Suite 300 Email Ronald@pcbinc.com
Newport Beach, CA 92660

Property Owner CT. Capital, LLC and Cal-Equity, LP Phone Number 949-660-8988
Mailing Address 1000 Dove Street Suite 300 Email Ronald@pcbinc.com
Newport Beach, CA 92660

LOCAL JURISDICTION AGENCY

Local Agency Name County of Riverside Phone Number 951-955-5719
Staff Contact David Alvarez Email daalvarez@RIVCO.ORG
Mailing Address 4080 Lemon Street Case Type

Riverside, CA 92501
Local Agency Project No TTM 37358
CZ1800012
 General Plan / Specific Plan Amendment
 Zoning Ordinance Amendment
 Subdivision Parcel Map / Tentative Tract
 Use Permit
 Site Plan Review/Plot Plan
 Other

PROJECT LOCATION

Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways

Street Address Mapes Road, between Ball Road and Antelope Road
Assessor's Parcel No. 327-463-010; 327-462-028; 327-180-005, 006, 013 Gross Parcel Size 45.6
Subdivision Name Pacific Melrose II Nearest Airport and distance from Airport March AFB
Lot Number Proposed 154 residential lots

PROJECT DESCRIPTION

If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed

Existing Land Use (describe) Vacant Land. No structures.

Proposed Land Use (describe)	154 Single Family Residential Houses	
For Residential Uses	Number of Parcels or Units on Site (exclude secondary units)	154
For Other Land Uses (See Appendix C)	Hours of Operation	
	Number of People on Site	Maximum Number
	Method of Calculation	
Height Data	Site Elevation (above mean sea level)	1438 to 1453 ft.
	Height of buildings or structures (from the ground)	26' above finished slab ft.
Flight Hazards	Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
	If yes, describe	

- A. **NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.
- B. **REVIEW TIME:** Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.
- C. **SUBMISSION PACKAGE:**
1. Completed ALUC Application Form
 1. ALUC fee payment
 1. Plans Package (24x36 folded) (site plans, floor plans, building elevations, grading plans, subdivision maps)
 1. Plans Package (8.5x11) (site plans, floor plans, building elevations, grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
 1. CD with digital files of the plans (pdf)
 1. Vicinity Map (8.5x11)
 1. Detailed project description
 1. Local jurisdiction project transmittal
 3. Gummed address labels for applicant/representative/property owner/local jurisdiction planner
 3. Gummed address labels of all surrounding property owners within a 300 foot radius of the project site. (Only required if the project is scheduled for a public hearing Commission meeting)

**COUNTY OF RIVERSIDE
AIRPORT LAND USE COMMISSION**

STAFF REPORT

AGENDA ITEM: 3.2

HEARING DATE: September 12, 2019

CASE NUMBER: ZAP1371MA19 – Sares-Regis Group/SRG Perris, LP.
(Representatives: EPD Solutions and Johnson Aviation
[regarding the solar energy glare study])

APPROVING JURISDICTION: County of Riverside

JURISDICTION CASE NO: PPT190011 (Plot Plan)

MAJOR ISSUES: The County of Riverside Climate Action Plan requires nonresidential development to utilize on-site renewable energy production (usually from photovoltaic solar panels) to meet 20 percent of total energy demand, as a means to offset greenhouse gas emissions, unless infeasible. (A determination that a project would be hazardous to air traffic in conjunction with an Airport Land Use Commission review is acknowledged as a factor that may result in infeasibility. In that case, the applicant is nevertheless required to install on-site renewable energy production to the greatest extent feasible.) An initial proposal to overlay all of Building “B” with solar panels without anti-reflective coating with a tilt of 15 degrees and an orientation of 140 degrees resulted in a finding that Flight Path 1 would be affected by 21 minutes of “yellow” level glare (potential for temporary after-image), a level which the Federal Aviation Administration (FAA) has determined to be unacceptable. The applicant worked with the project consultants (including Johnson Aviation) to identify a solar panel configuration that provides for renewable energy production to the greatest feasible extent consistent with maintaining glare at the acceptable “green” level. The revised proposal provides for 30,000 square feet of solar panels on each of the two buildings with anti-reflective coating, a fixed tilt of 10 degrees with no rotation, and an orientation of 160 degrees. This proposal would result in “green” level glare (low potential for temporary after-image) within the 2 mile approach to Runway 32 and within the Air Force traffic patterns; “green” level glare complies with the FAA Interim Policy pertaining to acceptable levels of glare. Given that the proposal meets the FAA policy requirements with the applicable flight tracks modeled, March Air Reserve Base has indicated no further concerns.

RECOMMENDATION: Staff recommends that the Commission open the public hearing, consider testimony, and find the proposed Plot Plan CONSISTENT with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, subject to the conditions included herein.

PROJECT DESCRIPTION: The applicant proposes to construct two industrial manufacturing buildings with mezzanines totaling 710,736 square feet on a combined total 35.76 acres. The applicant also proposes rooftop solar panels totaling 60,000 square feet (30,000 square feet on each building).

PROJECT LOCATION: The site is located northerly of Oleander Avenue, westerly of Decker Road, southerly of Nandina Avenue, and easterly of Day Street, in the unincorporated community of Mead Valley, approximately 5,400 feet southwesterly of the southerly end of Runway 14-32 at March Air Reserve Base.

LAND USE PLAN: 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan

- a. Airport Influence Area: March Air Reserve Base
- b. Land Use Policy: Zone C2 High Terrain Zone
- c. Noise Levels: Below 60 CNEL from aircraft

BACKGROUND:

Non-Residential Average Land Use Intensity: Pursuant to the Airport Land Use Compatibility Plan for the March Air Reserve Base/Inland Port Airport, the site is located within Compatibility Zone C2 and partially within the High Terrain Zone (although the project's development footprint is located outside of the High Terrain Zone). Zone C2 limits average intensity to 200 people per acre.

Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan, and the March Air Reserve Base/Inland Port Airport Compatibility Plan, the following rates were used to calculate potential occupancy for the proposed building in Compatibility Zone C2:

- Office – 1 person per 200 square feet (with 50% reduction),
- Manufacturing – 1 person per 200 square feet.

The project proposes two industrial manufacturing buildings with mezzanines totaling 710,736 square feet on a combined total 35.76 acres, accommodating 3,554 people, resulting in an average intensity of 99 people per acre, which is consistent with the Compatibility Zone C2 criterion of 200.

Looking at each building individually (as adjusted through a future lot line adjustment):

- Building A consists of 363,367 square feet, including 348,367 square feet of industrial manufacturing area and 15,000 square feet second floor office mezzanine area, accommodating 1,817 people on 18.5 acres, resulting in an average intensity of 98 people per acre, which is consistent with the Compatibility Zone C2 criterion of 200; and
- Building B consists of 347,369 square feet, including 332,369 square feet of industrial manufacturing area and 15,000 square feet of second floor office mezzanine area,

accommodating 1,737 people on 17.26 acres, resulting in an average intensity of 101 people per acre, which is also consistent with the Compatibility Zone C2 criterion of 200.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per vehicle and 1.0 persons per truck trailer parking/dock space in the absence of more precise data). Based on the number of parking spaces (471 spaces) and truck trailer spaces (111 spaces) provided, the total occupancy would be estimated at 818 people for an average intensity of 23 people per acre, which is consistent with the Compatibility Zone C2 average criterion of 200.

Non-Residential Single-Acre Land Use Intensity: Compatibility Zone C2 limits maximum single-acre intensity to 500 people. There are no risk-reduction design bonuses available, as March Air Reserve Base/Inland Port Airport is primarily utilized by large aircraft weighing more than 12,500 pounds.

Based on the site plan provided and the occupancies as previously noted, the maximum single-acre area for each building would consist of 43,560 square feet of manufacturing area and 15,000 square feet of second floor office mezzanine area, resulting in a single acre occupancy of 293 people, which is consistent with the Compatibility Zone C2 single acre criterion of 500.

March Air Reserve Base/United States Air Force Input: Given that the project site is located in Zone C2 southwesterly of the southerly runway at March Air Reserve Base, the March Air Reserve Base staff was notified of the project, specifically the rooftop solar panels, and sent a solar glare hazard analysis study for their review. On August 22, 2019, staff received an e-mail from the Base Civil Engineer at March Air Reserve Base advising that the Base had no further concerns, given that the project complies with the Federal Aviation Administration requirements relating to allowable glare levels. (See analysis, below.)

Renewable Energy and Flight Hazards: The applicant proposes that photovoltaic (PV) panel structures totaling 60,000 square feet (30,000 square feet for each building) be located on the rooftops of the two industrial buildings within Compatibility Zone C2.

The County of Riverside Climate Action Plan requires nonresidential development to utilize on-site renewable energy production (usually from photovoltaic solar panels) to meet 20 percent of total energy demand, as a means to offset greenhouse gas emissions, unless infeasible. (A determination that a project would be hazardous to air traffic in conjunction with an Airport Land Use Commission review is acknowledged as a factor that may result in infeasibility. In that case, the applicant is nevertheless required to install on-site renewable energy production to the greatest extent feasible.)

Glint and Glare/Reflectivity

Based on the Federal Aviation Administration's Interim Policy for Review of Solar Energy System Projects on Federally Obligated Airports, no glare potential or low potential for temporary after-

image (“green” level) are acceptable levels of glare on final approach (within 2 miles from end of runway) for solar facilities located on airport property. However, potential for temporary after-image (“yellow” level) is not an acceptable level of glare on final approach. No glare is permitted at air traffic control towers.

An initial proposal to overlay all of Building “B” with solar panels without anti-reflective coating with a fixed tilt of 15 degrees with no rotation and an orientation of 140 degrees resulted in a finding using the web-based Forge Solar analysis that Flight Path 1 would be affected by 21 minutes of “yellow” level glare (potential for temporary after-image), a level which the FAA has determined to be unacceptable.

The applicant worked with project consultants to identify a solar panel configuration that provides for renewable energy production to the greatest extent feasible consistent with maintaining glare at the acceptable “green” level. The revised proposal provides for 30,000 square feet of solar panels on each of the two buildings with anti-reflective coating, a fixed tilt of 10 degrees with no rotation, and an orientation of 160 degrees. Johnson Aviation has submitted a glare analysis utilizing the web-based Forge Solar, a copy of which is attached hereto. The analysis was based on a 2 mile straight in approach (as per FAA Interim Policy standards) to runway 32, and also based on the traffic patterns as identified by March Air Reserve Base staff (Runway 12/30 General Aviation, Runway 14/32 General Aviation, Runway 14/32 C-17/KC-135, Runway 14/32 Overhead). The analysis utilized a glide slope approach of 3.0 degrees for the approach. No glare would affect the Air Traffic Control Tower.

The analysis concluded that glare would occur on the approach to runway 32 and also within the Air Force traffic patterns. Evaluation of the 2 mile flight approach to runway 32 indicates that the panels would result in low potential for temporary after-image (“green” level glare) at distances of 0.25 to 0.75 miles during the late afternoon from May through September. Evaluation of the Air Force traffic patterns indicates that the panels would also result in low potential for temporary after-image (“green” level glare) within each of the traffic patterns, during mornings and late afternoons throughout the year.

The total amount of glare time experienced annually is 82,310 minutes for “green” level glare (11,103 minutes within the 2 mile flight approach, 71,207 minutes within the Air Force traffic patterns).

- A total of 2,838 minutes (annually) of low potential “green” glare is projected to occur within the Runway 12/30 General Aviation traffic pattern, of which 0 minutes of glare would occur within the 2 mile approach to the runway, and 2,838 minutes of glare would occur within the Runway 30 General Aviation pattern route, and would last up to 20 minutes a day from October through March between 4:00 p.m. to 5:00 p.m. (pacific standard time).
- A total of 61,101 minutes (annually) of low potential “green” glare is projected to occur

within the Runway 14/32 General Aviation traffic pattern, of which 3,429 minutes of glare would occur within the 2 mile approach to the runway, and would last up to 20 minutes a day from April through August between 6:00 p.m. to 7:00 p.m. (pacific daylight time) at a distance of 0.25 miles to 0.75 miles from the runway. A total of 57,672 minutes of glare would occur within the Runway 14 and 32 traffic patterns, and would last up to 80 minutes a day throughout the year in the late mornings through to late afternoons.

- A total of 6,449 minutes (annually) of low potential “green” glare is projected to occur within the Runway 14/32 C-17/KC-135 traffic pattern, of which 3,429 minutes of glare would occur within the 2 mile approach to the runway, and would last up to 20 minutes a day from April through August between 6:00 p.m. to 7:00 p.m. (pacific daylight time) at a distance of 0.25 miles to 0.75 miles from the runway. A total of 3,020 minutes of glare would occur within the Runway 32 traffic pattern, and would last up to 15 minutes a day from March to October in the early mornings through to late afternoons.
- A total of 11,922 minutes (annually) of low potential “green” glare is projected to occur within the Runway 14/32 Overhead traffic pattern, of which 4,245 minutes of glare would occur within the 2 mile approach to the runway, and would last up to 30 minutes a day from April through August between 5:30 p.m. to 6:30 p.m. (pacific daylight time) at a distance of 0.25 miles to 0.75 miles from the runway. A total of 7,677 minutes of glare would occur within the Runway 32 overhead traffic pattern, and would last up to 40 minutes a day from April to October in the late afternoons.

Electrical and Communication Interference

The applicant has indicated that they do not plan to utilize equipment that would interfere with aircraft communications. The PV panels themselves present little risk of interfering with radar transmission due to their low profiles. In addition, solar panels do not emit electromagnetic waves over distances that could interfere with radar signal transmissions, and any electrical facilities that do carry concentrated current will be buried beneath the ground and away from any signal transmission. There is no radar transmission or receiving facilities within the site.

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited or discouraged in Compatibility Zone C2.

Noise: The March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan depicts the site as being outside the 60 CNEL range from aircraft noise. As a primarily industrial use not sensitive to noise (and considering typical anticipated building construction noise attenuation of approximately 20 dBA), the manufacturing area would not require special measures to mitigate aircraft-generated noise. However, a condition is included to provide for adequate noise attenuation within office areas of the building.

Part 77: The elevation of Runway 14-32 at its southerly terminus is 1,488 feet above mean sea level (1,488 feet AMSL). At a distance of approximately 5,400 feet from the runway to the site, Federal Aviation Administration (FAA) review would be required for any structures with top of roof elevation exceeding 1,542 feet AMSL. The site's finished floor elevation is 1,626 feet AMSL and the proposed building height is 43 feet, for a top point elevation of 1,669 feet AMSL. Therefore, review by the FAA Obstruction Evaluation Service (FAA OES) is required. Submittal to the FAAOES was made, and Aeronautical Study Numbers 2019-AWP-8541-OE through 2019-AWP-8548-OE were assigned to this project. Determinations of no hazard to air navigation were issued on August 15, 2019.

While the project's parcels are partially located within a High Terrain Zone (which triggers the inclusion of a condition for an aviation easement dedication and disclosure), which would normally require FAA OES review, the project's development footprint is located outside the High Terrain Zone boundary. However, as noted above, FAA OES review was required and completed, as the project's top point elevation exceeds the notification threshold.

Open Area: None of the Compatibility Zones for the March Air Reserve Base/Inland Port ALUCP require open area specifically.

CONDITIONS:

1. Any outdoor lighting installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site, in accordance with Note A on Table 4 of the Mead Valley Area Plan.
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area.
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.

3. The following uses/activities are specifically prohibited at this location: trash transfer stations that are open on one or more sides; recycling centers containing putrescible wastes; construction and demolition debris facilities; wastewater management facilities; incinerators; noise-sensitive outdoor nonresidential uses; and hazards to flight. Children's schools are discouraged.
4. The following uses/activities are not included in the proposed project, but, if they were to be proposed through a subsequent use permit or plot plan, would require subsequent Airport Land Use Commission review:

Restaurants and other eating establishments; day care centers; health and exercise centers; churches, temples, or other uses primarily for religious worship; theaters.
5. Prior to issuance of any building permits, the landowner shall convey and have recorded an aviation easement to the March Inland Port Airport Authority. Contact March Joint Powers Authority at (951) 656-7000 for additional information.
6. The attached notice shall be given to all prospective purchasers of the property and lessees/tenants of the building.
7. Any detention basins on the site (including water quality management basins) shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention basins that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.
8. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.
9. Noise attenuation measures shall be incorporated into the design of the office areas of the structures, to the extent such measures are necessary to ensure that interior noise levels from aircraft operations are at or below 45 CNEL.
10. This project has been evaluated for a total of 710,736 square feet of manufacturing area, including 30,000 square feet of office area. Any increase in building area or change in use other than for warehouse, office and manufacturing uses will require an amended review by the Airport Land Use Commission.

11. Solar panels shall incorporate anti-reflective coating and shall be fixed with no rotation. Panels shall have a tilt of 10 degrees and orientation of 160 degrees. Solar panel areas shall be limited to 30,000 square feet per building.
12. Any revisions to the solar panels will require a new solar glare analysis to ensure that the project does not create “yellow” level glare, and require ALUC review.
13. In the event that any incidence of glint, glare, or flash affecting the safety of air navigation occurs as a result of project operation, upon notification to the airport operator of an incidence, the airport operator shall notify the project operator in writing. Within 30 days of written notice, the project operator shall be required to promptly take all measures necessary to eliminate such glint, glare, or flash. An “incidence” includes any situation that results in an accident, incident, “near-miss,” or specific safety complaint regarding an in-flight experience to the airport operator or to federal, state, or county authorities responsible for the safety of air navigation. The project operator shall work with the airport operator to prevent recurrence of the incidence. Suggested measures may include, but are not limited to, reprogramming the alignment of the panels, covering them at the time of day when incidences of glare occur, or wholly removing panels to diminish or eliminate the source of the glint, glare, or flash. For each such incidence made known to the project operator, the necessary remediation shall only be considered to have been fulfilled when the airport operator states in writing that the situation has been remediated to the airport operator’s satisfaction.
14. In the event that any incidence of electrical interference affecting the safety of air navigation occurs as a result of project operation, upon notification to the airport operator of an incidence, the airport operator shall notify the project operator in writing. Within 30 days of written notice, the project operator shall be required to promptly take all measures necessary to eliminate such interference. An “incidence” includes any situation that results in an accident, incident, “near-miss,” report by airport personnel, or specific safety complaint to the airport operator or to federal, state, or county authorities responsible for the safety of air navigation. The project operator shall work with the airport operator to prevent recurrence of the incidence. For each such incidence made known to the project operator, the necessary remediation shall only be considered to have been fulfilled when the airport operator states in writing that the situation has been remediated to the airport operator’s satisfaction.
15. The Federal Aviation Administration has conducted aeronautical studies of the proposed buildings (Aeronautical Study Nos. 2019-AWP-8541-OE through 2019-AWP-8548-OE) and has determined that neither marking nor lighting of the structures is necessary for aviation safety. However, if marking and/or lighting for aviation safety are accomplished on a voluntary basis, such marking and/or lighting (if any) shall be installed in accordance with FAA Advisory Circular 70/7460-1 L Change 2 and shall be maintained in accordance therewith for the life of the project.

16. The proposed buildings shall not exceed a height of 62 feet above ground level and a maximum elevation at top point of 1,669 feet above mean sea level.
17. The maximum height and top point elevation specified above shall not be amended without further review by the Airport Land Use Commission and the Federal Aviation Administration; provided, however, that reduction in structure height or elevation shall not require further review by the Airport Land Use Commission.
18. Temporary construction equipment used during actual construction of the buildings shall not exceed 62 feet in height and a maximum elevation of 1,669 feet above mean sea level, unless separate notice is provided to the Federal Aviation Administration through the Form 7460-1 process.
19. Within five (5) days after construction of each proposed structure reaches its greatest height, FAA Form 7460-2 (Part II), Notice of Actual Construction or Alteration, shall be completed by the project proponent or his/her designee and e-filed with the Federal Aviation Administration. (Go to <https://oeaaa.faa.gov> for instructions.) This requirement is also applicable in the event the project is abandoned or a decision is made not to construct the applicable structure(s).

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



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Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2019-AWP-8541-OE

Issued Date: 08/15/2019

Patrick Russell
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**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Building MV-A-1
Location:	Perris, CA
Latitude:	33-51-42.70N NAD 83
Longitude:	117-16-19.21W
Heights:	1630 feet site elevation (SE) 34 feet above ground level (AGL) 1664 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
 Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 2.

This determination expires on 02/15/2021 unless:

- the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- extended, revised, or terminated by the issuing office.
- the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (424) 405-7643, or karen.mcdonald@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2019-AWP-8541-OE.

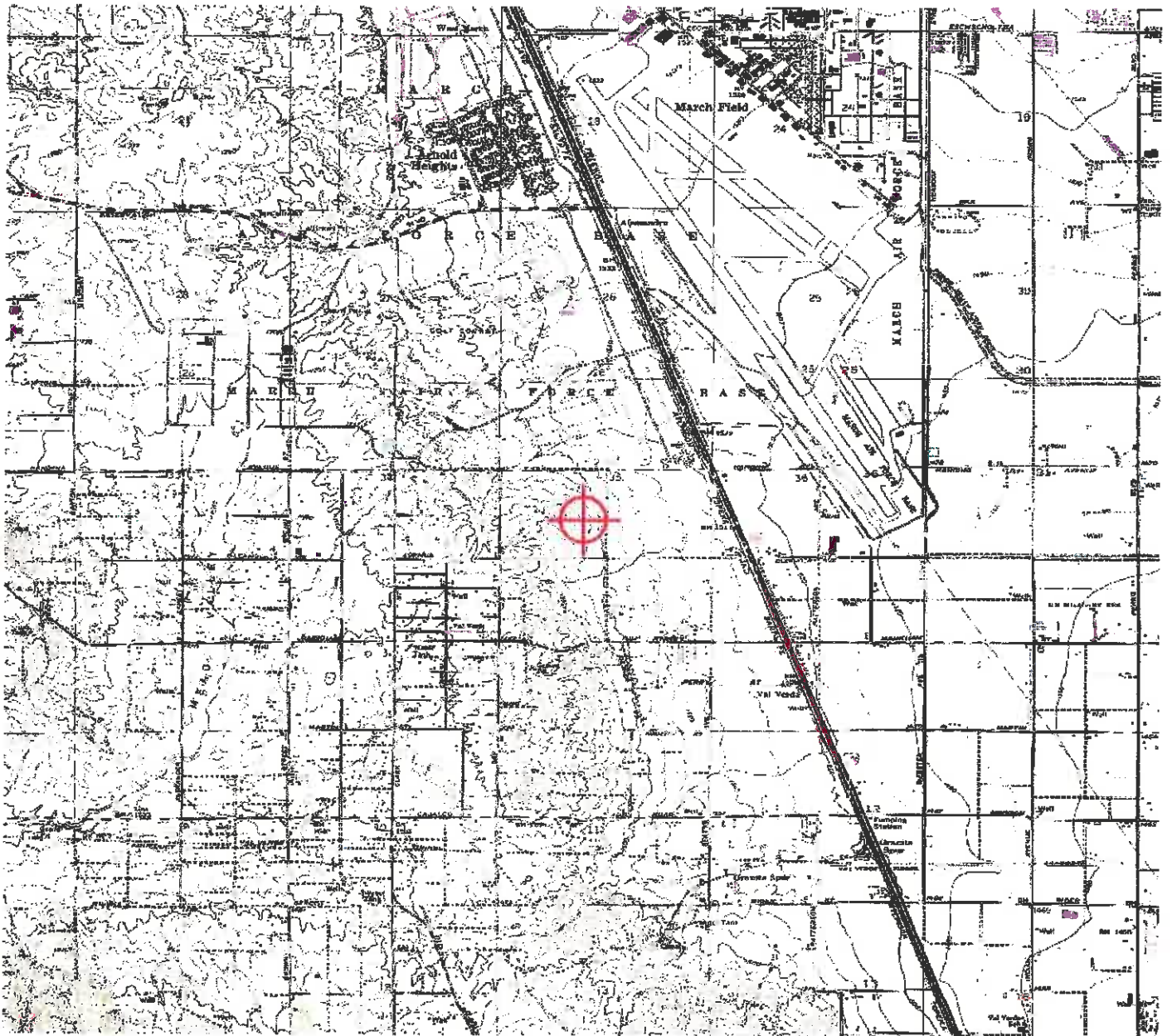
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(DNE)

Karen McDonald
Specialist

Attachment(s)
Map(s)

TOPO Map for ASN 2019-AWP-8541-OE





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Aeronautical Study No.
2019-AWP-8542-OE

Issued Date: 08/15/2019

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**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building MV-A-2
Location: Perris, CA
Latitude: 33-51-42.31N NAD 83
Longitude: 117-16-14.33W
Heights: 1600 feet site elevation (SE)
65 feet above ground level (AGL)
1665 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
 Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 2.

This determination expires on 02/15/2021 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (424) 405-7643, or karen.mcdonald@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2019-AWP-8542-OE.

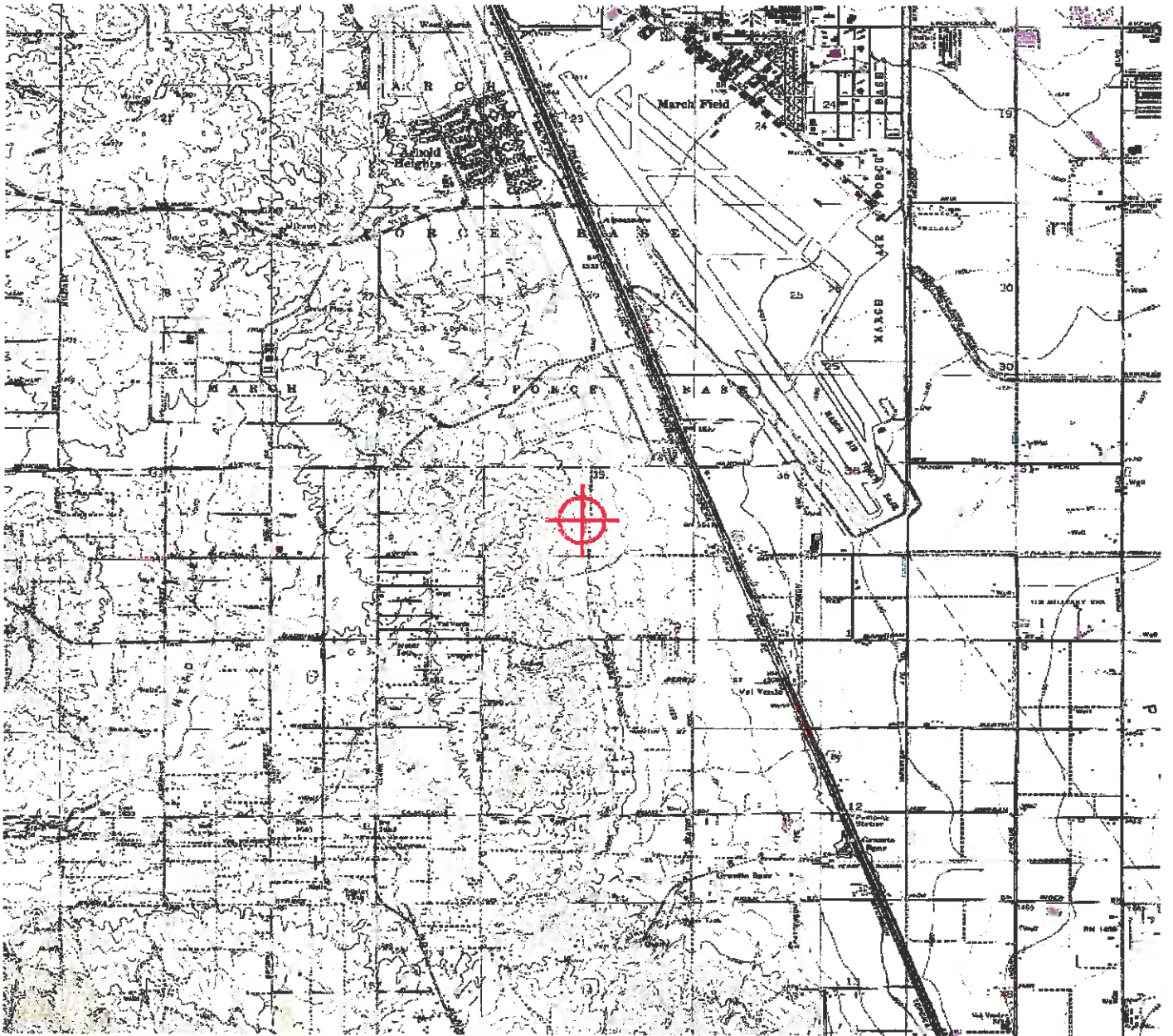
Signature Control No: 411360904-414489802

(DNE)

Karen McDonald
Specialist

Attachment(s)
Map(s)

TOPO Map for ASN 2019-AWP-8542-OE





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Aeronautical Study No.
2019-AWP-8543-OE

Issued Date: 08/15/2019

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**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building MV-A-3
Location: Perris, CA
Latitude: 33-51-33.38N NAD 83
Longitude: 117-16-14.45W
Heights: 1607 feet site elevation (SE)
62 feet above ground level (AGL)
1669 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
 Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 2.

This determination expires on 02/15/2021 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (424) 405-7643, or karen.mcdonald@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2019-AWP-8543-OE.

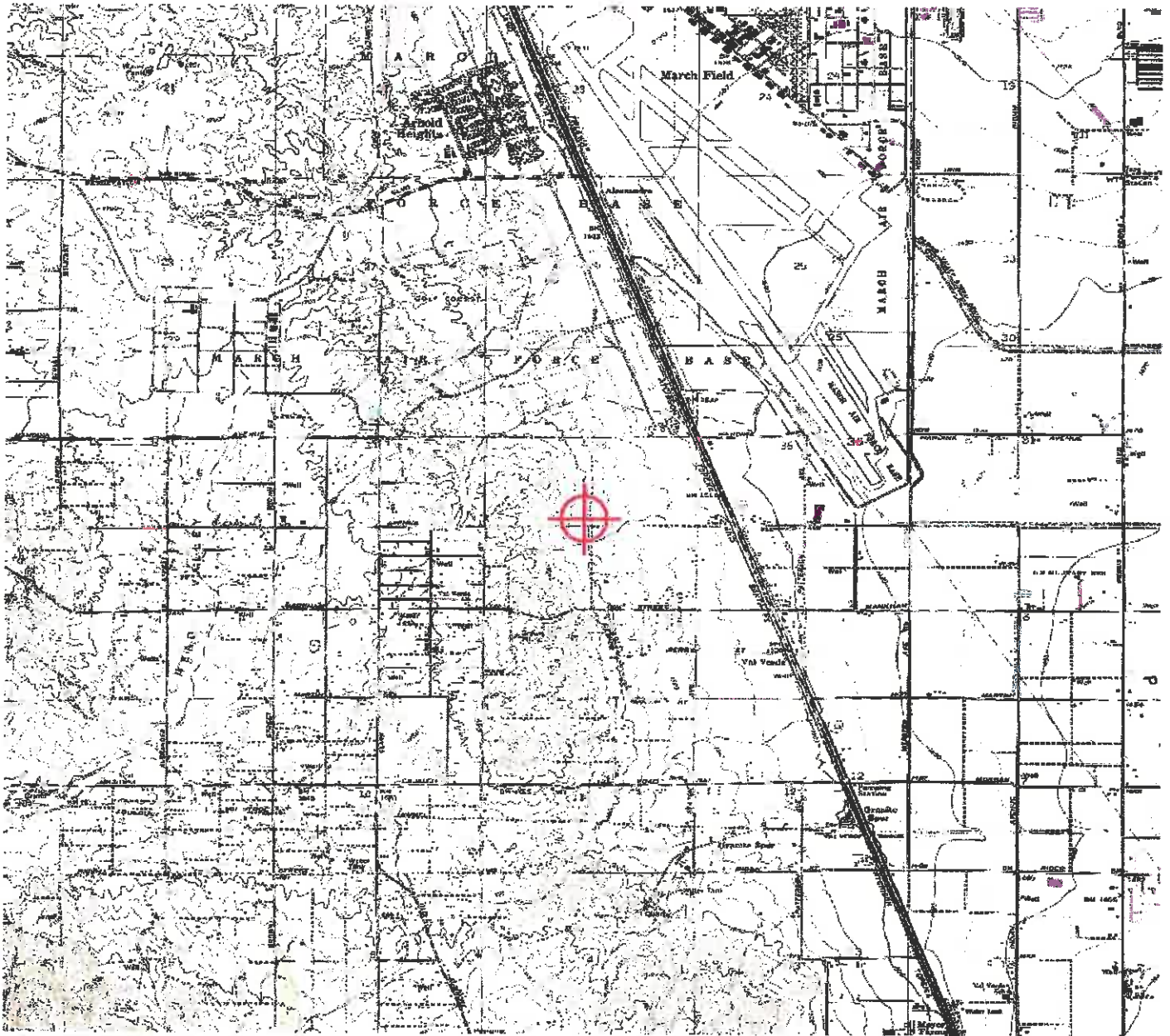
Signature Control No: 411360905-414489807

(DNE)

Karen McDonald
Specialist

Attachment(s)
Map(s)

TOPO Map for ASN 2019-AWP-8543-OE





Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2019-AWP-8544-OE

Issued Date: 08/15/2019

Patrick Russell
SRG Perris, L.P.
18802 Bardeen Avenue
Irvine, CA 92612-1521

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building MV-A-4
Location: Perris, CA
Latitude: 33-51-33.05N NAD 83
Longitude: 117-16-19.31W
Heights: 1620 feet site elevation (SE)
48 feet above ground level (AGL)
1668 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
 Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 2.

This determination expires on 02/15/2021 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

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This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (424) 405-7643, or karen.mcdonald@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2019-AWP-8544-OE.

Signature Control No: 411360906-414489803

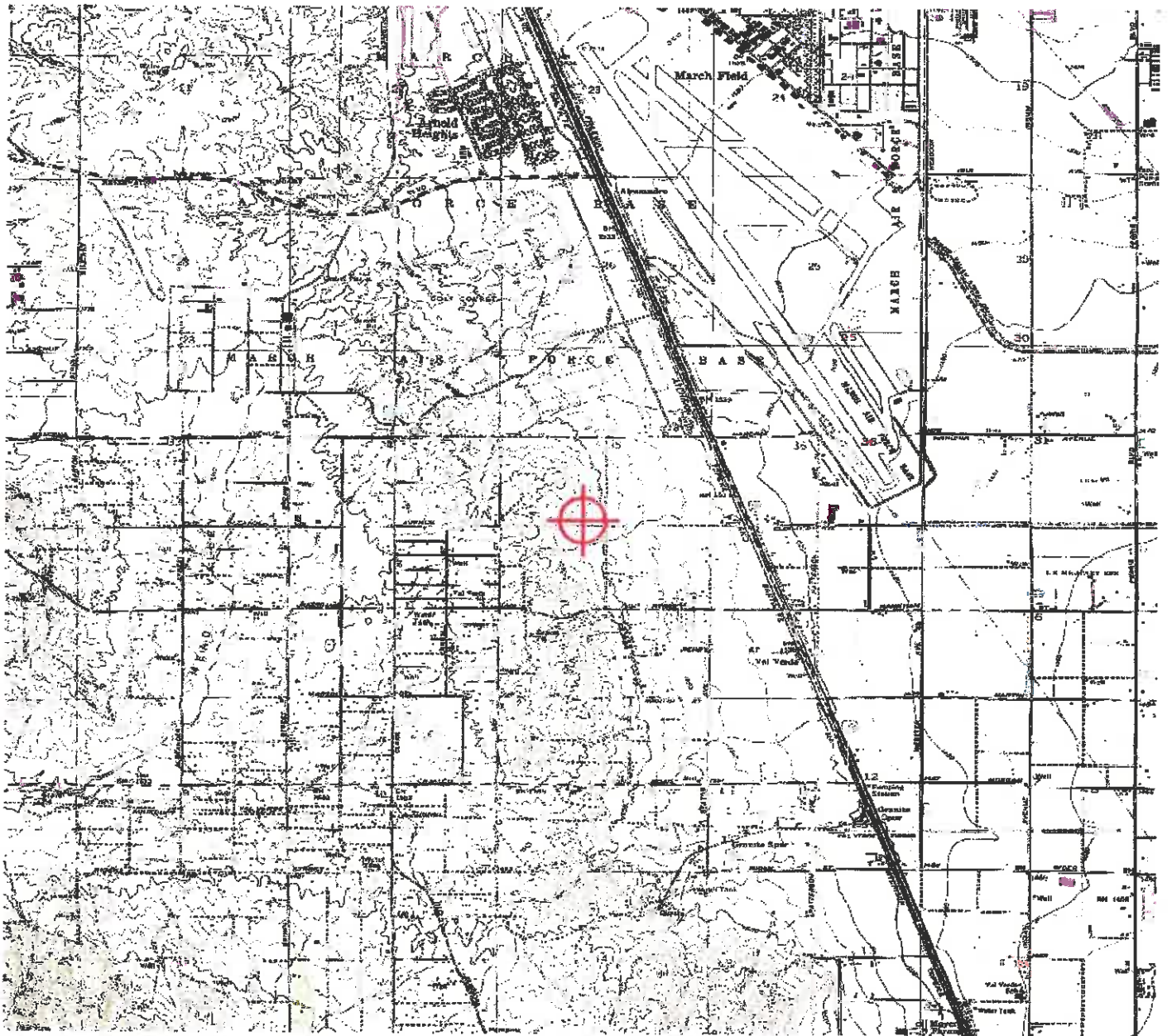
(DNE)

Karen McDonald
Specialist

Attachment(s)

Map(s)

TOPO Map for ASN 2019-AWP-8544-OE





Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2019-AWP-8545-OE

Issued Date: 08/15/2019

Patrick Russell
 SRG Perris, L.P.
 18802 Bardeen Avenue
 Irvine, CA 92612-1521

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building MV-B-1
 Location: Perris, CA
 Latitude: 33-51-56.21N NAD 83
 Longitude: 117-16-19.06W
 Heights: 1597 feet site elevation (SE)
 47 feet above ground level (AGL)
 1644 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 2.

This determination expires on 02/15/2021 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

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This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (424) 405-7643, or karen.mcdonald@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2019-AWP-8545-OE.

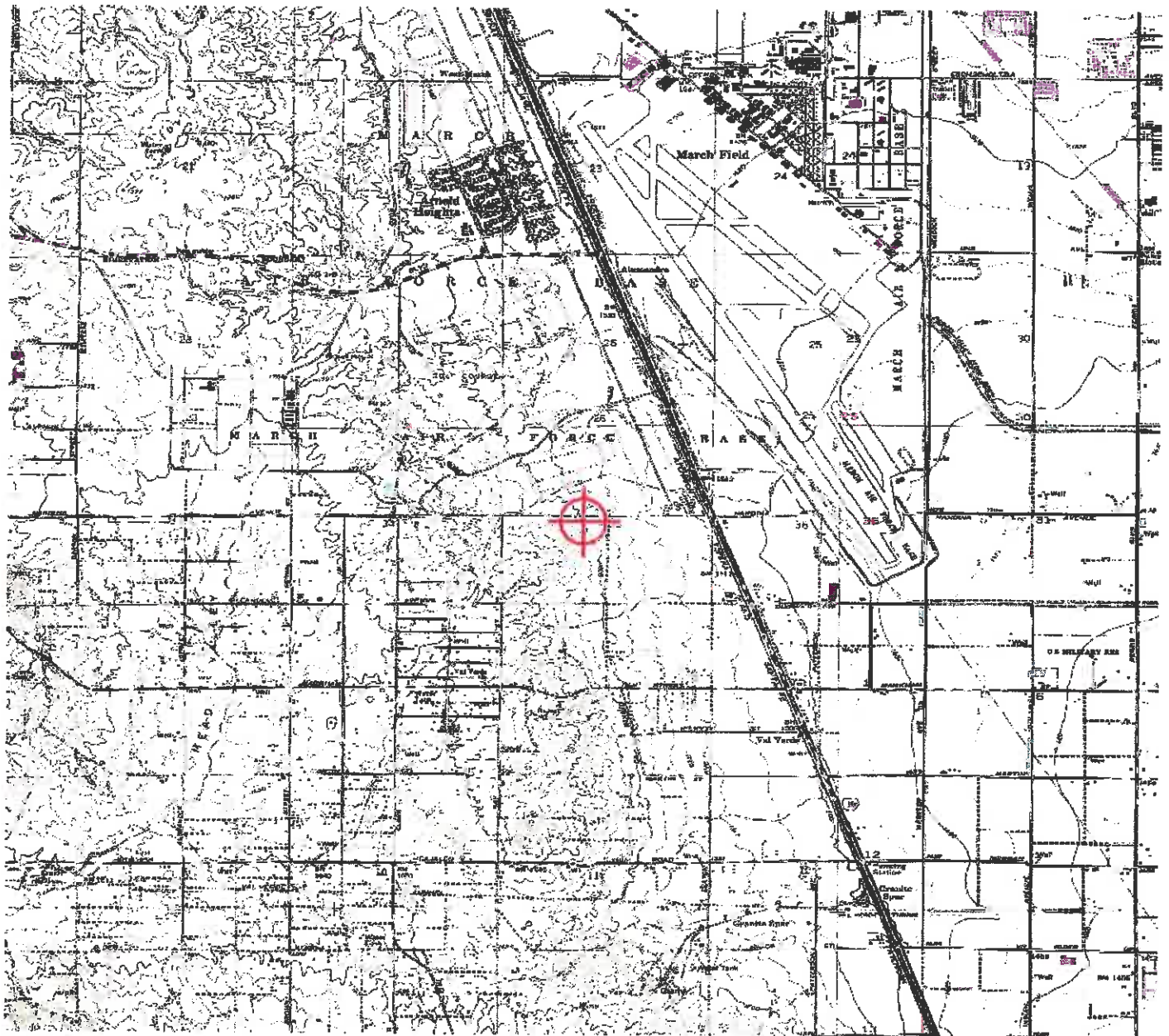
Signature Control No: 411360907-414489809

(DNE)

Karen McDonald
Specialist

Attachment(s)
Map(s)

TOPO Map for ASN 2019-AWP-8545-OE





Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2019-AWP-8546-OE

Issued Date: 08/15/2019

Patrick Russell
 SRG Perris, L.P.
 18802 Bardeen Avenue
 Irvine, CA 92612-1521

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building MV-B-2
 Location: Perris, CA
 Latitude: 33-51-55.75N NAD 83
 Longitude: 117-16-14.22W
 Heights: 1576 feet site elevation (SE)
 69 feet above ground level (AGL)
 1645 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 2.

This determination expires on 02/15/2021 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

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This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (424) 405-7643, or karen.mcdonald@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2019-AWP-8546-OE.

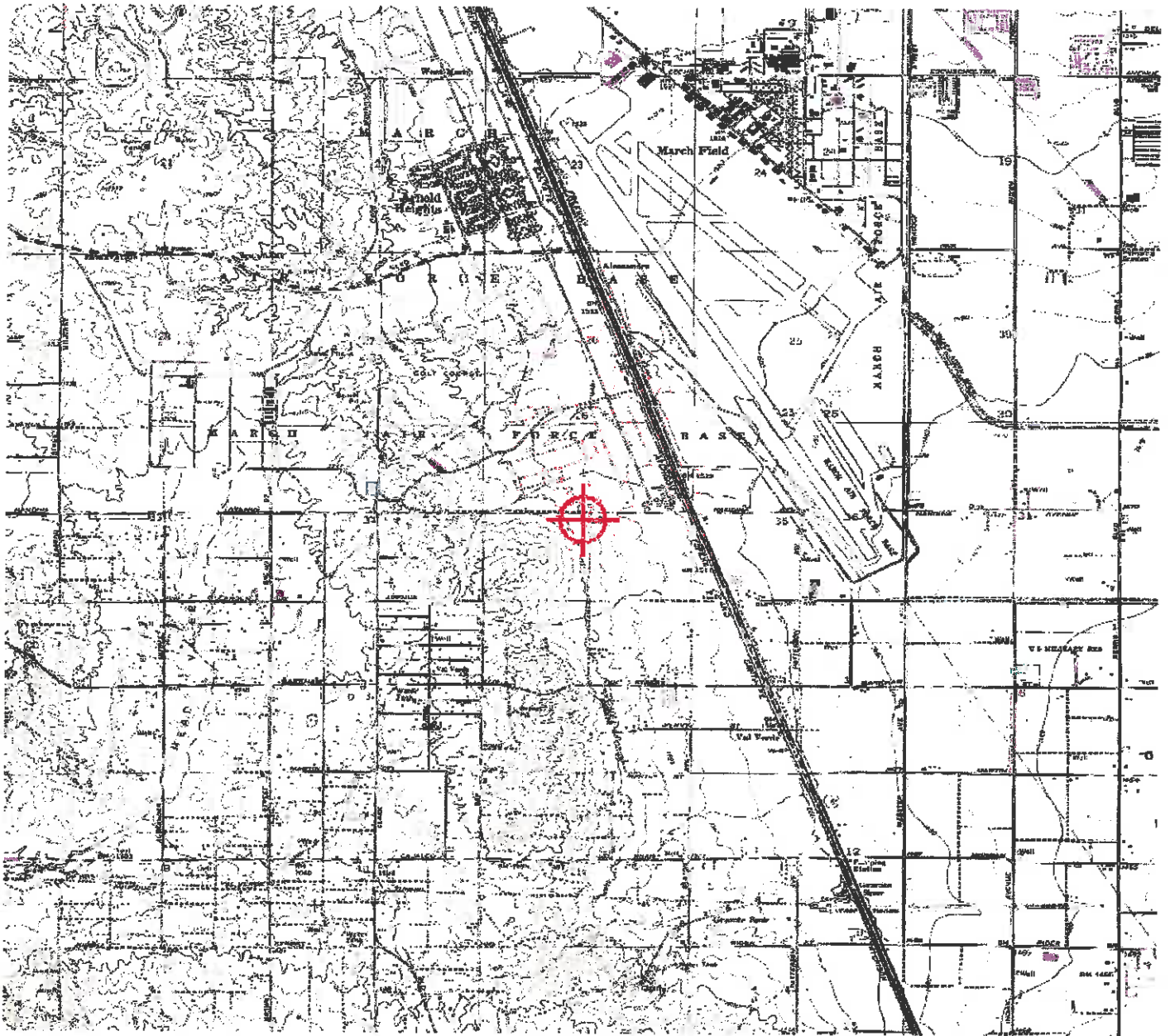
Signature Control No: 411360908-414489805

Karen McDonald
Specialist

(DNE)

Attachment(s)
Map(s)

TOPO Map for ASN 2019-AWP-8546-OE





Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2019-AWP-8547-OE

Issued Date: 08/15/2019

Patrick Russell
SRG Perris, L.P.
18802 Bardeen Avenue
Irvine, CA 92612-1521

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building MV-B-3
Location: Perris, CA
Latitude: 33-51-47.43N NAD 83
Longitude: 117-16-14.31W
Heights: 1597 feet site elevation (SE)
53 feet above ground level (AGL)
1650 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
 Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 2.

This determination expires on 02/15/2021 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

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This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (424) 405-7643, or karen.mcdonald@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2019-AWP-8547-OE.

Signature Control No: 411360909-414489806

(DNE)

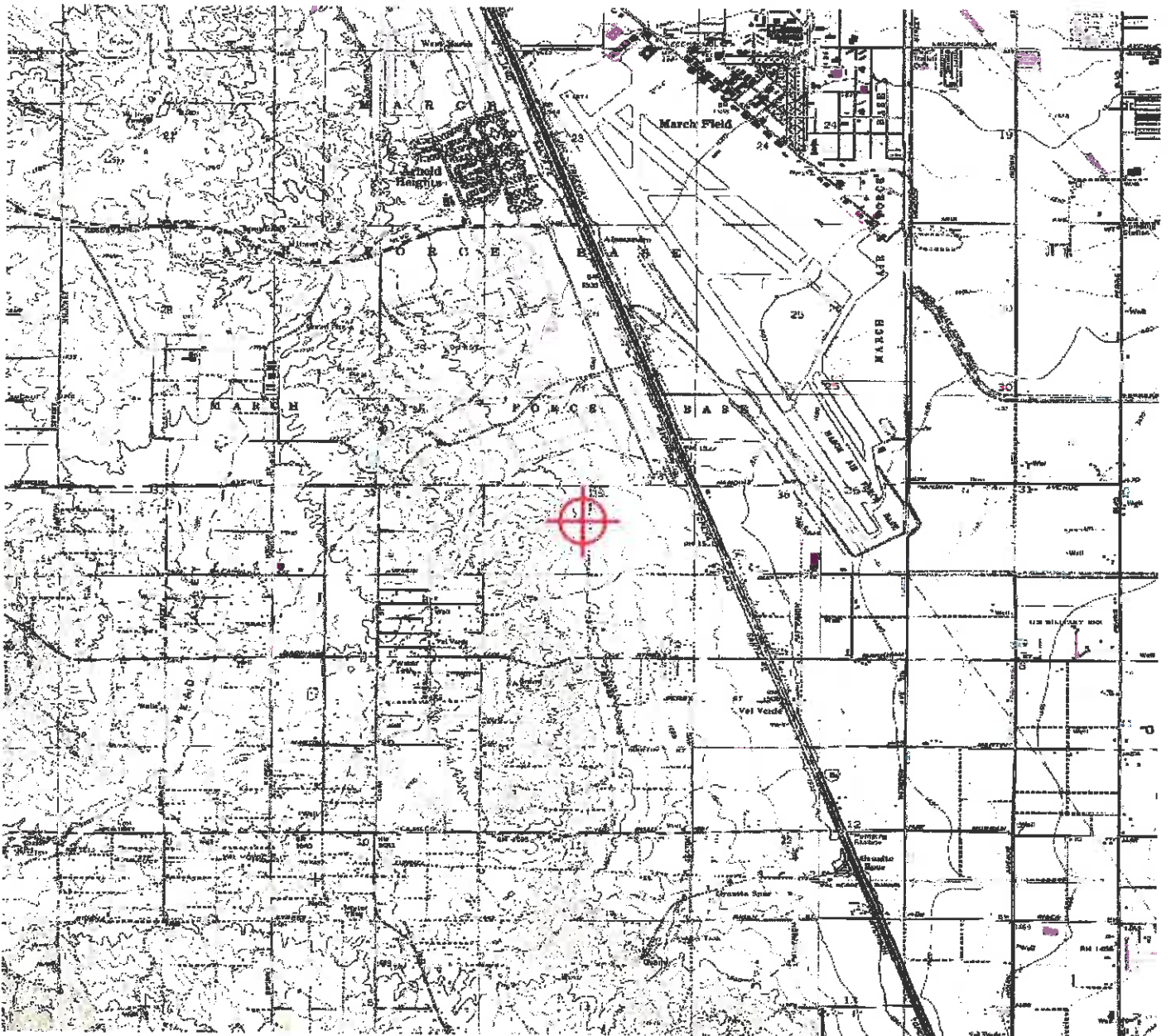
Karen McDonald

Specialist

Attachment(s)

Map(s)

TOPO Map for ASN 2019-AWP-8547-OE





Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2019-AWP-8548-OE

Issued Date: 08/15/2019

Patrick Russell
SRG Perris, L.P.
18802 Bardeen Avenue
Irvine, CA 92612-1521

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building MV-B-4
Location: Perris, CA
Latitude: 33-51-47.06N NAD 83
Longitude: 117-16-19.16W
Heights: 1625 feet site elevation (SE)
24 feet above ground level (AGL)
1649 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
 Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 2.

This determination expires on 02/15/2021 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

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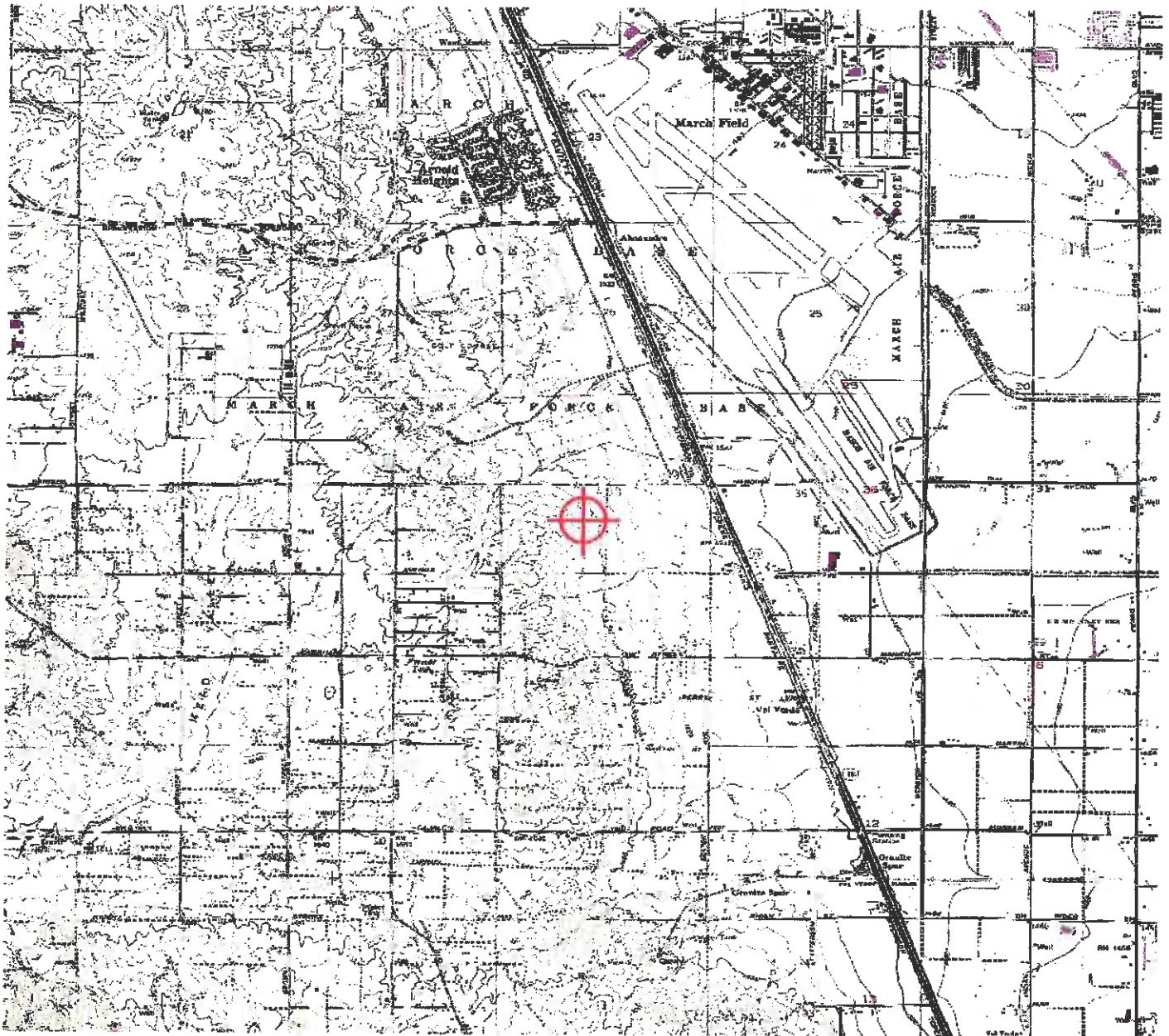
If we can be of further assistance, please contact our office at (424) 405-7643, or karen.mcdonald@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2019-AWP-8548-OE.

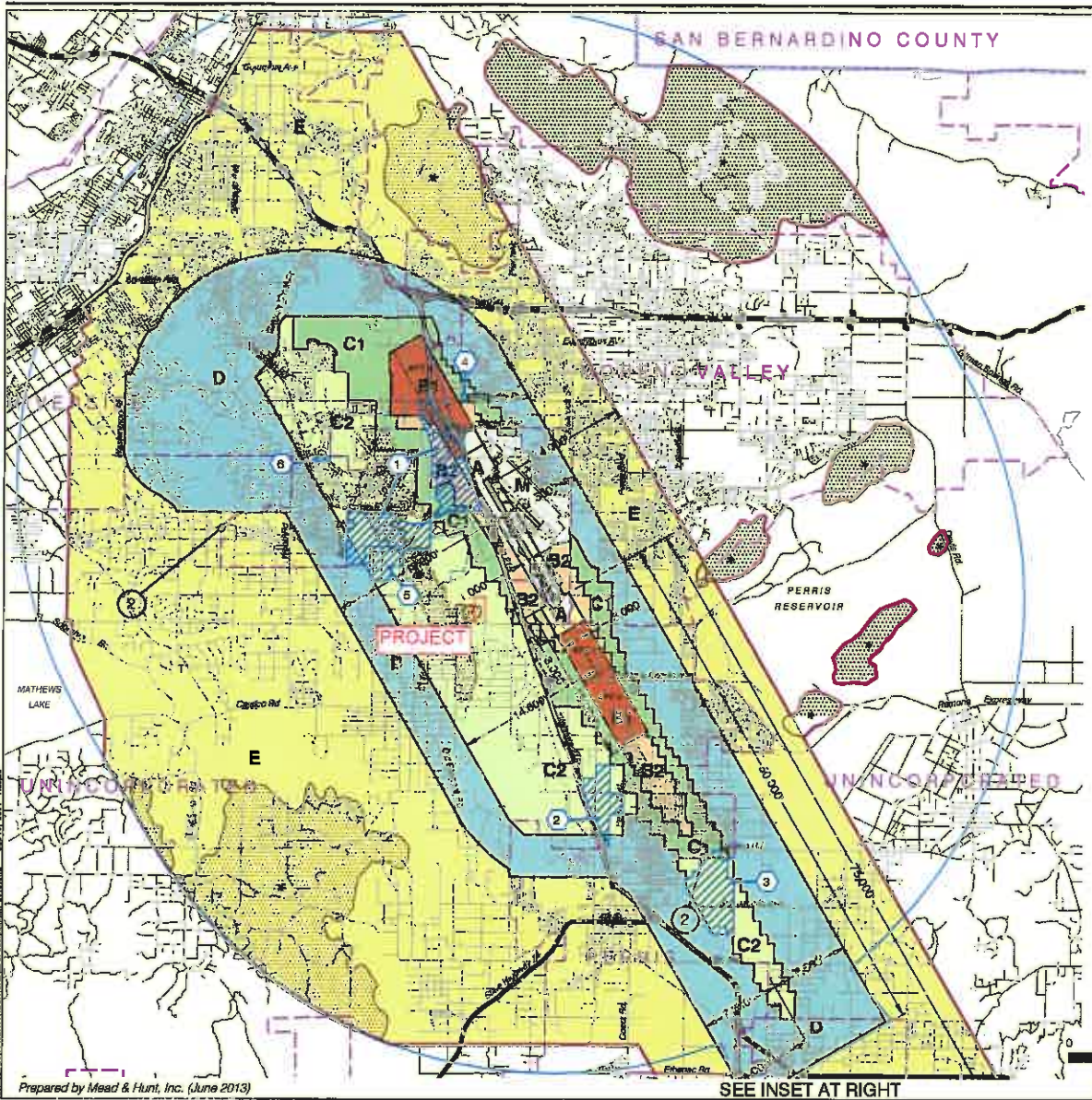
Signature Control No: 411360910-414489804
Karen McDonald
Specialist

(DNE)

Attachment(s)
Map(s)

TOPO Map for ASN 2019-AWP-8548-OE





LEGEND

Compatibility Zones

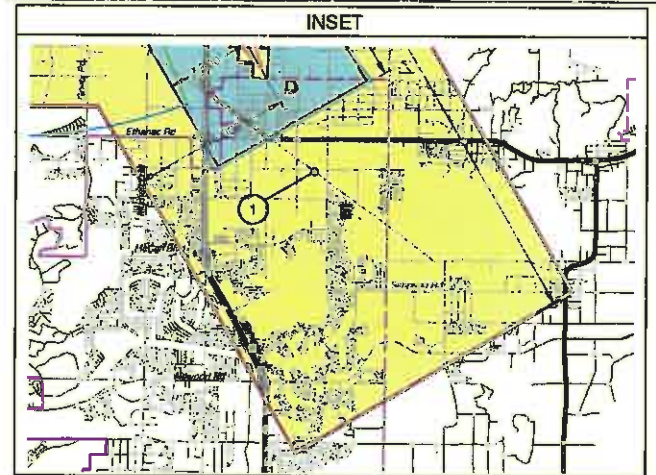
- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- Site-Specific Exceptions (existing local agency commitments to development projects)

- 1 Point at which aircraft on Runway 32 ILS approach descend below 9,000 feet above runway end. Airport Elevation is 1,535 feet MSL.
- 2 Point at which departing aircraft typically reach 3,000 feet above runway end.

- March JPA: March Business Center/Meridian
- Perris: Harvest Landing
- Perris: Park West
- Moreno Valley: Affordable Housing
- March JPA: Ben Clark Training Center
- Riverside: Ridge Crest Subdivision

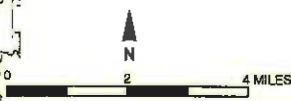


**Riverside County
Airport Land Use Commission
March Air Reserve Base / Inland Port Airport
Land Use Compatibility Plan
(Adopted November 13, 2014)**

Map MA-1

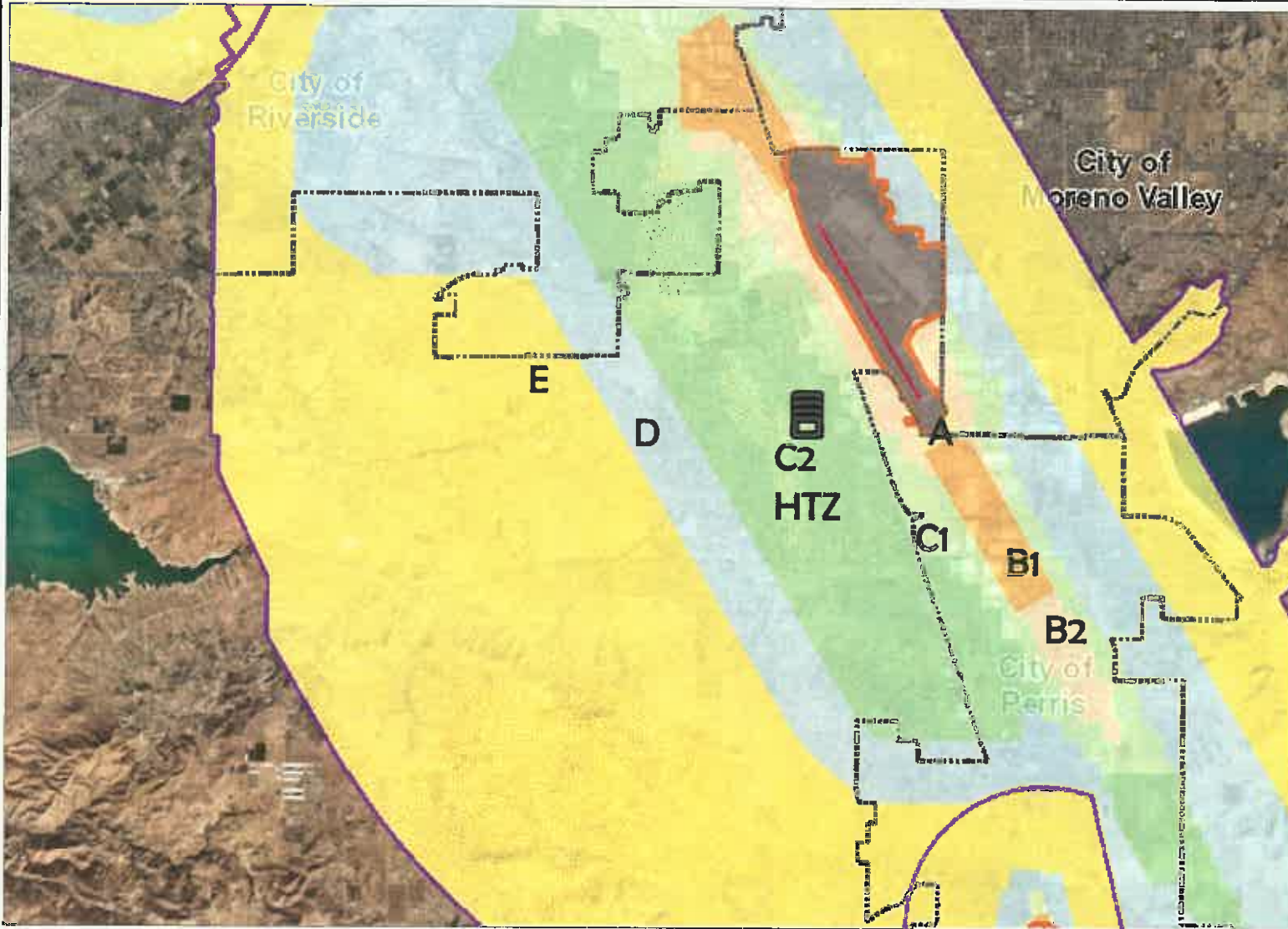
Compatibility Map
March Air Reserve Base / Inland Port Airport

Note:
All dimensions are measured from runway ends and centerlines.



Base map source: County of Riverside 2013

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas

Airport Compatibility Zones

OTHER COMPATIBILITY ZONE

- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC8



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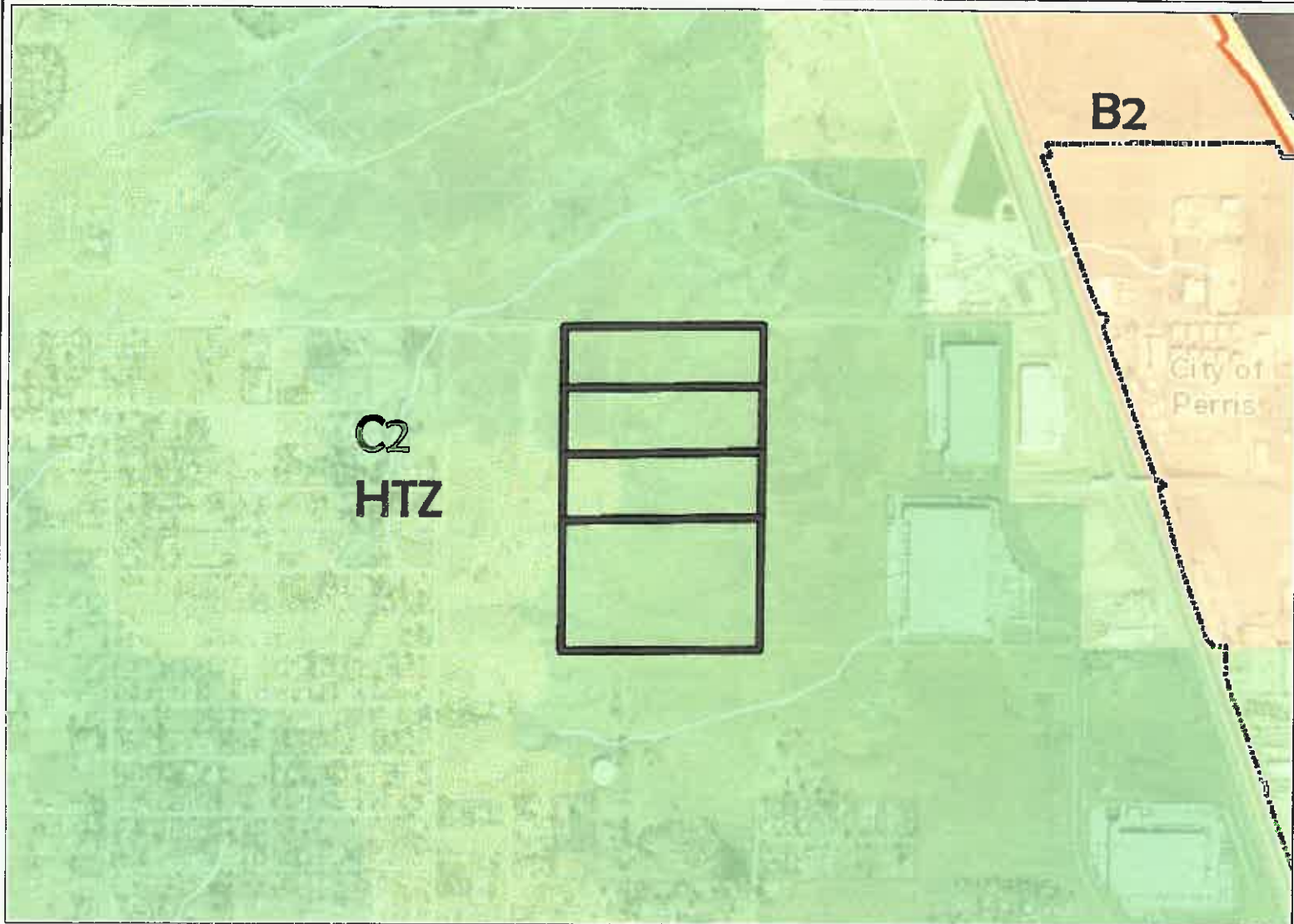


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Notes

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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Notes

Map My County Map



Legend

- City Areas
- World Street Map



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Notes

Map My County Map



Legend

-  Blueline Streams
-  City Areas
-  World Street Map



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Notes

Map My County Map



Legend

- Blueline Streams
- City Areas
- World Street Map



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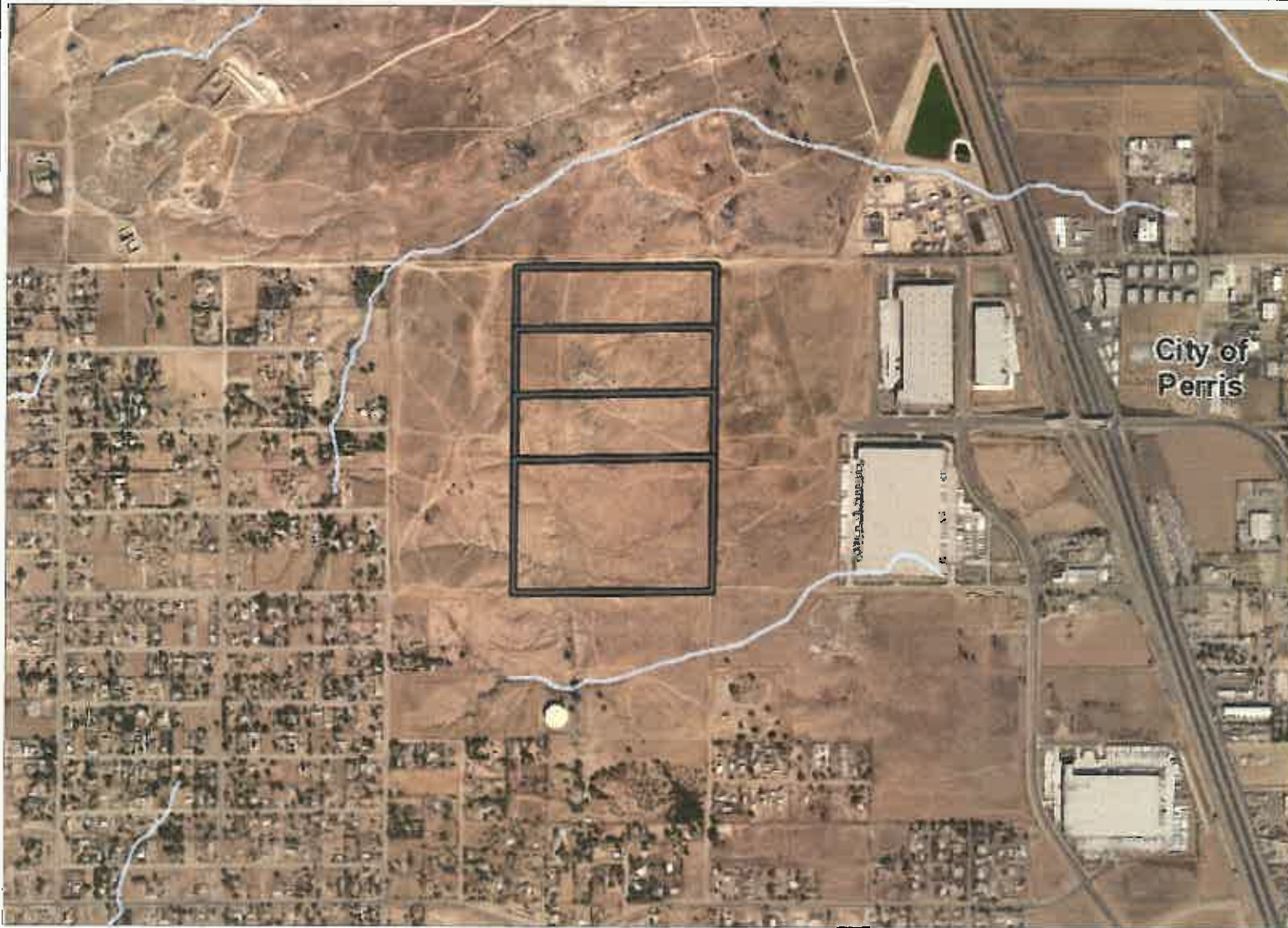
0 3,032 6,064 Feet

REPORT PRINTED ON... 6/6/2019 7:57:12 AM

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Notes

Map My County Map



Legend

- Blue line Streams
- City Areas
- World Street Map



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Notes

OWNER:
 LAND VALLEY GROUP
 14000 BAYVIEW AVE.
 SUITE 200
 FORT MYERS, FL 33907
 PHONE: (888) 236-5659
 FAX: (888) 236-5659
 WWW.LANDVALLEY-RETI.COM

APPLICANT:
 SUKES PESTER
 1802 SANDHILL AVE.
 FRESNO, CA 93720
 PHONE: (549) 758-9659
 FAX: (549) 758-9659
 WWW.SUKESPESTERS-RETI.COM

ENGINEER:
 MICHAEL BAKER INTERNATIONAL
 4000 4TH AVE.
 2 HASTON CENTRE DRIVE, SUITE 200
 SANTA ANA, CA 92707
 PHONE: (949) 400-7071
 WWW.MBIINTERNATIONAL.COM

ARCHITECT:
 MEA OFFICE OF ARCHITECTURAL DESIGN
 1021 W. CHERRY AVENUE, SUITE 102
 IRVINE, CA 92614
 PHONE: (949) 261-0020
 FAX: (949) 261-0922
 WWW.MEADOFFICE.COM

ADJACENT PARCEL NUMBER(S):

LAND USE/ZONING:
 PROPOSED LAND USE:
 BUSINESS PARK
 PROPOSED ZONING:
 MEAD VALLEY AREA PLAN
 EXISTING COUNTY SPECIFIC PLAN

UTILITY COMPANIES:
 WATER: CALIFORNIA WATER SERVICE DISTRICT
 SEWER: EASTERN MARICOPA WATER DISTRICT
 ELECTRIC: CALIFORNIA ELECTRIC
 TELEPHONE: SOUTHWEST CALIFORNIA GAS COMPANY
 GAS: SOUTHWEST CALIFORNIA GAS COMPANY
 SCHOOL DISTRICT:
 VAL VOICE UNIFIED SCHOOL DISTRICT

SITE ADDRESS:
 WEST SIDE OF DECKER ROAD BETWEEN NORDEN AVENUE & CLELAND AVENUE, UNINCORPORATED INDUSTRIAL QUARTER, CALIFORNIA

EARTHWORK ESTIMATE:
 CUT: 300,000 CY
 FILL: 111,000 CY
 TOTAL: 411,000 CY
 PLACEMENT OF EMBANK: 100% BY EXPORT

ACREAGE:
 LEGAL DESCRIPTION (FUT+)
 PROPOSED ACQUISITION OF STREET
 NET+ GROSS - DEDUCTION
 ACQUISITION

TOPOGRAPHY:
 GEOTECHNICAL REPORT: DRG. 2007-TECHNOLOGY DRIVE, SUITE 200, MARICOPA, CALIFORNIA 92541
 PHONE: (916) 828-9200

**IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA
 MEAD VALLEY BUSINESS PARK - PLOT PLAN NO.**

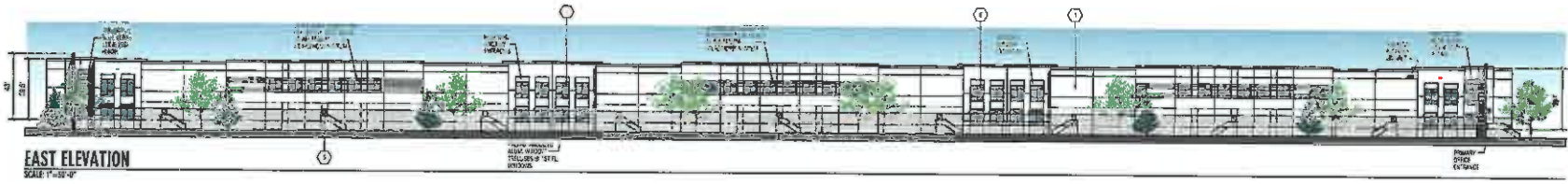
SITE TABULATION		BUILDING AREA A	
PROPOSED PARCEL GROSS AREA	62,048 SF (1.42 AC)		
BUILDING AREA			
-FOOTPRINT	349,387 SF		
-MEZANINE	15,000 SF		
-TOTAL	364,387 SF		
LOT COVERAGE	45.00 %		
TOTAL PARKING REQUIRED	217 STALLS		
PARKING PROVIDED:			
-STANDARD	220 STALLS		
-ADA	5 STALLS		
-TOTAL	225 STALLS		
TRAILER STALLS	92 STALLS		
LANDSCAPE REQUIRED	120,000 SF / 15.00 AC		
LANDSCAPE PROVIDED:	120,000 SF / 15.00 AC		

SITE TABULATION		BUILDING AREA B	
PROPOSED PARCEL GROSS AREA	782,076 SF (17.92 AC)		
BUILDING AREA			
-FOOTPRINT	832,320 SF		
-MEZANINE	15,000 SF		
-TOTAL	847,320 SF		
LOT COVERAGE	45.11 %		
TOTAL PARKING REQUIRED	200 STALLS		
PARKING PROVIDED:			
-STANDARD	200 STALLS		
-ADA	8 STALLS		
-TOTAL	208 STALLS		
TRAILER STALLS	81 STALLS		
LANDSCAPE REQUIRED	110,000 SF / 12.50 AC		
LANDSCAPE PROVIDED:	107,000 SF / 12.20 AC		

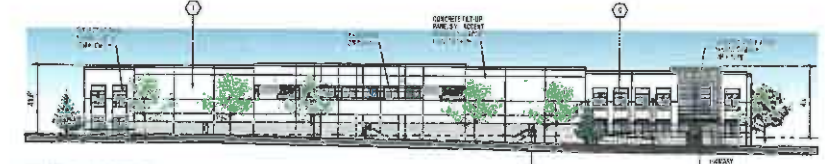
PROJECT DESCRIPTION:
 PROPOSED SITE IS TO BE DEVELOPED FOR INDUSTRIAL/COMMERCIAL PURPOSES. DEVELOPMENT WILL INCLUDE (1) NEW BUILDINGS, WOOD DRIVE AISLES, TRUCK DOORS, GRAVEL LOTS, AND EXISTING SUPERMARKET PROPERTY, AND UTILITIES TO SUPPORT PROPOSED DEVELOPMENT, INCLUDING A 15,000,000 SF COMBINATION OF PARCELS 1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17, 18, 19, 20, 21, 22, 23, 24, 25, 26, 27, 28, 29, 30, 31, 32, 33, 34, 35, 36, 37, 38, 39, 40, 41, 42, 43, 44, 45, 46, 47, 48, 49, 50, 51, 52, 53, 54, 55, 56, 57, 58, 59, 60, 61, 62, 63, 64, 65, 66, 67, 68, 69, 70, 71, 72, 73, 74, 75, 76, 77, 78, 79, 80, 81, 82, 83, 84, 85, 86, 87, 88, 89, 90, 91, 92, 93, 94, 95, 96, 97, 98, 99, 100, 101, 102, 103, 104, 105, 106, 107, 108, 109, 110, 111, 112, 113, 114, 115, 116, 117, 118, 119, 120, 121, 122, 123, 124, 125, 126, 127, 128, 129, 130, 131, 132, 133, 134, 135, 136, 137, 138, 139, 140, 141, 142, 143, 144, 145, 146, 147, 148, 149, 150, 151, 152, 153, 154, 155, 156, 157, 158, 159, 160, 161, 162, 163, 164, 165, 166, 167, 168, 169, 170, 171, 172, 173, 174, 175, 176, 177, 178, 179, 180, 181, 182, 183, 184, 185, 186, 187, 188, 189, 190, 191, 192, 193, 194, 195, 196, 197, 198, 199, 200, 201, 202, 203, 204, 205, 206, 207, 208, 209, 210, 211, 212, 213, 214, 215, 216, 217, 218, 219, 220, 221, 222, 223, 224, 225, 226, 227, 228, 229, 230, 231, 232, 233, 234, 235, 236, 237, 238, 239, 240, 241, 242, 243, 244, 245, 246, 247, 248, 249, 250, 251, 252, 253, 254, 255, 256, 257, 258, 259, 260, 261, 262, 263, 264, 265, 266, 267, 268, 269, 270, 271, 272, 273, 274, 275, 276, 277, 278, 279, 280, 281, 282, 283, 284, 285, 286, 287, 288, 289, 290, 291, 292, 293, 294, 295, 296, 297, 298, 299, 300, 301, 302, 303, 304, 305, 306, 307, 308, 309, 310, 311, 312, 313, 314, 315, 316, 317, 318, 319, 320, 321, 322, 323, 324, 325, 326, 327, 328, 329, 330, 331, 332, 333, 334, 335, 336, 337, 338, 339, 340, 341, 342, 343, 344, 345, 346, 347, 348, 349, 350, 351, 352, 353, 354, 355, 356, 357, 358, 359, 360, 361, 362, 363, 364, 365, 366, 367, 368, 369, 370, 371, 372, 373, 374, 375, 376, 377, 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REVISIONS

- 1. CORRECTED DRAWING
- 2. CORRECTED DRAWING
- 3. CORRECTED DRAWING
- 4. CORRECTED DRAWING
- 5. CORRECTED DRAWING



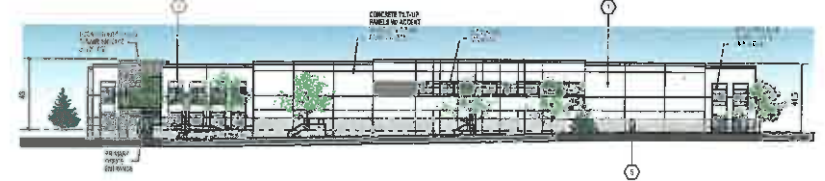
EAST ELEVATION
SCALE: 1/8"=1'-0"



SOUTH ELEVATION
SCALE: 1/8"=1'-0"



WEST ELEVATION
SCALE: 1/8"=1'-0"

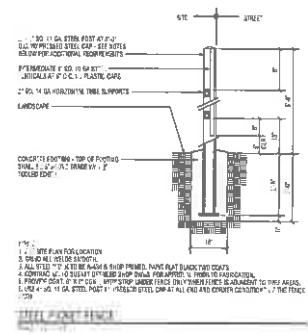


NORTH ELEVATION
SCALE: 1/8"=1'-0"

FINISH SCHEDULE

P-1	MC-01
P-2	MC-1
P-3	MC-1
P-4	MC-1

FINISH	DESCRIPTION
MC-01	CONCRETE FILL UP PANELS IN ACCENT
MC-1	CONCRETE FILL UP PANELS IN ACCENT
MC-2	CONCRETE FILL UP PANELS IN ACCENT
MC-3	CONCRETE FILL UP PANELS IN ACCENT
MC-4	CONCRETE FILL UP PANELS IN ACCENT
MC-5	CONCRETE FILL UP PANELS IN ACCENT
MC-6	CONCRETE FILL UP PANELS IN ACCENT
MC-7	CONCRETE FILL UP PANELS IN ACCENT

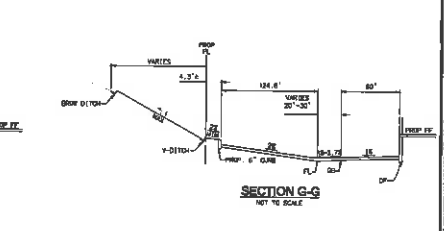
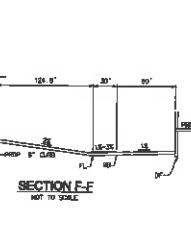
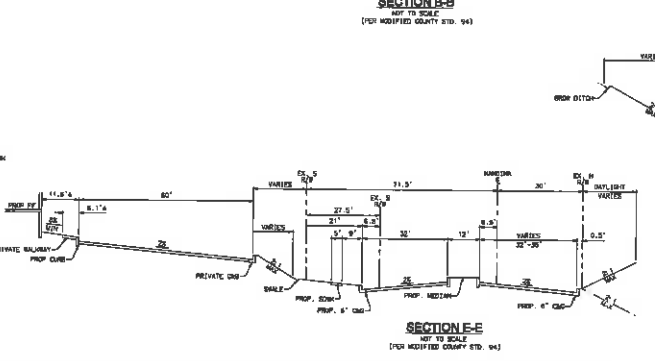
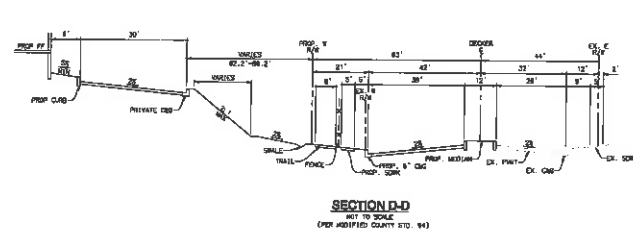
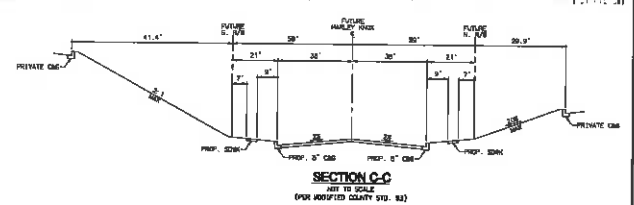
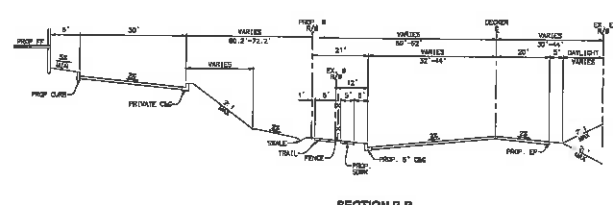
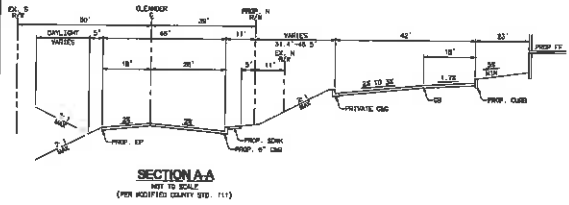
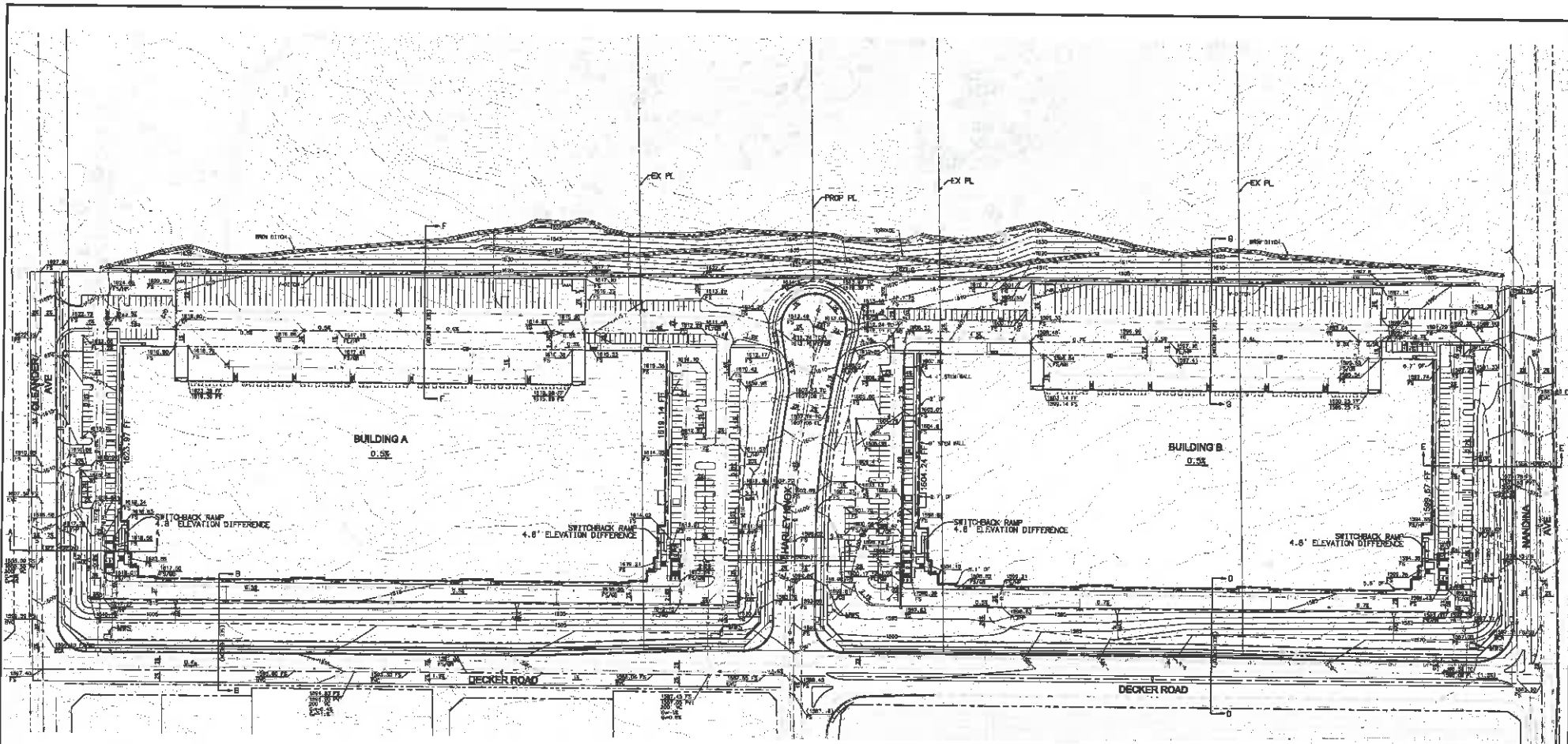


MEAD VALLEY INDUSTRIAL PARK

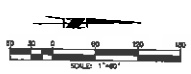
0000 HARLEY KNOX ROAD
COUNTY OF RIVERSIDE, CA

APPLICANT:
RGA ARCHITECTURAL DESIGN
15331 ALTON PARKWAY SUITE 100
IRVINE, CA 92618
TEL: 949.453.1100
CONTACT: MING GILL
minggill@rga.com

DEVELOPER:
SARGIS PARTNER GROUP
10000 SANDHURST AVENUE
IRVINE, CA 92618
TEL: 949.259.8414
CONTACT: PATRICK RUSSELL
patrick@sgp.com



REVISION	DATE	BY	CHKD



COUNTY OF RIVERIDE

MEAD VALLEY BUSINESS PARK
CONCEPTUAL GRADING PLAN

DATE: 11/22/22
PROJECT: MEAD VALLEY BUSINESS PARK
DRAWN BY: J. BRADSHAW
SCALE: AS SHOWN
SHEET NO. 1 OF 1

DESIGNED BY: J. BRADSHAW
CHECKED BY: J. BRADSHAW
DATE: 11/22/22

Technical Memorandum



To: Janine Padia, SRG Perris, L.P.
From: Nick Johnson, Johnson Aviation, Inc.
Date: August 13, 2019

Subject: Solar Glare Analysis – Solar Photovoltaic (PV) Installation, Mead Valley Development Project

Findings

The findings of this Solar Glare Analysis are that the Proposed Project **PASSES** the FAA’s recommended solar glare tests and **PASSES** these same tests for four critical flight paths required by the March Air Reserve Base. This Technical Memorandum outlines the study of the proposed solar PV project and substantiates these findings.

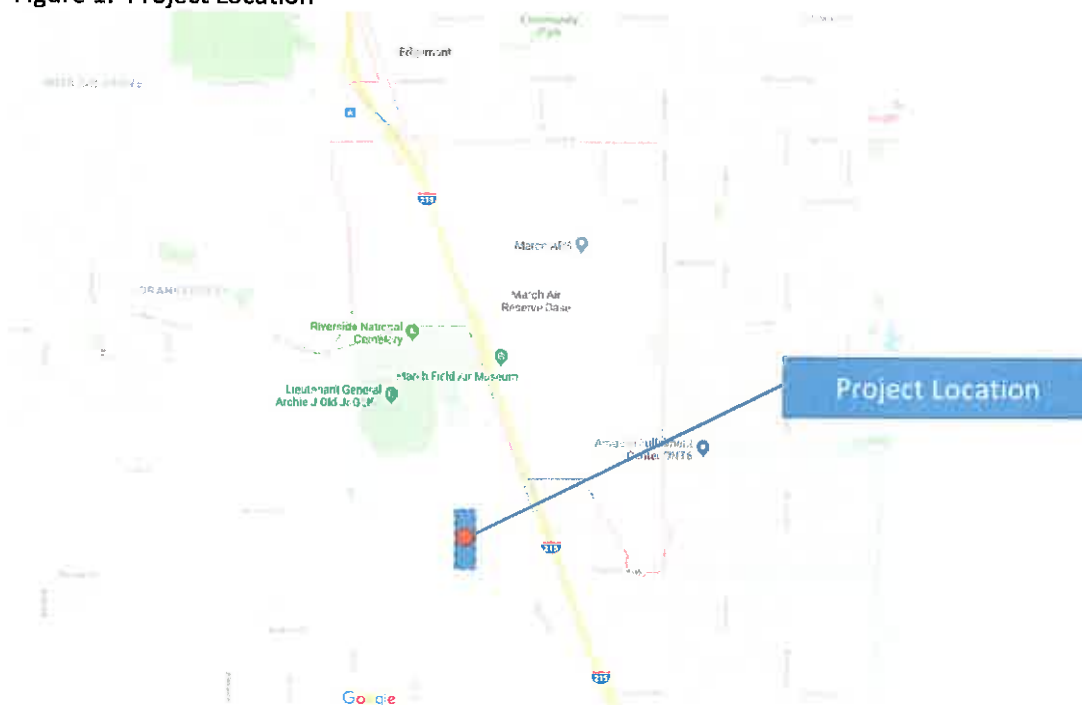
Introduction

The purpose of this technical memorandum is to assess the airport compatibility of two proposed solar (PV) installations on a portion of the roof of each of two buildings that comprise the Mead Valley Development Project. The Project is to be located at the southwest corner of Nandina Avenue and Decker Road in the County of Riverside and within the March Air Reserve Base (March ARB) airport influence area (AIA) (See Figure 1). The analysis and findings of this memo are intended for review and acceptance by Riverside County, Riverside County Airport Land Use Commission (ALUC) and the March ARB.

Project Description

SRG Perris, L.P., the Project Owner, proposes to develop two roof-top solar PV installations on the Mead Valley Development Project. The Project site is located south of Nandina Avenue, west of Decker Road, North of Oleander Avenue and bisected by Harley Knox Boulevard, west of I-215. This site is southwest of March ARB (See Figure 1).

Figure 1: Project Location



The proposed solar PV installations are located on the northern portion of Building A and Building B (See Figure 2) in a total site area of approximately 30,000 square feet for each installation.

Figure 2: Mead Valley Development Project – Solar PV Installations



Standard of Review

This study and its findings have been prepared consistent with the Federal Aviation Administration's (FAA) policy to eliminate hazards to air navigation that may arise as the result of implementing solar energy facilities on and near airports. The FAA adopted an Interim Policy¹ for Solar PV project review in 2013. The FAA was finding that solar PV reflections of sunlight glint and glare were affecting pilots' vision, particularly on final approach to runways, and was also impacting some air traffic controllers' vision when controlling aircraft near airports. In conjunction with Sandia National Laboratories, the FAA developed a computer analysis tool to measure the potential impact of reflected glint and glare from Solar PV installations. The analysis of this impact is achieved through use of the Solar Glare Hazard Assessment Tool (SGHAT). At the time of the Interim Policy, Sandia Labs produced the tool to meet the analysis requirement. Since then, Sandia Labs has licensed the tool to other providers to sell commercially for solar glare analysis. ForgeSolar licensed the SGHAT tool and incorporated its software into their Glare Analysis tool. Johnson Aviation, Inc. uses the ForgeSolar Glare Analysis tool under subscription license from Sims Industries d/b/a ForgeSolar.

¹ Background on the Interim Policy, FAA Review of Solar Energy System Projects on Federally Obligated Airports, Federal Register, October 23, 2013.

The FAA Interim Policy is for federally-obligated airports for development on those airports to be included on the Airport Layout Plan (ALP). Solar energy systems located on an airport that is not federally-obligated or located outside the property of a federally-obligated airport are not subject to this policy because the FAA (and in this case, the US Department of Defense (DOD) does not control land use off of airport property. According to the FAA's Interim Policy, "***Proponents of solar energy systems located off-airport property or on non-federally-obligated airports are strongly encouraged to consider the requirements of this policy when siting such systems [emphasis added].***" The following is the Standard for Measuring Ocular Impact from the FAA's Interim Policy:

Standard for Measuring Ocular Impact

FAA adopts the Solar Glare Hazard Analysis Plot as the standard for measuring the ocular impact of any proposed solar energy system on a federally-obligated airport. To obtain FAA approval to revise an airport layout plan to depict a solar installation and/or a "no objection" to a Notice of Proposed Construction Form 7460-1, the airport sponsor will be required to demonstrate that the proposed solar energy system meets the following standards:

1. No potential for glint or glare in the existing or planned Airport Traffic Control Tower (ATCT) cab; and
2. No potential for glare or "low potential for after-image" along the final approach path for any existing landing threshold or future landing thresholds (including any planned interim phases of the landing thresholds) as shown on the current FAA-approved Airport Layout Plan (ALP). The final approach path is defined as two (2) miles from fifty (50) feet above the landing threshold using a standard three (3) degree glidepath.
3. Ocular impact must be analyzed over the entire calendar year in one (1) minute intervals from when the sun rises above the horizon until the sun sets below the horizon.

In addition to the FAA's standards for runway final approach paths and air traffic control tower visibility, the March ARB staff in conjunction with the Riverside County ALUC staff have established a series of air traffic patterns for the two runways located at the Base. Their concern is to ensure that land uses around the base are compatible with its air operations and that solar PV installations will not create a hazard to air navigation as a result of reflected sunlight and the associated potential glare. March ARB staff have provided four sets of geographic coordinates to define the standard traffic patterns listed below:

- Runway 12/30 General Aviation Traffic Pattern (See Attachment A)
- Runway 14/32 General Aviation Traffic Pattern (See Attachment B)
- Runway 14/32 C-17/KC-135 Traffic Pattern (See Attachment C)
- Runway 14/32 Overhead Traffic Pattern (See Attachment D)

Solar Glare Analysis Reports

The following pages of this Technical Memorandum provide the solar glare analysis reports for each of the required studies. The FAA standard study of the final approach paths to the runway ends and the Air Traffic Control Tower analysis is included in each individual report. The four reports are grouped by the flight path studies required by the March ARB and ALUC staff using the SGHAT program.

Attachment A
March ARB Runway 12/30 General Aviation Traffic Pattern Analysis



FORGESOLAR GLARE ANALYSIS

Project: **SRG Perris - Mead Valley**

Proposed solar PV installation under the traffic pattern at March Air Reserve Base, Riverside, California

Site configuration: **Mead Valley-MARB Runway 12-30 GA Analysis**

Analysis conducted by Nick Johnson (nick.johnson@johnson-aviation.com) at 23:17 on 12 Aug, 2019.

U.S. FAA 2013 Policy Adherence

The following table summarizes the policy adherence of the glare analysis based on the 2013 U.S. Federal Aviation Administration Interim Policy 78 FR 63276. This policy requires the following criteria be met for solar energy systems on airport property:

- No "yellow" glare (potential for after-image) for any flight path from threshold to 2 miles
- No glare of any kind for Air Traffic Control Tower(s) ("ATCT") at cab height.
- Default analysis and observer characteristics (see list below)

ForgeSolar does not represent or speak officially for the FAA and cannot approve or deny projects. Results are informational only.

COMPONENT	STATUS	DESCRIPTION
Analysis parameters	PASS	Analysis time interval and eye characteristics used are acceptable
Flight path(s)	PASS	Flight path receptor(s) do not receive yellow glare
ATCT(s)	PASS	Receptor(s) marked as ATCT do not receive glare

Default glare analysis parameters and observer eye characteristics (for reference only):

- Analysis time interval: 1 minute
- Ocular transmission coefficient: 0.5
- Pupil diameter: 0.002 meters
- Eye focal length: 0.017 meters
- Sun subtended angle: 9.3 milliradians

FAA Policy 78 FR 63276 can be read at <https://www.federalregister.gov/d/2013-24729>



GlareGauge Glare Analysis Results

Site Configuration: Mead Valley-MARB Runway 12-30 GA Analysis

Project site configuration details and results.



Created Aug. 12, 2019 5:36 p.m.
 Updated Aug. 12, 2019 7:20 p.m.
 DNI varies and peaks at 1,000.0 W/m²
 Analyze every 1 minute(s)
 0.5 ocular transmission coefficient
 0.002 m pupil diameter
 0.017 m eye focal length
 9.3 mrad sun subtended angle
 Timezone UTC-8
 Site Configuration ID: 30305.5333

Summary of Results Glare with low potential for temporary after-image predicted

PV name	Tilt	Orientation	"Green" Glare	"Yellow" Glare	Energy Produced
	deg	deg	min	min	kWh
Mead Valley Business Park-Bldg A	10.0	160.0	969	0	-
Mead Valley Business Park-Bldg B	10.0	160.0	1,869	0	-

Component Data

PV Array(s)

Name: Mead Valley Business Park-Bldg A
Axis tracking: Fixed (no rotation)
Tilt: 10.0 deg
Orientation: 160.0 deg
Rated power: -
Panel material: Smooth glass with AR coating
Vary reflectivity with sun position? Yes
Correlate slope error with surface type? Yes
Slope error: 8.43 mrad

Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.861655	-117.271672	1617.08	48.00	1665.08
2	33.861644	-117.270914	1617.08	48.00	1665.08
3	33.861294	-117.270909	1617.08	48.00	1665.08
4	33.861293	-117.271675	1617.08	48.00	1665.08



Name: Mead Valley Business Park-Bldg B
Axis tracking: Fixed (no rotation)
Tilt: 10.0 deg
Orientation: 160.0 deg
Rated power: -
Panel material: Smooth glass with AR coating
Vary reflectivity with sun position? Yes
Correlate slope error with surface type? Yes
Slope error: 8.43 mrad

Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.865497	-117.271631	1597.08	48.00	1645.08
2	33.865494	-117.270857	1597.08	48.00	1645.08
3	33.865144	-117.270858	1597.08	48.00	1645.08
4	33.865148	-117.271642	1597.08	48.00	1645.08



2-Mile Flight Path Receptor(s)

Name: RWY 12 Final
Description: None
Threshold height: 50 ft
Direction: 135.0 deg
Glide slope: 3.0 deg
Pilot view restricted? Yes
Vertical view restriction: 30.0 deg
Azimuthal view restriction: 50.0 deg

Point	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
Threshold	33.890258	-117.260681	1500.07	50.00	1550.08
2-mile point	33.898508	-117.270608	1500.07	1300.06	2800.14



Name: RWY 30 Final
Description: None
Threshold height: 50 ft
Direction: 315.0 deg
Glide slope: 3.0 deg
Pilot view restricted? Yes
Vertical view restriction: 30.0 deg
Azimuthal view restriction: 50.0 deg

Point	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
Threshold	33.884319	-117.253536	1500.07	50.00	1550.08
2-mile point	33.876069	-117.243611	1500.07	1300.06	2800.14



Route Receptor(s)

Name: RWY 12 GA Pattern Route
Route type One-way
View angle: 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.884319	-117.253536	1500.07	50.00	1550.08
2	33.876069	-117.243611	1500.07	1300.06	2800.14
3	33.876081	-117.235119	1500.07	1300.06	2800.14
4	33.880814	-117.229467	1500.07	1300.06	2800.14
5	33.887897	-117.229483	1500.07	1300.06	2800.14
6	33.910333	-117.256469	1500.07	1300.06	2800.14
7	33.910322	-117.264967	1500.07	1300.06	2800.14
8	33.905592	-117.270622	1500.07	1300.06	2800.14
9	33.898508	-117.270608	1500.07	1300.06	2800.14
10	33.890258	-117.260681	1500.07	50.00	1550.08

Name: RWY 30 GA Pattern Route
Route type One-way
View angle: 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.890258	-117.260681	1500.07	50.00	1550.08
2	33.898508	-117.270608	1500.07	1300.06	2800.14
3	33.905592	-117.270622	1500.07	1300.06	2800.14
4	33.910322	-117.264967	1500.07	1300.06	2800.14
5	33.910333	-117.256469	1500.07	1300.06	2800.14
6	33.887897	-117.229483	1500.07	1300.06	2800.14
7	33.880814	-117.229467	1500.07	1300.06	2800.14
8	33.876081	-117.235119	1500.07	1300.06	2800.14
9	33.876069	-117.243611	1500.07	1300.06	2800.14
10	33.884319	-117.253536	1500.07	50.00	1550.08

Discrete Observation Receptors

Number	Latitude	Longitude	Ground elevation	Height above ground	Total Elevation
	deg	deg	ft	ft	ft
1-ATCT	33.891572	-117.251203	1511.07	118.01	1629.08

1-ATCT map image



PV Array Results

Mead Valley Business Park-Bldg A low potential for temporary after-image

Component	Green glare (min)	Yellow glare (min)
FP: RWY 12 Final	0	0
FP: RWY 30 Final	0	0
OP: 1-ATCT	0	0
Route: RWY 12 GA Pattern Route	0	0
Route: RWY 30 GA Pattern Route	969	0

Mead Valley Business Park-Bldg A - Receptor (RWY 12 Final)

No glare found

Mead Valley Business Park-Bldg A - Receptor (RWY 30 Final)

No glare found

Mead Valley Business Park-Bldg A - OP Receptor (1-ATCT)

No glare found

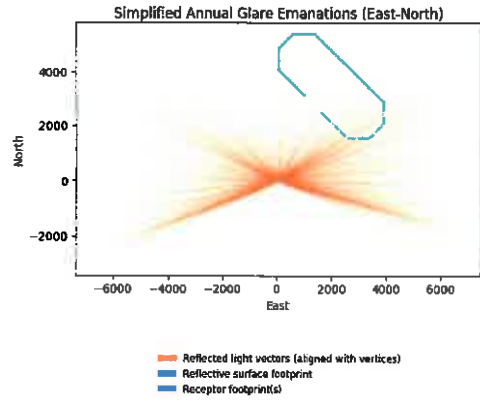
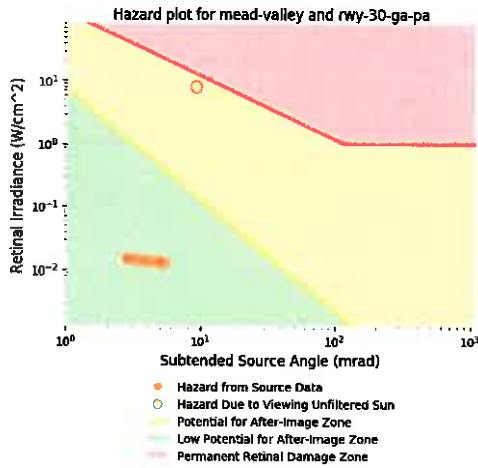
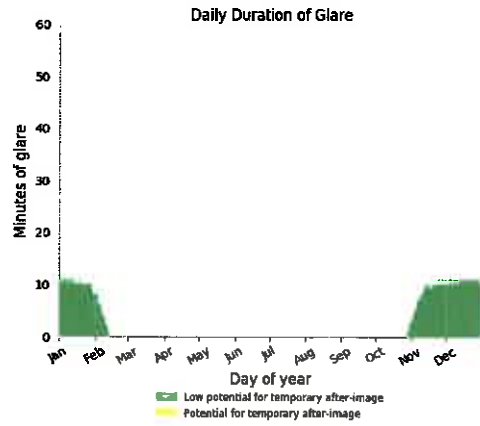
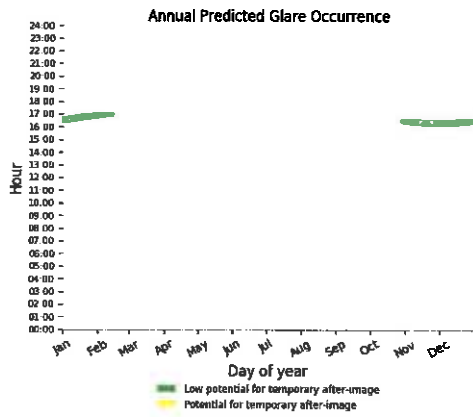
Mead Valley Business Park-Bldg A - Route Receptor (RWY 12 GA Pattern Route)

No glare found

Mead Valley Business Park-Bldg A - Route Receptor (RWY 30 GA Pattern Route)

PV array is expected to produce the following glare for receptors at this location:

- 969 minutes of "green" glare with low potential to cause temporary after-image.
- 0 minutes of "yellow" glare with potential to cause temporary after-image.



Glare vectors placed at PV centroid for clarity. Actual glare-spot locations vary.

Mead Valley Business Park-Bldg B low potential for temporary after-image

Component	Green glare (min)	Yellow glare (min)
FP: RWY 12 Final	0	0
FP: RWY 30 Final	0	0
OP: 1-ATCT	0	0
Route: RWY 12 GA Pattern Route	0	0
Route: RWY 30 GA Pattern Route	1869	0

Mead Valley Business Park-Bldg B - Receptor (RWY 12 Final)

No glare found

Mead Valley Business Park-Bldg B - Receptor (RWY 30 Final)

No glare found

Mead Valley Business Park-Bldg B - OP Receptor (1-ATCT)

No glare found

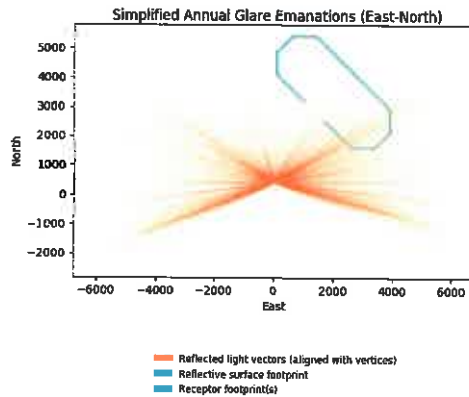
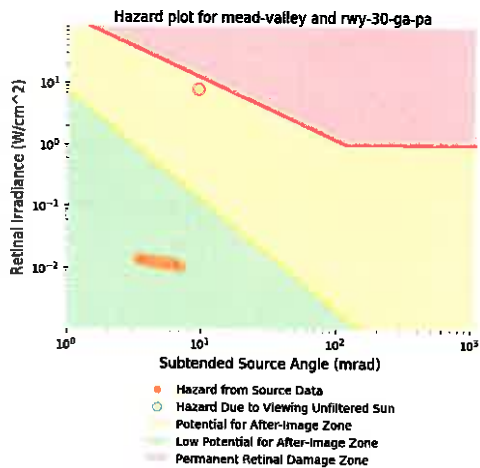
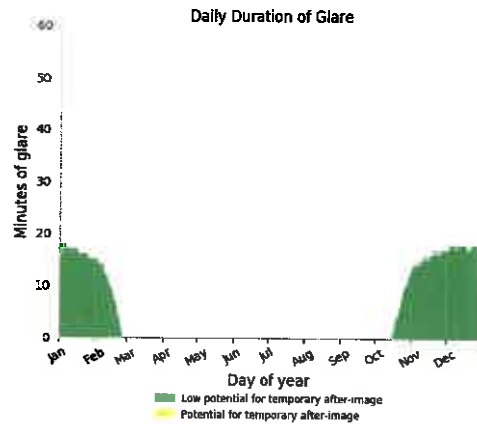
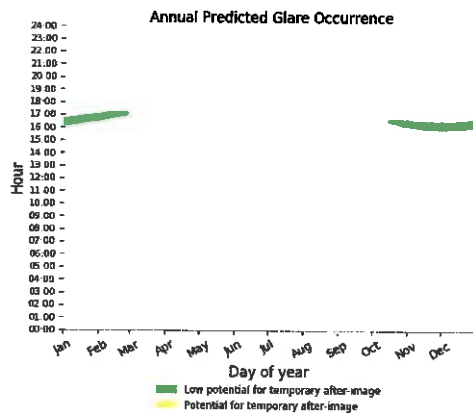
Mead Valley Business Park-Bldg B - Route Receptor (RWY 12 GA Pattern Route)

No glare found

Mead Valley Business Park-Bldg B - Route Receptor (RWY 30 GA Pattern Route)

PV array is expected to produce the following glare for receptors at this location:

- 1,869 minutes of "green" glare with low potential to cause temporary after-image.
- 0 minutes of "yellow" glare with potential to cause temporary after-image.



Glare vectors placed at PV centroid for clarity. Actual glare-spot locations vary.

Assumptions

- Times associated with glare are denoted in Standard time. For Daylight Savings, add one hour.
- Glare analyses do not account for physical obstructions between reflectors and receptors. This includes buildings, tree cover and geographic obstructions.
- Detailed system geometry is not rigorously simulated.
- The glare hazard determination relies on several approximations including observer eye characteristics, angle of view, and typical blink response time. Actual values and results may vary.
- Several calculations utilize the PV array centroid, rather than the actual glare spot location, due to algorithm limitations. This may affect results for large PV footprints. Additional analyses of array sub-sections can provide additional information on expected glare.
- The subtended source angle (glare spot size) is constrained by the PV array footprint size. Partitioning large arrays into smaller sections will reduce the maximum potential subtended angle, potentially impacting results if actual glare spots are larger than the sub-array size. Additional analyses of the combined area of adjacent sub-arrays can provide more information on potential glare hazards. (See previous point on related limitations.)
- Hazard zone boundaries shown in the Glare Hazard plot are an approximation and visual aid. Actual ocular impact outcomes encompass a continuous, not discrete, spectrum.
- Glare locations displayed on receptor plots are approximate. Actual glare-spot locations may differ.
- Glare vector plots are simplified representations of analysis data. Actual glare emanations and results may differ.
- Refer to the **Help page** for assumptions and limitations not listed here.

Attachment B
March ARB Runway 14/32 General Aviation Traffic Pattern Analysis



FORGESOLAR GLARE ANALYSIS

Project: **SRG Perris - Mead Valley**

Proposed solar PV installation under the traffic pattern at March Air Reserve Base, Riverside, California

Site configuration: **Mead Valley-MARB Runway 14-32 GA Analysis3**

Analysis conducted by Nick Johnson (nick.johnson@johnson-aviation.com) at 22:55 on 12 Aug, 2019.

U.S. FAA 2013 Policy Adherence

The following table summarizes the policy adherence of the glare analysis based on the 2013 U.S. Federal Aviation Administration Interim Policy 78 FR 63276. This policy requires the following criteria be met for solar energy systems on airport property:

- No "yellow" glare (potential for after-image) for any flight path from threshold to 2 miles
- No glare of any kind for Air Traffic Control Tower(s) ("ATCT") at cab height.
- Default analysis and observer characteristics (see list below)

ForgeSolar does not represent or speak officially for the FAA and cannot approve or deny projects. Results are informational only.

COMPONENT	STATUS	DESCRIPTION
Analysis parameters	PASS	Analysis time interval and eye characteristics used are acceptable
Flight path(s)	PASS	Flight path receptor(s) do not receive yellow glare
ATCT(s)	PASS	Receptor(s) marked as ATCT do not receive glare

Default glare analysis parameters and observer eye characteristics (for reference only):

- Analysis time interval: 1 minute
- Ocular transmission coefficient: 0.5
- Pupil diameter: 0.002 meters
- Eye focal length: 0.017 meters
- Sun subtended angle: 9.3 milliradians

FAA Policy 78 FR 63276 can be read at <https://www.federalregister.gov/d/2013-24729>



GlareGauge Glare Analysis Results

Site Configuration: Mead Valley-MARB Runway 14-32 GA Analysis3

Project site configuration details and results.



Created Aug. 12, 2019 5:42 p.m.
 Updated Aug. 12, 2019 6:59 p.m.
 DNI varies and peaks at 1,000.0 W/m²
 Analyze every 1 minute(s)
 0.5 ocular transmission coefficient
 0.002 m pupil diameter
 0.017 m eye focal length
 9.3 mrad sun subtended angle
 Timezone UTC-8
 Site Configuration ID: 30306.5333

Summary of Results Glare with low potential for temporary after-image predicted

PV name	Tilt	Orientation	"Green" Glare	"Yellow" Glare	Energy Produced
	deg	deg	min	min	kWh
Mead Valley Business Park-Bldg A	10.0	160.0	23,077	0	-
Mead Valley Business Park-Bldg B	10.0	160.0	38,024	0	-

Component Data

PV Array(s)

Name: Mead Valley Business Park-Bldg A
Axis tracking: Fixed (no rotation)
Tilt: 10.0 deg
Orientation: 160.0 deg
Rated power: -
Panel material: Smooth glass with AR coating
Vary reflectivity with sun position? Yes
Correlate slope error with surface type? Yes
Slope error: 8.43 mrad

Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.861655	-117.271672	1617.08	48.00	1665.08
2	33.861644	-117.270914	1617.08	48.00	1665.08
3	33.861294	-117.270909	1617.08	48.00	1665.08
4	33.861293	-117.271675	1617.08	48.00	1665.08



Name: Mead Valley Business Park-Bldg B
Axis tracking: Fixed (no rotation)
Tilt: 10.0 deg
Orientation: 160.0 deg
Rated power: -
Panel material: Smooth glass with AR coating
Vary reflectivity with sun position? Yes
Correlate slope error with surface type? Yes
Slope error: 8.43 mrad

Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.865497	-117.271631	1597.08	48.00	1645.08
2	33.865494	-117.270857	1597.08	48.00	1645.08
3	33.865144	-117.270858	1597.08	48.00	1645.08
4	33.865148	-117.271642	1597.08	48.00	1645.08



2-Mile Flight Path Receptor(s)

Name: RWY 14 Final
Description: None
Threshold height: 50 ft
Direction: 149.5 deg
Glide slope: 3.0 deg
Pilot view restricted? Yes
Vertical view restriction: 30.0 deg
Azimuthal view restriction: 50.0 deg

Point	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
Threshold	33.896431	-117.270636	1500.07	50.00	1550.08
2-mile point	33.906486	-117.277783	1500.07	1500.07	3000.15



Name: RWY 32 Final
Description: None
Threshold height: 50 ft
Direction: 329.5 deg
Glide slope: 3.0 deg
Pilot view restricted? Yes
Vertical view restriction: 30.0 deg
Azimuthal view restriction: 50.0 deg

Point	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
Threshold	33.864994	-117.248281	1500.07	50.00	1550.08
2-mile point	33.854942	-117.241136	1500.07	1500.07	3000.15



Route Receptor(s)

Name: RWY 14 GA Pattern Route
 Route type One-way
 View angle: 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.864994	-117.248281	1500.07	50.00	1550.08
2	33.854942	-117.241136	1500.07	1500.07	3000.15
3	33.848078	-117.243236	1500.07	1500.07	3000.15
4	33.844669	-117.250119	1500.07	1500.07	3000.15
5	33.846422	-117.258344	1500.07	1500.07	3000.15
6	33.897972	-117.295011	1500.07	1500.07	3000.15
7	33.904833	-117.292903	1500.07	1500.07	3000.15
8	33.908242	-117.286017	1500.07	1500.07	3000.15
9	33.906486	-117.277783	1500.07	1500.07	3000.15
10	33.896431	-117.270636	1500.07	50.00	1550.08

Name: RWY 32 GA Pattern Route
 Route type One-way
 View angle: 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.896431	-117.270636	1500.07	50.00	1550.08
2	33.906486	-117.277783	1500.07	1500.07	3000.15
3	33.908242	-117.286017	1500.07	1500.07	3000.15
4	33.904833	-117.292903	1500.07	1500.07	3000.15
5	33.897972	-117.295011	1500.07	1500.07	3000.15
6	33.846422	-117.258344	1500.07	1500.07	3000.15
7	33.844669	-117.250119	1500.07	1500.07	3000.15
8	33.848078	-117.243236	1500.07	1500.07	3000.15
9	33.854942	-117.241136	1500.07	1500.07	3000.15
10	33.864994	-117.248281	1500.07	50.00	1550.08

Discrete Observation Receptors

Number	Latitude	Longitude	Ground elevation	Height above ground	Total Elevation
	deg	deg	ft	ft	ft
1-ATCT	33.891572	-117.251203	1511.07	118.01	1629.08

1-ATCT map image



PV Array Results

Mead Valley Business Park-Bldg A low potential for temporary after-image

Component	Green glare (min)	Yellow glare (min)
FP: RWY 14 Final	0	0
FP: RWY 32 Final	1047	0
OP: 1-ATCT	0	0
Route: RWY 14 GA Pattern Route	8973	0
Route: RWY 32 GA Pattern Route	13057	0

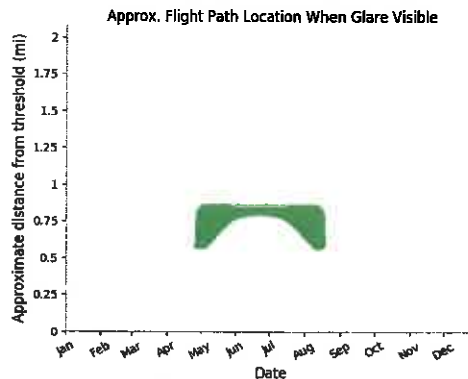
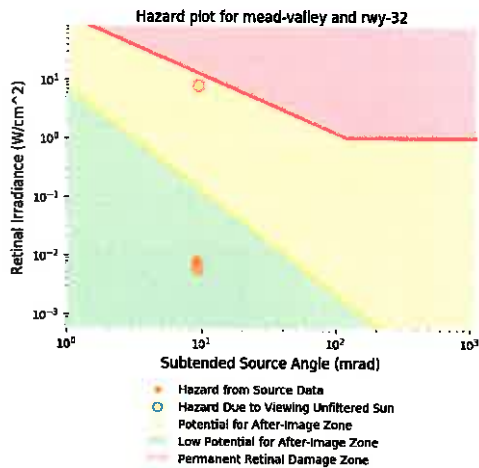
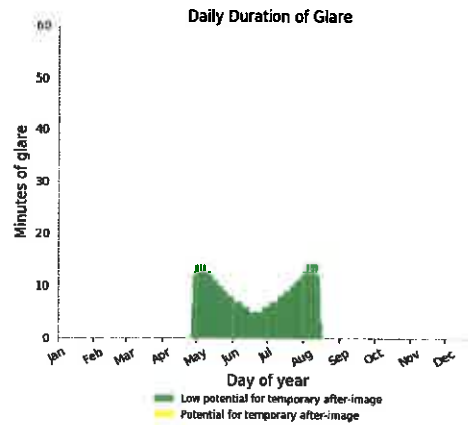
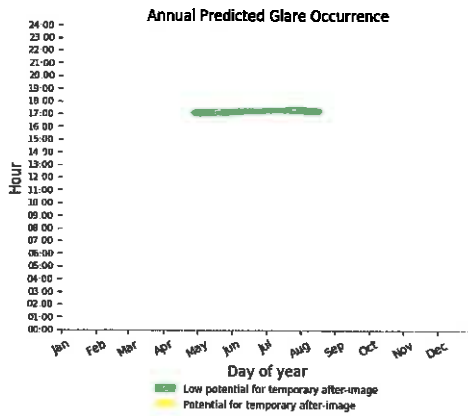
Mead Valley Business Park-Bldg A - Receptor (RWY 14 Final)

No glare found

Mead Valley Business Park-Bldg A - Receptor (RWY 32 Final)

PV array is expected to produce the following glare for observers on this flight path:

- 1,047 minutes of "green" glare with low potential to cause temporary after-image.
- 0 minutes of "yellow" glare with potential to cause temporary after-image.



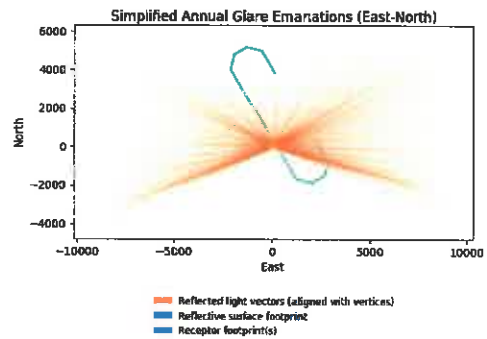
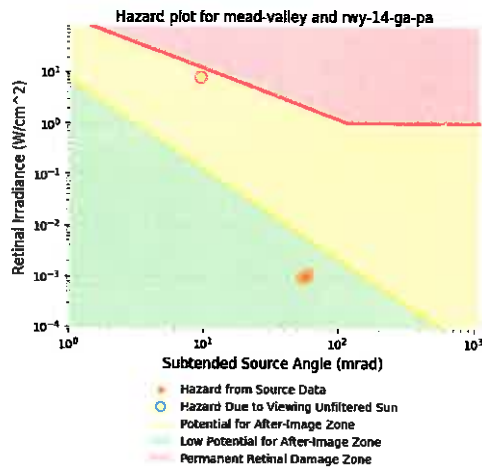
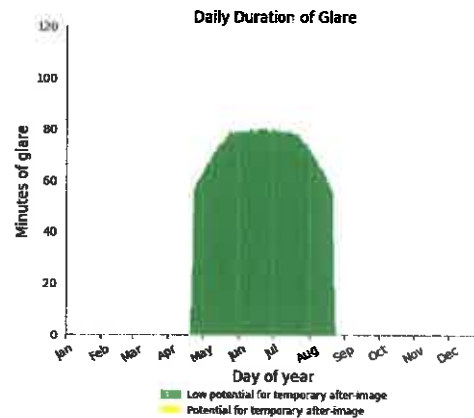
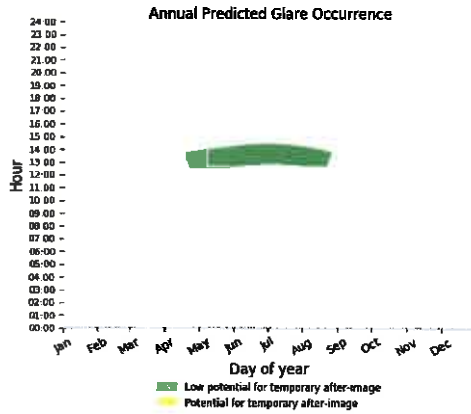
Mead Valley Business Park-Bldg A - OP Receptor (1-ATCT)

No glare found

Mead Valley Business Park-Bldg A - Route Receptor (RWY 14 GA Pattern Route)

PV array is expected to produce the following glare for receptors at this location:

- 8,973 minutes of "green" glare with low potential to cause temporary after-image.
- 0 minutes of "yellow" glare with potential to cause temporary after-image.

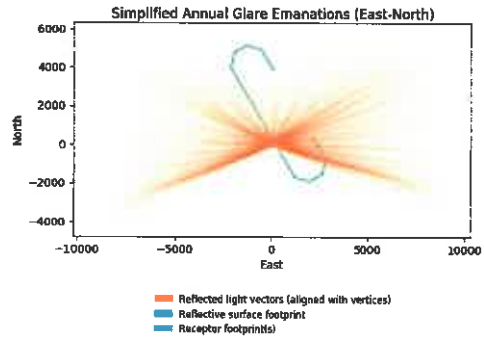
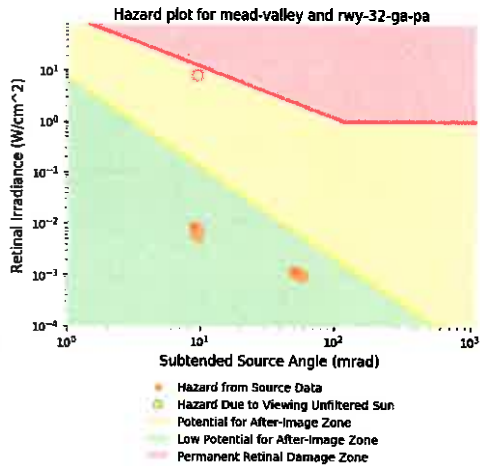
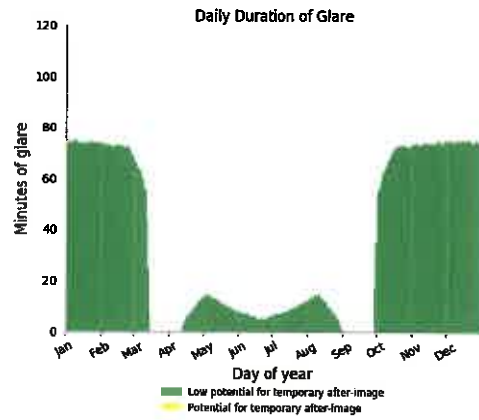
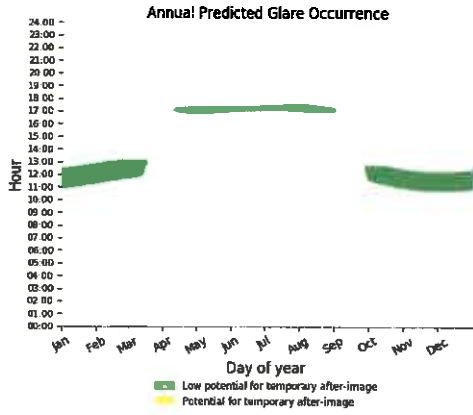


Glare vectors placed at PV centroid for clarity. Actual glare-spot locations vary.

Mead Valley Business Park-Bldg A - Route Receptor (RWY 32 GA Pattern Route)

PV array is expected to produce the following glare for receptors at this location:

- 13,057 minutes of "green" glare with low potential to cause temporary after-image.
- 0 minutes of "yellow" glare with potential to cause temporary after-image.



Glare vectors placed at PV centroid for clarity. Actual glare-spot locations vary.

Mead Valley Business Park-Bldg B low potential for temporary after-image

Component	Green glare (min)	Yellow glare (min)
FP: RWY 14 Final	0	0
FP: RWY 32 Final	2382	0
OP: 1-ATCT	0	0
Route: RWY 14 GA Pattern Route	9913	0
Route: RWY 32 GA Pattern Route	25729	0

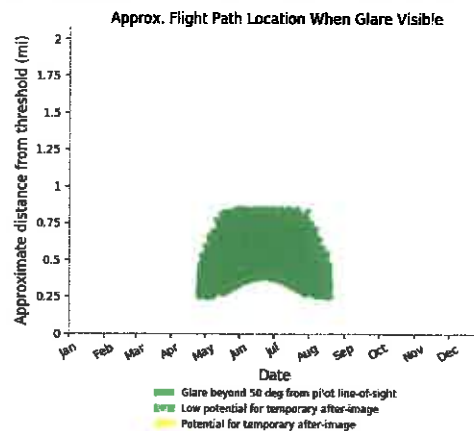
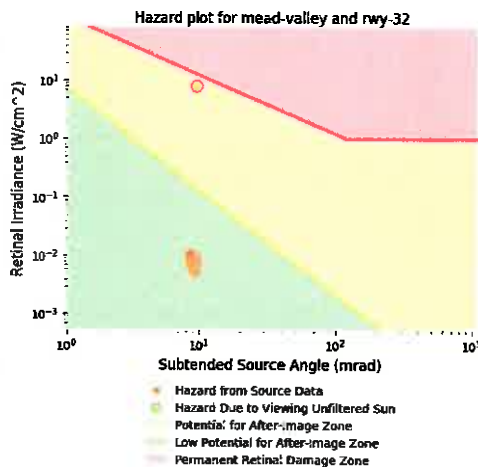
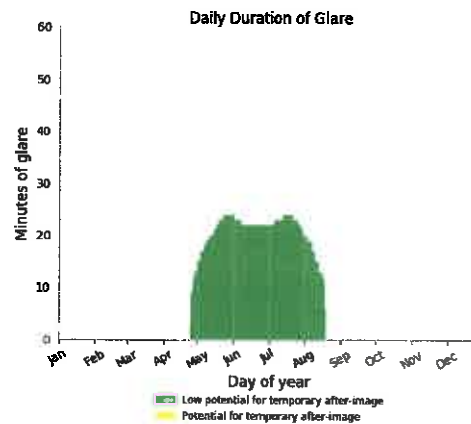
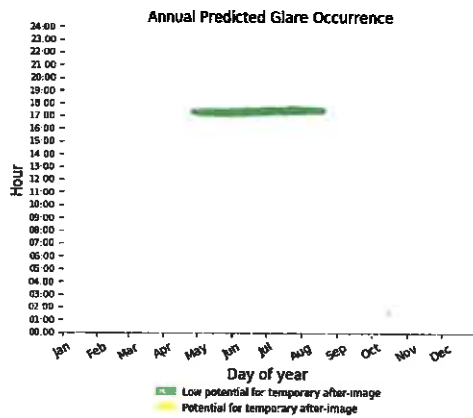
Mead Valley Business Park-Bldg B - Receptor (RWY 14 Final)

No glare found

Mead Valley Business Park-Bldg B - Receptor (RWY 32 Final)

PV array is expected to produce the following glare for observers on this flight path:

- 2,382 minutes of "green" glare with low potential to cause temporary after-image.
- 0 minutes of "yellow" glare with potential to cause temporary after-image.



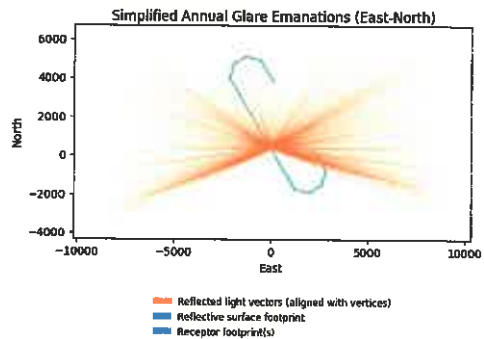
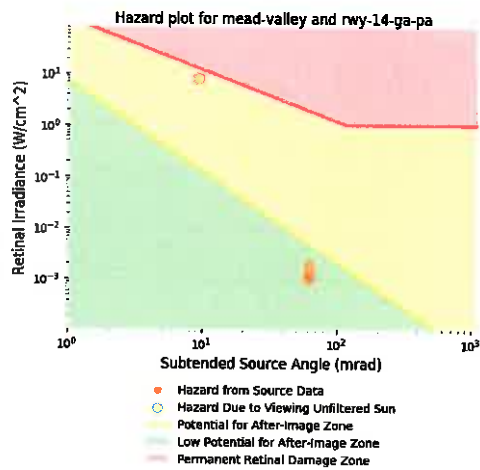
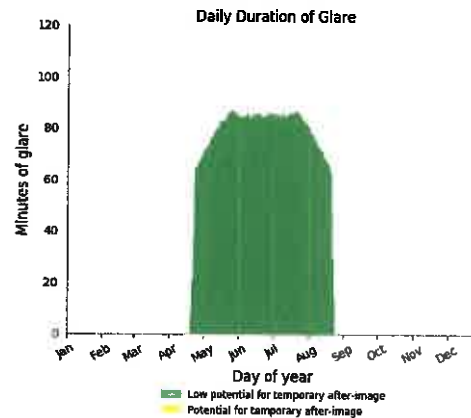
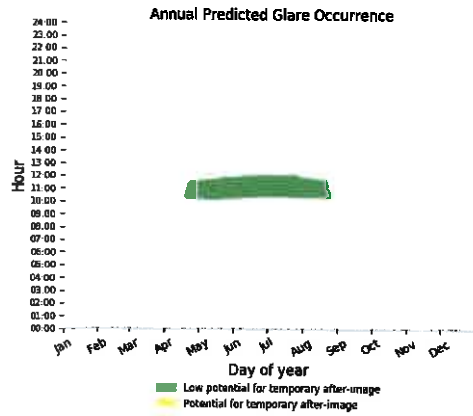
Mead Valley Business Park-Bldg B - OP Receptor (1-ATCT)

No glare found

Mead Valley Business Park-Bldg B - Route Receptor (RWY 14 GA Pattern Route)

PV array is expected to produce the following glare for receptors at this location:

- 9,913 minutes of "green" glare with low potential to cause temporary after-image.
- 0 minutes of "yellow" glare with potential to cause temporary after-image.

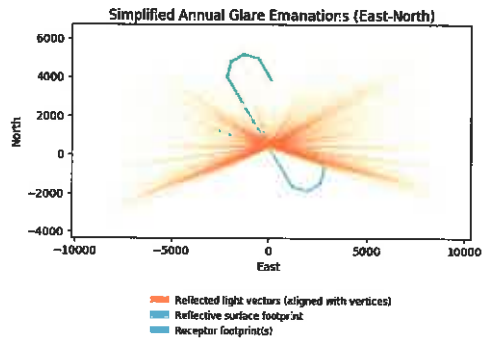
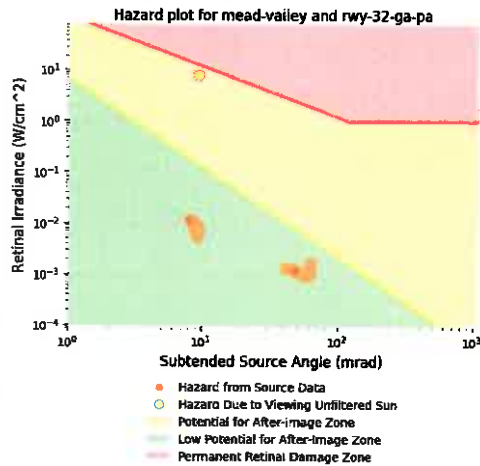
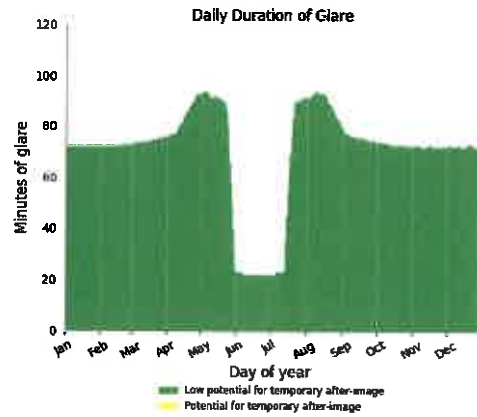
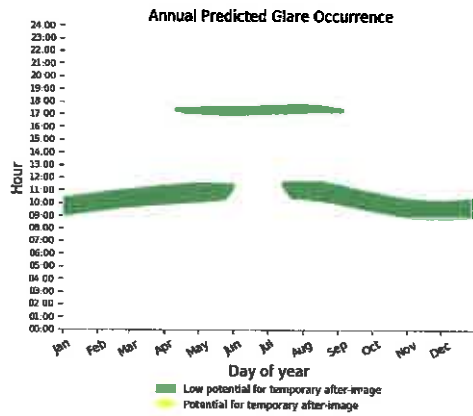


Glare vectors placed at PV centroid for clarity. Actual glare-spot locations vary.

Mead Valley Business Park-Bldg B - Route Receptor (RWY 32 GA Pattern Route)

PV array is expected to produce the following glare for receptors at this location:

- 25,729 minutes of "green" glare with low potential to cause temporary after-image.
- 0 minutes of "yellow" glare with potential to cause temporary after-image.



Glare vectors placed at PV centroid for clarity. Actual glare-spot locations vary.

Assumptions

- Times associated with glare are denoted in Standard time. For Daylight Savings, add one hour.
- Glare analyses do not account for physical obstructions between reflectors and receptors. This includes buildings, tree cover and geographic obstructions.
- Detailed system geometry is not rigorously simulated.
- The glare hazard determination relies on several approximations including observer eye characteristics, angle of view, and typical blink response time. Actual values and results may vary.
- Several calculations utilize the PV array centroid, rather than the actual glare spot location, due to algorithm limitations. This may affect results for large PV footprints. Additional analyses of array sub-sections can provide additional information on expected glare.
- The subtended source angle (glare spot size) is constrained by the PV array footprint size. Partitioning large arrays into smaller sections will reduce the maximum potential subtended angle, potentially impacting results if actual glare spots are larger than the sub-array size. Additional analyses of the combined area of adjacent sub-arrays can provide more information on potential glare hazards. (See previous point on related limitations.)
- Hazard zone boundaries shown in the Glare Hazard plot are an approximation and visual aid. Actual ocular impact outcomes encompass a continuous, not discrete, spectrum.
- Glare locations displayed on receptor plots are approximate. Actual glare-spot locations may differ.
- Glare vector plots are simplified representations of analysis data. Actual glare emanations and results may differ.
- Refer to the **Help page** for assumptions and limitations not listed here.

Attachment C
March ARB Runway 14/32 C-17/KC-135 Traffic Pattern Analysis



FORGESOLAR GLARE ANALYSIS

Project: **SRG Perris - Mead Valley**

Proposed solar PV Installation under the traffic pattern at March Air Reserve Base, Riverside, California

Site configuration: **Mead Valley-MARB Runway 14-32 C-17 Analysis**

Analysis conducted by Nick Johnson (nick.johnson@johnson-aviation.com) at 23:10 on 12 Aug, 2019.

U.S. FAA 2013 Policy Adherence

The following table summarizes the policy adherence of the glare analysis based on the 2013 U.S. Federal Aviation Administration Interim Policy 78 FR 63276. This policy requires the following criteria be met for solar energy systems on airport property:

- No "yellow" glare (potential for after-image) for any flight path from threshold to 2 miles
- No glare of any kind for Air Traffic Control Tower(s) ("ATCT") at cab height.
- Default analysis and observer characteristics (see list below)

ForgeSolar does not represent or speak officially for the FAA and cannot approve or deny projects. Results are informational only.

COMPONENT	STATUS	DESCRIPTION
Analysis parameters	PASS	Analysis time interval and eye characteristics used are acceptable
Flight path(s)	PASS	Flight path receptor(s) do not receive yellow glare
ATCT(s)	PASS	Receptor(s) marked as ATCT do not receive glare

Default glare analysis parameters and observer eye characteristics (for reference only):

- Analysis time interval: 1 minute
- Ocular transmission coefficient: 0.5
- Pupil diameter: 0.002 meters
- Eye focal length: 0.017 meters
- Sun subtended angle: 9.3 milliradians

FAA Policy 78 FR 63276 can be read at <https://www.federalregister.gov/d/2013-24729>



GlareGauge Glare Analysis Results

Site Configuration: Mead Valley-MARB Runway 14-32 C-17 Analysis

Project site configuration details and results.



Created Aug. 12, 2019 5:51 p.m.
 Updated Aug. 12, 2019 7:14 p.m.
 DNI varies and peaks at 1,000.0 W/m²
 Analyze every 1 minute(s)
 0.5 ocular transmission coefficient
 0.002 m pupil diameter
 0.017 m eye focal length
 9.3 mrad sun subtended angle
 Timezone UTC-8
 Site Configuration ID: 30307.5333

Summary of Results Glare with low potential for temporary after-image predicted

PV name	Tilt	Orientation	"Green" Glare	"Yellow" Glare	Energy Produced
	deg	deg	min	min	kWh
Mead Valley Business Park-Bldg A	10.0	160.0	2,528	0	-
Mead Valley Business Park-Bldg B	10.0	160.0	3,921	0	-

Component Data

PV Array(s)

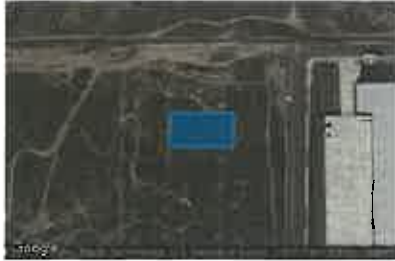
Name: Mead Valley Business Park-Bldg A
 Axis tracking: Fixed (no rotation)
 Tilt: 10.0 deg
 Orientation: 160.0 deg
 Rated power: -
 Panel material: Smooth glass with AR coating
 Vary reflectivity with sun position? Yes
 Correlate slope error with surface type? Yes
 Slope error: 8.43 mrad

Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.861655	-117.271672	1617.08	48.00	1665.08
2	33.861644	-117.270914	1617.08	48.00	1665.08
3	33.861294	-117.270909	1617.08	48.00	1665.08
4	33.861293	-117.271675	1617.08	48.00	1665.08



Name: Mead Valley Business Park-Bldg B
Axis tracking: Fixed (no rotation)
Tilt: 10.0 deg
Orientation: 160.0 deg
Rated power: -
Panel material: Smooth glass with AR coating
Vary reflectivity with sun position? Yes
Correlate slope error with surface type? Yes
Slope error: 8.43 mrad

Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.865497	-117.271631	1597.08	48.00	1645.08
2	33.865494	-117.270857	1597.08	48.00	1645.08
3	33.865144	-117.270858	1597.08	48.00	1645.08
4	33.865148	-117.271642	1597.08	48.00	1645.08



2-Mile Flight Path Receptor(s)

Name: RWY 14 Final
Description: None
Threshold height: 50 ft
Direction: 149.5 deg
Glide slope: 3.0 deg
Pilot view restricted? Yes
Vertical view restriction: 30.0 deg
Azimuthal view restriction: 50.0 deg

Point	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
Threshold	33.896431	-117.270636	1500.07	50.00	1550.08
2-mile point	33.906486	-117.277783	1500.07	1500.07	3000.15



Name: RWY 32 Final
Description: None
Threshold height: 50 ft
Direction: 329.5 deg
Glide slope: 3.0 deg
Pilot view restricted? Yes
Vertical view restriction: 30.0 deg
Azimuthal view restriction: 50.0 deg

Point	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
Threshold	33.864994	-117.248281	1500.07	50.00	1550.08
2-mile point	33.854942	-117.241136	1500.07	1500.07	3000.15



Route Receptor(s)

Name: RWY 14 C-17 - KC-135 Pattern Route
Route type: One-way
View angle: 50.0 deg



Vertex	Latitude deg	Longitude deg	Ground elevation ft	Height above ground ft	Total elevation ft
1	33.864994	-117.248281	1500.07	50.00	1550.08
2	33.836269	-117.227869	1500.07	1500.07	3000.15
3	33.821961	-117.228367	1500.07	1500.07	3000.15
4	33.813147	-117.244350	1500.07	1500.07	3000.15
5	33.819225	-117.262269	1500.07	1500.07	3000.15
6	33.908131	-117.325528	1500.07	1500.07	3000.15
7	33.922394	-117.325047	1500.07	1500.07	3000.15
8	33.931244	-117.309014	1500.07	1500.07	3000.15
9	33.925156	-117.291061	1500.07	1500.07	3000.15
10	33.896431	-117.270636	1500.07	50.00	1550.08

Name: RWY 32 C-17 - KC-135 Pattern Route
Route type: One-way
View angle: 50.0 deg



Vertex	Latitude deg	Longitude deg	Ground elevation ft	Height above ground ft	Total elevation ft
1	33.896431	-117.270636	1500.07	50.00	1550.08
2	33.925156	-117.291061	1500.07	1500.07	3000.15
3	33.931244	-117.309014	1500.07	1500.07	3000.15
4	33.922394	-117.325047	1500.07	1500.07	3000.15
5	33.908131	-117.325528	1500.07	1500.07	3000.15
6	33.819225	-117.262269	1500.07	1500.07	3000.15
7	33.813147	-117.244350	1500.07	1500.07	3000.15
8	33.821961	-117.228367	1500.07	1500.07	3000.15
9	33.836269	-117.227869	1500.07	1500.07	3000.15
10	33.864994	-117.248281	1500.07	50.00	1550.08

Discrete Observation Receptors

Number	Latitude deg	Longitude deg	Ground elevation ft	Height above ground ft	Total Elevation ft
1-ATCT	33.891572	-117.251203	1511.07	118.01	1629.08

1-ATCT map image



PV Array Results

Mead Valley Business Park-Bldg A low potential for temporary after-image

Component	Green glare (min)	Yellow glare (min)
FP: RWY 14 Final	0	0
FP: RWY 32 Final	1047	0
OP: 1-ATCT	0	0
Route: RWY 14 C-17 - KC-135 Pattern Route	0	0
Route: RWY 32 C-17 - KC-135 Pattern Route	1481	0

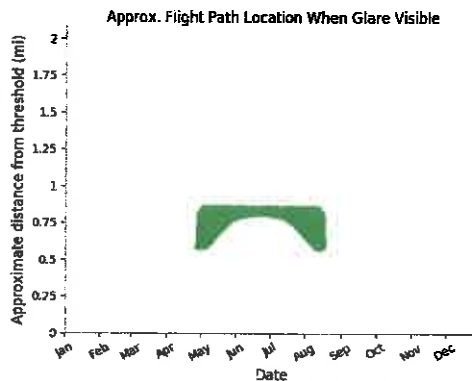
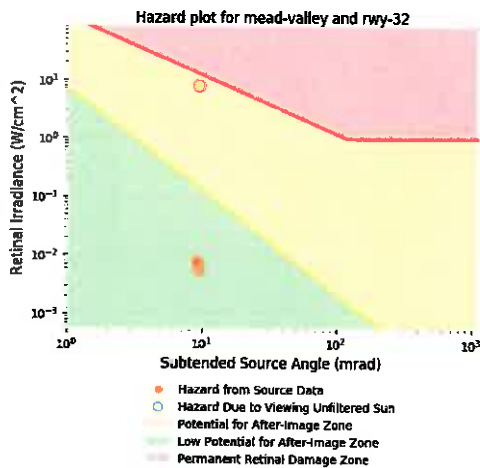
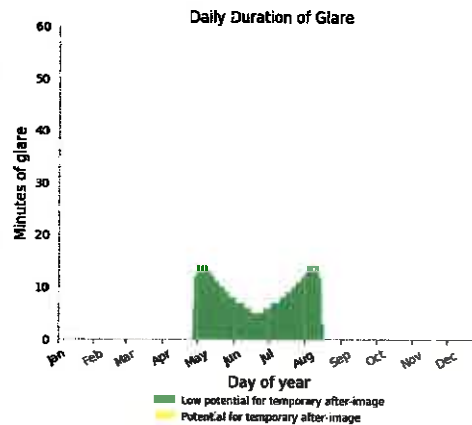
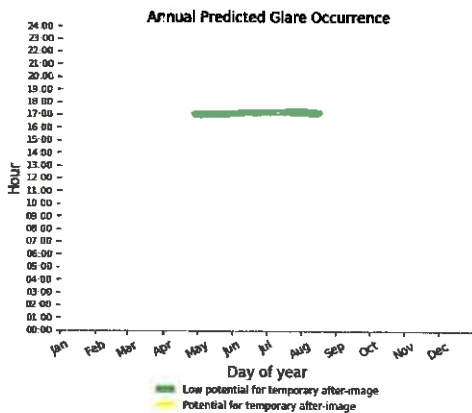
Mead Valley Business Park-Bldg A - Receptor (RWY 14 Final)

No glare found

Mead Valley Business Park-Bldg A - Receptor (RWY 32 Final)

PV array is expected to produce the following glare for observers on this flight path:

- 1,047 minutes of "green" glare with low potential to cause temporary after-image.
- 0 minutes of "yellow" glare with potential to cause temporary after-image.



Mead Valley Business Park-Bldg A - OP Receptor (1-ATCT)

No glare found

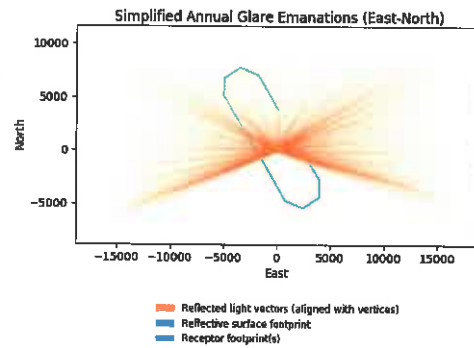
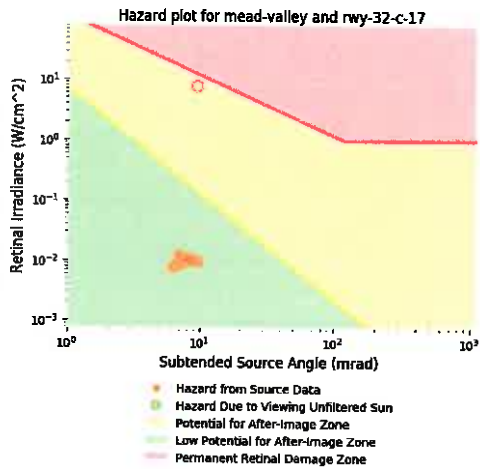
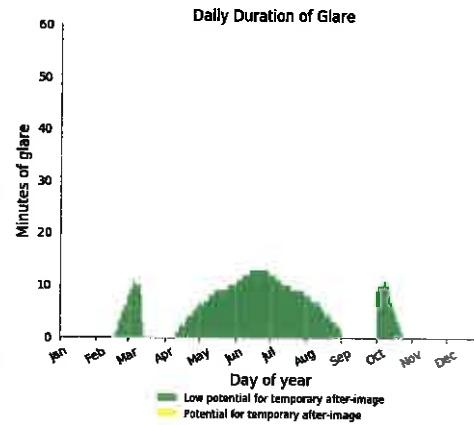
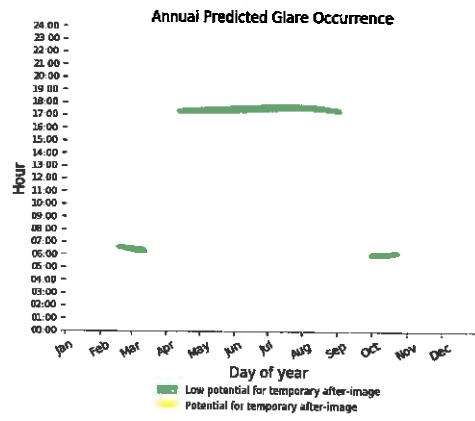
Mead Valley Business Park-Bldg A - Route Receptor (RWY 14 C-17 - KC-135 Pattern Route)

No glare found

Mead Valley Business Park-Bldg A - Route Receptor (RWY 32 C-17 - KC-135 Pattern Route)

PV array is expected to produce the following glare for receptors at this location:

- 1,481 minutes of "green" glare with low potential to cause temporary after-image.
- 0 minutes of "yellow" glare with potential to cause temporary after-image.



Glare vectors placed at PV centroid for clarity. Actual glare-spot locations vary.

Mead Valley Business Park-Bldg B low potential for temporary after-image

Component	Green glare (min)	Yellow glare (min)
FP: RWY 14 Final	0	0
FP: RWY 32 Final	2382	0
OP: 1-ATCT	0	0
Route: RWY 14 C-17 - KC-135 Pattern Route	0	0
Route: RWY 32 C-17 - KC-135 Pattern Route	1539	0

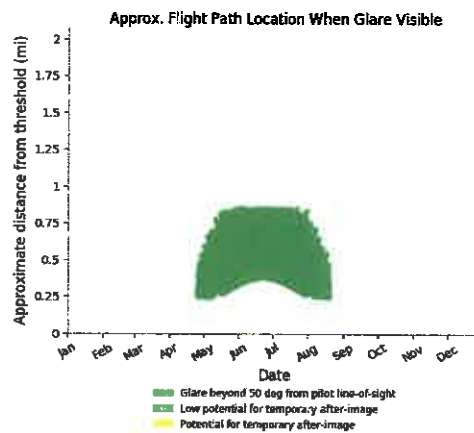
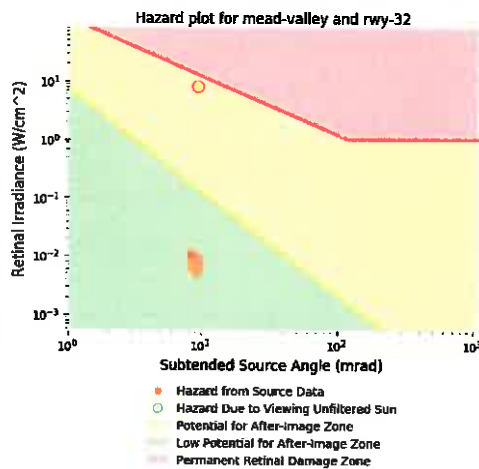
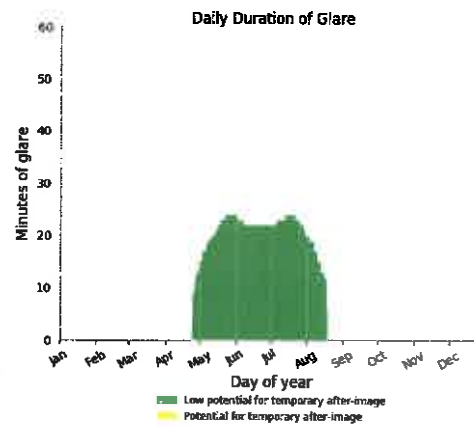
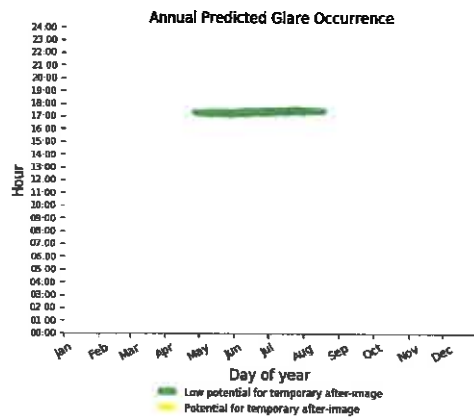
Mead Valley Business Park-Bldg B - Receptor (RWY 14 Final)

No glare found

Mead Valley Business Park-Bldg B - Receptor (RWY 32 Final)

PV array is expected to produce the following glare for observers on this flight path:

- 2,382 minutes of "green" glare with low potential to cause temporary after-image.
- 0 minutes of "yellow" glare with potential to cause temporary after-image.



Mead Valley Business Park-Bldg B - OP Receptor (1-ATCT)

No glare found

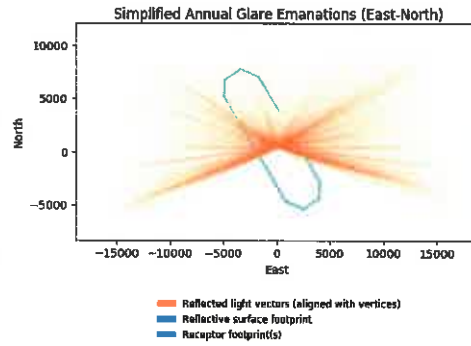
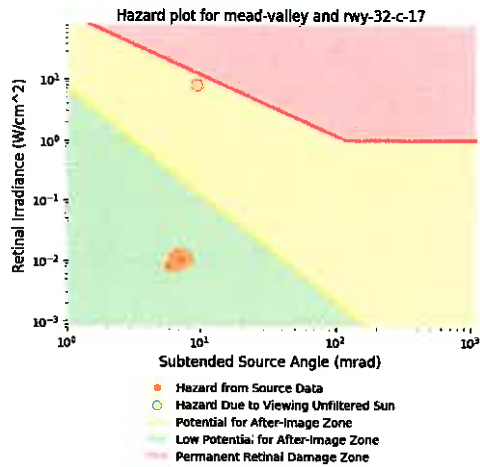
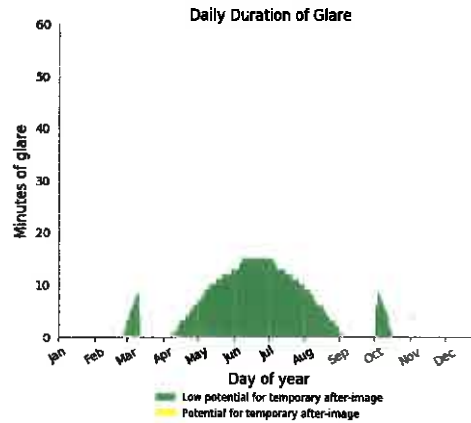
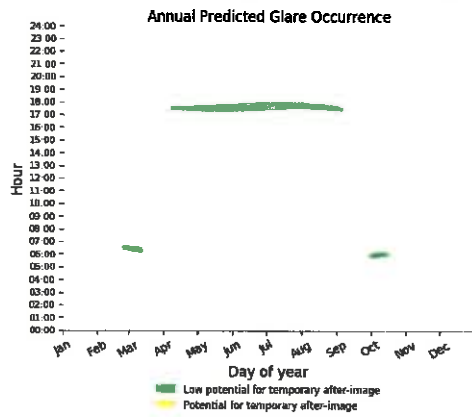
Mead Valley Business Park-Bldg B - Route Receptor (RWY 14 C-17 - KC-135 Pattern Route)

No glare found

Mead Valley Business Park-Bldg B - Route Receptor (RWY 32 C-17 - KC-135 Pattern Route)

PV array is expected to produce the following glare for receptors at this location:

- 1,539 minutes of "green" glare with low potential to cause temporary after-image.
- 0 minutes of "yellow" glare with potential to cause temporary after-image.



Glare vectors placed at PV centroid for clarity. Actual glare-spot locations vary.

Assumptions

- Times associated with glare are denoted in Standard time. For Daylight Savings, add one hour.
- Glare analyses do not account for physical obstructions between reflectors and receptors. This includes buildings, tree cover and geographic obstructions.
- Detailed system geometry is not rigorously simulated.
- The glare hazard determination relies on several approximations including observer eye characteristics, angle of view, and typical blink response time. Actual values and results may vary.
- Several calculations utilize the PV array centroid, rather than the actual glare spot location, due to algorithm limitations. This may affect results for large PV footprints. Additional analyses of array sub-sections can provide additional information on expected glare.
- The subtended source angle (glare spot size) is constrained by the PV array footprint size. Partitioning large arrays into smaller sections will reduce the maximum potential subtended angle, potentially impacting results if actual glare spots are larger than the sub-array size. Additional analyses of the combined area of adjacent sub-arrays can provide more information on potential glare hazards. (See previous point on related limitations.)
- Hazard zone boundaries shown in the Glare Hazard plot are an approximation and visual aid. Actual ocular impact outcomes encompass a continuous, not discrete, spectrum.
- Glare locations displayed on receptor plots are approximate. Actual glare-spot locations may differ.
- Glare vector plots are simplified representations of analysis data. Actual glare emanations and results may differ.
- Refer to the [Help page](#) for assumptions and limitations not listed here.

Attachment D
March ARB Runway 14/32 Overhead Traffic Pattern Analysis



FORGESOLAR GLARE ANALYSIS

Project: **SRG Perris - Mead Valley**

Proposed solar PV installation under the traffic pattern at March Air Reserve Base, Riverside, California

Site configuration: **Mead Valley-MARB Runway 14-32 Overhead Analysis**

Analysis conducted by Nick Johnson (nick.johnson@johnson-aviation.com) at 23:25 on 12 Aug, 2019.

U.S. FAA 2013 Policy Adherence

The following table summarizes the policy adherence of the glare analysis based on the 2013 U.S. Federal Aviation Administration Interim Policy 78 FR 63276. This policy requires the following criteria be met for solar energy systems on airport property:

- No "yellow" glare (potential for after-image) for any flight path from threshold to 2 miles
- No glare of any kind for Air Traffic Control Tower(s) ("ATCT") at cab height.
- Default analysis and observer characteristics (see list below)

ForgeSolar does not represent or speak officially for the FAA and cannot approve or deny projects. Results are informational only.

COMPONENT	STATUS	DESCRIPTION
Analysis parameters	PASS	Analysis time interval and eye characteristics used are acceptable
Flight path(s)	PASS	Flight path receptor(s) do not receive yellow glare
ATCT(s)	PASS	Receptor(s) marked as ATCT do not receive glare

Default glare analysis parameters and observer eye characteristics (for reference only):

- Analysis time interval: 1 minute
- Ocular transmission coefficient: 0.5
- Pupil diameter: 0.002 meters
- Eye focal length: 0.017 meters
- Sun subtended angle: 9.3 milliradians

FAA Policy 78 FR 63276 can be read at <https://www.federalregister.gov/d/2013-24729>



GlareGauge Glare Analysis Results

Site Configuration: Mead Valley-MARB Runway 14-32 Overhead Analysis

Project site configuration details and results.



Created Aug. 12, 2019 5:16 p.m.
 Updated Aug. 12, 2019 7:28 p.m.
 DNI varies and peaks at 1,000.0 W/m²
 Analyze every 1 minute(s)
 0.5 ocular transmission coefficient
 0.002 m pupil diameter
 0.017 m eye focal length
 9.3 mrad sun subtended angle
 Timezone UTC-8
 Site Configuration ID: 30300.5333

Summary of Results Glare with low potential for temporary after-image predicted

PV name	Tilt	Orientation	"Green" Glare	"Yellow" Glare	Energy Produced
	deg	deg	min	min	kWh
Mead Valley Business Park-Bldg A	10.0	160.0	4,911	0	-
Mead Valley Business Park-Bldg B	10.0	160.0	7,011	0	-

Component Data

PV Array(s)

Name: Mead Valley Business Park-Bldg A
 Axis tracking: Fixed (no rotation)
 Tilt: 10.0 deg
 Orientation: 160.0 deg
 Rated power: -
 Panel material: Smooth glass with AR coating
 Vary reflectivity with sun position? Yes
 Correlate slope error with surface type? Yes
 Slope error: 8.43 mrad

Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.861655	-117.271672	1617.08	48.00	1665.08
2	33.861644	-117.270914	1617.08	48.00	1665.08
3	33.861294	-117.270909	1617.08	48.00	1665.08
4	33.861293	-117.271675	1617.08	48.00	1665.08



Name: Mead Valley Business Park-Bldg B
Axis tracking: Fixed (no rotation)
Tilt: 10.0 deg
Orientation: 160.0 deg
Rated power: -
Panel material: Smooth glass with AR coating
Vary reflectivity with sun position? Yes
Correlate slope error with surface type? Yes
Slope error: 8.43 mrad

Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.865497	-117.271631	1597.08	48.00	1645.08
2	33.865494	-117.270857	1597.08	48.00	1645.08
3	33.865144	-117.270858	1597.08	48.00	1645.08
4	33.865148	-117.271642	1597.08	48.00	1645.08



2-Mile Flight Path Receptor(s)

Name: RWY 14 Final
Description: None
Threshold height: 50 ft
Direction: 149.5 deg
Glide slope: 3.0 deg
Pilot view restricted? Yes
Vertical view restriction: 30.0 deg
Azimuthal view restriction: 50.0 deg

Point	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
Threshold	33.896431	-117.270636	1500.07	50.00	1550.08
2-mile point	33.906486	-117.277783	1500.07	2000.10	3500.17



Name: RWY 32 Final
Description: None
Threshold height: 50 ft
Direction: 329.5 deg
Glide slope: 3.0 deg
Pilot view restricted? Yes
Vertical view restriction: 30.0 deg
Azimuthal view restriction: 50.0 deg

Point	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
Threshold	33.864994	-117.248281	1500.07	50.00	1550.08
2-mile point	33.854942	-117.241136	1500.07	2000.10	3500.17



Route Receptor(s)

Name: RWY 14 Overhead Route
Route type: One-way
View angle: 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.968036	-117.322128	1500.07	2000.10	3500.17
2	33.880706	-117.259453	1500.07	2000.10	3500.17
3	33.863564	-117.293808	1500.07	2000.10	3500.17
4	33.908131	-117.325528	1500.07	2000.10	3500.17
5	33.925156	-117.291061	1500.07	2000.10	3500.17
6	33.896431	-117.270636	1500.07	50.00	1550.08

Name: RWY 32 Overhead Route
Route type: One-way
View angle: 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.793375	-117.196878	1500.07	2000.10	3500.17
2	33.880706	-117.259453	1500.07	2000.10	3500.17
3	33.863564	-117.293808	1500.07	2000.10	3500.17
4	33.819225	-117.262269	1500.07	2000.10	3500.17
5	33.836269	-117.227869	1500.07	2000.10	3500.17
6	33.864994	-117.248281	1500.07	50.00	1550.08

Discrete Observation Receptors

Number	Latitude	Longitude	Ground elevation	Height above ground	Total Elevation
	deg	deg	ft	ft	ft
1-ATCT	33.891572	-117.251203	1511.07	118.01	1629.08

1-ATCT map image



PV Array Results

Mead Valley Business Park-Bldg A low potential for temporary after-image

Component	Green glare (min)	Yellow glare (min)
FP: RWY 14 Final	0	0
FP: RWY 32 Final	1336	0
OP: 1-ATCT	0	0
Route: RWY 14 Overhead Route	0	0
Route: RWY 32 Overhead Route	3575	0

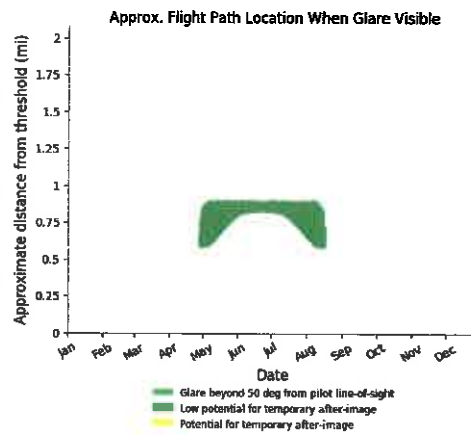
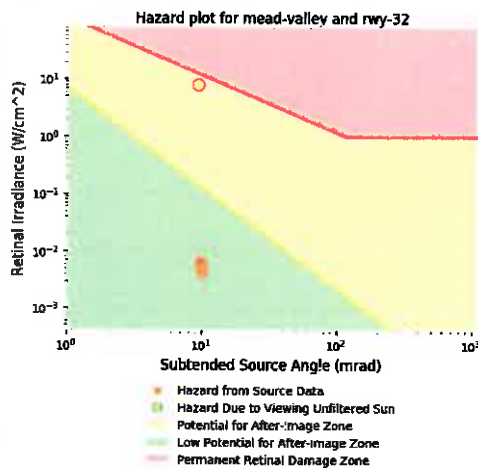
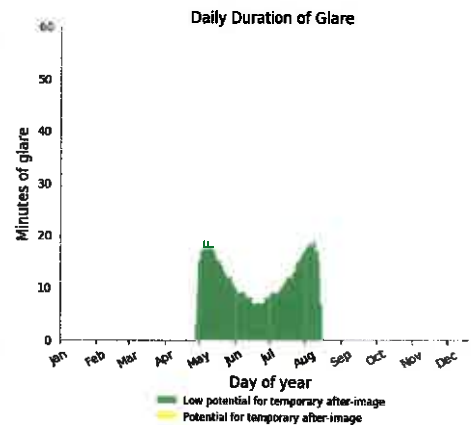
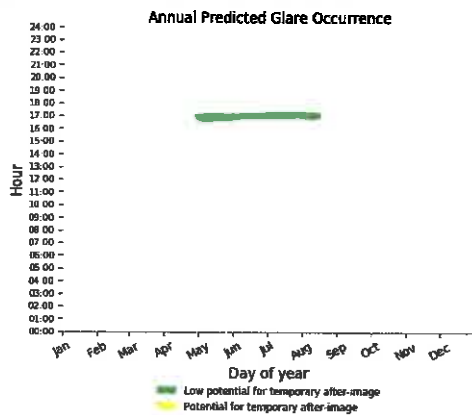
Mead Valley Business Park-Bldg A - Receptor (RWY 14 Final)

No glare found

Mead Valley Business Park-Bldg A - Receptor (RWY 32 Final)

PV array is expected to produce the following glare for observers on this flight path:

- 1,336 minutes of "green" glare with low potential to cause temporary after-image.
- 0 minutes of "yellow" glare with potential to cause temporary after-image.



Mead Valley Business Park-Bldg A - OP Receptor (1-ATCT)

No glare found

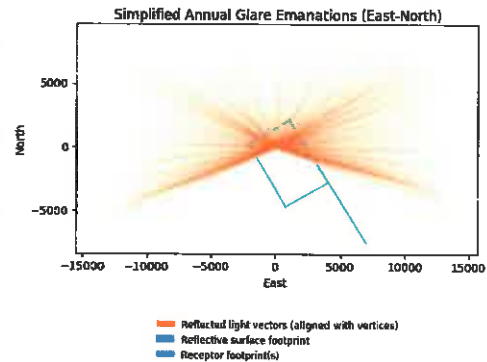
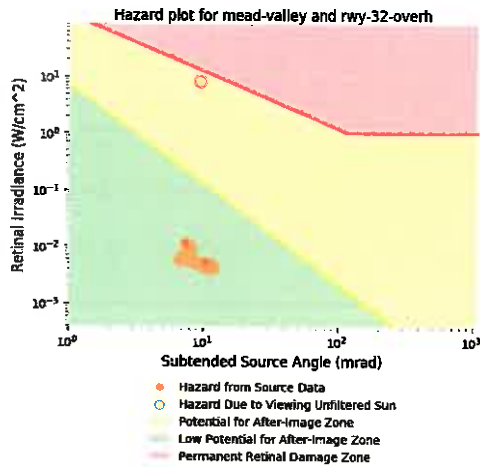
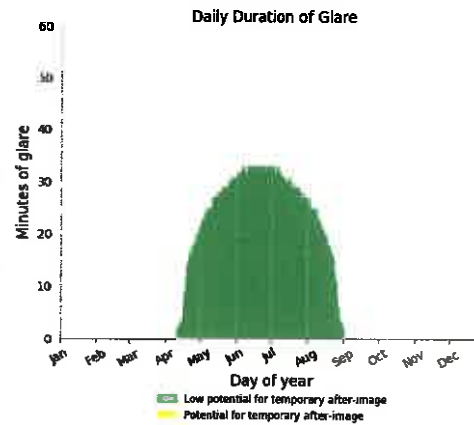
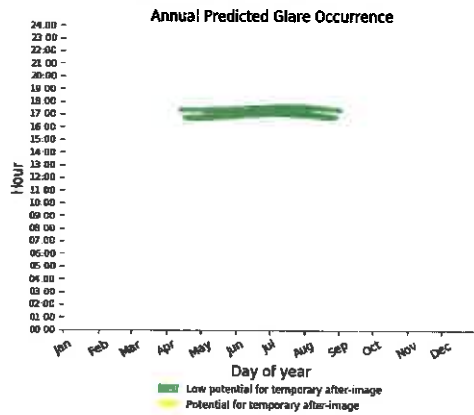
Mead Valley Business Park-Bldg A - Route Receptor (RWY 14 Overhead Route)

No glare found

Mead Valley Business Park-Bldg A - Route Receptor (RWY 32 Overhead Route)

PV array is expected to produce the following glare for receptors at this location:

- 3,575 minutes of "green" glare with low potential to cause temporary after-image.
- 0 minutes of "yellow" glare with potential to cause temporary after-image.



Glare vectors placed at PV centroid for clarity. Actual glare-spot locations vary.

Mead Valley Business Park-Bldg B low potential for temporary after-image

Component	Green glare (min)	Yellow glare (min)
FP: RWY 14 Final	0	0
FP: RWY 32 Final	2909	0
OP: 1-ATCT	0	0
Route: RWY 14 Overhead Route	0	0
Route: RWY 32 Overhead Route	4102	0

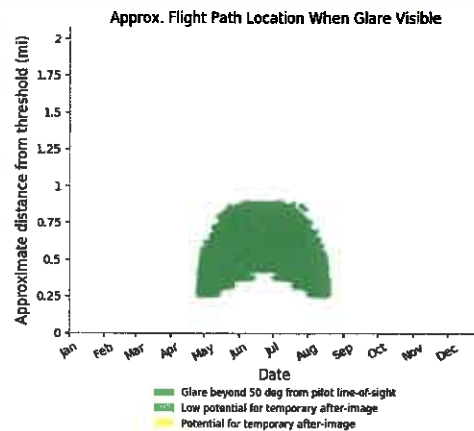
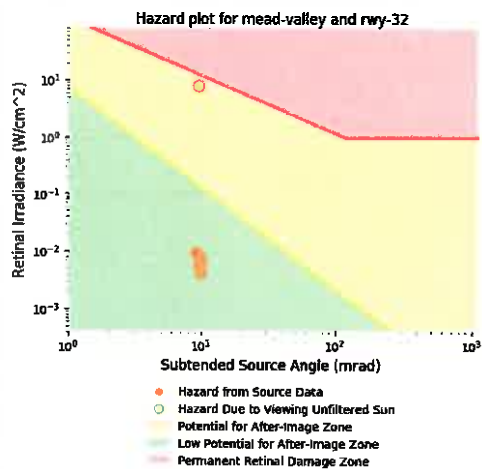
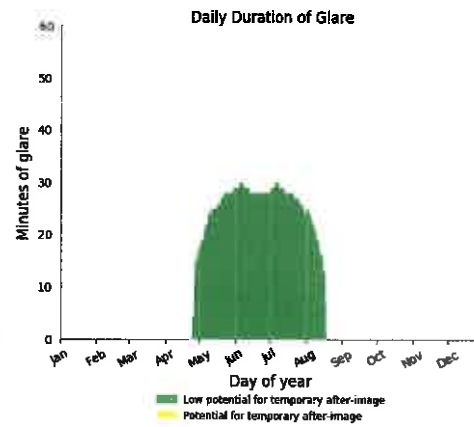
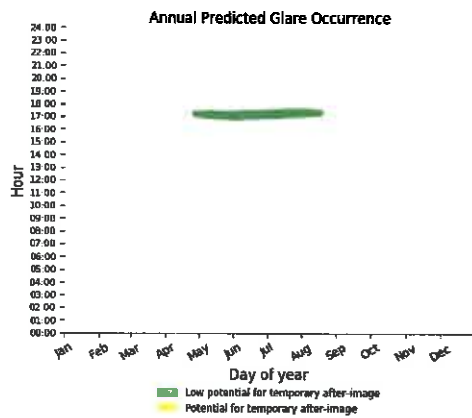
Mead Valley Business Park-Bldg B - Receptor (RWY 14 Final)

No glare found

Mead Valley Business Park-Bldg B - Receptor (RWY 32 Final)

PV array is expected to produce the following glare for observers on this flight path:

- 2,909 minutes of "green" glare with low potential to cause temporary after-image.
- 0 minutes of "yellow" glare with potential to cause temporary after-image.



Mead Valley Business Park-Bldg B - OP Receptor (1-ATCT)

No glare found

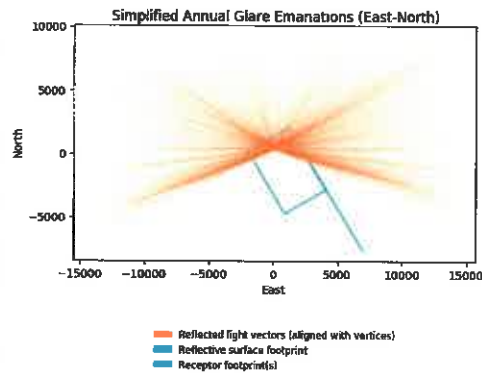
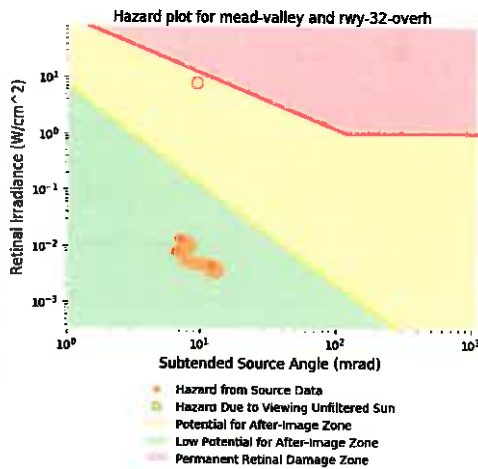
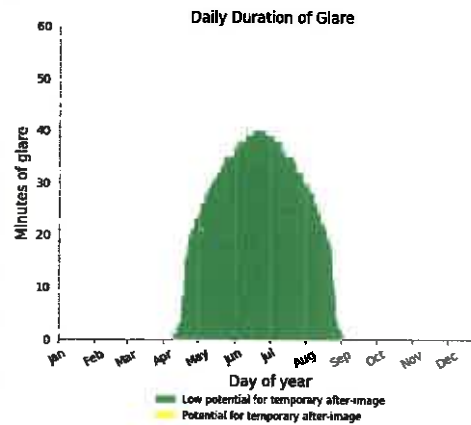
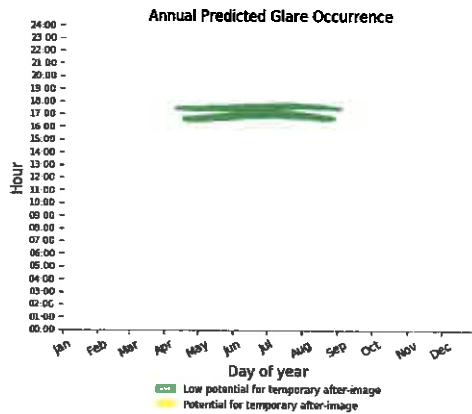
Mead Valley Business Park-Bldg B - Route Receptor (RWY 14 Overhead Route)

No glare found

Mead Valley Business Park-Bldg B - Route Receptor (RWY 32 Overhead Route)

PV array is expected to produce the following glare for receptors at this location:

- 4,102 minutes of "green" glare with low potential to cause temporary after-image.
- 0 minutes of "yellow" glare with potential to cause temporary after-image.



Glare vectors placed at PV centroid for clarity. Actual glare-spot locations vary.

Assumptions

- Times associated with glare are denoted in Standard time. For Daylight Savings, add one hour.
- Glare analyses do not account for physical obstructions between reflectors and receptors. This includes buildings, tree cover and geographic obstructions.
- Detailed system geometry is not rigorously simulated.
- The glare hazard determination relies on several approximations including observer eye characteristics, angle of view, and typical blink response time. Actual values and results may vary.
- Several calculations utilize the PV array centroid, rather than the actual glare spot location, due to algorithm limitations. This may affect results for large PV footprints. Additional analyses of array sub-sections can provide additional information on expected glare.
- The subtended source angle (glare spot size) is constrained by the PV array footprint size. Partitioning large arrays into smaller sections will reduce the maximum potential subtended angle, potentially impacting results if actual glare spots are larger than the sub-array size. Additional analyses of the combined area of adjacent sub-arrays can provide more information on potential glare hazards. (See previous point on related limitations.)
- Hazard zone boundaries shown in the Glare Hazard plot are an approximation and visual aid. Actual ocular impact outcomes encompass a continuous, not discrete, spectrum.
- Glare locations displayed on receptor plots are approximate. Actual glare-spot locations may differ.
- Glare vector plots are simplified representations of analysis data. Actual glare emanations and results may differ.
- Refer to the **Help** page for assumptions and limitations not listed here.

**NOTICE OF PUBLIC HEARING
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION**

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the application described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. For more information please contact **ALUC Planner Paul Rull at (951) 955-6893**. The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan.

The County of Riverside Planning Department may hold hearings on this item and should be contacted on non-ALUC issues. For more information please contact County of Riverside Planner Mr. Tim Wheeler at (951) 955-6060.

The proposed project application may be viewed and written comments may be submitted at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Thursday from 8:00 a.m. to 5:00 p.m., except Monday September 2 (Labor Day), and by prescheduled appointment on Friday, September 6, from 9:00 a.m. to 5:00 p.m.

PLACE OF HEARING: Riverside County Administration Center
4080 Lemon Street, 1st Floor Board Chambers
Riverside California

DATE OF HEARING: September 12, 2019

TIME OF HEARING: 9:30 A.M.

CASE DESCRIPTION:

ZAP1371MA19 – Sares-Regis Group/SRG Perris, LP (Representative: EPD Solutions) – County of Riverside Case No. PPT190011 (Plot Plan). A proposal to construct two industrial manufacturing buildings with mezzanines totaling 710,736 square feet on a combined total 35.76 acres located northerly of Oleander Avenue, westerly of Decker Road, southerly of Nandina Avenue, and easterly of Day Street. The applicant also proposes rooftop solar panels totaling 60,000 square feet (30,000 square feet on each building) (Airport Compatibility Zone C2 High Terrain Zone of the March Air Reserve Base/Inland Port Airport Influence Area).



RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

March
C2/
C2-HT

APPLICATION FOR MAJOR LAND USE ACTION REVIEW			
ALUC CASE NUMBER: <u>2AP1371MA19</u>		DATE SUBMITTED: <u>6-3-19</u>	
APPLICANT / REPRESENTATIVE / PROPERTY OWNER CONTACT INFORMATION			
Applicant	<u>Sares- Regis Group / SRG Perris, LP</u>	Phone Number	<u>949-809-2414</u>
Mailing Address	<u>18802 Bardeen Ave Irvine CA 92612</u>	Email	<u>prussell@sares-regis.com</u>
Representative	<u>EPD Solutions</u>	Phone Number	<u>949-226-1854</u>
Mailing Address	<u>2 Park Plaza Suite 1120 Irvine CA 92614</u>	Email	<u>norah@epdsolution.com</u>
Property Owner	<u>SRG Perris LP</u>	Phone Number	<u>949-809-2414</u>
Mailing Address	<u>18802 Bardeen Ave Irvine CA 92612</u>	Email	<u>prussell@sares-regis.com</u>
LOCAL JURISDICTION AGENCY			
Local Agency Name	<u>County of Riverside</u>	Phone Number	<u>951-955-6060</u>
Staff Contact	<u>Timothy Wheeler</u>	Email	<u>TWHEELER@RIVCO.ORG</u>
Mailing Address	<u>4080 Lemon St 12th Floor Riverside CA 92501</u>	Case Type	<u>Plot Plan</u>
Local Agency Project No	<u>PPT190011</u>	<input type="checkbox"/> General Plan / Specific Plan Amendment <input type="checkbox"/> Zoning Ordinance Amendment <input type="checkbox"/> Subdivision Parcel Map / Tentative Tract <input type="checkbox"/> Use Permit <input checked="" type="checkbox"/> Site Plan Review/Plot Plan <input type="checkbox"/> Other	
PROJECT LOCATION			
<i>Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways</i>			
Street Address	<u>SWC of Nandina Ave and Decker Rd</u>		
Assessor's Parcel No.	<u>295-310-012-3, 4, 5, 6</u>	Gross Parcel Size	<u>36 acres</u>
Subdivision Name	Nearest Airport and distance from Air- port		
Lot Number			
PROJECT DESCRIPTION			
<i>If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed</i>			
Existing Land Use (describe)	<u>Site is currently vacant</u>		

Proposed Land Use (describe)	Development of one story + mezzanine buildings for speculative industrial/commercial warehouse use.	
For Residential Uses	Number of Parcels or Units on Site (exclude secondary units)	NA
For Other Land Uses (See Appendix C)	Hours of Operation	TBD
	Number of People on Site	Maximum Number
	Method of Calculation	
Height Data	Site Elevation (above mean sea level)	ft.
	Height of buildings or structures (from the ground)	43 ft.
Flight Hazards	Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
	If yes, describe	

- A. **NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.
- B. **REVIEW TIME:** Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.
- C. **SUBMISSION PACKAGE:**
1. Completed ALUC Application Form
 1. ALUC fee payment — *1521 (incl. 190)*
 1. Plans Package (*24x36 folded*) (site plans, floor plans, building elevations, grading plans, subdivision maps)
 - ①. Plans Package (8.5x11) (site plans, floor plans, building elevations, grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
 1. CD with digital files of the plans (pdf)
 1. Vicinity Map (8.5x11)
 1. Detailed project description
 1. Local jurisdiction project transmittal
 - ③. Gummed address labels for applicant/representative/property owner/local jurisdiction planner
 - ③. Gummed address labels of all surrounding property owners within a 300 foot radius of the project site. (**Only required if the project is scheduled for a public hearing Commission meeting**)

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

STAFF REPORT

ADMINISTRATIVE ITEMS

4.1 Director's Approvals.

- A. During the period of July 16, 2019 through August 15, 2019, as authorized pursuant to Section 1.5.2(d) of the 2004 Riverside County Airport Land Use Compatibility Plan, ALUC Director Simon Housman reviewed three non-legislative cases within Zone D of the Jacqueline Cochran Regional Airport, March Air Reserve Base/Inland Port and Palm Springs International Airport Influence Areas and issued determinations of consistency.

ZAP1078PS19 (Palm Springs International Airport Influence Area, Zone D) pertains to City of Cathedral City Case No. CUP16-008A (Conditional Use Permit), a proposal to construct a new two-story 10,834 square foot building and occupy an existing 13,648 square foot building to be used as a cannabis cultivation facility on 0.97 acres located at 36555 Bankside Drive (on the westerly side of Bankside Drive, northerly of Commercial Road). The site is located within Compatibility Zone D of the Palm Springs International Airport Influence Area. Within Compatibility Zone D of the Palm Springs International Airport Land Use Compatibility Plan, nonresidential intensity is limited to an average of 100 persons per acre and a maximum of 300 persons in any given single-acre area. The proposed new building and occupancy of the existing building would cumulatively accommodate a total of 94 persons, resulting in an average intensity of 97 persons per acre and a single-acre intensity of 94 persons, both of which are consistent with Zone D intensity criteria. The elevation of Runway 13R-31L at Palm Springs International Airport at its southerly terminus is approximately 395.5 feet above mean sea level (AMSL). At a distance of approximately 14,900 feet from the runway to the project property line, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any new structures with top of roof exceeding 544.5 feet AMSL. The site elevation is 302 feet AMSL, and the tallest proposed structure height is 36 feet, resulting in a maximum top point elevation of 338 feet AMSL, which is lower than the runway elevation. Therefore, FAA OES review for height/elevation reasons was not required. ALUC Director Simon Housman issued a determination of consistency for this project on July 18, 2019.

ZAP1374MA19 (March Air Reserve Base/Inland Port Airport Influence Area, Zone D) pertains to County of Riverside Case No. PPA180065 (Plot Plan), a proposal to establish three 3,500 square foot pre-fabricated buildings on a 1.96-acre lot located westerly of Wood Road, easterly of Prairie Way, northerly of Van Buren Boulevard, and southerly of Orrick Road in the unincorporated community of Woodcrest for use as an indoor bird aviary. The site is located within Airport Compatibility Zone D of the March Air Reserve Base/Inland Port Airport Influence Area, where nonresidential intensity is not restricted. The elevation of Runway 14-32 at March Air Reserve Base/Inland Port Airport at its northerly terminus is approximately 1,535 feet above mean sea level (AMSL). At a distance of 18,100 feet from the runway to the project, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review could be required for any structures with top of roof exceeding 1,716 feet AMSL. The site's elevation is 1,624 feet AMSL, and the proposed maximum building height is 26 feet, resulting in a top point elevation of 1,650 feet AMSL. Therefore, FAA OES review was not required. ALUC Director Simon Housman issued a determination of consistency for this project on July 18, 2019.

ZAP1047TH19 (Jacqueline Cochran Regional Airport Influence Area, Zone D) pertains to County of Riverside Case No. CUP190004 (Conditional Use Permit), a proposal to establish a truck storage yard with a 480 square foot office trailer, an above ground 10,000 gallon diesel fuel storage tank, a metal storage container, and a weigh station on 2.63 acres located at 56061 Orange Street (on the westerly side of Orange Street, southerly of Airport Boulevard) in the unincorporated community of Thermal. The site is located within Compatibility Zone D of the Jacqueline Cochran Regional Airport Influence Area, where nonresidential intensity is restricted to an average of 100 persons per acre and a maximum of 300 persons in any given single-acre area. The 480 square foot office trailer would accommodate 2 people. Even if 30 trucks with two drivers each were on-site at any one time, the total number of persons would not exceed 60, which would still be consistent. The elevation at the northerly end of Runway 12-30 at Jacqueline Cochran Regional Airport is -117 feet mean sea level (MSL) [i.e., 117 feet below mean sea level]. At a distance of 5,150 feet from the runway to the project, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with top of roof exceeding -65.5 feet mean sea level. The site's elevation is -116 feet MSL. The proposed building height is 10 feet, resulting in a top point elevation of -106 feet MSL. Therefore, FAA OES review for height/elevation reasons was not required. ALUC Director Simon Housman issued a determination of consistency for this project on August 15, 2019.

- B. Additionally, as authorized pursuant to ALUC Resolution No. 2015-01, as extended by Resolution Nos. 2016-02 and 2018-02, ALUC Director Simon Housman reviewed two legislative case sets with associated non-legislative cases and one stand-alone legislative case within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area and issued determinations of consistency.

ZAP1375MA19 pertains to City of Moreno Valley Case Nos. GPA19-0151 (General Plan Amendment), a proposal to amend the General Plan designations on 49.61 acres located northerly of State Highway Route 60, easterly of Day Street, southerly of Ironwood Avenue, and westerly of Brixton Court from 30.43 acres of Commercial and 19.17 acres of Residential/Office to 30.56 acres of Commercial and 19.05 acres of Residential/Office, ZC19-0152 (Zone Change), a proposal to amend the boundaries of zoning classifications on the same site from 30.66 acres of Community Commercial and 18.94 acres of Residential 15 to 30.56 acres of Community Commercial and 19.05 acres of Residential 15 (essentially shifting the General Plan land use designation and zoning boundaries), and PEN19-0150 (Tentative Parcel Map No. 37750), a proposal to divide the land specified above into four (4) parcels. The site is located within Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area, where nonresidential intensity is not limited. Both the existing and proposed General Plan and zoning designations and boundaries are consistent with the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan. The elevation of Runway 14-32 at March Air Reserve Base/Inland Port Airport at its northerly terminus is approximately 1,535 feet above mean sea level (AMSL). At a distance of 15,900 feet from the project site to the nearest point on the runway, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any new structures with an elevation at top of roof exceeding 1,694 feet AMSL. Elevations on-site range from 1,656 to 1,772 feet AMSL. No new buildings are proposed at this time. Review by the FAA OES is not a prerequisite to land division, but will be required prior to construction of new buildings or any other structures on the parcels. A condition has been included requiring that the permittee obtain a "Determination of No Hazard to Air Navigation" letter from the FAA OES prior to issuance of building permits for any new structures on the property with a top point elevation greater than 1,694 feet AMSL. A determination of consistency for this project was issued by ALUC Director Simon Housman on July 25, 2019.

ZAP1377MA19 pertains to City of Menifee Case No. SPA2010-090 (Specific Plan Amendment), a proposal to amend the land use designations within a portion of Specific Plan No. 260 (SP260 A3) Menifee North Specific Plan located in the City of Menifee, specifically the portion located northerly of State Highway Route 74, westerly of Menifee Road, and easterly of Palomar Road. (The Specific Plan was initially approved by the County of Riverside prior to incorporation of the City of Menifee, and a portion of the Specific Plan lies outside city limits.) The area proposed for amendment constitutes Planning Areas 11 through 14, which are currently proposed to provide 28.3 acres of Business Park uses (Planning Areas 11 and 12), 14.6 acres of Commercial/Business Park uses (Planning Area 13), and 11.7 acres of Commercial uses (Planning Area 14). (In addition, there is a 9.12-acre Southern California Edison ["SCE"] transmission line easement.) The proposed amendment would provide for 22.03 acres of Commercial uses, 24.43 acres of Very High Density Residential uses, and 7.66 acres that could be developed with either Commercial or Very High Density Residential uses (the above acreages excluding land within the SCE easement). Planning Areas 11 through 14 would be reconfigured as Planning Areas 11A, 11B, 12A, 12B, 13A, 13B, and 14. Junipero Road would separate Planning Areas 11A from 11B, 12A from 12B, and 13A from 13B. (The SCE easement would be included within Planning Areas 11B, 12B, and 13B, although that area would not be available for development of residential or commercial uses.) Planning Areas 11A and 11B would be designated for Very High Density Residential uses, and Planning Areas 13A and 13B would be designated for Commercial uses. Planning Areas 12A and 12B would be designated to allow for either Commercial or Very High Density Residential land uses. The site is located within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area, where residential density and non-residential intensity are not restricted. ALUC Director Simon Housman issued a determination of consistency for this project on July 25, 2019, subject to the proviso that the City of Menifee incorporate in the text of the amended Specific Plan an acknowledgement that the Specific Plan is located within Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area and that subsequent underlying entitlements will be reviewed in light of the then-applicable Airport Land Use Compatibility Plan.

ZAP1378MA19 pertains to County of Riverside Case Nos. SP260SC2 (Specific Plan No. 260, Substantial Conformance No. 2), a proposal to (1) re-allocate the number of dwelling units among Planning Areas 24, 25, 26, and 28 within the portions of the Menifee North Specific Plan, as amended through Amendment No. 2, located within the unincorporated area, specifically the portion located southerly of Watson Road, easterly of Briggs Road, westerly of Sultana Road, and northerly of (but not contiguous to) State Highway Route 74; (2) modify the development standards for these Planning Areas; and (3) correct the land use designations for Planning Areas 26 and 28 to correspond to the approved tentative tract map for that area, CZ180018 (Change of Zone), a proposal to amend the Specific Plan Zoning Ordinance for Specific Plan No. 260 to modify the minimum lot size requirements and other development standards applicable to Planning Areas 24, 25, 26, and 28 in accordance with the Substantial Conformance proposal, TR37553 (Tentative Tract Map No. 37553), a proposal to divide 70.7 acres (Planning Areas 24 and 25 of Specific Plan No. 260, as amended by Substantial Conformance No. 2) into 363 residential lots (with a 3.2-acre park), TR29322MC1 (Tentative Tract Map No. 29322, Minor Change No. 1), a proposal to modify approved, but as yet unrecorded, Tentative Tract Map No. 29322 so as to reduce the number of residential lots therein from 210 to 188 while increasing its area by 2 acres, and PM37690 (Tentative Parcel Map No. 37690), a proposal to divide the larger 114.14 gross acres constituting the combined areas of the two tracts into five (5) parcels for financing purposes. The site is located within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area, where residential density and non-residential intensity are not restricted. The actual nearest runway to the site is Runway 15-33 at Perris Valley Airport, but the southerly terminus of this runway is located approximately 24,900 feet from the project site, so the site is beyond the 20,000-foot radius within which notice to the Federal Aviation Administration Obstruction Evaluation Service (FAA OES) could be required. Runway 14-32 at March Air Reserve Base/Inland Port Airport is 10 miles from the

site. Therefore, FAA OES review for height/elevation reasons was not required. ALUC Director Simon Housman issued a determination of consistency for this project on August 5, 2019.

- C. Additionally, ALUC Director Simon Housman reviewed two County-initiated non-impact legislative cases (ordinance amendments) pursuant to ALUC Resolution No. 2011-02 and issued determinations of consistency.

ZAP1041RG19 (Countywide unincorporated areas) pertains to County of Riverside Change of Zone Case No. 1900008 (Ordinance Amendment), a proposal to amend County Ordinance No. 348 by amending the zones within which kennels and catteries are permitted, reorganizing Section 18.45 addressing Kennels and Catteries, and updating definitions of catteries (Section 21.20) and kennels (Section 21.40a). The definitions of kennels and catteries would be amended to be consistent with the provisions of County Ordinance No. 630. Additionally, reference to allowances for kennels and catteries would be deleted from the individual zoning classification lists of permitted land uses, with the zoning classifications specified in Section 18.45. Presently, the R-R and R-R-O zones allow for Class II (11 to 25 dogs) and Class III (26 to 40 dogs) kennels, subject to plot plan approval, and Class IV kennels (41 or more dogs) and Sentry Dog kennels, subject to conditional use permit approval. Additionally, Class II kennels are permitted in the R-A, W-2, R-T-R, R-D, and W-2-M zones, subject to plot plan approval. These provisions would be deleted, such that the maximum size kennel allowed in residential zones (other than the agricultural zones A-1, A-2, A-P, and A-D and the N-A [Natural Assets] zone) would be Class I kennels (5 to 10 dogs). The amendment would also add the I-P, MU, C-1/C-P, C-P-S, and CR zones to the list of zones that would permit Class I, Class II, and Class III kennels, subject to plot plan approval, and Class IV kennels, subject to conditional use permit approval. Sentry Dog kennels would also be permitted in the I-P one, subject to conditional use permit approval. Finally, the I-P, MU, C-1/C-P, C-P-S, and CR zones would be added to the list of zones permitting Class I (10 to 25 cats) and Class II (26 or more cats) catteries, subject to plot plan approval. The proposed amendment does not change development standards or land uses in a manner that would affect residential densities or the human intensity of nonresidential uses. Therefore, this amendment has no possibility for having an impact on the safety of air navigation or on the consistency/inconsistency status of the County's General Plan relative to any given Airport Land Use Compatibility Plan. ALUC Director Simon Housman issued a determination of consistency for this project on August 8, 2019.

ZAP1042RG19 (Countywide unincorporated areas) pertains to County of Riverside Change of Zone Case No. 1900010 (Ordinance Amendment), a proposal to amend County Ordinance No. 348 by adding Section 18.54 relating to Mobile Food Trucks. The purpose of this amendment is to establish regulations related to Mobile Food Truck operations on private property in the unincorporated areas of the County. The new section includes permitting requirements, a list of zones in which they would be prohibited, and specified location, operational, and approval requirements for Mobile Food Truck operations. Mobile Food Truck operations would not be permitted in Compatibility Zone A of any airport. Mobile Food Trucks operating in conjunction with and on the same site as events consistent with a legally permitted land use or temporary event are exempted from these provisions, as are those operating (1) on the property of a legally established school, college, university, hospital, or place of religious worship, (2) in the public right-of-way pursuant to Ordinance No. 853, or (3) in conjunction with an active construction site. The proposed amendment does not change development standards or land uses in a manner that would affect residential density or nonresidential intensity. Therefore, this amendment has no possibility for having an impact on the safety of air navigation within the County's Airport Influence Areas. ALUC Director Simon Housman issued a determination of consistency for this project on August 15, 2019.

4.2 Strategic Vision Chart

The ALUC Strategic Vision Chart presents priorities for staff's long-term planning work, involving the preparation and amendment of Airport Land Use Compatibility Plans (ALUCPs). Presently, our Plans can be considered as dividing into two groups: those which have been adopted or amended before the *Muzzy v. Solano* decision, and those which have been adopted or amended after that decision. In *Muzzy v. Solano*, the California Supreme Court determined that the adoption of an ALUCP, or of an amendment to an ALUCP, constitutes a "project" pursuant to the California Environmental Quality Act (CEQA), and, therefore, requires environmental analysis or a finding of exemption. The French Valley, Chino, Perris Valley, March Air Reserve Base/Inland Port, and Hemet-Ryan ALUCPs, the 2006 amendment to the Jacqueline Cochran ALUCP, and the 2016 amendment to the Banning ALUCP required concurrent environmental determinations. The other ALUCPs approved in 2004 and 2005 – Riverside Municipal, Palm Springs International, Bermuda Dunes, Flabob, Blythe, Corona Municipal, and Chiriaco Summit – have not been environmentally assessed. Such assessment, involving an inventory of land use and General Plan designations, would be required prior to amending these Plans, in order to determine potential displacement resulting from the differences between General Plan and Compatibility Plan development allowances.

Y:\ALUC\ALUC Administrative Items\Admin Item 09-12-19.doc



AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY

July 18, 2019

Mr. Salvador Quintanilla, Project Planner
City of Cathedral City Planning Department
68-700 Avenida Lalo Guerrero
Cathedral City, CA 92234

CHAIR
Steve Manos
Lake Elsinore

VICE CHAIR
Russell Betts
Desert Hot Springs

COMMISSIONERS

Arthur Butler
Riverside

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Riverside

Russell Betts
Desert Hot Springs

Steven Stewart
Palm Springs

Richard Stewart
Moreno Valley

Gary Youmans
Temecula

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW –
DIRECTOR’S DETERMINATION**

File No.: ZAP1078PS19
Related File No.: CUP16-008A (Conditional Use Permit)
APN: 687-311-027

Dear Mr. Quintanilla:

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to ALUC’s general delegation as per Policy 1.5.2(d) of the Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan, staff reviewed City of Cathedral City Case No. CUP16-008A (Conditional Use Permit), a proposal to construct a new two-story 10,834 square foot building and occupy an existing 13,648 square foot building to be used as a cannabis cultivation facility on 0.97 acres located at 36555 Bankside Drive (on the west side of Bankside Drive, northerly of Commercial Road).

STAFF

Director
Simon A. Housman

John Guerin
Paul Rull
Barbara Santos

County Administrative Center
4080 Lerron St., 14th Floor
Riverside, CA 92501
(951) 955-5132

The site is located within Airport Compatibility Zone D of the Palm Springs International Airport Influence Area (AIA). Within Compatibility Zone D of the Palm Springs International Airport Land Use Compatibility Plan, non-residential intensity is limited to 100 people per average acre and 300 people per single acre. The proposed new building and occupancy of the existing building (conservatively estimated as all office use) would cumulatively accommodate a total of 94 persons, resulting in an average intensity of 97 persons per acre, and a single-acre intensity of 94 persons, both of which are consistent with Zone D intensity criteria.

The elevation of Runway 13R-31L at Palm Springs International Airport at its southerly terminus is approximately 395.5 feet above mean sea level (395.5 feet AMSL). At a distance of approximately 14,900 feet from the runway to the project property line, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with top of roof exceeding 544.5 feet AMSL. The site elevation is 302 feet AMSL, and the tallest proposed structure height is 36 feet, resulting in a maximum top point elevation of 338 feet AMSL, which is lower than the runway elevation. Therefore, review by the FAA OES was not required.

www.rcaluc.org


As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2005 Palm Springs Airport Land Use Compatibility Plan, provided that the City of Cathedral City applies the following recommended conditions:

CONDITIONS:

1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses shall be prohibited:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, outdoor production of cereal grains, sunflower, and row crops, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, and construction and demolition debris facilities.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
3. The attached "Notice of Airport in Vicinity" shall be provided to all potential purchasers of the property and to the tenants of the buildings, and be recorded as a deed notice.
4. Any new detention basins on the site shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention basin(s) that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.
5. Any increase in building area or change in use that would increase the size of the conference room, provide additional area open to the public, or convert storage area to additional conference, office, or customer space will require an amended review by the Airport Land Use Commission.

If you have any questions, please contact Paul Rull, ALUC Principal Planner, at (951) 955-6893.

Sincerely,
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Simon A. Housman, ALUC Director

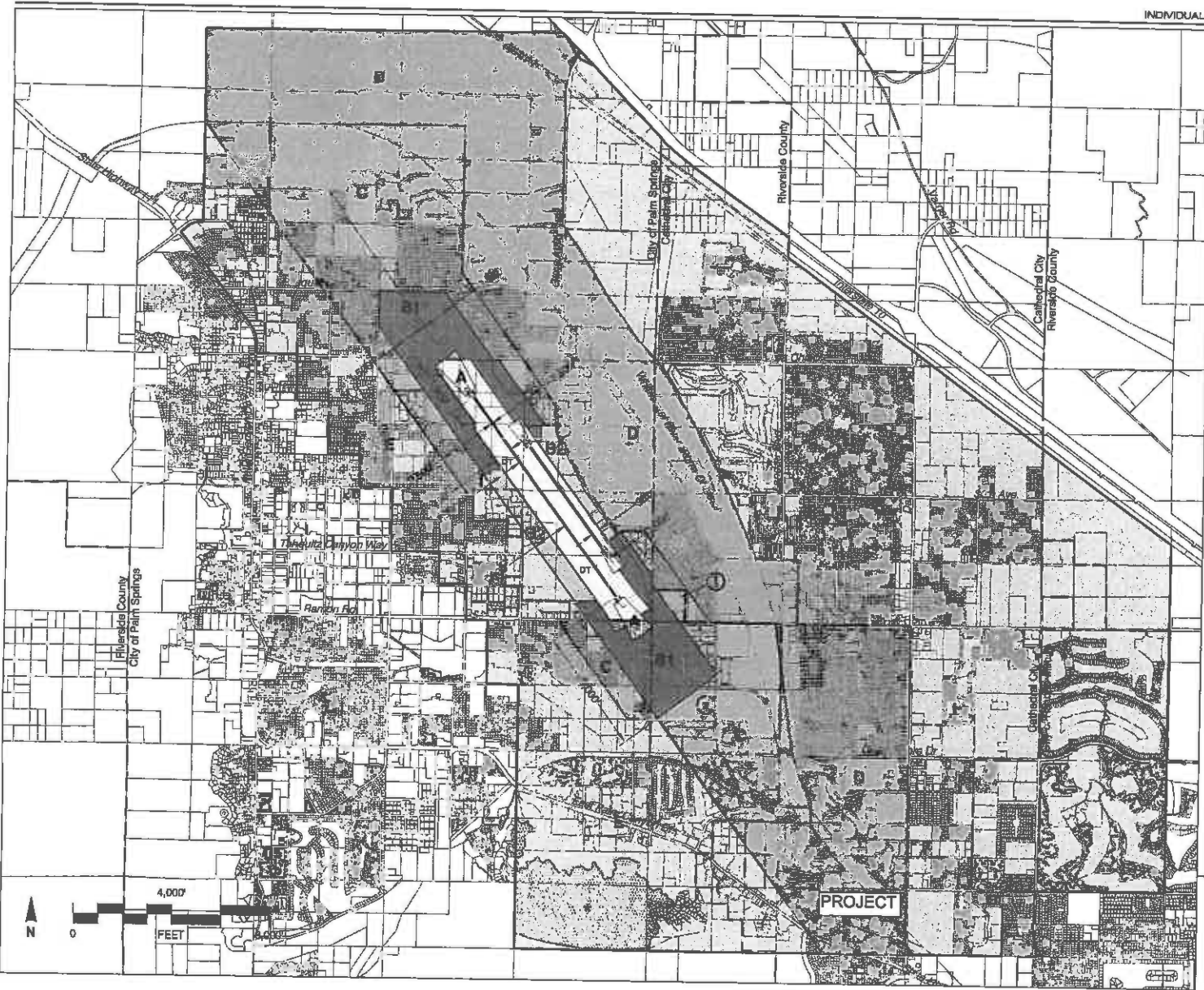
Attachment: Notice of Airport in Vicinity

cc: Mayan Properties, LLC/Medicinal Healing, LLC (property owner/applicant)
Hundred Mile House, Duane Smith (representative)
Thomas Nolan, Executive Director, Palm Springs International Airport
ALUC Case File

Y:\AIRPORT CASE FILES\Palm Springs\ZAP1078PS19\ZAP1078PS19.LTR.doc

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



Legend

- Compatibility Zones**
- Airport Influence Area Boundary
 - Zone A
 - Zone B1
 - Zone B2
 - Zone C
 - Zone D
 - Zone E
 - Height Review Overlay Zone

- Boundary Lines**
- Airport Property Line
 - City Limits

- Notes**
- All dimensions measured from runway ends and centerlines.
- DT = Displaced Threshold
- See Chapter 2, Table 2A for compatibility criteria associated with this map.
- Ⓟ See Policy PS.2.1.

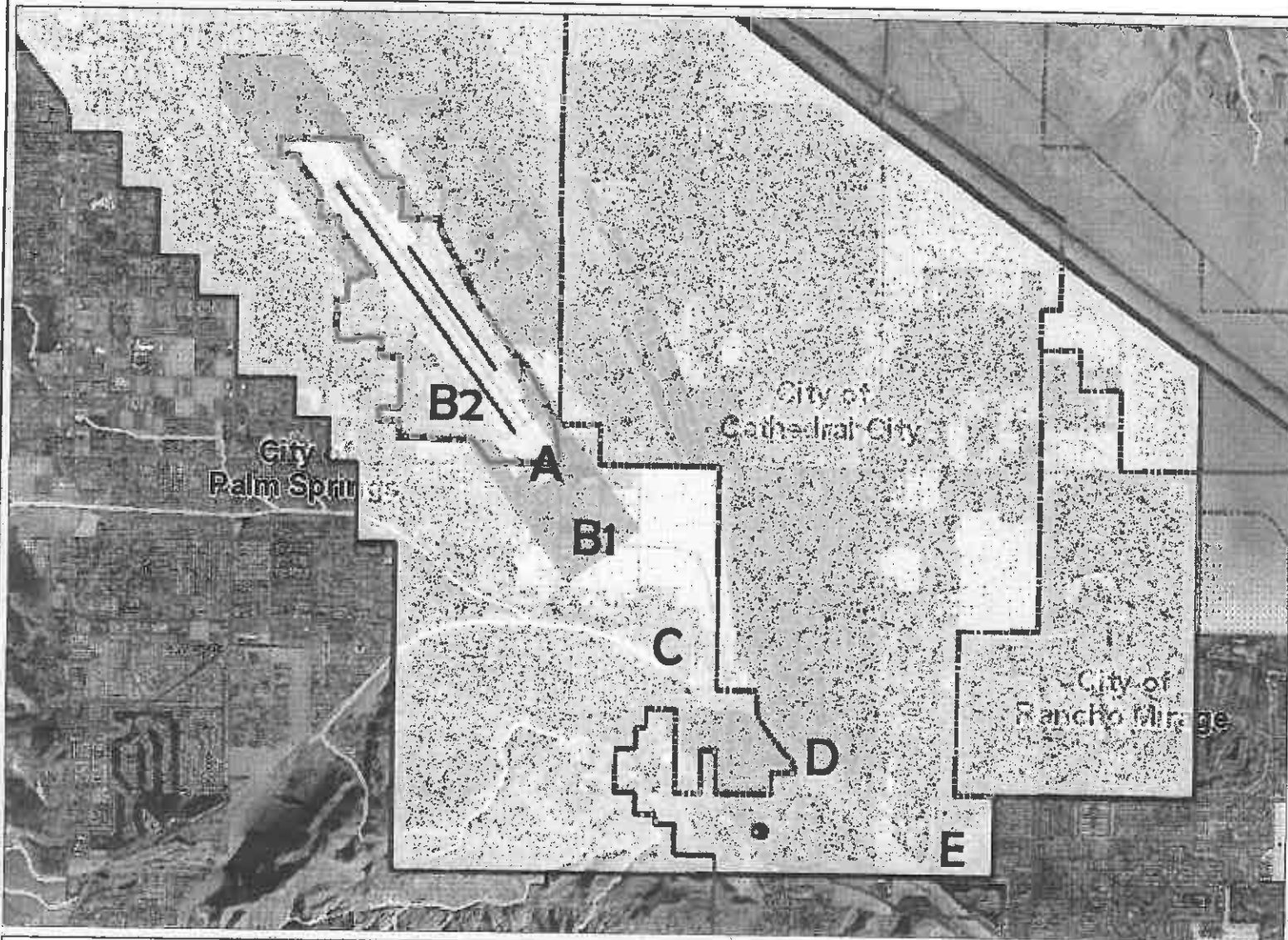
Riverside County
 Airport Land Use Commission
 Riverside County
 Airport Land Use Compatibility Plan
 Policy Document
 (Adopted March 2005)

Map PS-1

Compatibility Map
 Palm Springs International Airport

PS-1-comma.tiff.tty

Map My County Map



Legend

- Runways
- ◻ Airports
- ◻ Airport Influence Areas
- Airport Compatibility Zones
- ◻ OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC8



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Notes

Map My County Map



Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5



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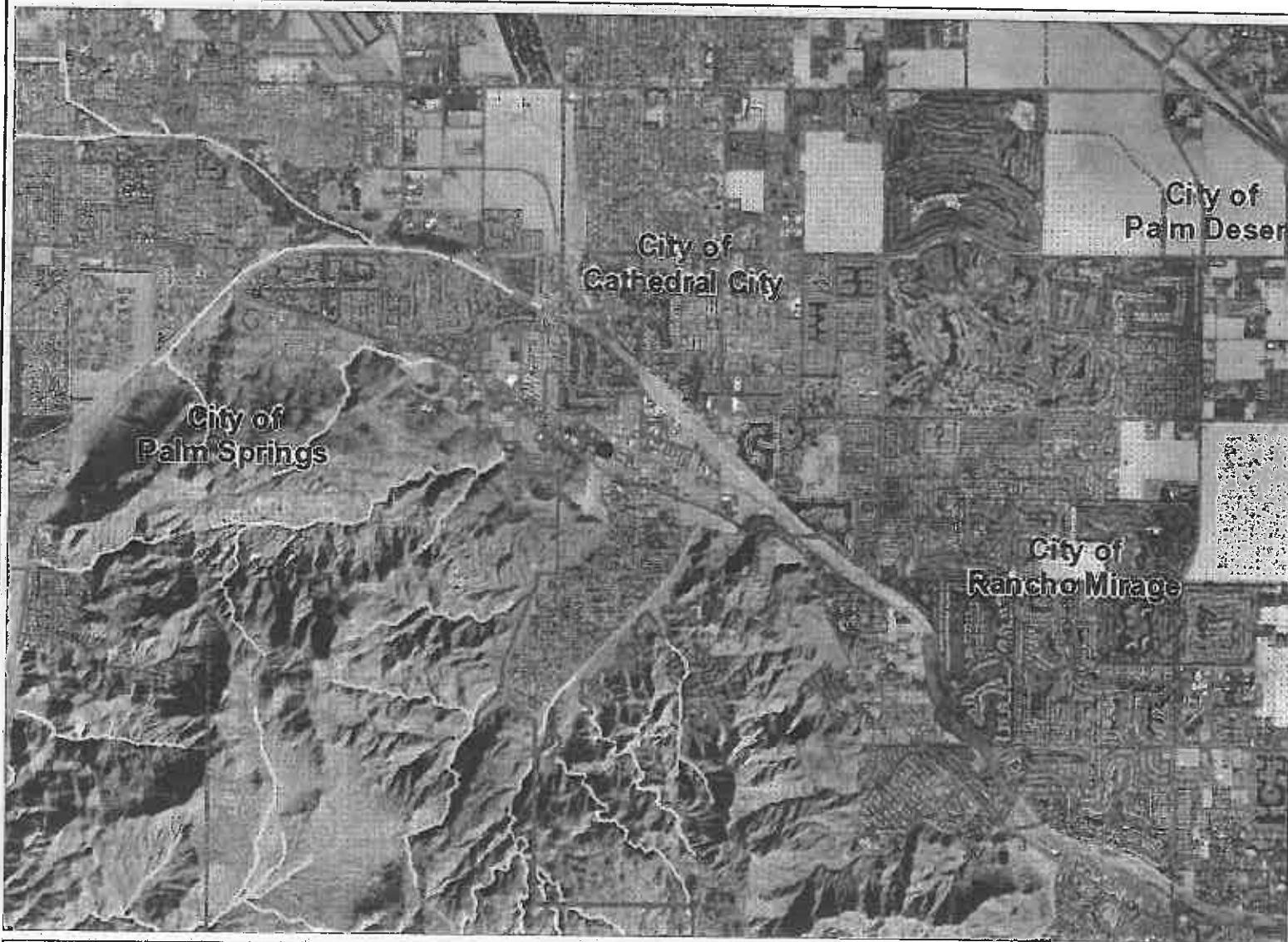
Notes

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Map My County Map



- Legend**
- Blueline Streams
 - ▨ City Areas
 - World Street Map



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Notes

Map My County Map



Legend

- Blueline Streams
- City Areas
- World Street Map



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516

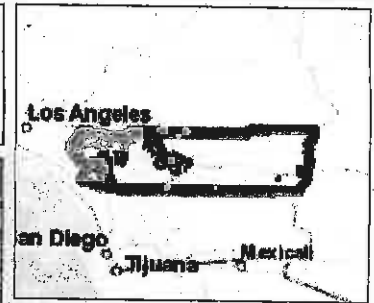
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Notes

Map My County Map



Legend

- Parcels
- Blue line Streams
- City Areas
- World Street Map



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0 379 758 Feet

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Notes



PROJECT DESCRIPTION

Project Name: Bankside Cultivation Facility
Project Address: 36555 Bankside Drive, Cathedral City, CA
Owner Name: Mayan Properties
APN: 687-311-027
Zone: D

Detailed Description:

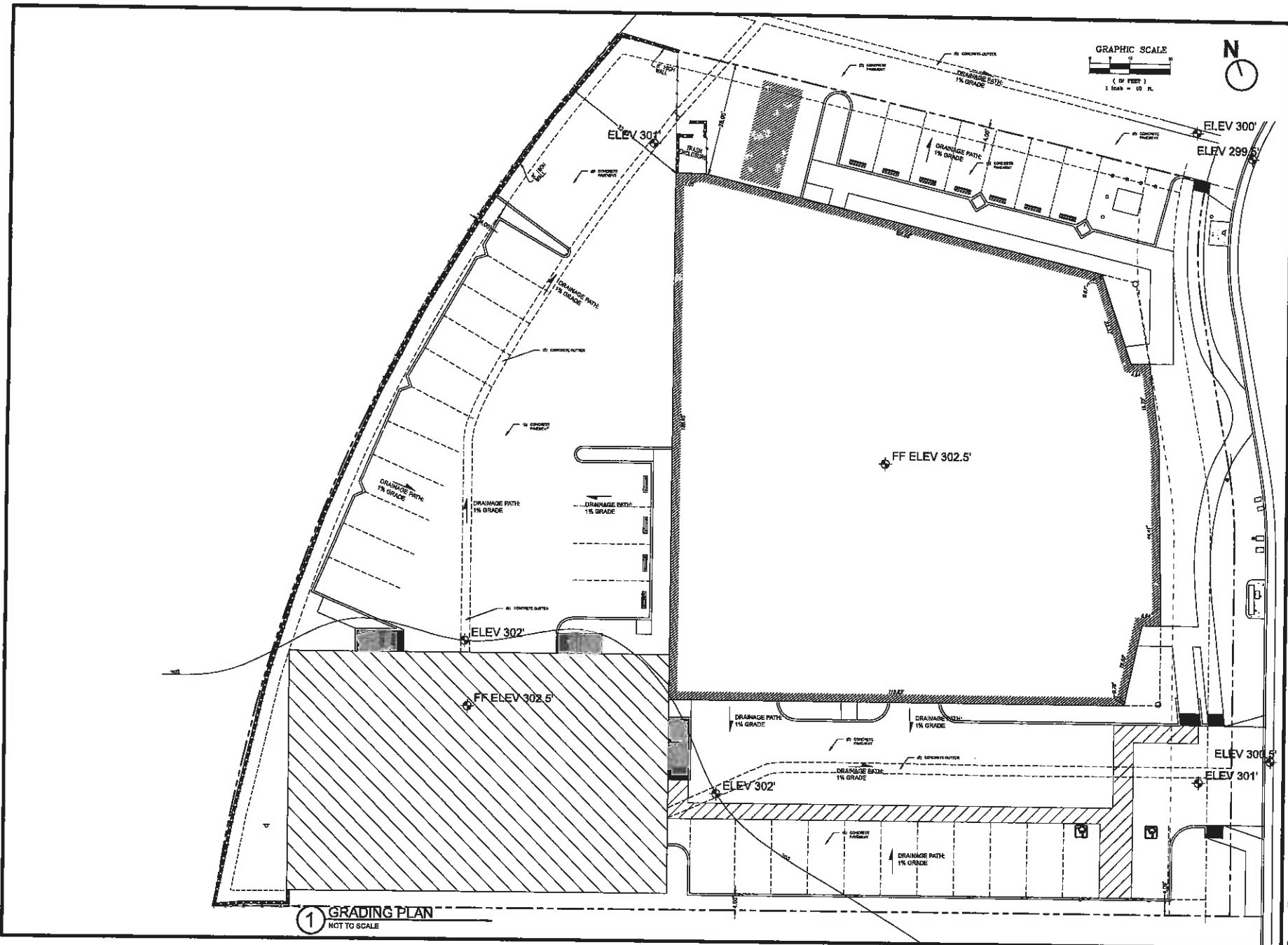
The owner proposes to construct a new 10,834 SF, 2-story commercial building on this commercial parcel zoned CBP-2 (Airport Zone D) in Cathedral City. The parcel currently has a 13,648 SF single story commercial building with a height of 18'. The existing building houses an approved cannabis cultivation business. The proposed additional building will abut the existing building but will have separate utilities for an independent tenant from the existing building. The proposed use of the new building is also cannabis cultivation. There is no distribution (public access) or extraction proposed at this site.

The proposed building is 10,834 SF divided equally over 2 stories, with a height of 35'-6". Roof top HVAC units will extend a maximum of 2' above the top of parapet wall.

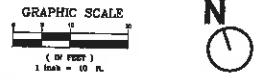
The existing building is a tilt-up concrete construction, V-B with fire sprinklers.

The proposed building is CMU and steel trusses, V-B with fire sprinklers.

There are no proposed elements that would cause any electrical interference, confusing lights, glare, smoke, or other hazards to aircraft.



1 GRADING PLAN
NOT TO SCALE



Architectural Design
2208 North 14th Street
Palo Alto, CA 94301
P: 310.893.4302
C: 310.893.4303
W: hundredmilehouse.com
Dustin Smith, Principal Designer

General Note
NOTE: Designer for Design Intent
Professional (P.E.) is not responsible
for the engineering, construction,
or installation of any equipment for all related
tasks including on the part of all
volunteers and other parties to be
submitted to HHS for final review
approval, or shall comply prior to
construction.

**BANKSIDE
CULTIVATION
FACILITY**

3655 BANKSIDE DR.
CATHEDRAL CITY, CA
92234
887-311-0277
MAYN PROPERTIES LLC
C/O STEVE HIGLEY
47742 LINDERO CYN RD. #403
WESTLAKE VILLAGE, CA 91382
310.893.5007
info@mayn.com

Date: 5/27/2019
Scale: AS SHOWN
Sheet: 101 of 101

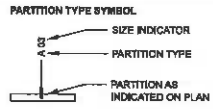
GRADING PLAN

Sheet:
B
5/27/2019
AS SHOWN

L104

LEGEND

- 1 HR RATED WALL ASSEMBLY (102.2)
- 1 HR RATED CMU WALL ASSEMBLY (102.2)
- NON-RATED INTERIOR WALL ASSEMBLY



PARTITION TYPE DESCRIPTION

CODE	DESCRIPTION
A	TYPE A - FULL HEIGHT NON-RATED PARTITION WALL; SEE DETAIL 23A/81
B	TYPE B - PARTIAL HEIGHT PARTITION WALL; SEE DETAIL 19A/01
C	TYPE C - FULL HEIGHT CONCRETE WALL; SEE DETAIL 47A/81
F	TYPE F - FULL HEIGHT 1-HR. RATED WALL; SEE DETAIL 39A/01
G	TYPE G - PARTIAL HEIGHT 1-HR. RATED TUNNEL CONSTRUCTION; SEE DETAIL 23A/55

STUD SIZES INDICATOR

INDICATOR	DESCRIPTION
1	1" METAL
2	2" METAL
3	3" METAL
4	4" METAL
6	6" METAL
7	2X8 WOOD

CMU SIZES INDICATOR

INDICATOR	DESCRIPTION
6	6X8X16" CMU BLOCK
8	8X8X16" CMU BLOCK
12	12X8X16" CMU BLOCK

SYMBOL LEGEND

	WINDOW NUMBER
	DOOR NUMBER
	KEY NOTE NUMBER
	SECTION NUMBER / SHEET NUMBER
	SPOT ELEVATION
	COLUMN LINE
	DETAIL NUMBER / SHEET NUMBER

CONSTRUCTION KEY NOTES

- 1 SURFACE MOUNT ELECTRIC PANELS, 3 PHASE
- 2 WATER FILTRATION / TREATMENT EQUIPMENT
- 3 NEVER DRINK TAP WATER; HOT WATER HEATER
- 4 ELEVATOR SHAFT OFF
- 5 HYDRAULIC ELEVATOR EQUIPMENT
- 6 1500 GPM FIBERGLASS WATER TANK
- 7 10" x 18" INSULATED HOLLOW PIPE DOOR
- 8 EQUIPMENT RACK
- 9 MICROCLIMATE
- 10 ICE REF
- 11 FREIGHT ELEVATOR; SEE PAGE A303
- 12 ACCESSIBLE RAMP; SEE A303 FOR DETAILS
- 13 CONCRETE WALKWAY
- 14 LANDSCAPED AREA
- 15 WATER PUMP
- 16 WALL UNDER URINAL
- 17 SECOND FLOOR ACCESS HATCH; WATER TIGHT
- 18 FLOOR DRAIN; SURROUNDING FLR TO SLOPE MIN 1% TO DRAIN
- 19 HOSE BIB
- 20 FUTURE WALK-IN REEF
- 21 CONCRETE DEWEAVY SURFACE
- 22 EXISTING 8" HIGH CMU WALL
- 23 COLUMN; SEE STRUCTURAL PLANS



Architectural Design

3800 Rock Vista Grande Ave
Palo Alto, CA 94304
P: 310.391.4300
E: hhouse@hundredmilehouse.com
V: hhouse@hundredmilehouse.com
Diana Smith, Principal Designer

General Notes
NOTES: Designer has designed based on information provided. Designer is not responsible for the information, construction, or installation of materials for all items not indicated on the drawings. All engineering and design changes to be indicated on the drawings. No field changes, omissions, or modifications shall be made without the written consent of the Designer.

PROJECT NAME
BANKSIDE CULTIVATION FACILITY

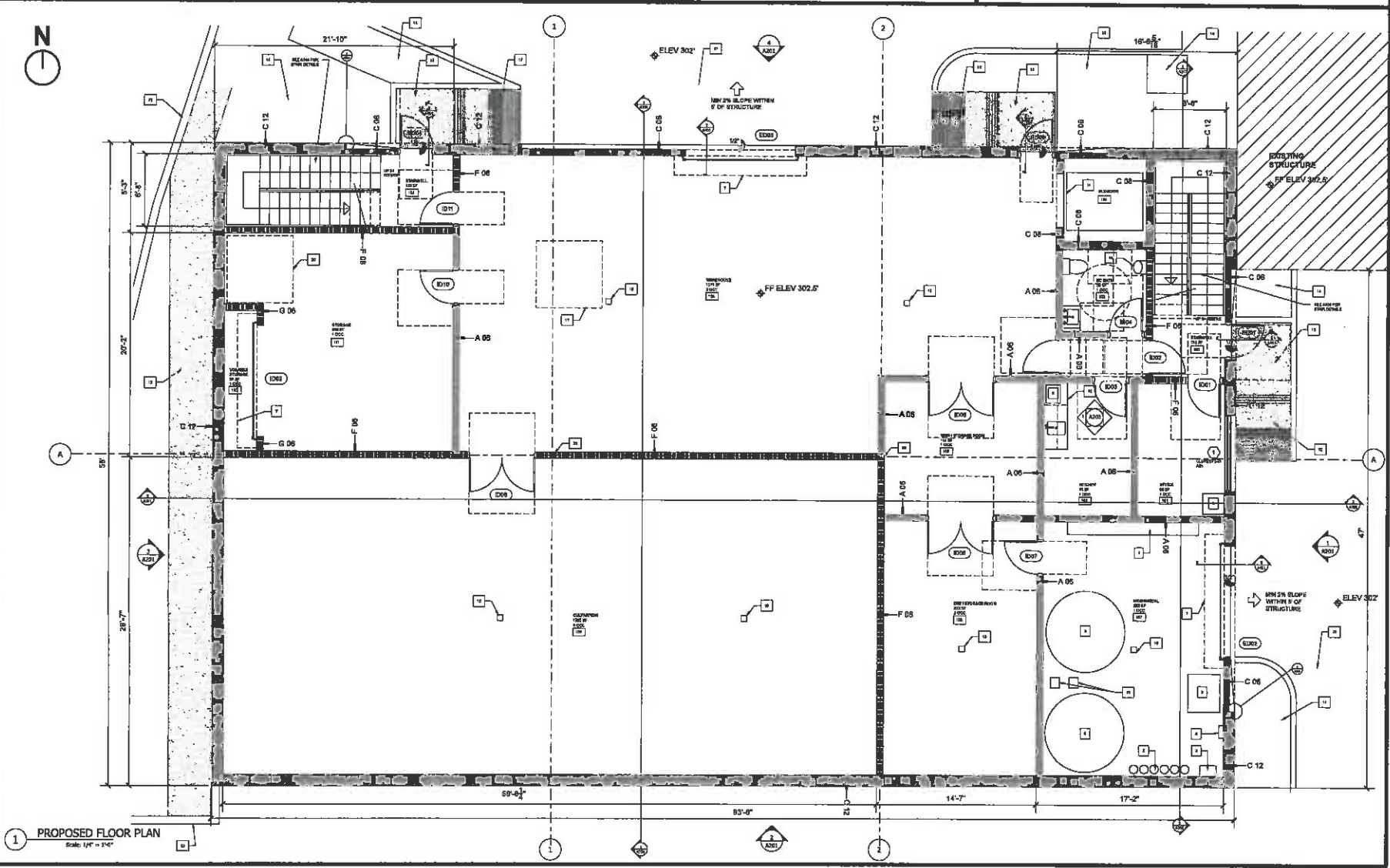
1875 Mission
58555 BANKSIDE DR.
CATHLAMET, OR, 97147
PH: 503-311-0277
WWW: BANKSIDEPROPERTIES.LLC
C/O: 818 W. AGUIRRE AVE.
STOCKTON, CA 95210
PH: 209-953-2801
BANKSIDE@GMAIL.COM

ARCHITECT
MEDICAL HEALTH LLC
5775 O LINDERO CYN RD, #403
WESTLAKE VILLAGE, CA 91362
PH: 818-362-8901
engineer@medhealth.com

Date: 5/27/2019
Revision: 01
Author: [Name]
Checker: [Name]

PROPOSED 1ST FLOOR PLAN
Sheet B
Date: 5/27/2019
AS SHOWN

A101



1 PROPOSED FLOOR PLAN
Scale: 1/4" = 1'-0"

General Notes
 NOTE: Drawings for Design Intent
 Foundation: 2017-18 ACC (2018/2019)
 For the engineering, construction,
 permitting or installation of all related
 trades including on this project. All
 engineering and final drawings to be
 submitted to PMEA for final review
 comments. If discrepancies prior to
 construction.

BANKSIDE CULTIVATION FACILITY

38555 BANKSIDE DR.
 CATHEDRAL CITY, CA
 887-311-027

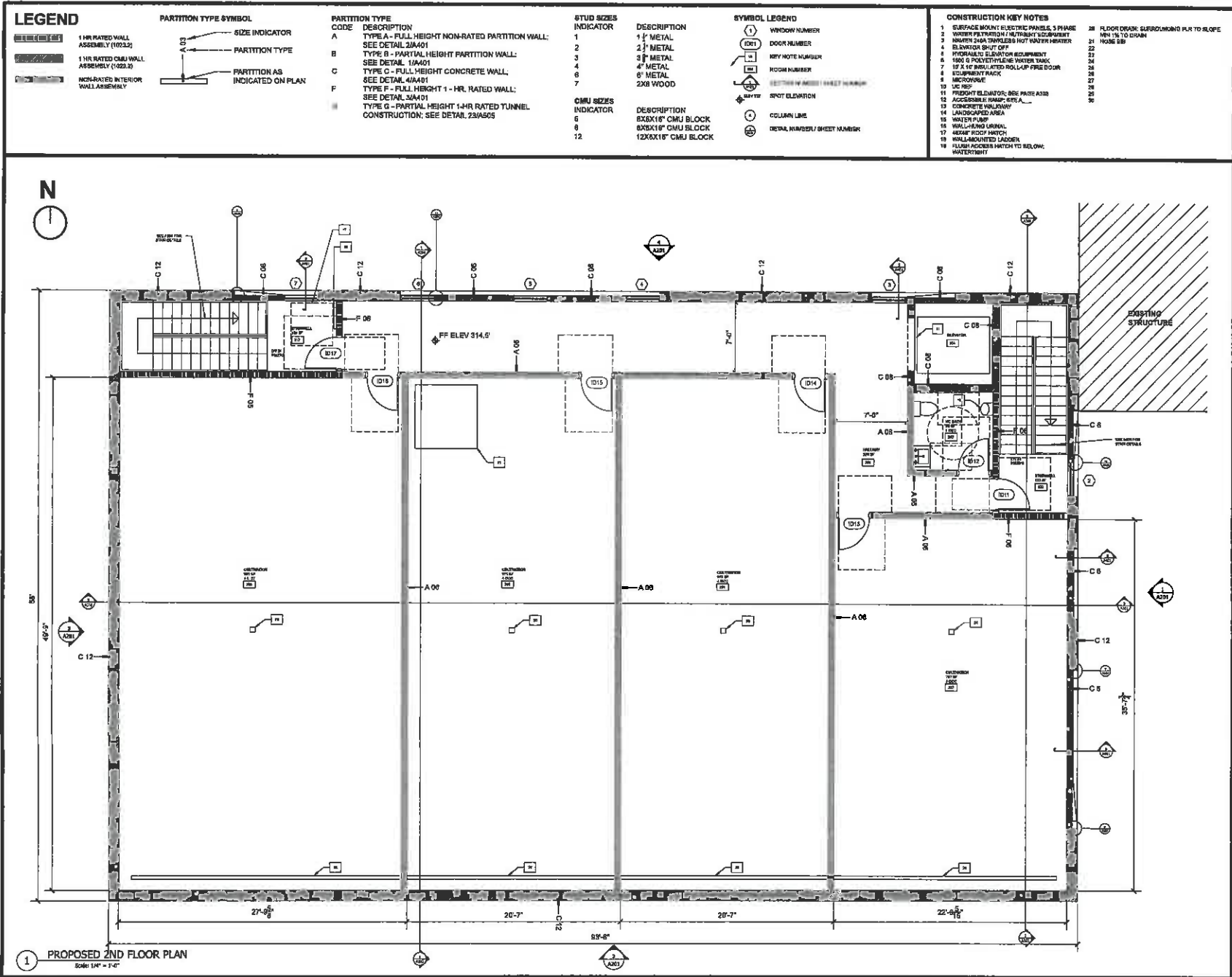
OWNER: BANKSIDE PROPERTIES LLC
 DESIGNER: MEDICAL HEALTH CARE LLC
 37750 LINDERO CIRCLE, SUITE 100
 WESTLAKE VILLAGE, CA 91382
 310-383-3901
 info@medhealthcare.com

DATE: 5/27/2019
 DRAWN BY: AS
 CHECKED BY: AS

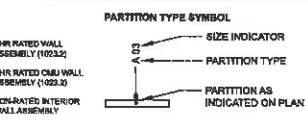
Date: 5/27/2019
 Drawn: AS
 Checked: AS

PROPOSED 2ND FLOOR PLAN
 SHEET B
 DATE: 5/27/2019
 DRAWN BY: AS

A102



LEGEND



PARTITION TYPE

CODE	DESCRIPTION
A	TYPE A - FULL HEIGHT NON-RATED PARTITION WALL: SEE DETAIL 23A401
B	TYPE B - PARTIAL HEIGHT PARTITION WALL: SEE DETAIL 19A401
C	TYPE C - FULL HEIGHT CONCRETE WALL: SEE DETAIL 44A401
F	TYPE F - FULL HEIGHT 1-HR. RATED WALL: SEE DETAIL 34A401
G	TYPE G - PARTIAL HEIGHT 1-HR. RATED TUNNEL CONSTRUCTION: SEE DETAIL 23A505

STUD SIZES

INDICATOR	DESCRIPTION
1	1 1/2" METAL
2	2" METAL
3	3" METAL
4	4" METAL
6	6" METAL
7	2X8 WOOD

CMU SIZES

INDICATOR	DESCRIPTION
6	8X8X16" CMU BLOCK
8	8X8X16" CMU BLOCK
12	12X8X16" CMU BLOCK

SYMBOL LEGEND

- WINDOW NUMBER
- DOOR NUMBER
- KEY NOTE NUMBER
- ROOM NUMBER
- SECTION NUMBER SHEET NUMBER
- SPOT ELEVATION
- COLUMN LINE
- DETAIL NUMBER SHEET NUMBER

CONSTRUCTION KEY NOTES

- 1 SURFACE MOUNT ELECTRIC PANELS, 3 PHASE
- 2 WATER FILTRATION / WATER TREATMENT
- 3 WATER TANK (WATERLESS HOT WATER HEATER)
- 4 ELEVATOR SHUT OFF
- 5 HYDRAULIC ELEVATOR EQUIPMENT
- 6 180 G POCKET STYLE WATER TANK
- 7 19" X 14" INSULATED ROLL-UP FIRE DOOR
- 8 EQUIPMENT RACK
- 9 MICROVAPE
- 10 LUG RIG
- 11 FREIGHT ELEVATOR: SEE PART A338
- 12 ACCESSIBLE RAMP: SEE A...
- 13 CONCRETE WALKWAY
- 14 LANDSCAPED AREA
- 15 WATER PUMP
- 16 WALL-FUNG URINAL
- 17 48564' ROOF PATCH
- 18 WALL-MOUNTED LADDER
- 19 FLOOR ACCESS PATCH TO BELOW: HOISTWAY

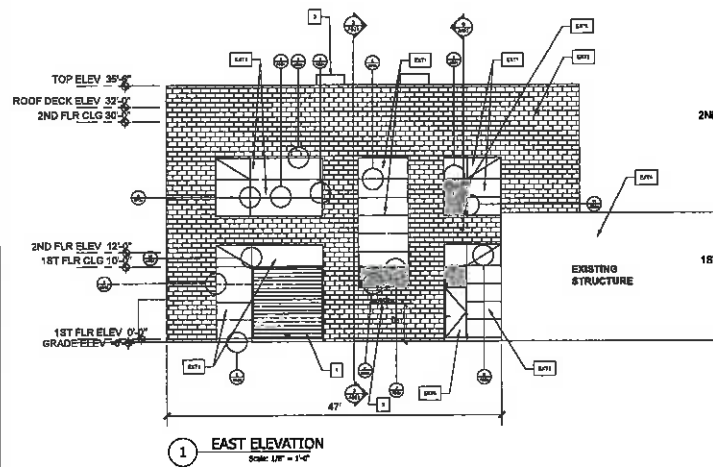
PROPOSED 2ND FLOOR PLAN
 Scale: 1/4" = 1'-0"

FINISH SCHEDULE

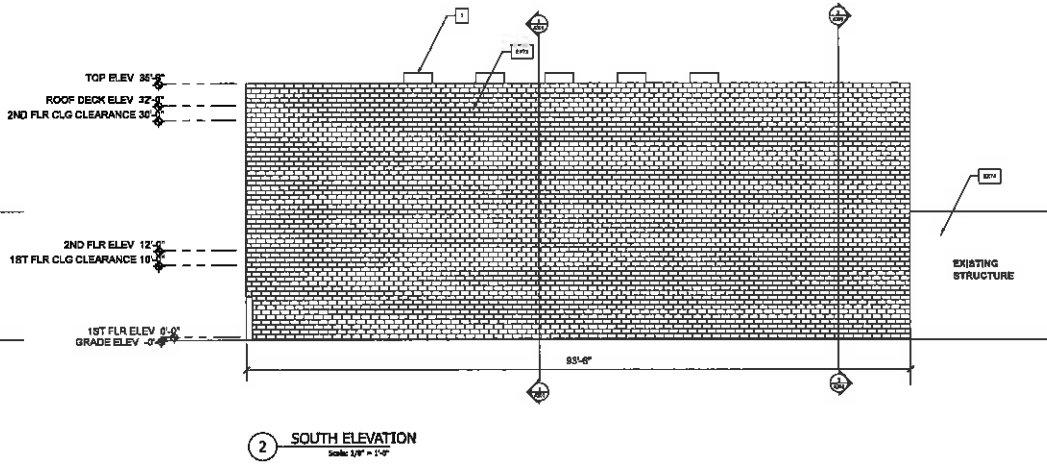
- EXT 1: METAL CLADDING PANEL
FINISH: WILSON JERUPSON GAL MATTE BLACK
SEE AIAA FOR METAL PANEL SPECIFICATIONS AND DETAILS
- EXT 2: ARCHITECTURAL CHALKGELAS PRECISION BLOCK
NATURAL GRAY SHOT BLAST FINISH
- EXT 4: PAINTED FINISH, DUNEDENWOODS POROUS STONE DESIGN, LOW GREEN
- EXT 5: BLACK ANODIZED ALUMINUM WINDOW FRAMES
- EXT 6: PAINTED FINISH TO MATCH WILSON JERUPSON GAL MATTE BLACK

CONSTRUCTION KEY NOTES

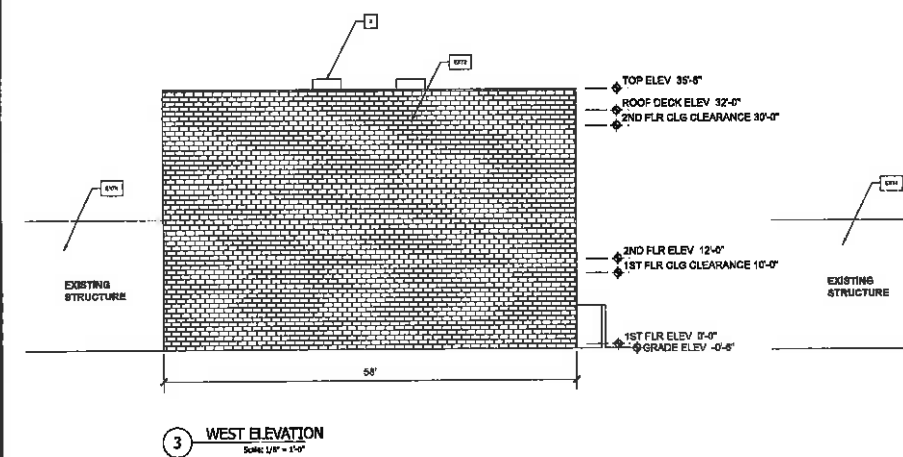
- 1 12 X 16' INSULATED ROLL-UP FIRE DOOR; PAINTED TO MATCH EXT 6
- 2 BUILDING ADDRESS
- 3 MATERIAL: STUCCO/MOUNTED ALUMINUM NUMBER, BLACK SATIN FINISH
- 4 POINT MECHANICAL, 1" HUB
- 5 DEAR ILLUMINATED NO. 2000 LED LIGHT STRIPS
- 6 VENDOR: WWW.BIPACTORIAL.COM
- 7 HANG RTU; SEE A301 FOR LIGHT LINES FROM STREET



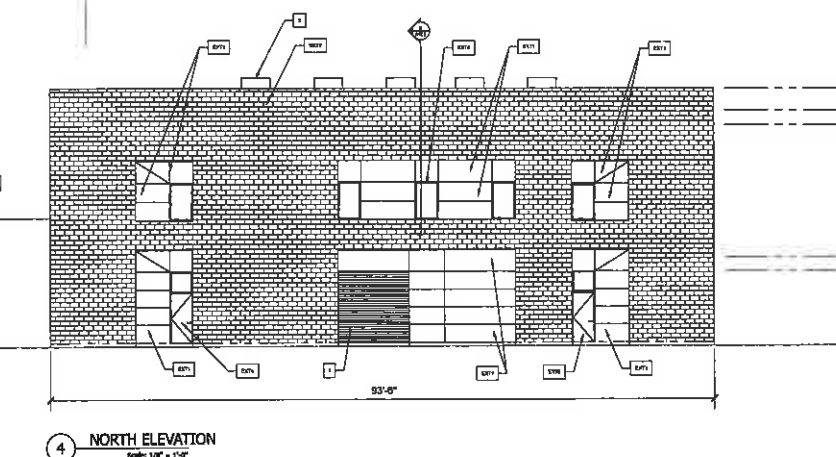
1 EAST ELEVATION
Scale: 1/8" = 1'-0"



2 SOUTH ELEVATION
Scale: 1/8" = 1'-0"



3 WEST ELEVATION
Scale: 1/8" = 1'-0"



4 NORTH ELEVATION
Scale: 1/8" = 1'-0"



HUNDRED MILE HOUSE
Architectural Design
2100 North Vista Grande Ave
Palo Alto, CA 94303
P: 310.991.4302
F: 310.991.4302
W: hundredmilehouse.com
Danae Bell, Principal Designer

GENERAL NOTES
NOTE: Drawings for Design Build
Contract. CHS shall be responsible
for the alignment, construction,
completion or installation of all related
items including on the project. All
engineering and other drawings to be
submitted to CHS for final review,
consistency, and coordination prior to
construction.

BANKSIDE CULTIVATION FACILITY

3855 BANKSIDE DR.
CATHEDRAL CITY, CA 91102
PH: 311-027
OWNER: MURRAY PROPERTIES LLC
OWNER ADDRESS:
C/O STEVE AGUILAR
5778 O LINDEN RD, #410
WESTLAKE VILLAGE, CA 91362
310-383-9001
c.aguilarmuray@gmail.com
ARCHITECT:
MEDICINAL HEALING LLC
5778 O LINDEN RD, #410
WESTLAKE VILLAGE, CA 91362
310-383-9001
c.aguilarmuray@gmail.com

Date: 5/27/2019
User: SLP/AM/UCM
User: SLP/AM/UCM

PROPOSED ELEVATIONS

Sheet
B
Date
5/27/2019
User
AS SHOWN

A201

PAGE BREAK





AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY

July 18, 2019

CHAIR
Steve Manos
Lake Elsinore

VICE CHAIR
Russell Betts
Desert Hot Springs

Ms. Bahellia Boothe, Urban Regional Planner IV
County of Riverside Planning Department
4080 Lemon Street, 9th Floor
Riverside CA 92501
(VIA HAND DELIVERY)

COMMISSIONERS

Arthur Butler
Riverside

John Lyon
Riverside

Steven Stewart
Palm Springs

Richard Stewart
Moreno Valley

Gary Youmans
Temecula

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW –
DIRECTOR’S DETERMINATION**

File No.: ZAP1374MA19
Related File No.: PPA180065 (Plot Plan)
APN: 280-250-015

Dear Ms. Boothe:

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to Policy 1.5.2(d) of the Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan, staff reviewed County of Riverside Case No. PPA180065 (Plot Plan), a proposal to construct three 3,500 square foot pre-fabricated buildings for use as an indoor bird aviary on a 1.96-acre lot, located westerly of Wood Road, easterly of Prairie Way, northerly of Van Buren Boulevard, and southerly of Orrick Road.

The site is located within Airport Compatibility Zone D of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone D of the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, non-residential intensity is not restricted.

The elevation of Runway 14-32 at March Air Reserve Base/Inland Port Airport at its northerly terminus is approximately 1,535 feet above mean sea level (AMSL). At a distance of 18,100 feet from the runway to the project, Federal Aviation Administration Obstruction Evaluation Services (FAA OES) review could be required for any structures with a top of roof exceeding 1,716 feet AMSL. The site’s elevation is 1,624 feet AMSL, and the proposed maximum building height is 26 feet, resulting in a top point elevation of 1,650 feet AMSL. Therefore, FAA OES review for height/elevation reasons is not required.

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, provided that the County of Riverside applies the following recommended conditions:

STAFF

Director
Simon A. Housman

John Guerin
Paul Rull
Barbara Santos

County Administrative Center
4080 Lemon St., 14th Floor
Riverside, CA 92501
(951) 955-5132

www.rcaluc.org

AIRPORT LAND USE COMMISSION

CONDITIONS:

1. Any new outdoor lighting installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site, in accordance with Note 1 on Table 4 of the Lake Mathews/Woodcrest Area Plan:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area.
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
3. The following uses/activities are specifically prohibited: wastewater management facilities; trash transfer stations that are open on one or more sides; recycling centers containing putrescible wastes; incinerators.
4. The attached notice shall be provided to all potential purchasers of the property and tenants/lessees of the buildings thereon, and shall be recorded as a deed notice.
5. Any new aboveground detention or water quality basins on the site shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention/water quality basin(s) that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.
6. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment of irrigation controllers, access gates, etc.

If you have any questions, please contact Paul Rull, ALUC Principal Planner, at (951) 955-6893.

AIRPORT LAND USE COMMISSION

Sincerely,
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Simon A. Housman, ALUC Director

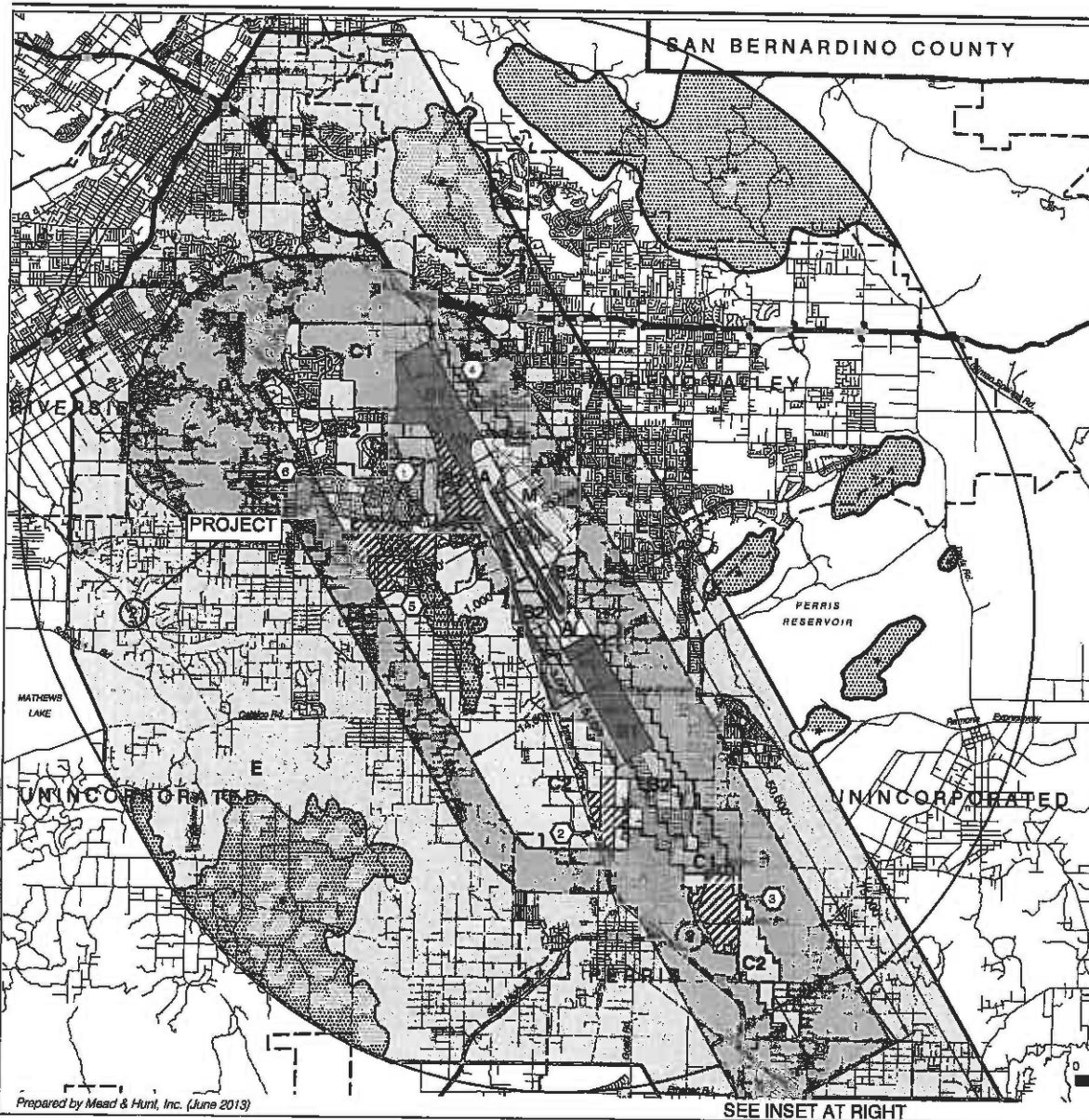
Attachments: Notice of Airport in Vicinity

cc: Maggie Wang/International Pet & Supply (applicants/landowner)
Itra Group., Inc., Roger Su (representative)
Margaret Wang (Rowland Heights)
Gary Gosliga, Airport Manager, March Inland Port Airport Authority
Daniel "Rock" Rockholt, March Air Reserve Base
ALUC Case File

Y:\AIRPORT CASE FILES\March\ZAP1374MA19\ZAP1374MA19.LTR.doc

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



LEGEND

Compatibility Zones

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

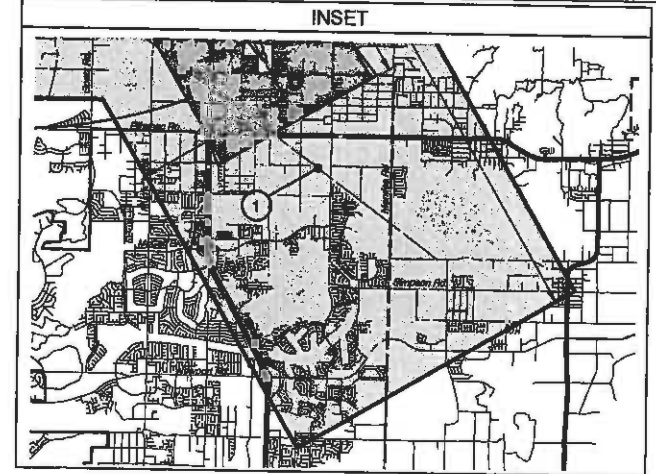
Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- ▨ Site-Specific Exceptions (existing local agency commitments to development projects)

- ① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.

- ① March JPA: March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA: Ben Clark Training Center
- ⑥ Riverside: Ridge Crest Subdivision

INSET



Riverside County
 Airport Land Use Commission
 March Air Reserve Base / Inland Port Airport
 Land Use Compatibility Plan
 (Adopted November 13, 2014)

Note:
 All dimensions are measured from
 runway ends and centerlines.



Base map source: County of Riverside 2013

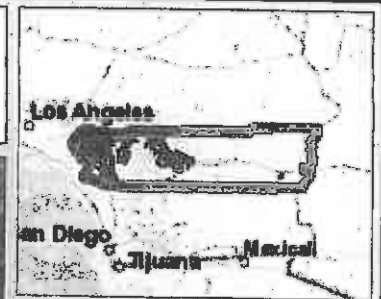
Prepared by Mead & Hunt, Inc. (June 2013)

SEE INSET AT RIGHT

Map MA-1

Compatibility Map
 March Air Reserve Base / Inland Port Airport

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones
- ▨ OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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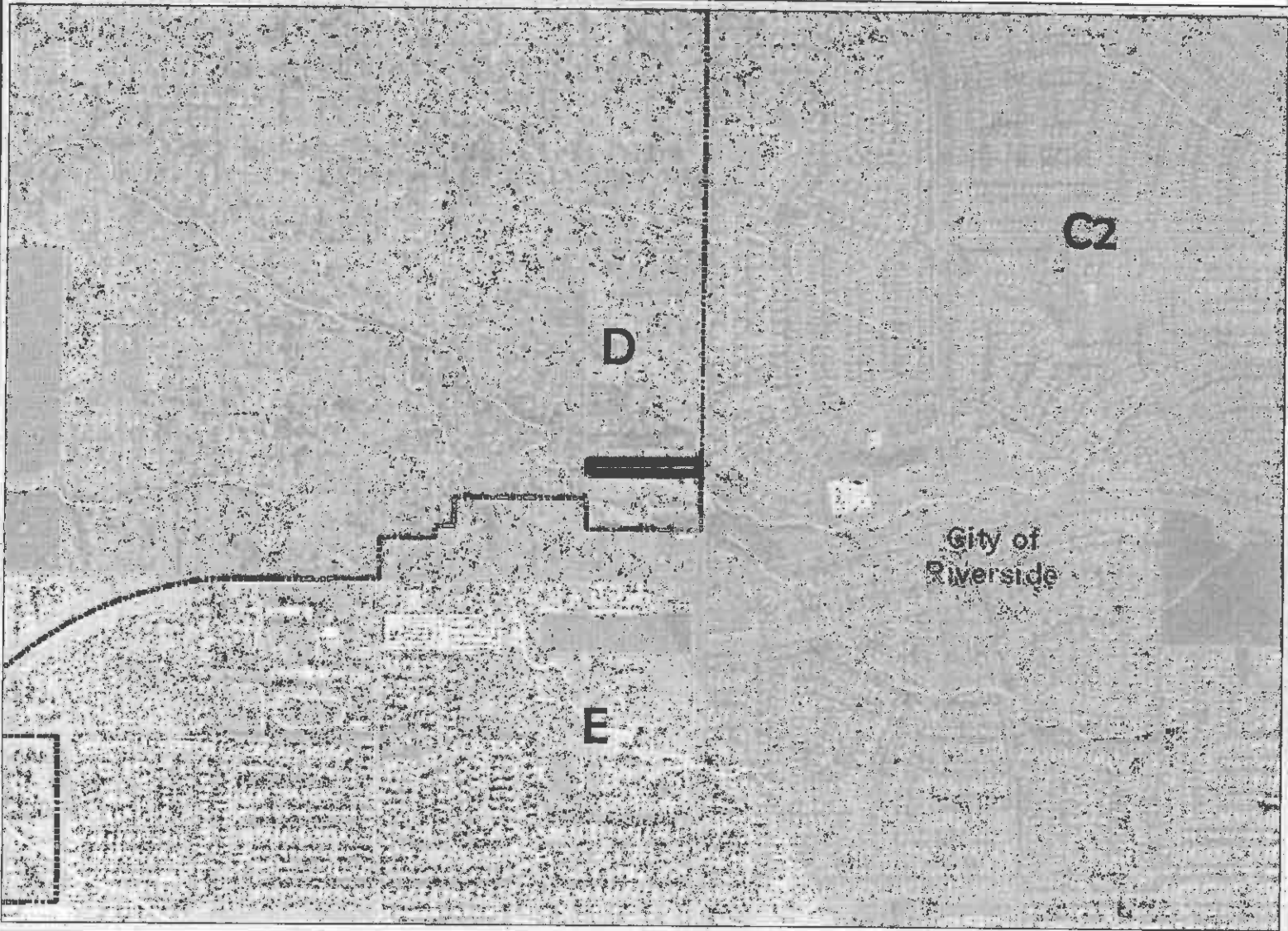
Notes



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Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- ▨ OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6

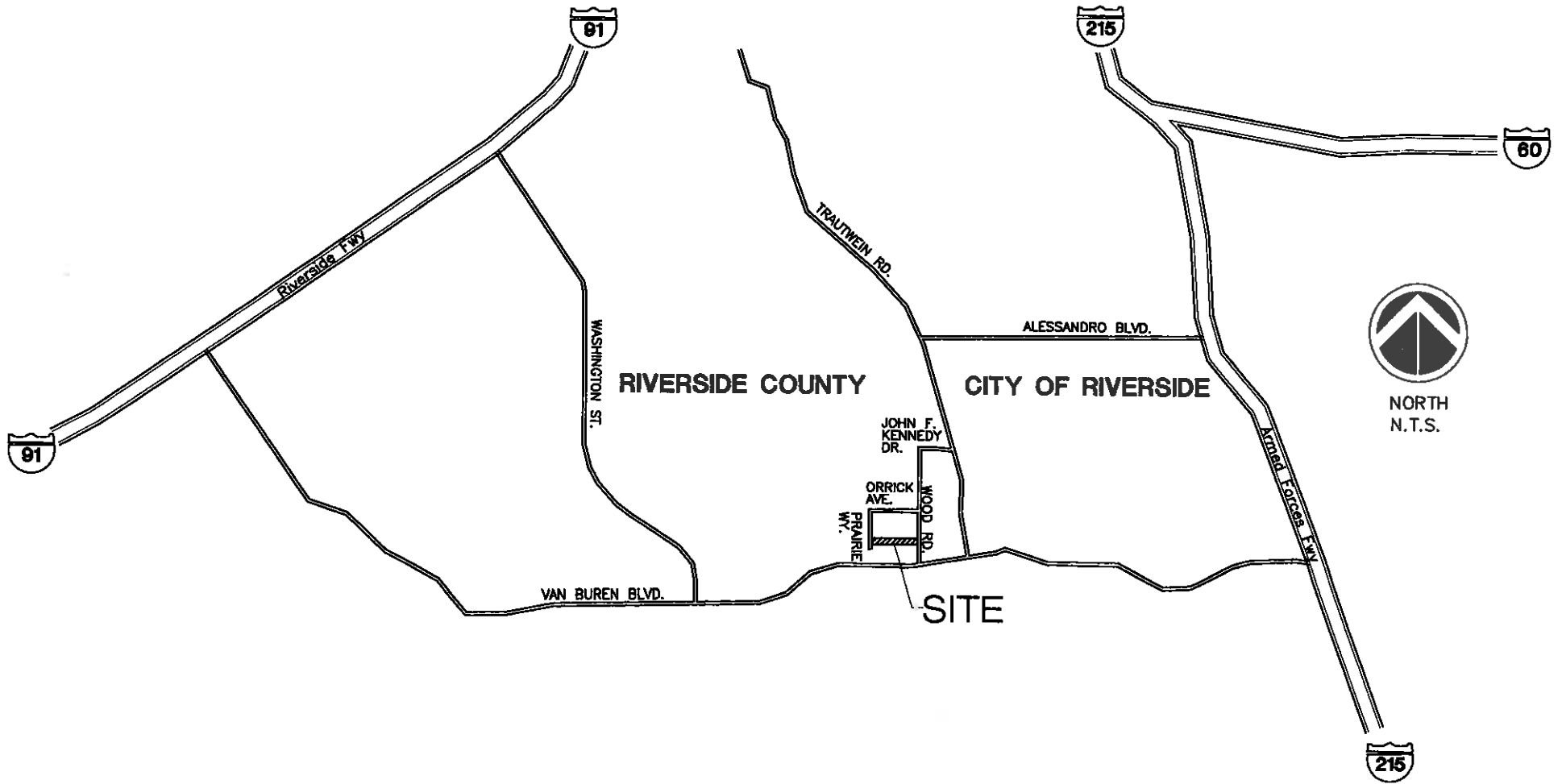


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Notes



SITE VICINITY MAP



Map My County Map



Legend

-  City Areas
-  World Street Map

Notes



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Map My County Map



Legend

- Blueline Streams
- City Areas
- World Street Map



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Notes

Map My County Map



Legend

- Blueline Streams
- ▣ City Areas
- World Street Map



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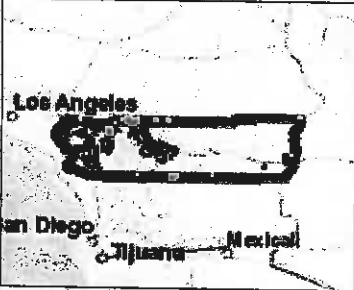
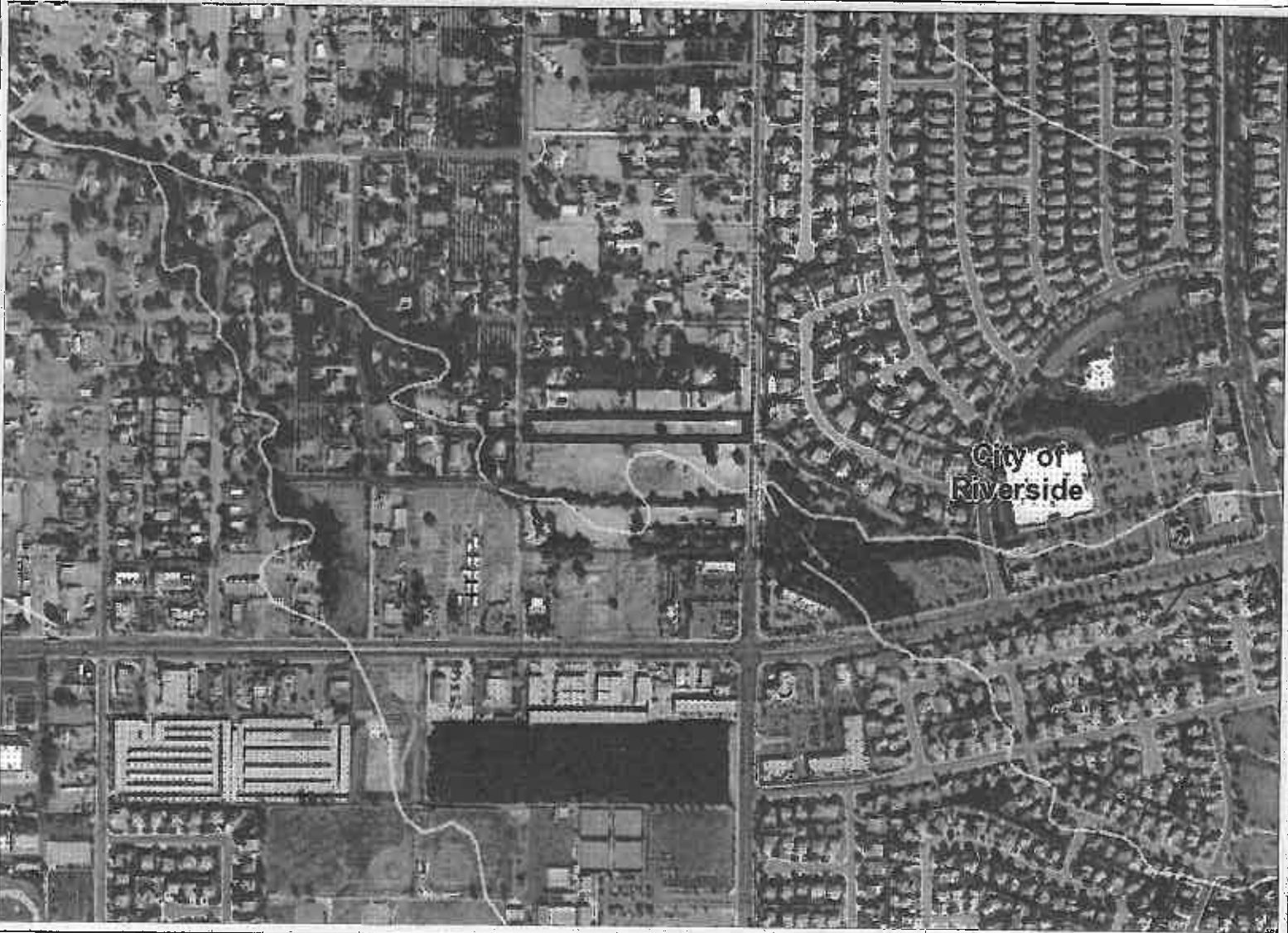
Notes

0 1 3,032 Feet
516

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Map My County Map



Legend

- Blueline Streams
- ▣ City Areas
- World Street Map



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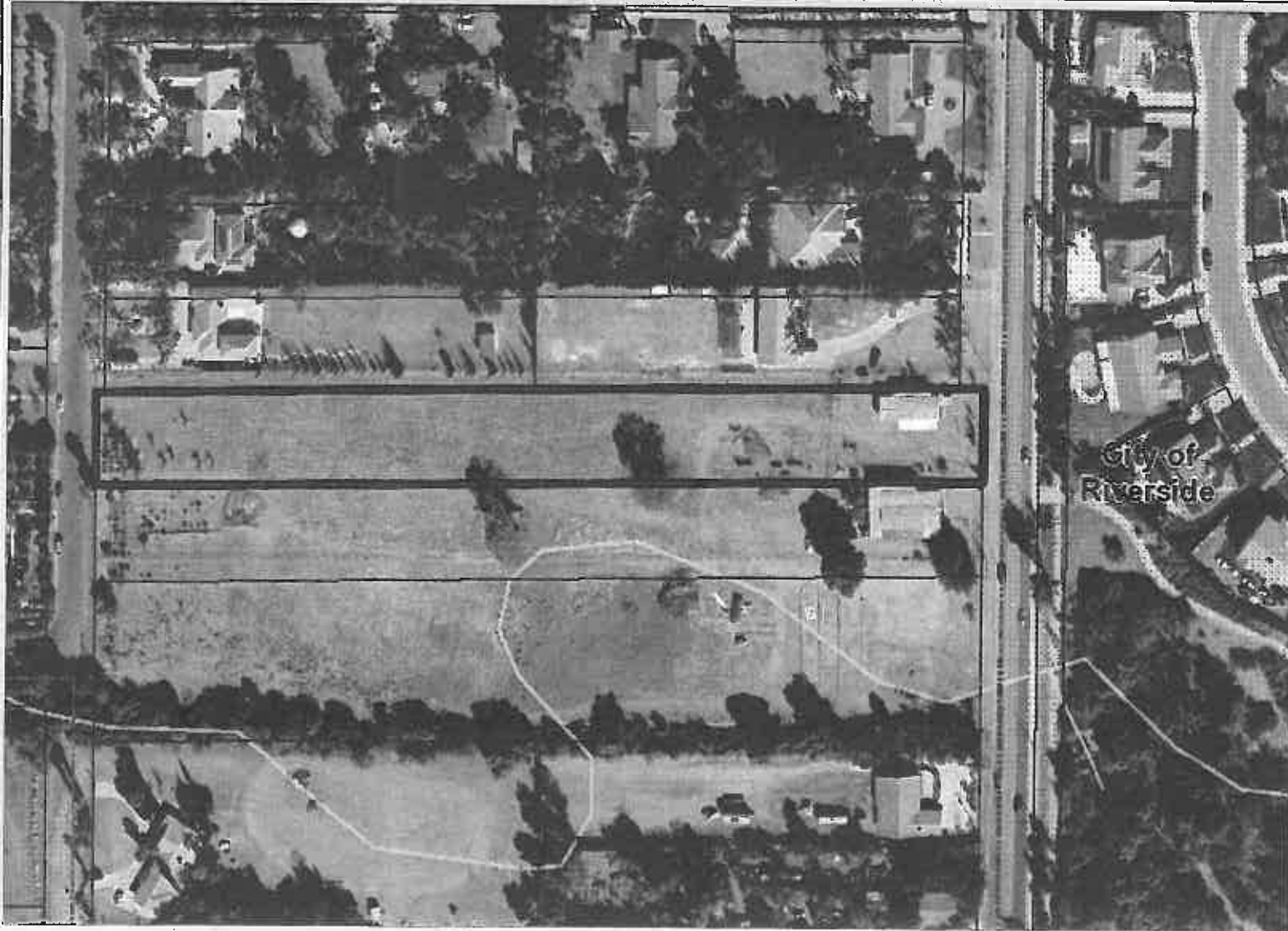
0 758 1,516 Feet

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Notes

Map My County Map



Legend

-  Parcels
-  Blueline Streams
-  City Areas
-  World Street Map



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0 189 379 Feet

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Notes

MINOR PLOT PLAN FOR PROPOSED AVIARIES

15825 WOOD ROAD, RIVERSIDE, CA 92508

PROJECT DESCRIPTION

PROPOSE TO ERECT THREE 3,500 SQ. FT. PRE-FABRICATED AVIARIES.

LEGAL DESCRIPTION

LOT 10 OF OLIVE GROVE PARK NO. 2, COUNTY OF RIVERSIDE, T3S, R4W, SECTION 19, AS PER MAP RECORDED IN BOOK 12, PAGE(S) 80, OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

EXCEPTING THE SOUTH 84.5 FEET THEREOF.

SITE INFORMATION

SITE LOCATION:
15825 WOOD ROAD
RIVERSIDE, CA 92508

OWNER/APPLICANT:
MAGGIE WANG
2550 ROSEMEAD BLVD.
SOUTH EL MONTE, CA 91733
928-258-7175 TEL
EMAIL: interpot@aol.com

ASSESSOR'S PARCEL #:
280-250-015

ZONING
EXISTING: A-1-1; W-2-M-1
PROPOSED: A-1-1; W-2-M-1

SITE
LOT GROSS AREA : 1.98 ACRE (85,377 SQ. FT.)
LOT NET AREA : 1.88 ACRE (82,542 SQ. FT.)

BUILDING AREA
A. EXISTING MOBILEHOME TO REMAIN : 1,440 SQ. FT.
B. PROPOSED AVIARIES 3 X 3,500 SQ. FT. = 10,500 SQ. FT.
TOTAL : 11,940 SQ. FT.

OCCUPANCY GROUP

A. EXISTING MOBILEHOME : R-S
B. PROPOSED AVIARIES : U

TYPE OF CONSTRUCTION

A. EXISTING MOBILEHOME : V-B
B. PROPOSED AVIARIES : V-B

DRAWING INDEX

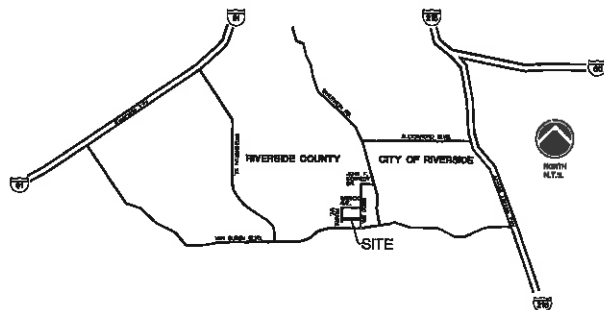
T-1.0	TITLE SHEET, PROJECT INFORMATION
T-2.0	EXEMPT FROM ACCESSIBILITY REQUIREMENTS
A-1.0	SITE PLAN
B-1.0	EXISTING MOBILEHOME ELEVATIONS
B-2.0	PROPOSED AVIARIES ELEVATIONS
C-1.0	EXISTING MOBILEHOME FLOOR PLAN
C-2.0	PROPOSED AVIARIES FLOOR PLAN
D-1.0	DRAINAGE, STREET SECTIONS
D-2.0	DRAINAGE MANAGEMENT

PROJECT TEAM

OWNER / APPLICANT
MAGGIE WANG
2550 ROSEMEAD BLVD.
SOUTH EL MONTE, CA 91733
928-258-7175 TEL
EMAIL: interpot@aol.com

EXHIBIT PREPARER
ITRA GROUP, INC.
984 CRYSTAL WATER LANE
WALNUT, CA 91789
908-524-2146 TEL
CONTACT: ROGER SU
E-MAIL: itragroup@gmail.com

SITE VICINITY MAP



DEVELOPMENT CONTACTS

PLANNING
COUNTY OF RIVERSIDE
4080 LEMON STREET
RIVERSIDE, CA 92501
951.955.6800

WATER
WESTERN MUNICIPAL W.D.
14205 MERIDIAN PARKWAY
RIVERSIDE, CA 92518
951.571.7100

SCHOOL DISTRICT
RIVERSIDE UNIFIED
3580 14th STREET
RIVERSIDE, CA 92501
951.788.7135

SEWER
WESTERN MUNICIPAL W.D.
14205 MERIDIAN PARKWAY
RIVERSIDE, CA 92518
951.571.7100

ELECTRICITY
SOUTHERN CALIFORNIA EDISON
288 KANSAS STREET
REDLANDS, CA 92373
908.274.3511

TELEPHONE
SPECTRUM
3802 RIVERSIDE PLAZA DR., #111
RIVERSIDE, CA 92508
866.874.2388

NATURAL GAS
SOUTHERN CALIFORNIA GAS CO.
7000 INDIANA AVE., #105
RIVERSIDE, CA 92506
800.427.2200

CABLE TELEVISION
SPECTRUM
3802 RIVERSIDE PLAZA DR., #111
RIVERSIDE, CA 92508
866.874.2388

PROPOSED AVIARIES

PPA180065

15825 WOOD ROAD
RIVERSIDE, CA 92508



ITRA GROUP INC.

984 Crystal Water Lane
Walnut, CA 91789

Tel: (909) 524-2148
E-mail: itragroup@gmail.com

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Revised	
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MINOR PLOT PLAN	01/30/18
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Drawn by	Checked by
S.H.	R.S.
Project No.	
161102	

DRIVING PLAN	TITLE SHEET
	PROJECT INFORMATION

Drawing No.

T-1.0

(6) A laundromat, dry-cleaner, bank, barber shop, beauty shop, travel service, shoe repair service, funeral parlor, gas station, office of an accountant or lawyer, pharmacy, insurance office, professional office of a health care provider, hospital, or other service establishment;

(7) A funeral, depot, or other station used for specified public transportation;

(8) A museum, library, gallery, or other place of public display or collection;

(9) A park, zoo, amusement park, or other place of recreation;

(10) A nursery, elementary, secondary, undergraduate, or postgraduate private school, or other place of education;

(11) A day care center, senior citizen center, homeless shelter, food bank, adoption agency, or other social service center establishment; and

(12) A gymnasium, health spa, bowling alley, golf course, or other place of exercise or recreation.

Exempt:

Christopher Bohanan, P.A., DABO, CAGI

Christopher Bohanan 7/26/18

TOTAL ACCESSIBILITY PLUS

Christopher Bohanan, P.A., CAGI, CAGI

Total Accessibility Plus, CAGI #4748

625-644-0025

Silena Gutierrez

Plano Examiner II

County of Riverside

TLMA-Building and Safety

4080 Lemon Street-9th Floor

Riverside, CA 92501

Dear Sir or Madam,

It is in my professional opinion that the facility detailed in the TTRA GROUP INC drawings for a proposed Accessory is EXEMPT from accessibility requirements of the 2010 ADA Title I and Title III, and 2010 CBC, chapter 11B because it is not a facility whose activities affect commerce and is not a place of public accommodation.

Additionally, the proposed project located at 15826 Wood Rd., Riverside, CA, drawings dated 07-02-2018 and per communication with TTRA GROUP INC Architects, can be considered an animal containment facility per the CBC and not opened to the public and not affecting commerce as defined by the ADA. Proposed staff consists of 22 caretakers, their job specific duties require a certain level of physical fitness, heavy lifting, general maintenance and security operations daily, and therefore EXEMPT from the codes and statutes noted below.

Exemptions are based on the following:

2010 CBC section 119-293.12 exempts animal containment areas not opened to the public are not required to meet the requirements of chapter 11B or be on an accessible path of travel.

2010 ADA Title I of the ADA covers employment but since the facility employs less than 15 workers it is not beholden to the accessibility requirements outlined in 2010 ADA Title III and CBC 11B.

Title I of the Americans with Disabilities Act of 1990 prohibits private employers, State and local governments, employment agencies and labor unions from discriminating against qualified individuals with disabilities in job application procedures, hiring, firing, advancement, compensation, job training, and other terms, conditions, and privileges of employment. The ADA covers employers with 15 or more employees, including State and local governments. It also applies to employment agencies and to labor organizations. Employees with more than one employee are only required to provide equal pay for equal work.

2010 ADA Title III of the ADA covers accessibility related to Places of Public Accommodation. To qualify as a PPA one would have to satisfy one of the following scenarios:

Places of public accommodation (PPA) means a facility operated by a private entity whose operations affect commerce and fall within at least one of the following categories:

(1) Place of lodging, except for an establishment located within a facility that contains not more than five rooms for rent or hire and that actually is occupied by the proprietor of the establishment as the residence of the proprietor. For purposes of this part, a facility is a "place of lodging" if it is --

(i) An inn, hotel, or motel; or

(ii) A facility that --

(A) Provides guest rooms for sleeping for stays that primarily are short-term in nature (generally, 30 days or less) where the occupant does not have the right to return to a specific room or unit after the conclusion of his or her stay; and

(B) Provides guest rooms under conditions and with amenities similar to a hotel, motel, or inn, including the following --

(1) On- or off-site management and reservations service;

(2) Rooms available on a walk-up or call-in basis;

(3) Availability of housekeeping or linen service; and

(4) Acceptance of reservations for a guest room type without guaranteeing a particular unit or room until check-in, and without a prior lease or security deposit.

(2) A restaurant, bar, or other establishment serving food or drink;

(3) A motion picture house, theater, concert hall, stadium, or other place of exhibition or entertainment;

(4) An auditorium, convention center, lecture hall, or other place of public gathering;

(5) A bakery, grocery store, clothing store, hardware store, shopping center, or other sales or rental establishment;

PROPOSED AVIARIES

PPA180065

15825 WOOD ROAD
RIVERSIDE, CA 92506



TTRA GROUP INC.

284 Crystal Vista Lane

Wheeler, CA 91769

Tel: (909) 524-2146

E-mail: tttagroup@gmail.com

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Revised

▲ MINOR PLOT PLAN 01/30/18

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Date

07/02/18 N.T.S.

Drawn by

S.H. R.S.

Project No.

181102

Drawing Title

EXEMPT FROM

ACCESSIBILITY REQUIREMENTS

Drawing No.

T-2.0

PROPOSED AVIARIES

PPA180085

15825 WOOD ROAD
RIVERSIDE, CA 92508



ITRA GROUP INC.

854 Crystal Gate Lane
Walnut, CA 91789

Tel: (909) 524-2148

E-mail: itragroup@gmail.com

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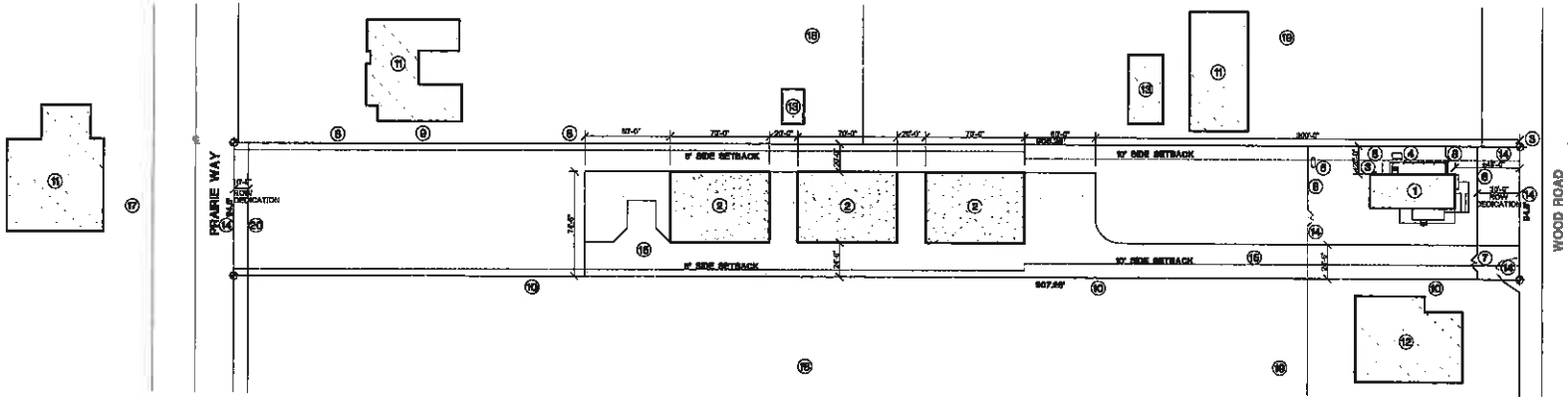
Revisions

MINOR PLOT PLAN	01/30/19

Date	Scale
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S.H.	R.S.
Project No.	
181102	
Drawing title	
	SITE PLAN

Drawing etc.

A-1.0



KEY NOTES:

- | | |
|--|--|
| <ul style="list-style-type: none"> ① EXISTING 1440 SF. MOBILEHOME TO REMAIN ② PROPOSED 3,800 SF. AVIARIES ③ 6" POWER POLE ④ 6" SEPTIC TANK TO BE DISCONTINUED, 6" SEWER TO CONNECT TO CITY OF RIVERSIDE PUBLIC SEWER MAIN ⑤ PROPANE TANK ⑥ 06 6" WROUGHT IRON FENCE ⑦ 06 5" 12" W WROUGHT IRON GATES ⑧ 6" CHAINLINK FENCE ⑨ 6" WOOD FENCE ⑩ 6" CHAINLINK FENCE | <ul style="list-style-type: none"> ⑪ ADJACENT SINGLE FAMILY RESIDENCE ⑫ ADJACENT MOBILEHOME ⑬ ADJACENT ACCESSORY STRUCTURE ⑭ 6" CHAINLINK FENCE & GATES TO BE REMOVED ⑮ 06 COMPACTED GRAVEL, FIRE APPARATUS ACCESS ROAD ⑯ CITY OF RIVERSIDE TERRITORY ⑰ R-A-1 ZONE ⑱ A-1-1 ZONE ⑲ W-2-M-1 ZONE ⑳ 06 6" CHAINLINK FENCE |
|--|--|

NOTES:
NO KNOWN EASEMENTS EXIST.

SITE PLAN

PROPOSED AVIARIES

PPA180065

15825 WOOD ROAD
RIVERSIDE, CA 92508



ITRA GROUP INC.

954 Crystal Gate Lane
Walnut, CA 91789

Tel: (909) 824-2148

E-mail: itragroup@aol.com

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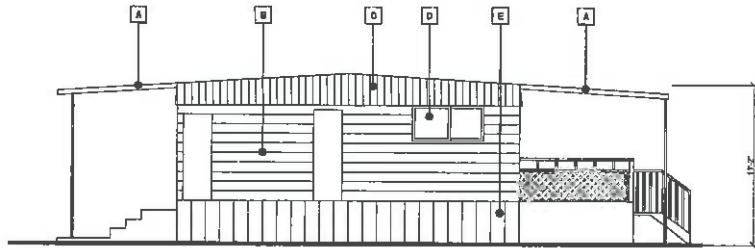
EXISTING MOBILEHOME
ELEVATIONS

181102

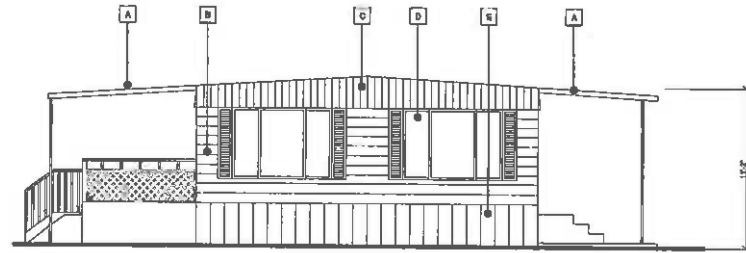
EXISTING MOBILEHOME
ELEVATIONS

181102

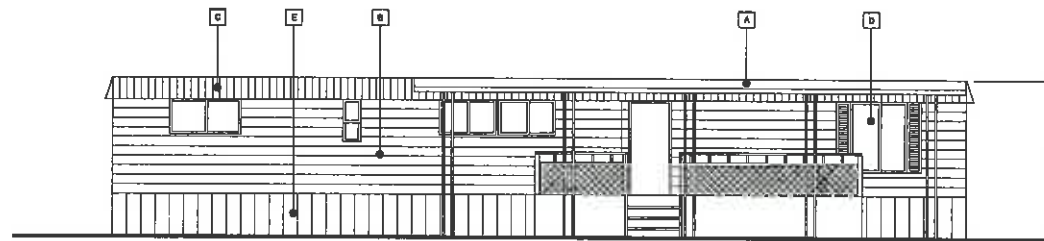
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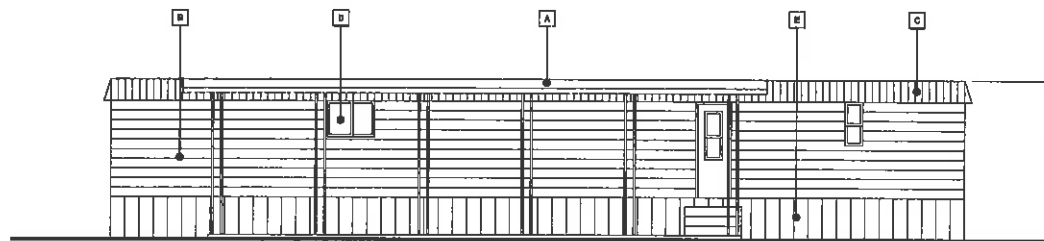
EXISTING MOBILEHOME WEST ELEVATION



EXISTING MOBILEHOME EAST ELEVATION



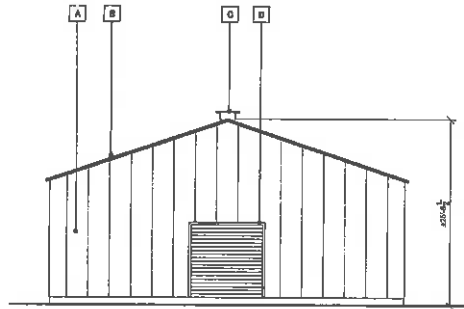
EXISTING MOBILEHOME SOUTH ELEVATION



EXISTING MOBILEHOME NORTH ELEVATION

EXTERIOR MATERIALS

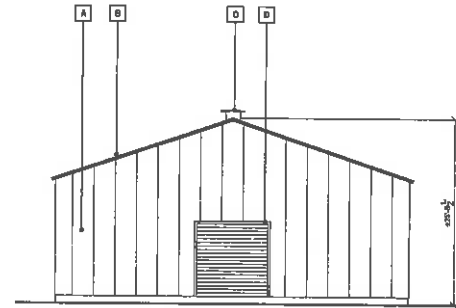
- A** METAL PORCH COVER
- B** VINYL SIDING
- C** VINYL HARBARD ROOF COVER
- D** ALUMINUM WINDOW
- E** METAL ROSSBAND SHOT



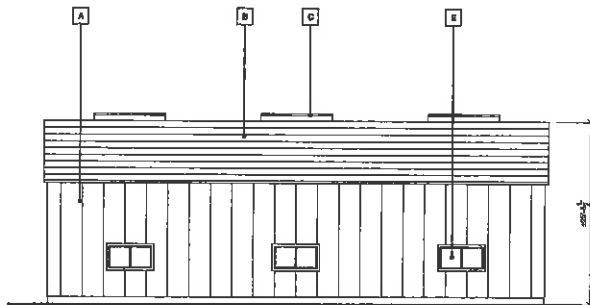
PROPOSED AVIARIES WEST (REAR) ELEVATION

EXTERIOR MATERIALS

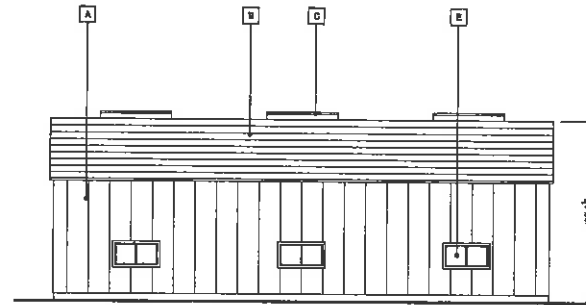
- A 26 GA PAINTED STEEL PANEL WALL
- B 26 GA GALVALUME ROOF
- C 1"X1" OPERABLE REAR VENT
- D 1/2"X1" STEEL ROLL-UP DOOR
- E 1/2"X1" WINDOW



PROPOSED AVIARIES EAST (FRONT) ELEVATION



PROPOSED AVIARIES SOUTH (SIDE) ELEVATION



PROPOSED AVIARIES NORTH (SIDE) ELEVATION

PROPOSED AVIARIES

PPA180065

15825 WOOD ROAD
RIVERSIDE, CA 92506



ITRA GROUP INC.

584 Crystal Vista Lane
Walnut, CA 91789

Tel: (909) 524-2145

E-mail: itragroup@gmail.com

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Revised

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2nd
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4th

Revisions

△	MINOR PLOT PLAN	01/30/19
△		
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Date Scale

07/22/18 1/8" = 1'-0"
 Drawn by: [Name] Checked by: [Name]

S.H. R.S.
 Project No. 181102

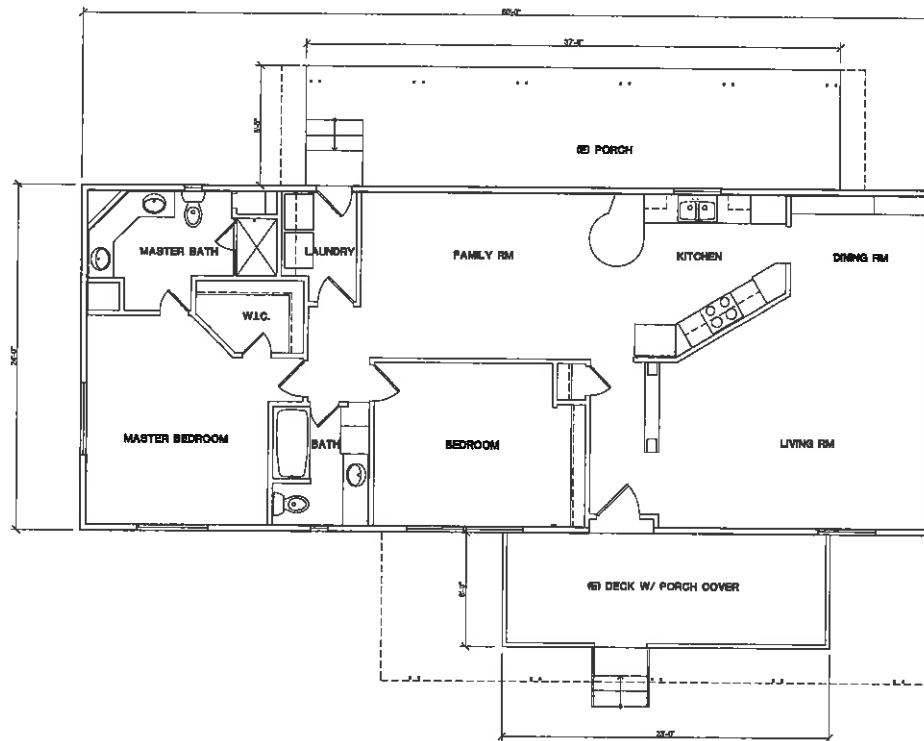
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Drawing Title

PROPOSED AVIARIES
ELEVATIONS

Drawing No.

B-2.0



EXISTING MOBILEHOME FLOOR PLAN 

PROPOSED AVIARIES

PPA180065

15825 WOOD ROAD
RIVERSIDE, CA 92508



ITRA GROUP INC.

964 Crystal Lake Lane
Walnut, CA 91788

Tel: (909) 524-2148

E-mail: itracorp@gmail.com

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Revised	By	Date

MINOR PLOT PLAN 01/20/19

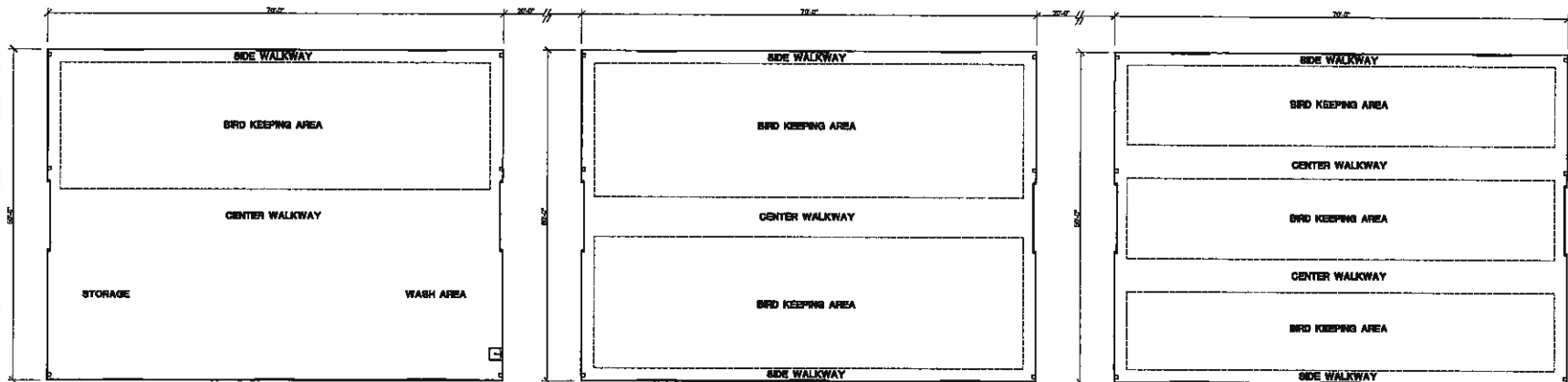
Date: 07/02/18 Scale: 1/8" = 1'-0"

Drawn by: S.H. Checked by: R.S.

Project No.: 161102

Drawing Title:
EXISTING MOBILEHOME FLOOR PLAN

Drawing No.: **C-1.0**



PROPOSED AVIARIES

PPA180065

15925 WOOD ROAD
RIVERSIDE, CA 92508



ITRA GROUP INC.

884 Crystal White Lane
Walnut, CA 91789

Tel: (909) 524-2148
E-mail: itragroup@gmail.com

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Revisions

REV	DATE	DESCRIPTION

MINOR PLOT PLAN 01/30/19

Date: 07/02/19

Drawn by: S.H. Checked by: R.S.

Project No. 181102

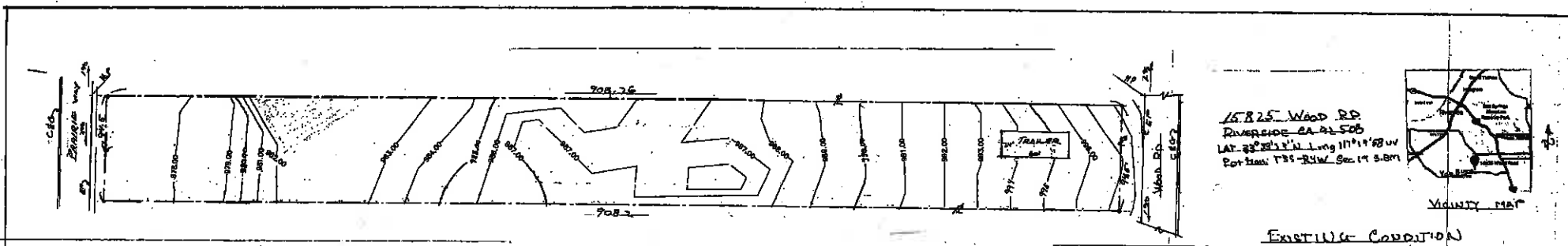
Drawing Title

PROPOSED AVIARIES
FLOOR PLAN

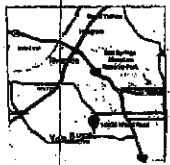
Drawing No.

PROPOSED AVIARIES FLOOR PLAN 

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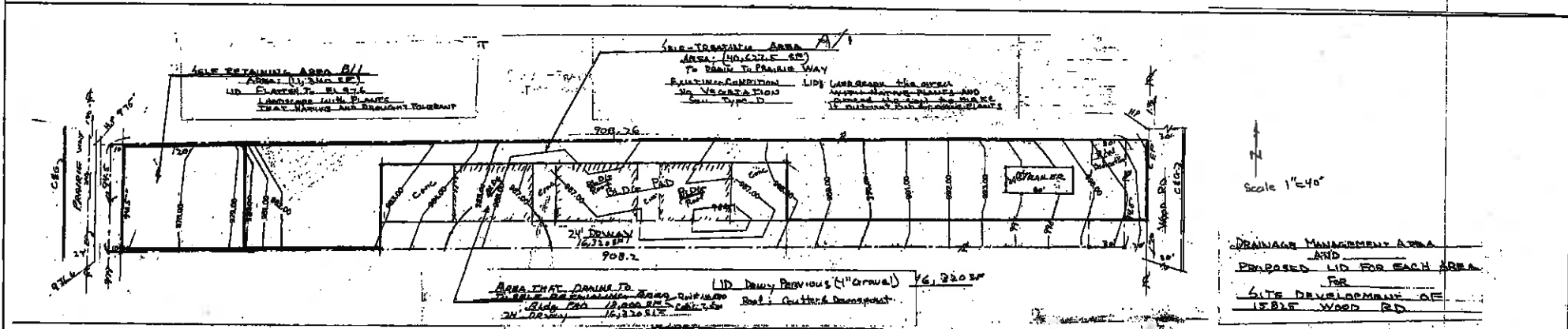


15825 Wood Rd
 Riverside CA 92506
 LAT: 33°21'N Long: 117°11'W
 Portion T2S-R4W Sec. 17 S.871

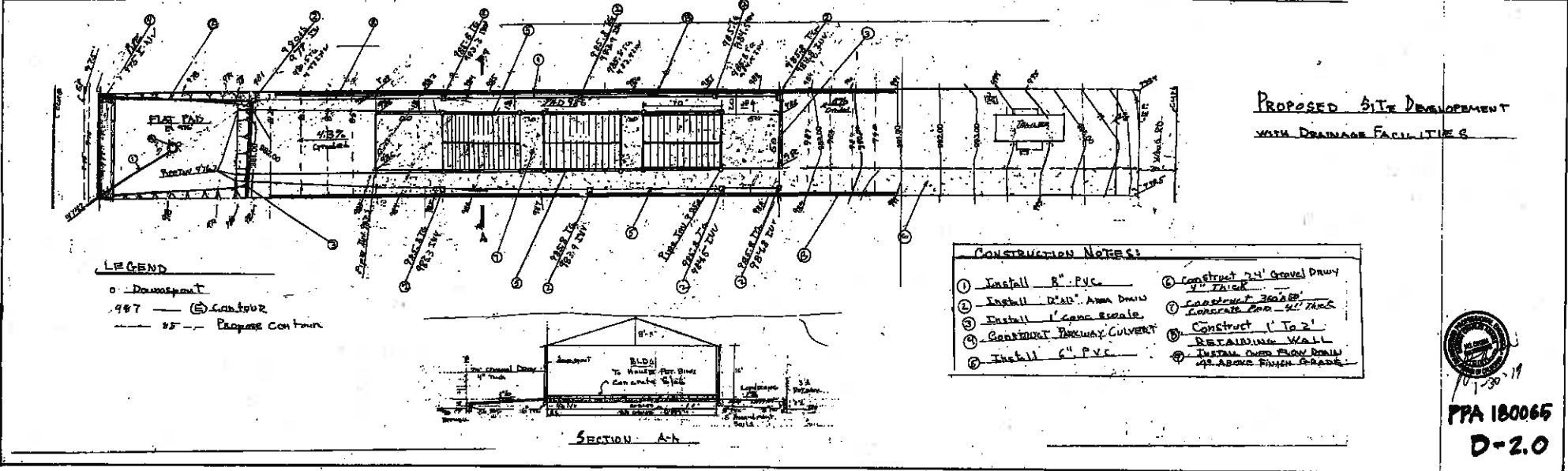


VICINITY MAP

EXISTING CONDITION



15825 WOOD RD
 DRAINAGE MANAGEMENT AREA
 AND
 PROPOSED LID FOR EACH AREA
 FOR
 SITE DEVELOPMENT OF
 15825 WOOD RD

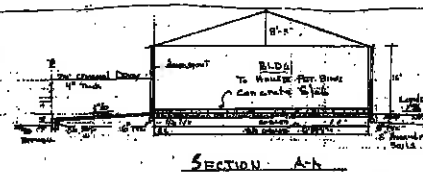


PROPOSED SITE DEVELOPMENT
 WITH DRAINAGE FACILITIES

LEGEND
 ○ Downspout
 987 — Contour
 --- 85 --- Proposed Contour

CONSTRUCTION NOTES:

1. Install 8" PVC	2. Construct 24' Gravel Drury 4" Thick
3. Install 24" Area Drain	4. Construct 36" x 36" Concrete Box - 4" Thick
5. Install 1" Catch Basins	6. Construct 1' to 2' Retaining Wall
7. Construct Driveway Culvert	8. Install 24" Box Drain 48" Above Finish Grade
9. Install 6" PVC	



SECTION A-A



PPA 180065
 D-2.0

PAGE BREAK





AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY

August 15, 2019

CHAIR
Steve Manos
Lake Elsinore

VICE CHAIR
Russell Betts
Desert Hot Springs

COMMISSIONERS

Arthur Butler
Riverside

John Lyon
Riverside

Steven Stewart
Palm Springs

Richard Stewart
Moreno Valley

Gary Youmans
Temecula

STAFF

Director
Simon A. Housman

John Guerin
Paul Rull
Barbara Santos

County Administrative Center
4080 Lerron St., 14th Floor
Riverside, CA 92501
(951) 955-5132

www.rcaluc.org

Mr. Jay Olivas, Project Planner
County of Riverside Planning Department
77-588 El Duna Court, Suite H
Palm Desert CA 92211

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW –
DIRECTOR'S DETERMINATION**

File No.: ZAP1047TH19
Related File No.: CUP190004 (Conditional Use Permit)
APN: 757-044-009

Dear Mr. Olivas:

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to Policy 1.5.2(d) of the Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan, staff reviewed County of Riverside Case No. CUP190004 (Conditional Use Permit), a proposal to establish a truck storage yard with a 480 square foot office trailer, an above ground 10,000 gallon diesel fuel storage tank, a metal storage container, and a weigh station on 2.63 acres located at 56051 Orange Street (on the westerly side of Orange Street, southerly of Airport Boulevard) in the unincorporated community of Thermal.

The site is located within Airport Compatibility Zone D of the Jacqueline Cochran Regional Airport Influence Area (AIA). Within Compatibility Zone D of the Jacqueline Cochran Regional Airport Land Use Compatibility Plan, non-residential intensity is restricted to an average of 100 people per acre and 300 people in any given single-acre area. The truck storage yard proposes a 480 square foot office trailer, accommodating 2 people. This results in an average intensity of 1 person per acre, and a single acre intensity of 2 people, which would be consistent with the Compatibility Zone D criteria. Even if 30 trucks with two drivers each were on-site at any one time, the total number of persons would not exceed 60, which would still be consistent.

The elevation at the northerly end of Runway 12-30 at Jacqueline Cochran Regional Airport is -117 feet mean sea level (MSL) [i.e., 117 feet below mean sea level]. At a distance of 5,150 feet from the runway to the project, Federal Aviation Administration Obstruction Evaluation Services (FAA OES) review would be required for any structures with a top of roof exceeding -65.5 feet mean sea level. The site's elevation is -116 feet MSL. The proposed building has a height of 10 feet, resulting in a top point elevation of -106 feet MSL. Therefore, FAA obstruction evaluation review for height/elevation reasons was not required.

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2005 Jacqueline Cochran Regional Airport Land Use Compatibility Plan, as amended in 2006, provided that the County of Riverside applies the following recommended conditions:

AIRPORT LAND USE COMMISSION

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The review of this Conditional Use Permit is based on the proposed uses and activities noted in the project description. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
 - (a) Any use or activity which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use or activity which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use or activity which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area.
 - (d) Any use or activity which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
3. The following uses/activities are specifically prohibited: wastewater management facilities; trash transfer stations that are open on one or more sides; recycling centers containing putrescible wastes; incinerators.
4. The attached notice shall be provided to all potential purchasers of the property and tenants/lessees of the buildings thereon, and shall be recorded as a deed notice.
5. Any new aboveground detention or water quality basins on the site shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention/water quality basin(s) that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.
6. This project has been evaluated as a truck storage yard with a 480 square foot office trailer, an above ground fuel storage tank, a metal storage container, and a 1,200 square foot weigh station. Any increase in building area or change in use will require an amended review by the Airport Land Use Commission.

If you have any questions, please contact Paul Rull, ALUC Principal Planner, at (951) 955-6893.

AIRPORT LAND USE COMMISSION

Sincerely,
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Simon A. Housman, ALUC Director

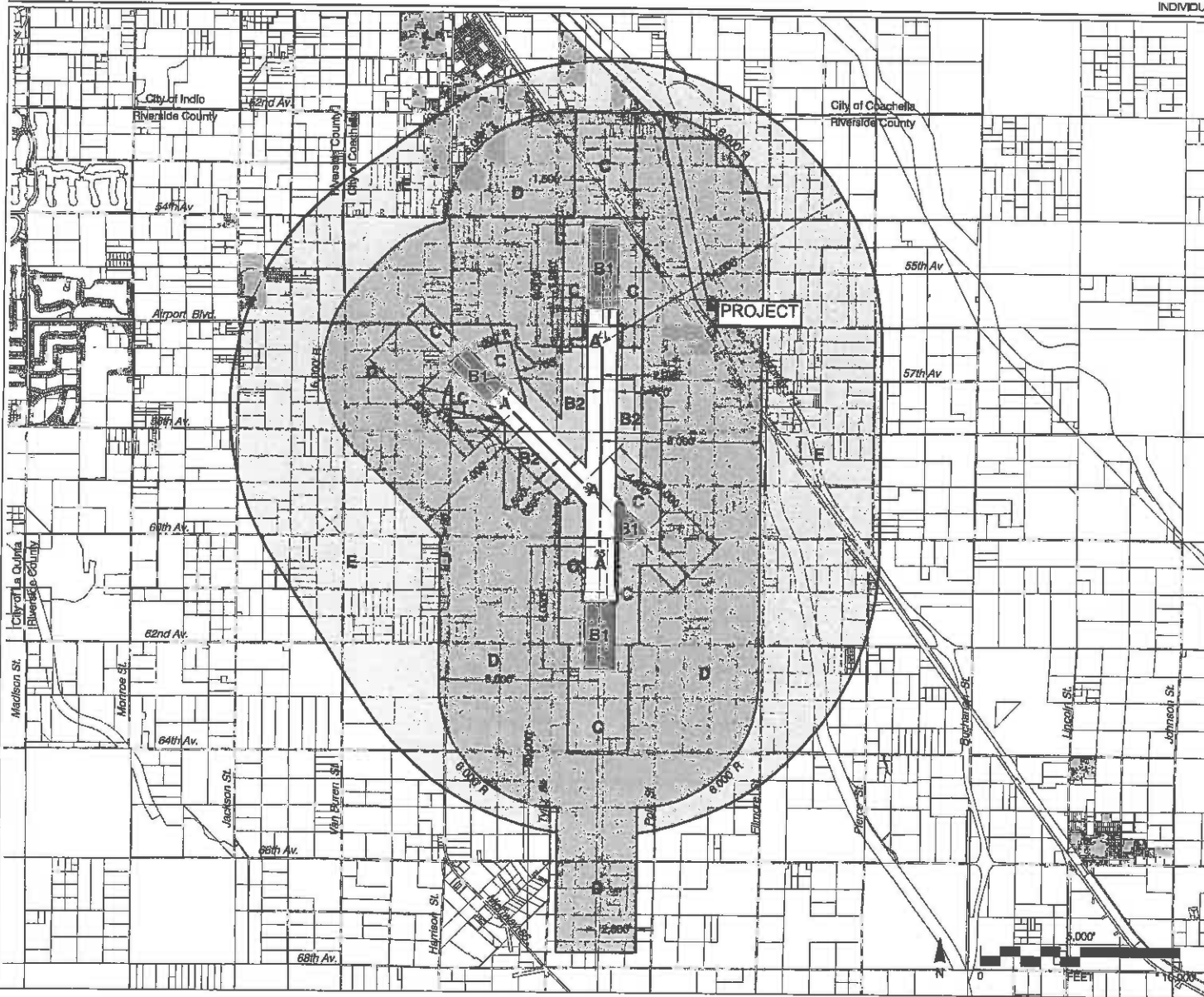
Attachments: Notice of Airport in Vicinity

cc: Villa Park Trucking/Clift Investments LLC (applicant)
James Fagelson (representative)
Juan Manuel Martinez (listed property owner)
Vince Yzaguirre, Assistant Director, Riverside County Economic Development Agency
Liliana Valle, County Airports Manager
ALUC Case File

Y:\AIRPORT CASE FILES\JCRA\ZAP1047TH19\ZAP1047TH19.LTR.doc

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



Legend

Compatibility Zones

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E

Boundary Lines

- Airport Property Line - Existing
- Airport Property Line - Planned
- City Limits

Note

Except for southern extension, Airport Influence Area boundary measured from a point 200 feet beyond runway ends in accordance with FAA airspace protection criteria (FAR Part 77). All other dimensions measured from runway ends and centerlines.

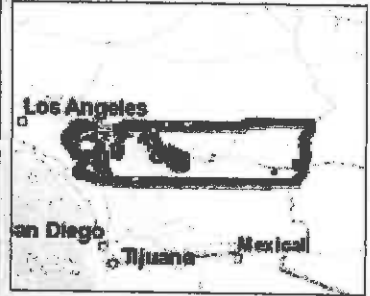
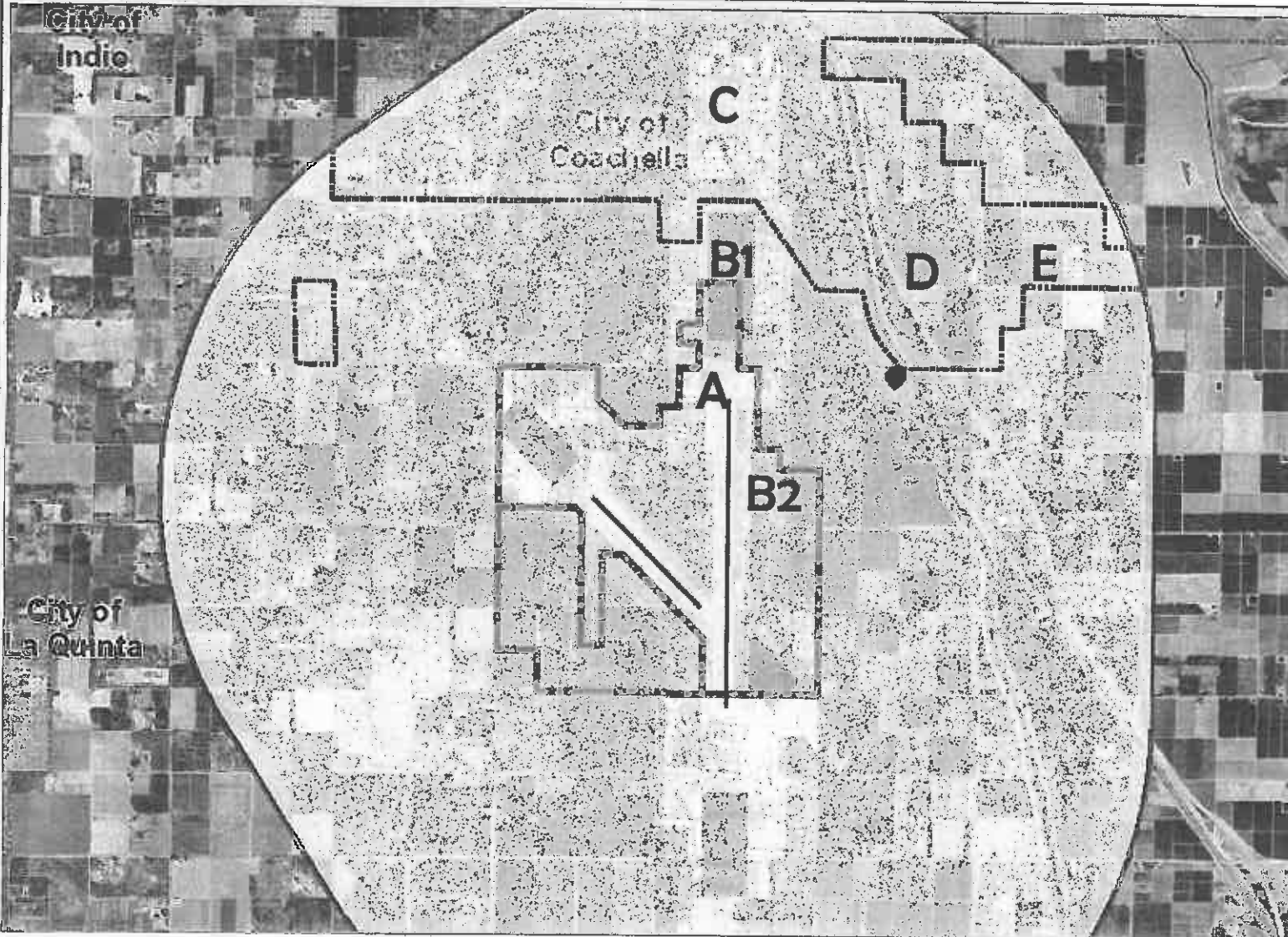
See Chapter 2, Table 2A for compatibility criteria associated with this map.

Riverside County
Airport Land Use Commission
Riverside County
Airport Land Use Compatibility Plan
Policy Document
 (Adopted June 2005)

Map JC-1

Compatibility Map
Jacqueline Cochran Regional Airport

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones
- ▨ OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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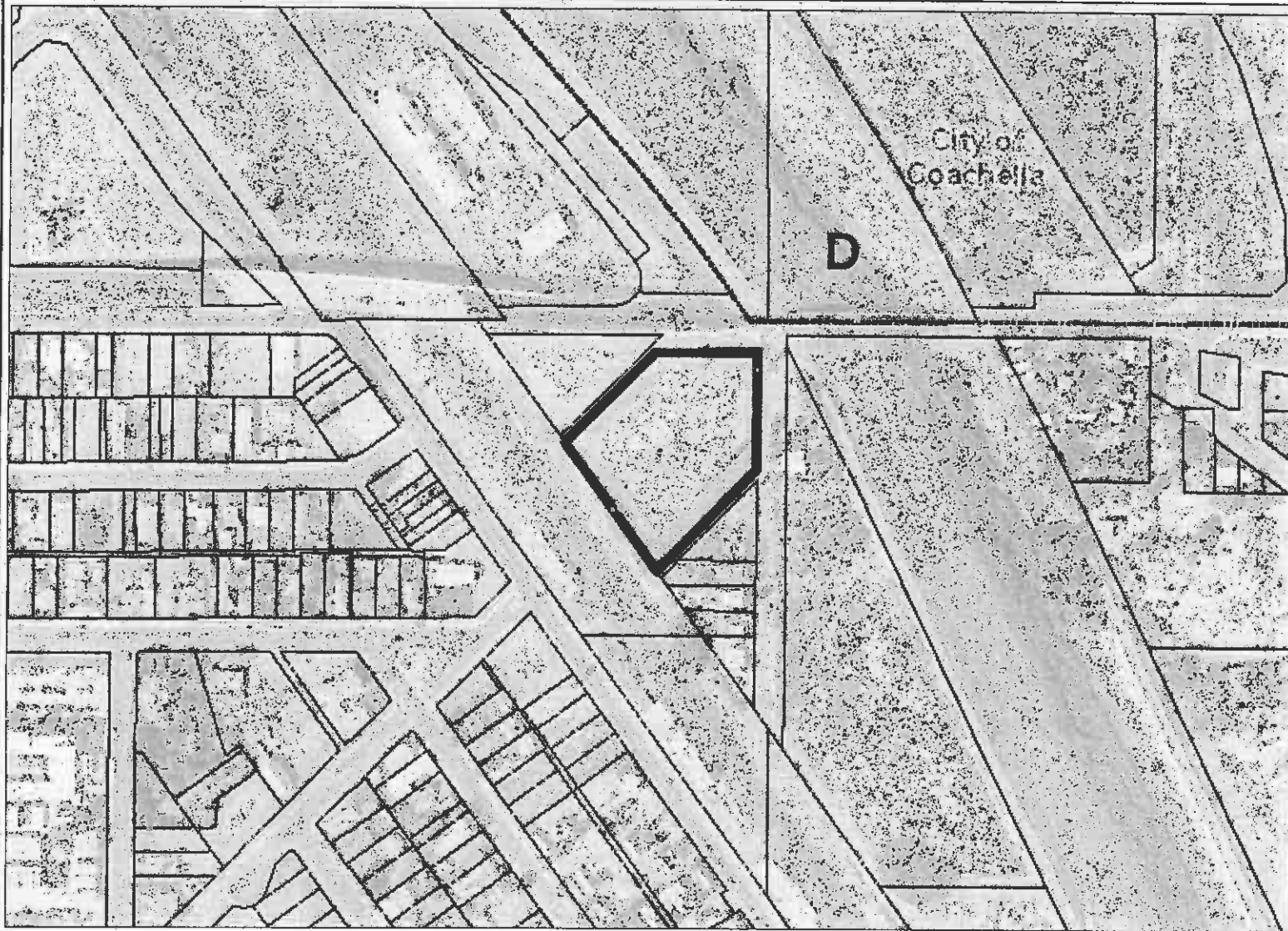


REPORT PRINTED ON... 8/12/2019 9:08:05 AM

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Notes

Map My County Map



Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas

Airport Compatibility Zones

- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5

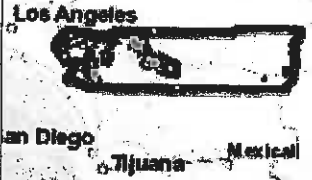
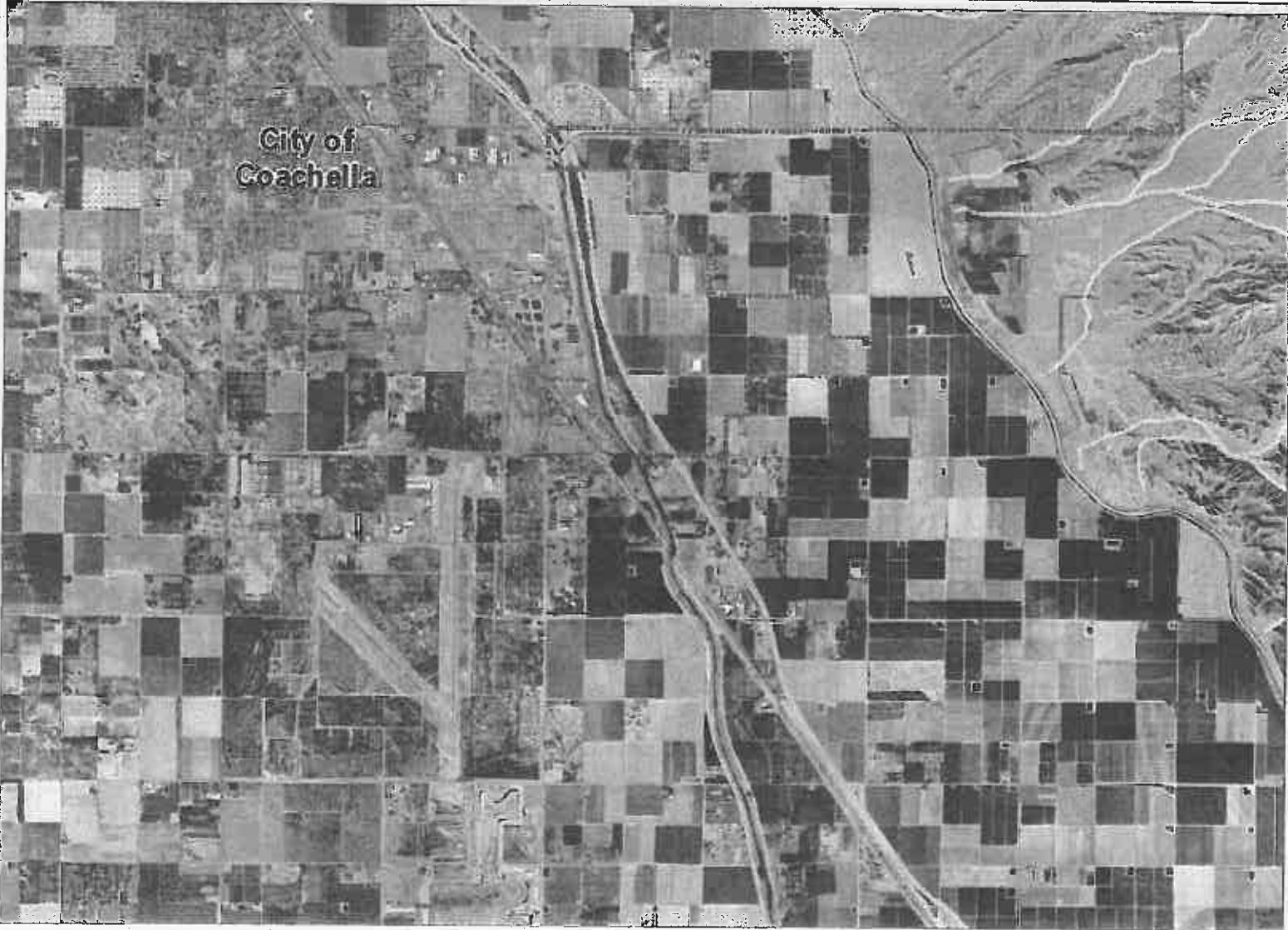


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Notes



Map My County Map



Legend

- Blueline Streams
- City Areas
- World Street Map



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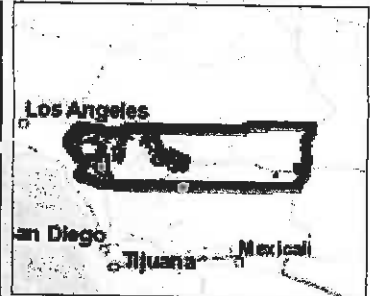
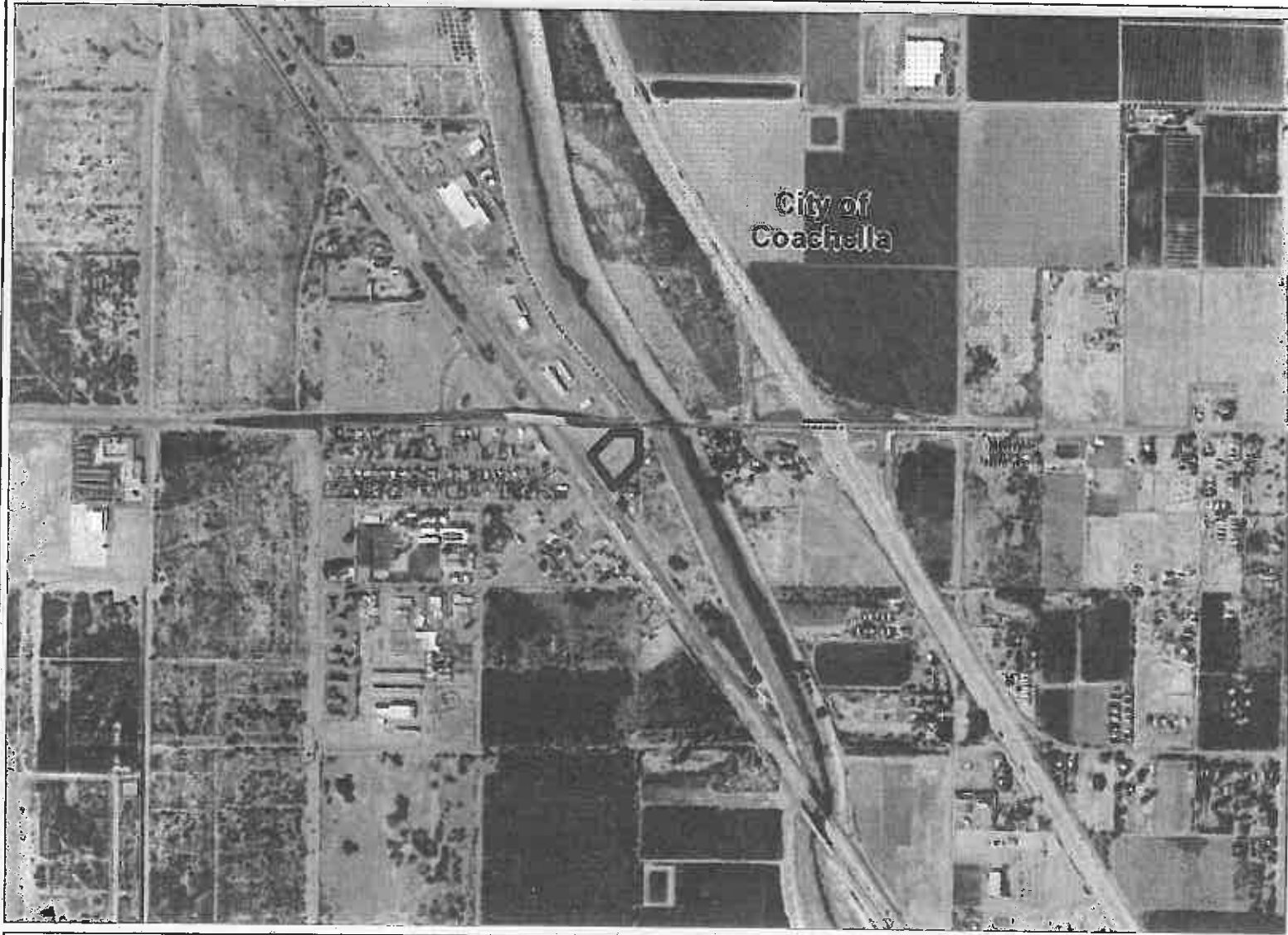


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Notes

Map My County Map



- Legend**
- Blueline Streams
 - City Areas
 - World Street Map



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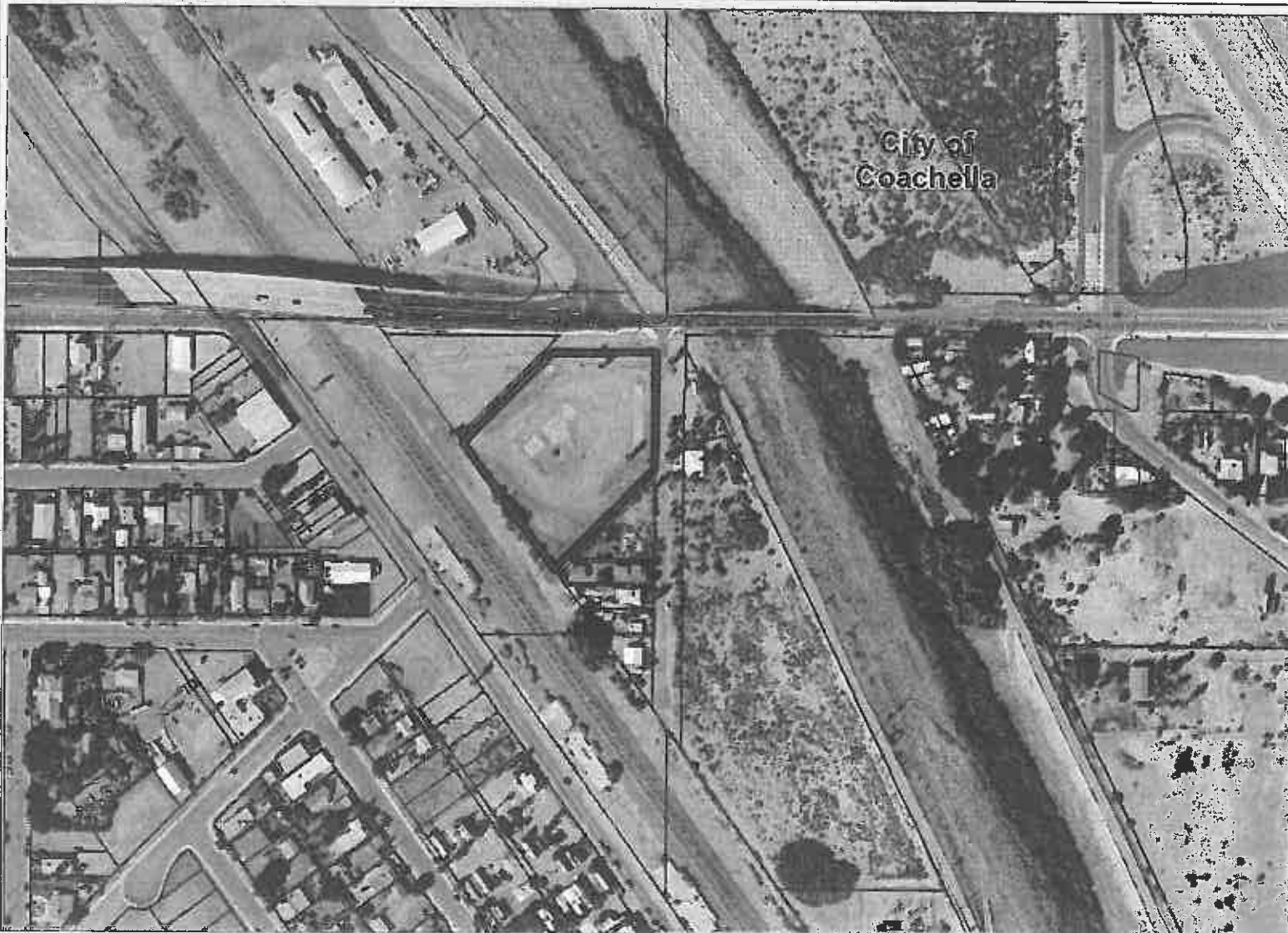
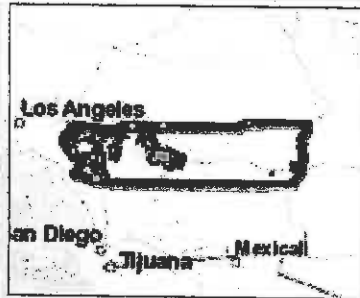


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Notes

Map My County Map



Legend

-  Parcels
-  Blueline Streams
-  City Areas
-  World Street Map



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Notes

0 379 758 Feet

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Conditional Use Permit 190004

Villa Park Trucking
2301 East Francis Street
Ontario, CA 91761

Project Description

Conditional Use Permit No. 190004 proposes a private truck storage yard. The main purpose for the development of this site is to fuel and weigh our private fleet while parking 2 to 4 semi-trailer trucks and approximately 4 to 8 sets of agricultural trailers at the facility during the citrus harvest in the Coachella Valley, a 480 square-foot office trailer containing one bathroom and a meeting room for use of the drivers to prepare paperwork and to take their breaks, an above ground diesel fuel storage tank containing up to 10,000 gallons with one (1) fuel pumps/dispenser, a 70 foot weigh station, and one metal storage container. The facility will be mostly unmanned throughout the year. There will be a landline with no exterior communication systems. Parking spots for private cars will be located next to and in front of the office trailer and will include one handicap space.

The storage yard will be for the exclusive use of Villa Park Trucking. The typical hours of operation will be between the hours of 7am and 5pm during both peak (Oct-April) and off peak (May-Sept) seasons. No use is expected during non-operational hours. We anticipate 6-30 trucks trips per day, Monday thru Saturday during the peak season and 0-6 trucks trips during the off season. There may be periods of time when the storage facility is not in use since the need to transport produce is not required.

There will be motion activated security lights on site. No security personnel will be kept on-site. The Orange Street will be a block wall with an automatic wrought iron gate. The remaining perimeter of the site will be a chain link fence to maintain visibility into the property.

The above-ground diesel fuel tank will be self-contained and will only be for the use of Villa Park Trucking. The drivers will pump their own fuel and will use the office to fill out the necessary paperwork. No truck repair will be allowed on-site. The metal storage container will house equipment necessary for the maintenance of the site and minimal equipment for the trucks.

PAGE BREAK





AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY

July 25, 2019

CHAIR

Steve Manos
Lake Elsinore

VICE CHAIR

Russell Betts
Desert Hot Springs

COMMISSIONERS

Arthur Butler
Riverside

John Lyon
Riverside

Steven Stewart
Palm Springs

Richard Stewart
Moreno Valley

Gary Youmans
Temecula

STAFF

Director
Simon A. Housman

John Guerin
Paul Rull
Barbara Santos

County Administrative Center
4080 Lerran St., 14th Floor.
Riverside, CA 92501
(951) 955-5132

www.aluc.org

Ms. Summer Woodend, Project Planner
City of Moreno Valley Planning Department
14177 Frederick Street
Moreno Valley CA 92552

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW –
DIRECTOR’S DETERMINATION**

File No.: ZAP1375MA19 (letter 1 of 2)
Related File Nos.: GPA19-0151 (General Plan Amendment); ZC19-0152 (Zone Change)
APNs: 291-100-054; 291-100-055

Dear Ms. Woodend:

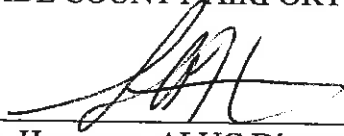
Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to ALUC Resolution No.15-01 (as adopted on August 13, 2015), staff reviewed City of Moreno Valley Case Nos. GPA19-0151 (General Plan Amendment), a proposal to amend the boundaries of the General Plan land use designations on 49.61 net acres located northerly of State Highway Route 60, easterly of Day Street, southerly of Ironwood Avenue, and westerly of Brixton Court from 30.43 acres of Commercial and 19.17 acres of Residential/Office to 30.56 acres of Commercial and 19.05 acres of Residential/Office, and ZC19-0152 (Zone Change), a proposal to amend the boundaries of zoning classifications on the same site from 30.66 acres of Community Commercial and 18.94 acres of Residential 15 to 30.56 acres of Community Commercial and 19.05 acres of Residential 15 (essentially shifting the general plan land use designation and zoning boundaries).

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan (“March ALUCP”).

This finding of consistency relates to airport compatibility issues and does not necessarily constitute an endorsement of this General Plan Amendment and zone change. Both the existing and proposed land use designation and zoning boundaries are consistent, as the site is located within Airport Compatibility Zone E, where non-residential intensity and residential density are not restricted.

If you have any questions, please contact Paul Rull, ALUC Principal Planner, at (951) 955-6893.

Sincerely,
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Simon A. Housman, ALUC Director

Attachments: Notice of Airport in Vicinity

cc: M&F Development Company, Paul Bernard (applicant/co-property owner)
Albert A Webb Associates, Haley Franco (representative)
MBBB, LLC, Matthew E. Webb (co-property owner)
Triple S Group, LLC, Scott Webb (co-property owner)
Irene Webb Salyer (co-property owner)
Fritz Duda Company/First National Bank of Iowa City (co-property owner)
MW Investment Company (listed property owner)
Gary Gosliga, Airport Manager, March Inland Port Airport Authority
Daniel "Rock" Rockholt, March Air Reserve Base
ALUC Case File

Y:\AIRPORT CASE FILES\March\ZAP1375MA19\ZAP1375MA19.LTR GPA.doc



AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY

July 25, 2019

CHAIR
Steve Manos
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VICE CHAIR
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County Administrative Center
4080 Lemon St., 14th Floor.
Riverside, CA 92501
(951) 955-5132

www.rcaluc.org

Ms. Summer Woodend, Project Planner
City of Moreno Valley Planning Department
14177 Frederick Street
Moreno Valley CA 92552

RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW – DIRECTOR'S DETERMINATION

File No.: ZAP1375MA19 (letter 2 of 2)
Related File Nos.: PEN19-0150 (Tentative Parcel Map)
APNs: 291-100-054 and -055

Dear Ms. Woodend:

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to Policy 1.5.2(d) of the Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan, staff reviewed City of Moreno Valley Case No. PEN19-0150 (Tentative Parcel Map), a proposal to divide 51.51 gross acres (49.61 net acres) located northerly of State Highway Route 60, easterly of Day Street, southerly of Ironwood Avenue, and westerly of Brixton Court into four (4) parcels.

The site is located within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone E, nonresidential intensity and residential are not restricted.

The elevation of Runway 14-32 at March Air Reserve Base/Inland Port Airport is approximately 1,535 feet above mean sea level (AMSL) at its northerly terminus. At a distance of 15,900 feet from the project to the nearest point on the runway, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any new structures with an elevation at top of roof exceeding 1,694 feet AMSL. Elevations on-site range from 1,656 to 1,772 feet AMSL. No new buildings or structures are proposed at this time. Review by the Federal Aviation Administration Obstruction Evaluation Services (FAA OES) is not a prerequisite to land division, but will be required prior to construction of new buildings or any other structures on the parcels. A condition has been included requiring that the permittee obtain a "Determination of No Hazard to Air Navigation" letter from the FAA OES prior to issuance of building permits for any new structures on the property with a top point elevation greater than 1,694 feet AMSL.

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, provided that the City of Moreno Valley applies the following recommended conditions:

AIRPORT LAND USE COMMISSION

CONDITIONS:

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site.
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
3. The attached notice shall be provided to all prospective purchasers of the property.
4. Any new aboveground detention or water quality basins on the site shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention basins that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.
5. Prior to issuance of building permits for any new structures with a top point elevation exceeding 1,694 feet above mean sea level, the permittee shall provide to the City of Moreno Valley Building and Safety Division a "Determination of No Hazard to Air Navigation" letter from the Federal Aviation Administration Obstruction Evaluation Service.

If you have any questions, please contact Paul Rull, ALUC Principal Planner, at (951) 955-6893.

AIRPORT LAND USE COMMISSION

Sincerely,
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Simon A. Housman, ALUC Director

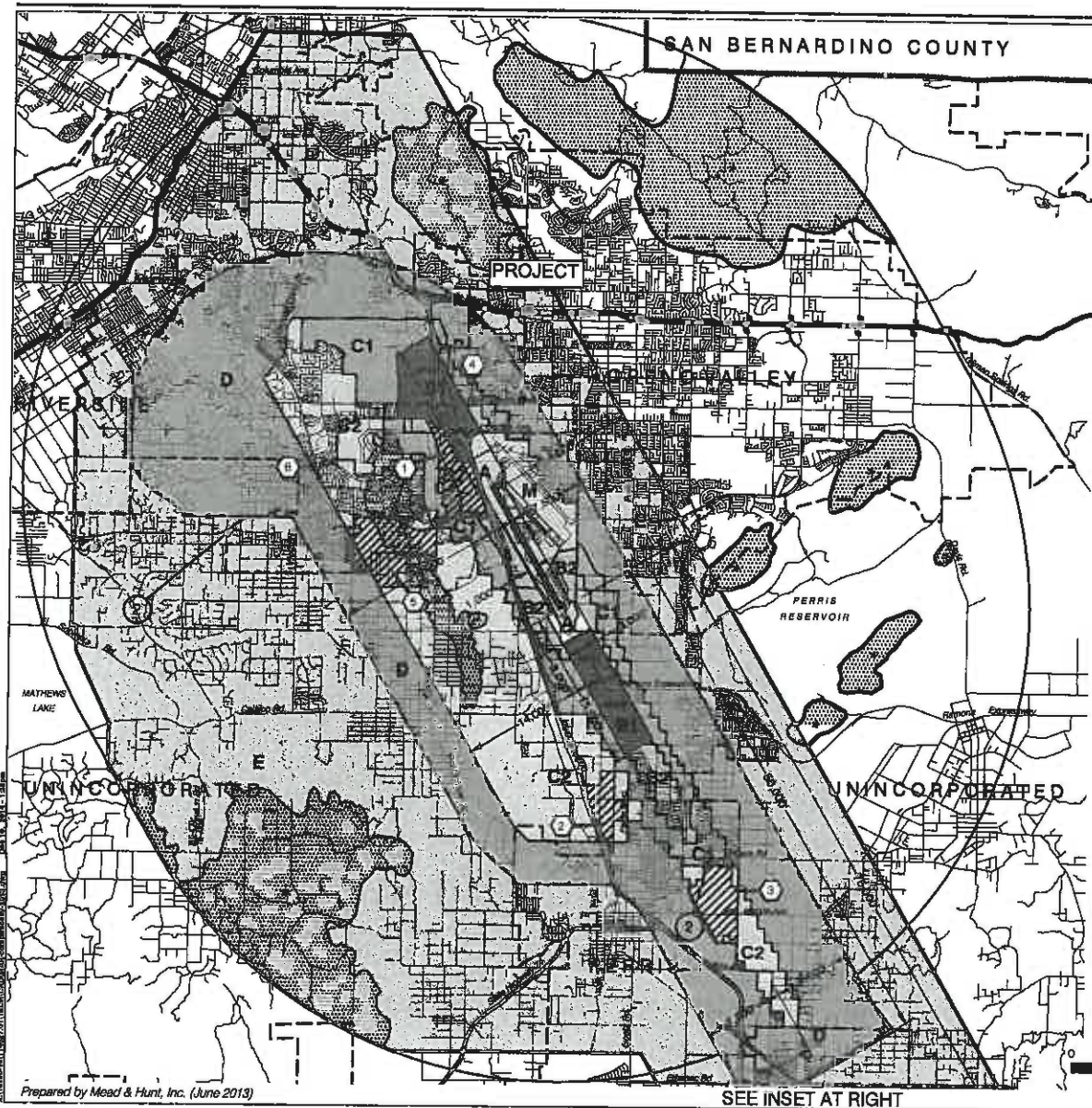
Attachments: Notice of Airport in Vicinity

cc: M&F Development Company, Paul Bernard (applicant/co-property owner)
Albert A Webb Associates, Haley Franco (representative)
MBBB, LLC, Matthew E. Webb (co-property owner)
Triple S Group, LLC, Scott Webb (co-property owner)
Irene Webb Salyer (co-property owner)
Fritz Duda Company/First National Bank of Iowa City (co-property owner)
MW Investment Company (listed property owner)
Gary Gosliga, Airport Manager, March Inland Port Airport Authority
Daniel "Rock" Rockholt, March Air Reserve Base
ALUC Case File

Y:\AIRPORT CASE FILES\March\ZAP1375MA19\ZAP1375MA19.LTR PM.doc

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



LEGEND

Compatibility Zones

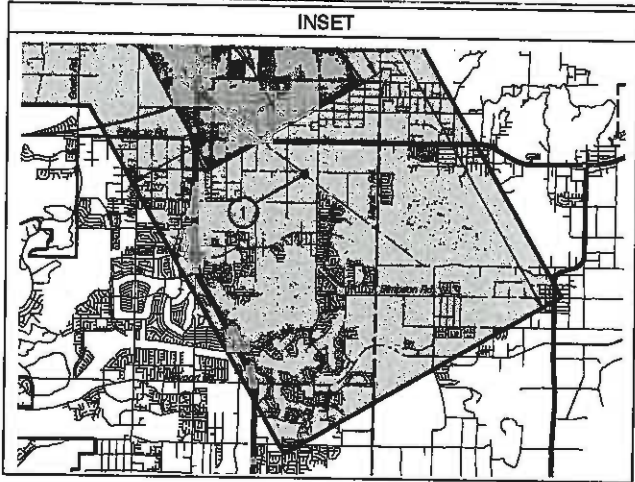
- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- ▨ Site-Specific Exceptions (existing local agency commitments to development projects)

- ① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,635 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.

- ① March JPA: March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA: Ben Clark Training Center
- ⑥ Riverside: Ridge Crest Subdivision



**Riverside County
Airport Land Use Commission
March Air Reserve Base / Inland Port Airport
Land Use Compatibility Plan
(Adopted November 13, 2014)**

Note:
All dimensions are measured from
runway ends and centerlines.



Base map source: County of Riverside 2013

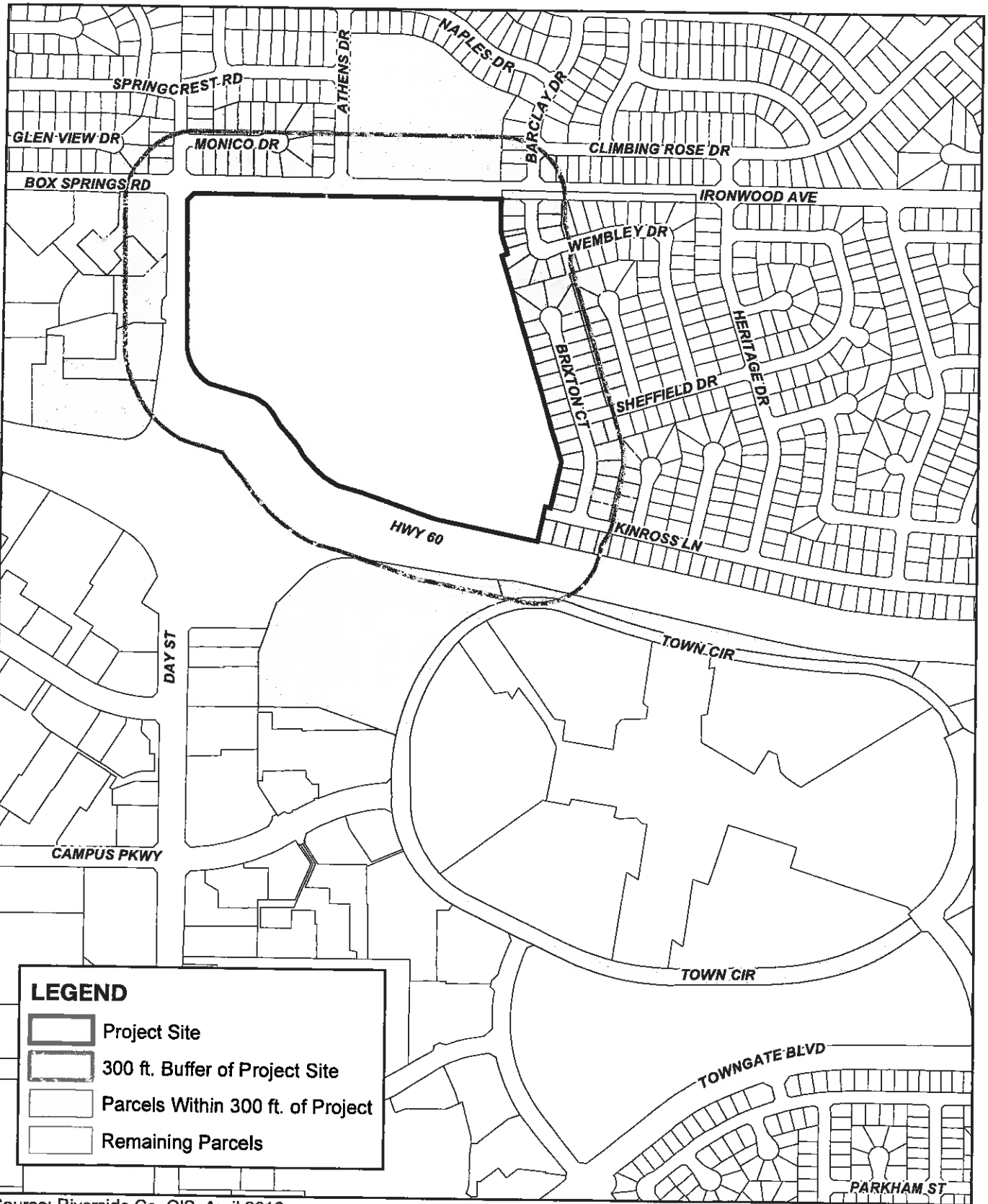
Prepared by Mead & Hunt, Inc. (June 2013)

SEE INSET AT RIGHT





**Compatibility Map
March Air Reserve Base / Inland Port Airport**

Map MA-1

H:\2019\19-0107\GIS\Parcels_300.mxd; Map created 29 May 2019

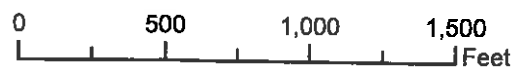


LEGEND

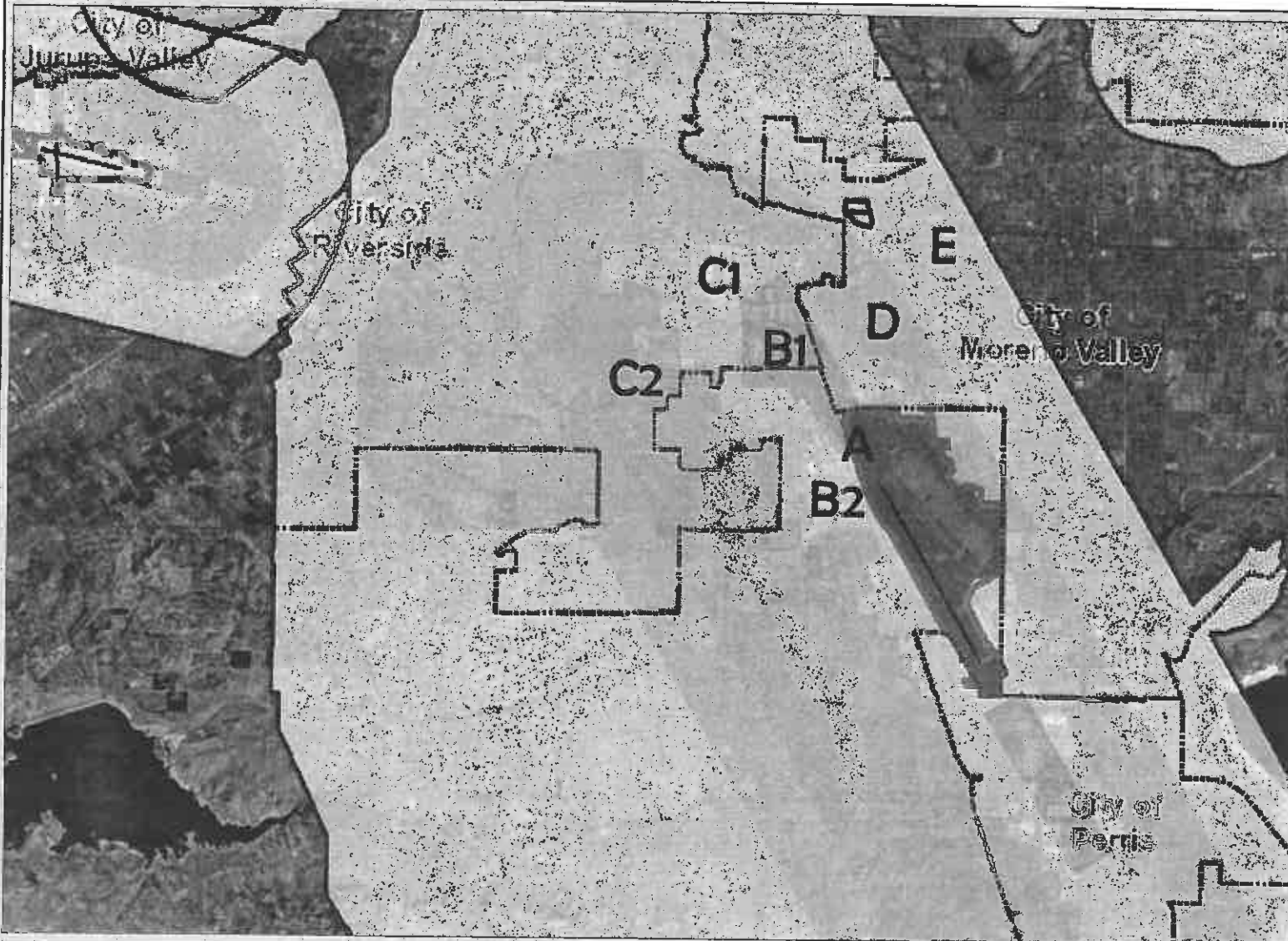
-  Project Site
-  300 ft. Buffer of Project Site
-  Parcels Within 300 ft. of Project
-  Remaining Parcels

Source: Riverside Co. GIS, April 2019.

Parcels Within 300 ft. of Project
 TPM 37750



Map My County Map



Legend

- Runways
- ▣ Airports
- ▭ Airport Influence Areas
- Airport Compatibility Zones
- ▨ OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
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- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6

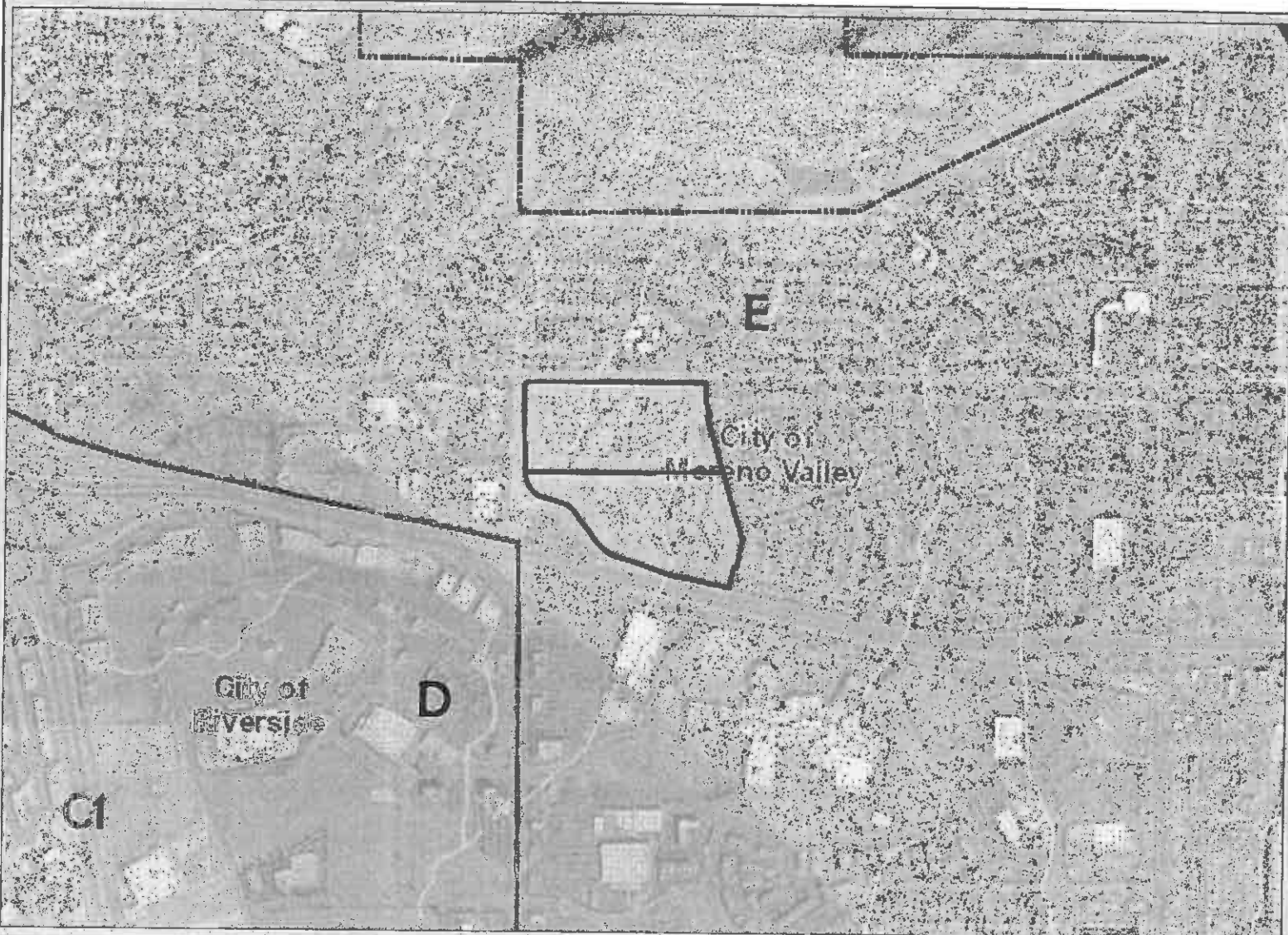


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Notes



Map My County Map



Legend

- Runways
- ▣ Airports
- ▣ Airport Influence Areas
- Airport Compatibility Zones
 - ▨ OTHER COMPATIBILITY ZONE
 - A
 - A-EXC1
 - B1
 - B1-APZ I
 - B1-APZ I-EXC1
 - B1-APZ II
 - B1-APZ II-EXC1
 - B1-EXC1
 - B2
 - B2-EXC1
 - C
 - C1
 - C1-EXC1
 - C1-EXC3
 - C1-EXC4
 - C1-HIGHT
 - C2
 - C2-EXC1
 - C2-EXC2
 - C2-EXC3
 - C2-EXC5
 - C2-EXC6



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Notes



Map My County Map



Legend

- City Areas
- World Street Map



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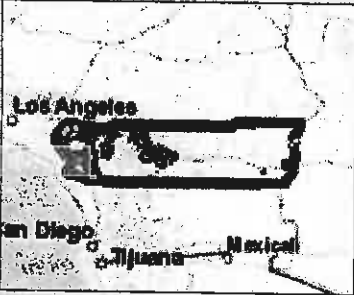


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Notes

Map My County Map



Legend

- Blueline Streams
- City Areas
- World Street Map



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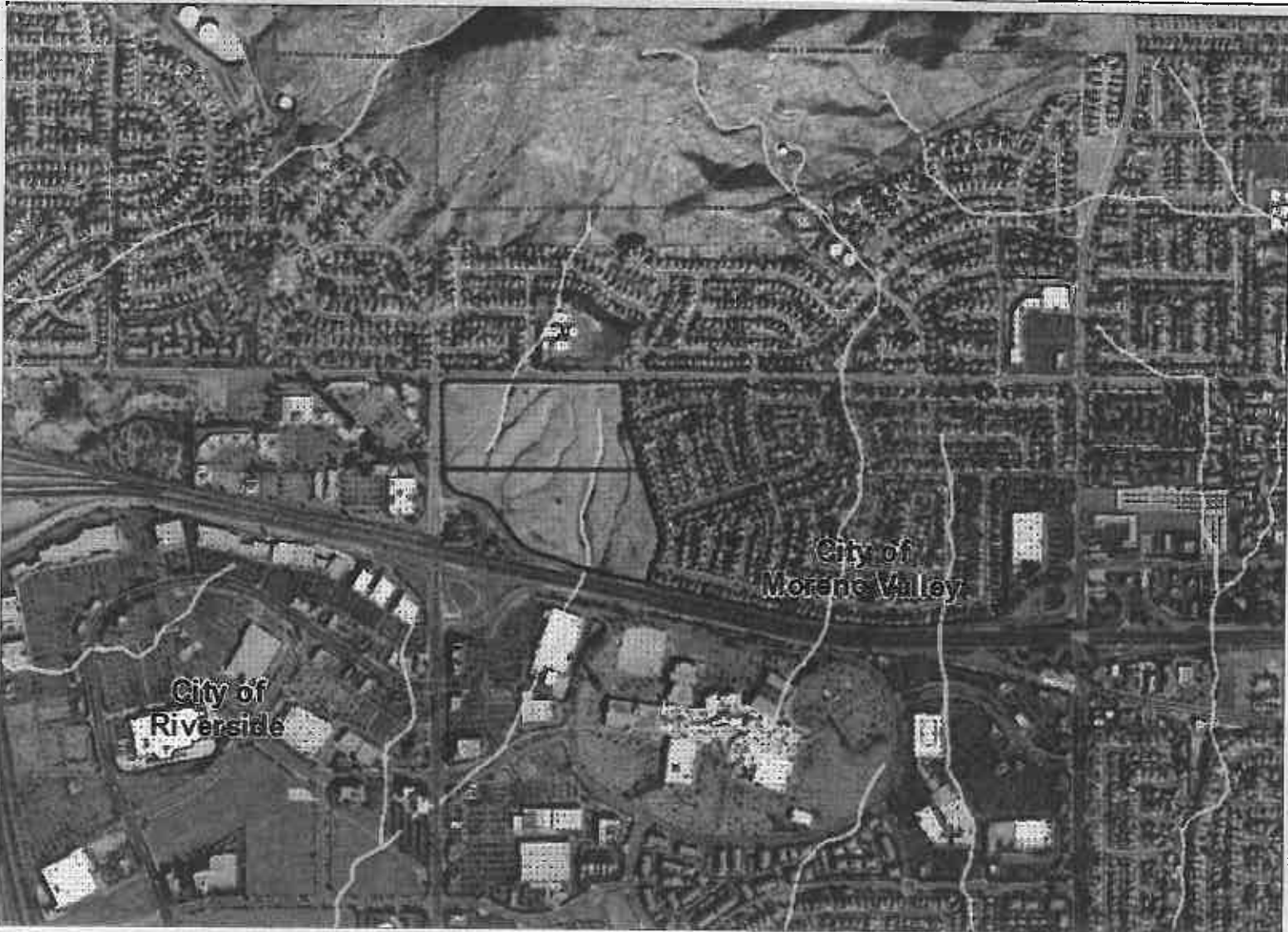
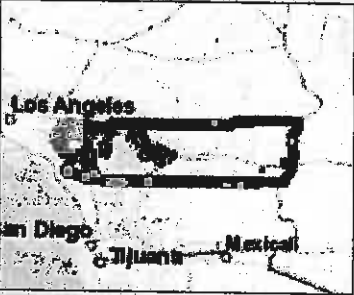


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Notes

Map My County Map



Legend

- Blueline Streams
- ▣ City Areas
- World Street Map



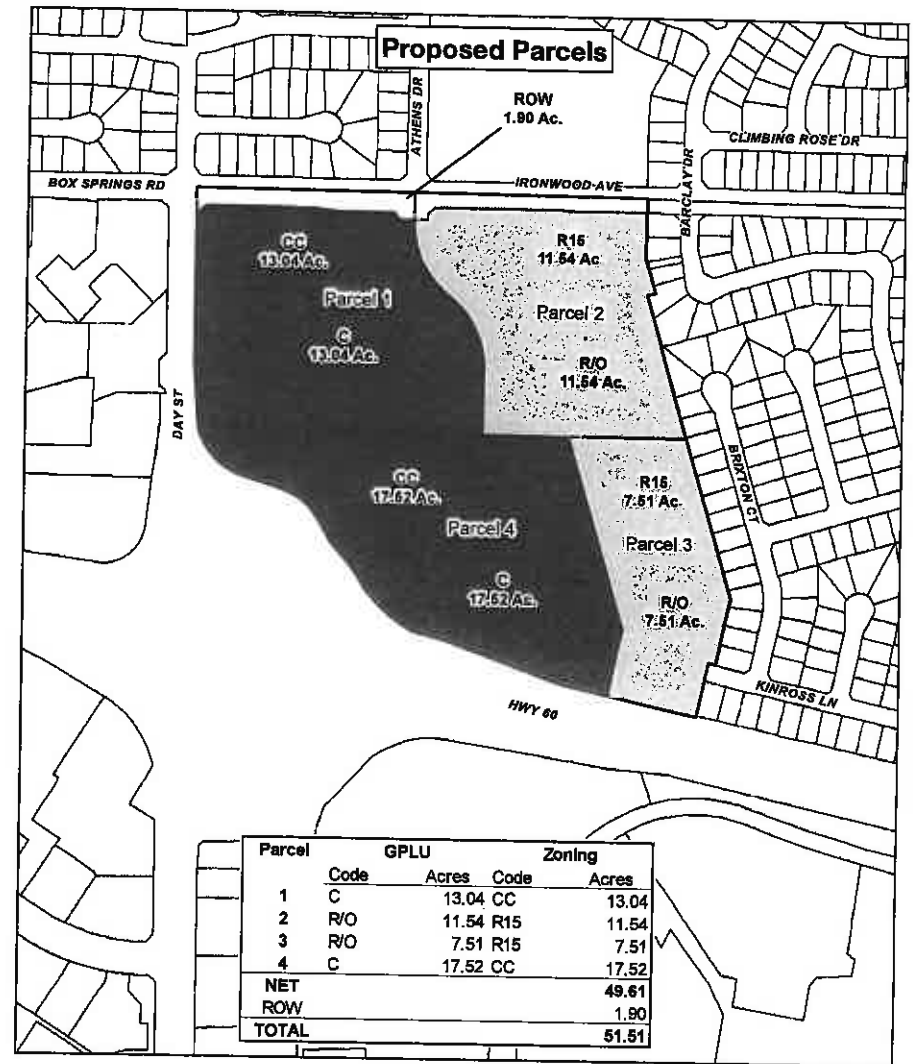
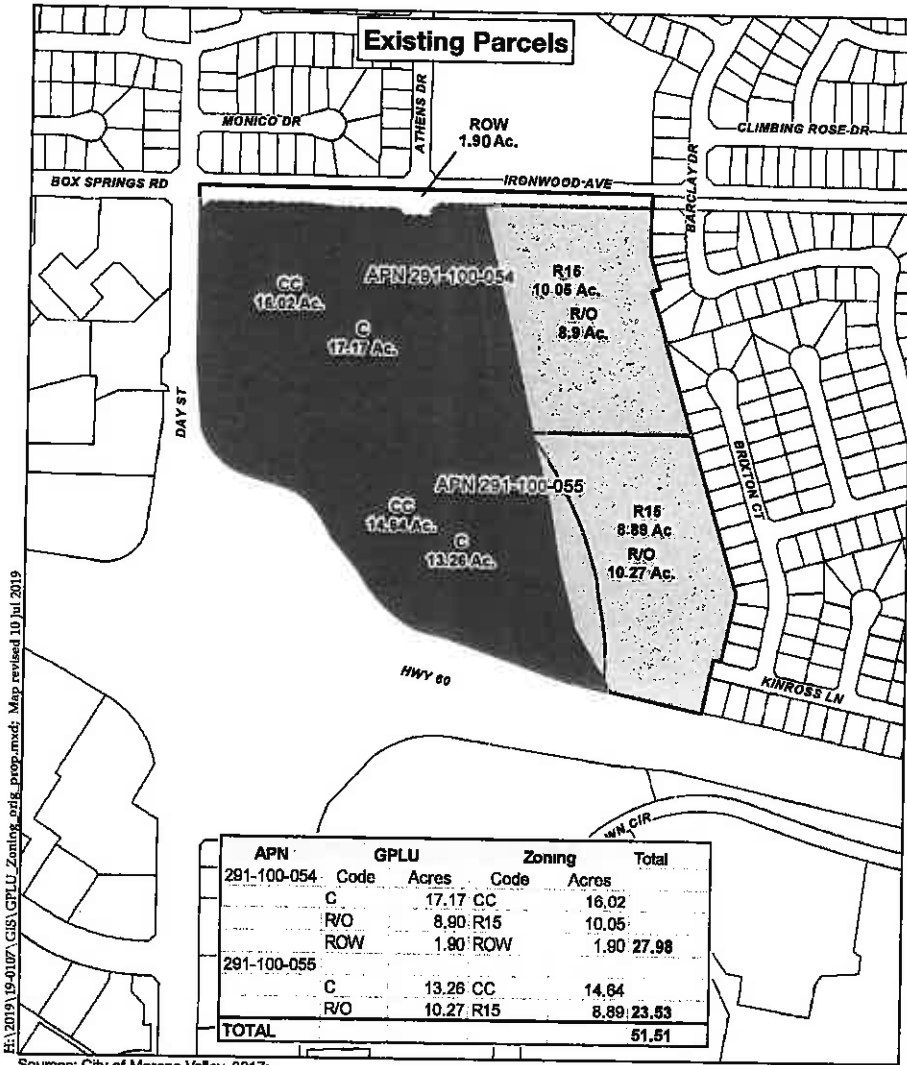
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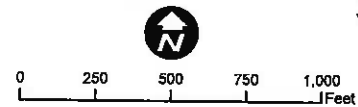
Notes



E:\2019\19-0107\GIS\GPLU_Zoning_orig_prop.mxd; Map revised 10 Jul 2019

Sources: City of Moreno Valley, 2017;
Riverside Co. GIS, 2019

Note: Acreages based on
Webb Survey



APN	Existing Parcels	GPLU Designation
1	Proposed Parcels	R/O Residential/Office
CC	Zoning	C Commercial

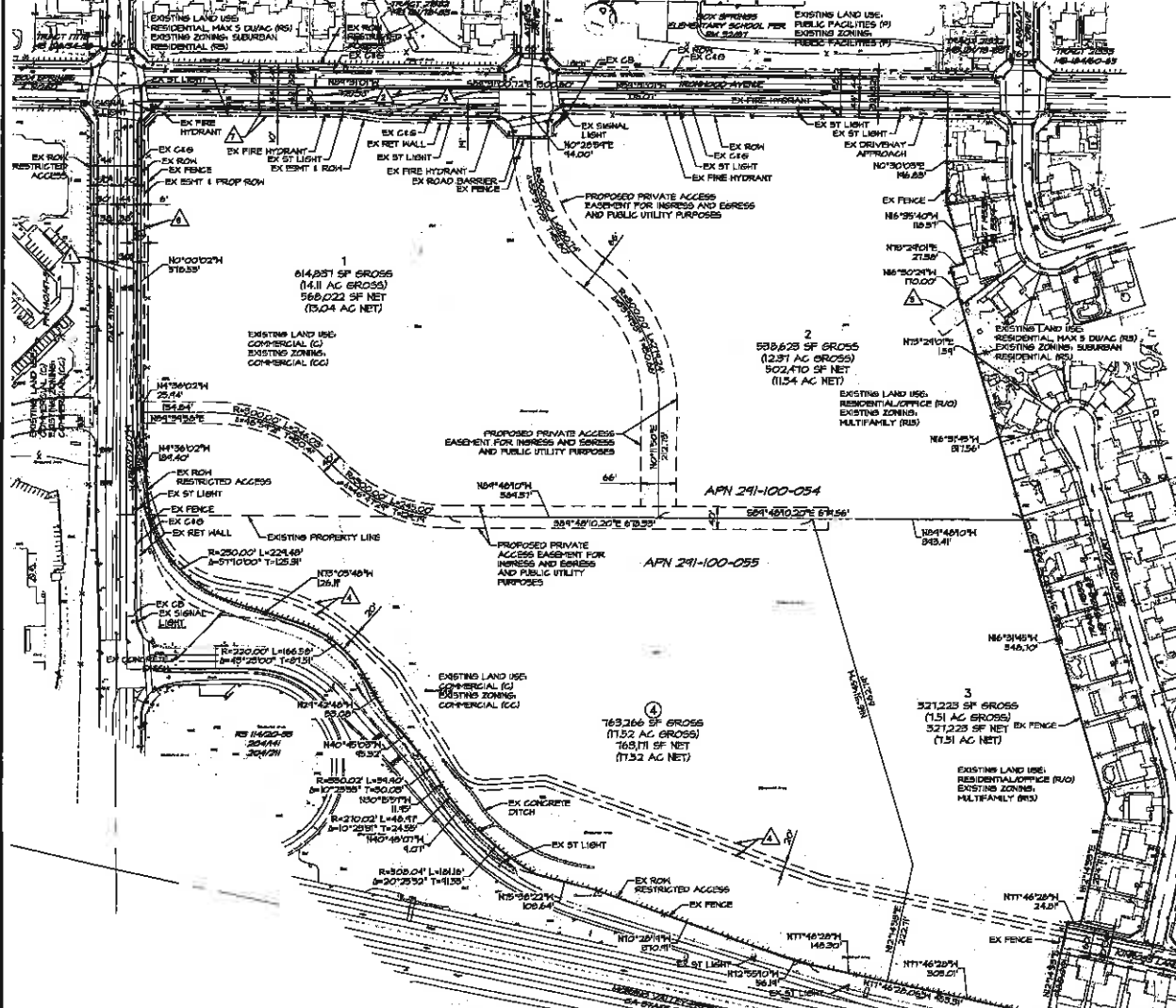
General Plan Land Use and Zoning

TPM 37750



IN THE CITY OF MORENO VALLEY, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA
TENTATIVE PARCEL NO. 37750

LOCATED IN SECTION 2, T. 3 S., R. 4 W., S.B.M.



PARCEL NUMBER	GROSS (SF)	GROSS (AC)	NET (SF)	NET (AC)
1	614,891	14.11	566,222	13.04
2	598,629	13.97	502,410	11.54
3	321,223	7.51	321,223	7.51
4	769,266	17.82	769,211	17.52
TOTAL	2,243,614	53.41	2,160,886	49.61

- OWNER (1 OF 3)**
 MFP DEVELOPMENT COMPANY
 3428 VIA LIDO, SUITE 250
 NEWPORT BEACH, CA 92663
 CONTACT: PAUL G. BERNARD
 PHONE: (949) 728-1100
 PBERNARD@FRITZDADA.COM
- OWNER (2 OF 5)**
 TRIPLE S GROUP, LLC
 3700 HOGAN STREET
 RIVERSIDE, CA 92506
 CONTACT: SCOTT S. HEBB
 PHONE: (951) 686-1070
 SCOTTHEBB@HEBBASSOCIATES.COM
- OWNER (3 OF 5)**
 HEBB LLC
 3700 HOGAN STREET
 RIVERSIDE, CA 92506
 CONTACT: MATTHEW E. HEBB
 PHONE: (951) 686-1070
 MATTHEBB@HEBBASSOCIATES.COM
- OWNER (4 OF 5)**
 IRENE HEBB SALTER, TRUSTEE OF THE BRIEVE HEBB CREDIT TRUST
 3700 HOGAN STREET
 RIVERSIDE, CA 92506
 CONTACT: IRENE SALTER
 PHONE: (951) 686-1070
 IRENE@HEBBASSOCIATES.COM
- OWNER (5 OF 5)**
 FRITZ DADA COMPANY,
 A TEXAS CORPORATION
 8020 NOEL ROAD, LEO, SUITE 2228
 DALLAS, TEXAS 75240
 CONTACT: FRITZ DADA
 PHONE: (949) 728-1100
 FRITZDADA@FRITZDADA.COM

- ABBREVIATIONS**
- APN ASSessor'S PARCEL NUMBER
 - CA CALIFORNIA
 - CB CATCH BASIN
 - G40 CURB AND GUTTER
 - E EAST
 - EX EXISTING
 - EXH EXHIBITION
 - F FIRE
 - FB FIRE BOOK
 - N NORTH
 - PH PARCEL MAP
 - R RAINFALL
 - ROH RECORD OF MAY
 - RS RECORD OF SURVEY
 - S SOUTH
 - SF SQUARE FEET
 - ST STREET
 - T TANGENT
 - WEST

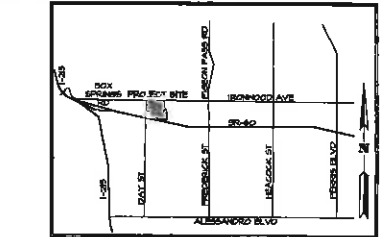
- LEGEND**
- EXISTING RETAINING WALL
 - EXISTING FENCE
 - EXISTING RIGHT OF WAY
 - EXISTING REAL RESTRICTED ACCESS
 - EXISTING EASEMENT
 - EXISTING STREET LIGHT
 - EXISTING FIRE HYDRANT

- EXPLANATIONS**
- NAD 83 1116 100 (PROJ 848554)
 ELEV. = 416.42 (NAVD 20)
 DESCRIBED BY METRO MATR. DIST. SO. CALIFORNIA 1494 (THE)
 RIVERSIDE FROM THE INTERSECTION OF INTERSTATE 215 AND ALEXANDER BLVD ON
 RAMP, PROCEED 0.8 MILE (0.8 MI) WEST ALONG ALEXANDER BLVD FROM 215
 PLY TO ALEXANDER ST, 44 FEET (0.4 MI) SOUTH OF ALEXANDER BLVD AND 20 FEET
 (0.2 MI) EAST OF ALEXANDER TO A POINT 0.2 MI BY 1 FOOT (2) IN CONCRETE
 CATCH BASIN ON SOUTH SIDE OF BLVD. SET A 1/2" DIA 6 INCH BRASS DISK
 STAMPED 10 INCH HIGH FLUSH ON TOP OF THE NORTHEAST CORNER OF CATCH BASIN
 AND 15 FEET (0.5 MI) BACK OF CURB FACE AND 4 INCHES ABOVE ROAD SURFACE.
 REFERENCE H&D PB 4205 OF 022.
 * CONVERSION FACTOR TO SVA 22 (NAVD 20) = -2.87, BASED ON RIV. CO. EN
 44. @ 210 WITH ESTABLISHED ELEVATION OF 864.02'

- GENERAL INFORMATION**
1. THIS MAP IS NOT A PART OF THE RECORD OF SURVEY OF THE LAND DIVIDER.
 2. THIS MAP INCLUDES THE ENTIRE CONTIGUOUS OWNERSHIP OF THE LAND DIVIDER.
 3. PROJECT IS NOT WITHIN A SPECIFIC PLAN.
 4. EASEMENTS OF RECORD ARE PLOTTED HEREON.
 5. PROJECT IS WITHIN MORENO VALLEY ZONE A (PARKS AND COMMUNITY SERVICES) AND ZONE C (CENTRAL STREET LIGHTING) COMMUNITY SERVICES DISTRICTS.
 6. LAND IS NOT WITHIN A SPECIAL STUDIES ZONE.
 7. LAND IS SUBJECT TO LOW LIQUIDATION.
 8. LAND IS SUSCEPTIBLE TO SUBSIDENCE.
 9. NO EXISTING STRUCTURES OR UTILITIES ON SITE.
 10. THE PROJECT IS IN THE SANTA ANA RIVER WATERSHED.
 11. FLOOD ZONE X AREA OF MINIMAL FLOODING PER FEMA PANEL 08060576-46.

SHEET INDEX

SHEET NO.	DESCRIPTION
1	TENTATIVE PARCEL MAP
2	LEGAL DESCRIPTION & STREET SECTIONS



- APPLICANT**
 MFP DEVELOPMENT COMPANY
 3428 VIA LIDO, SUITE 250
 NEWPORT BEACH, CA 92663
 CONTACT: PAUL G. BERNARD
 PHONE: (949) 728-1100
 PBERNARD@FRITZDADA.COM
- ENGINEER**
 ALBERT A. HEBB ASSOCIATES
 3700 HOGAN STREET
 RIVERSIDE, CA 92506
 CONTACT: SARAH KOHALSKI
 PHONE: (951) 686-1070
 SARAHKOHALSKI@HEBBASSOCIATES.COM
- TOPOGRAPHY SOURCE**
 TOPOGRAPHY PLAN BY
 ISLAND AERIAL SURVEYS, INC.
 ON 08/09/2024
- LAND USE**
 EXISTING GENERAL PLAN LAND USE:
 G (COMMERCIAL) AND R-10 (RESIDENTIAL OFFICE)
 EXISTING ZONING:
 G2 (COMMERCIAL) AND R-10 (MULTI-FAMILY)
- APN**
 291-100-054 AND 291-100-055
- ACREAGE**
 53.41 GROSS ACRES
 49.61 NET ACRES
 4 LOTS
- SCHOOL DISTRICT**
 MORENO VALLEY UNIFIED SCHOOL DISTRICT
- UTILITY PROVIDERS**
- WATER: EASTERN MUNICIPAL WATER DISTRICT
 - SEWER: EASTERN MUNICIPAL WATER DISTRICT
 - ELECTRICAL: SOUTHERN CALIFORNIA Edison
 - TELEPHONE: SOUTHERN CALIFORNIA GAS COMPANY
 - CABLE TV: AT&T
 - INTERNET: QUANTER COMMUNICATIONS

PROJECT DESCRIPTION

THIS TENTATIVE PARCEL MAP PROPOSES TO SUBDIVIDE 504 ACRES CONSISTING OF APNS 100-004 AND 100-005 LOCATED AT THE SOUTHWEST CORNER OF DAY STREET AND IRONWOOD AVENUE IN THE CITY OF MORENO VALLEY. THE SITE WILL BE SUBDIVIDED INTO FOUR PARCELS WITH THE INTENT TO NEITHER INCREASE NOR DECREASE THE TOTAL ACREAGE OF G AND R-10 GENERAL PLAN LAND USE DESIGNATIONS. THIS PARCEL MAP IS INTENDED FOR FINANCE AND CONVEYANCE PURPOSES ONLY.

- EASEMENT NOTES**
- 1. AN EASEMENT FOR ROADS AND PUBLIC UTILITIES AND INCIDENTAL PURPOSES PER BOOK 800, PAGE 808 OR, REC. 10/27/1948
 - 2. AN EASEMENT FOR ROADS AND UTILITIES AND INCIDENTAL PURPOSES PER BKT. 9173-39345, REC. 05/16/173
 - 3. 44' DEDICATION FOR PUBLIC ROAD AND FILE PER INST. 9416-28404 OR, REC. 10/27/173
 - 4. 20' R/W DEDICATION PER INST. 9416-18037 OR, REC. 10/26/173
 - 5. IRREVOCABLY DEDICATION IN FIDELITY FOR DRAINAGE PURPOSES PER INST. 9416-17856, REC. 10/26/173
 - 6. AN EASEMENT FOR ROADS AND PUBLIC UTILITIES AND INCIDENTAL PURPOSES PER INST. 9007-30071 OR, REC. 03/28/2007
 - 7. A PERPETUAL EASEMENT AND RIGHT OF WAY FOR PUBLIC ROADWAY PURPOSES AND FILE PER INST. 9204-22061 OR, REC. 6/19/2019

BASIS OF BEARINGS

THE BASIS OF BEARINGS FOR THIS SURVEY IS THE CALIFORNIA STATE PLAN COORDINATE SYSTEM, CGCS, ZONE 6, BASED LOCALLY ON CONTROL STATIONS 741P, AND 779P, NAD 83 REPROJ2011.

GENERAL INFORMATION

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SHEET INDEX

SHEET NO.	DESCRIPTION
1	TENTATIVE PARCEL MAP
2	LEGAL DESCRIPTION & STREET SECTIONS

SCALE 1" = 100'

DATE: 12/25/2024
 DRAWN BY: [Signature]
 CHECKED BY: [Signature]
 PLOTTED BY: [Signature]

FLAT DATE: 25-Nov-19

TENTATIVE PARCEL MAP No. 37760

CITY OF MORENO VALLEY, COUNTY OF RIVERSIDE

PAGE BREAK





AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY

July 25, 2019

CHAIR

Steve Manos
Lake Elsinore

Mr. Manny Baeza, Senior Planner

VICE CHAIR

Russell Betts
Desert Hot Springs

City of Menifee Community Development Department
29844 Haun Road
Menifee CA 92586

COMMISSIONERS

Arthur Butler
Riverside

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW –
DIRECTOR'S DETERMINATION**

John Lyon
Riverside

File No.: ZAP1377MA19

Related File No.: SPA2010-090 (Specific Plan Amendment)

Steven Stewart
Palm Springs

APNs: 329-090-025; 329-090-069 through 329-090-071; 329-090-075;
329-090-076; 329-100-025; 329-100-026; 329-100-030; 329-100-
031; 329-100-033; and 329-100-035.

Richard Stewart
Moreno Valley

Gary Youmans
Temecula

Dear Mr. Baeza:

STAFF

Director

Simon A. Housman

John Guerin

Paul Rull
Barbara Santos

County Administrative Center
4080 Lemon St., 14th Floor
Riverside, CA 92501
(951) 955-5132

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to ALUC Resolution No.15-01 (as adopted on August 13, 2015), staff reviewed City of Menifee Case No. SPA2010-090 (Specific Plan Amendment), a proposal to amend the land use designations within a portion of Specific Plan No. 260 (SP260 A3) Menifee North Specific Plan located in the City of Menifee, specifically the portion located northerly of State Highway Route 74, westerly of Menifee Road, and easterly of Palomar Road. (The Specific Plan was initially approved by the County of Riverside prior to incorporation of the City of Menifee, and a portion of the Specific Plan lies outside City limits.) The area proposed for amendment constitutes Planning Areas 11 through 14, which are currently proposed to provide 28.3 acres of Business Park uses (Planning Areas 11 and 12), 14.6 acres of Commercial/Business Park uses (Planning Area 13), and 11.7 acres of Commercial uses (Planning Area 14). (In addition, there is a 9.12-acre Southern California Edison ["SCE"] transmission line easement.) The proposed amendment would provide for 22.03 acres of Commercial uses, 24.43 acres of Very High Density Residential uses, and 7.66 acres that could be developed with either Commercial or Very High Density Residential uses, excluding land within the SCE easement.

www.rcauc.org

Planning Areas 11 through 14 would be reconfigured as Planning Areas 11A, 11B, 12A, 12B, 13A, 13B, and 14. Junipero Road would separate Planning Areas 11A from 11B, 12A from 12B, and 13A from 13B. (Technically, the SCE easement would be included within Planning Areas 11B, 12B, and 13B, although that area would not be available for development of residential or commercial uses.) Planning Areas 11A and 11B would be designated for Very High Density Residential uses, and Planning Areas 13A and 13B would be designated for Commercial uses. Planning Areas 12A and 12B would be designated to allow for either Commercial or Very High Density Residential land uses. The overall dwelling unit count for the portion of the Specific Plan within the City of Menifee would be capped at 1,506 dwelling units on 202.6 acres, with a

AIRPORT LAND USE COMMISSION

density of 7.4 dwelling units per acre within the Planning Areas allowing for residential development. As amended, the portions of the Specific Plan within the City of Menifee would provide 126.39 acres of Commercial area (if Planning Areas 12A and 12B are utilized for commercial development) [an increase of 18 acres], 36 acres of Commercial/Business Park area (a decrease of 14.6 acres), 197.5 acres of Industrial area, 8.7 acres of Schools area, and 24.5 acres of Community Park area.

The site is located within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area, residential density and non-residential intensity are not restricted.

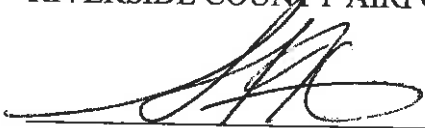
As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan ("March ALUCP"), provided that the City of Menifee incorporates in the text of the amended Specific Plan an acknowledgement that the Specific Plan is located within Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area and that subsequent underlying entitlements will be reviewed in light of the then-applicable Airport Land Use Compatibility Plan.

This finding of consistency relates to airport compatibility issues and does not necessarily constitute an endorsement of the proposed Specific Plan Amendment. As the site is located within Compatibility Zone E, both the existing and proposed Specific Plan land use designations are consistent with the March ALUCP.

If you have any questions, please contact Paul Rull, ALUC Principal Planner, at (951) 955-6893.

Sincerely,

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Simon A. Housman, ALUC Director

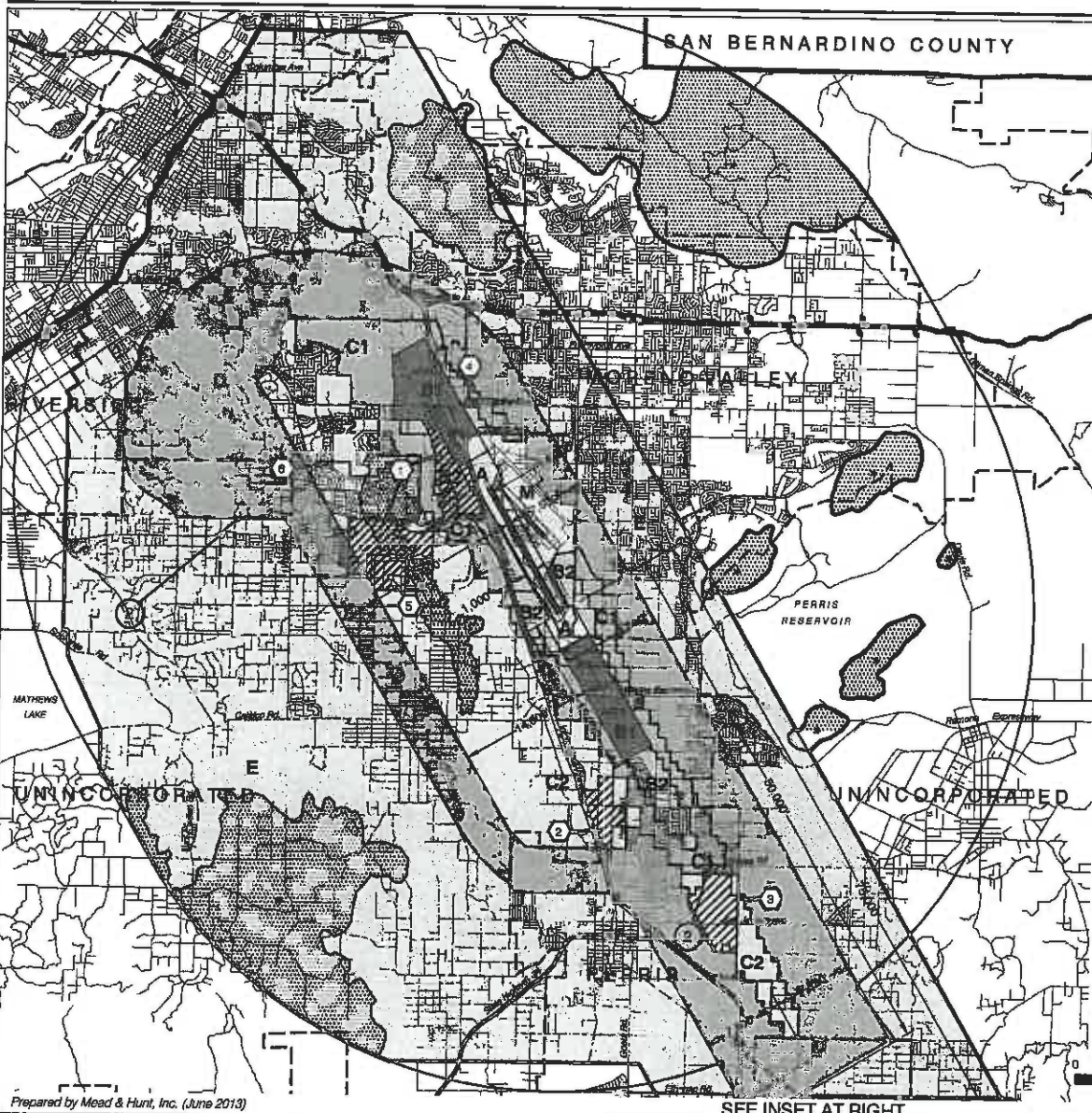
Attachments: Notice of Airport in Vicinity

cc: M.V. Ashley, Romola General Partnership/Malaga 74, LLC (applicant/landowner)
Carissa Hainsworth, Mike Naggar Associates, Inc. (representative)
Georgiann L. and G.L. Psaros (additional landowners)
Optimus Building Corporation (fee-payer) (Calgary address)
Gary Gosliga, Airport Manager, March Inland Port Airport Authority
Daniel Rockholt or Denise Hauser, March Air Reserve Base
ALUC Case File

Y:\AIRPORT CASE FILES\March\ZAP1377MA19\ZAP1377MA19SPA.LTR.doc

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



LEGEND

Compatibility Zones

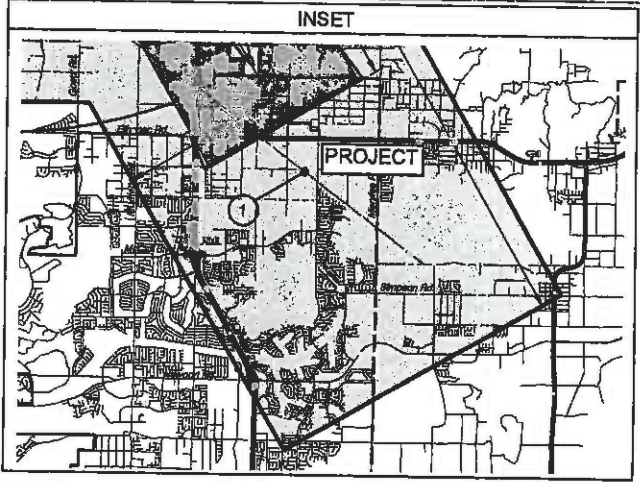
- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

- ① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.

Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- ▨ Site-Specific Exceptions (existing local agency commitments to development projects)

- ① March JPA: March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA: Ben Clark Training Center
- ⑥ Riverside: Ridge Crest Subdivision



Note:
All dimensions are measured from runway ends and centerlines.



0 2 4 MILES

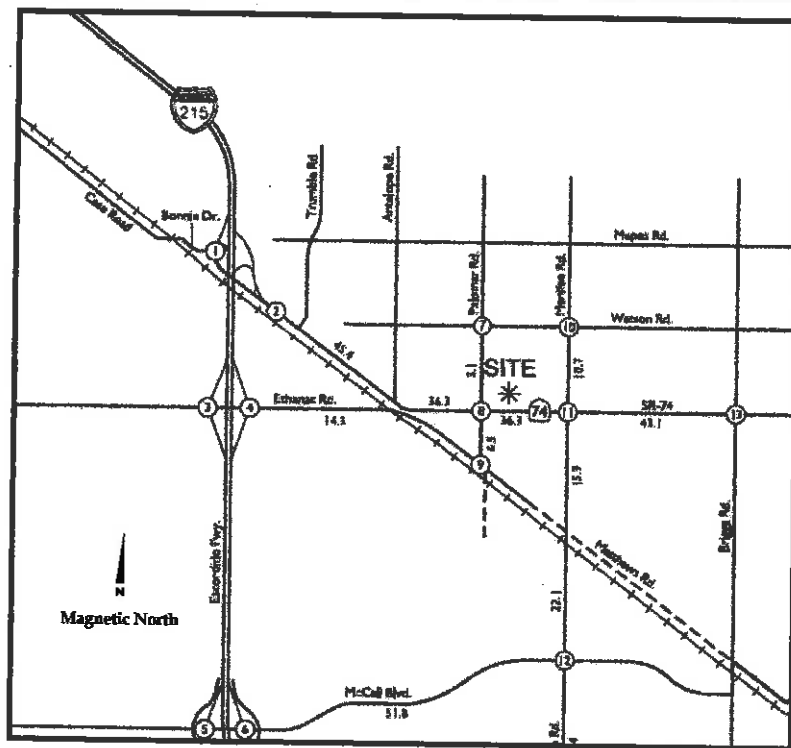
Base map source: County of Riverside 2013

**Riverside County
Airport Land Use Commission**
**March Air Reserve Base / Inland Port Airport
Land Use Compatibility Plan**
(Adopted November 13, 2014)

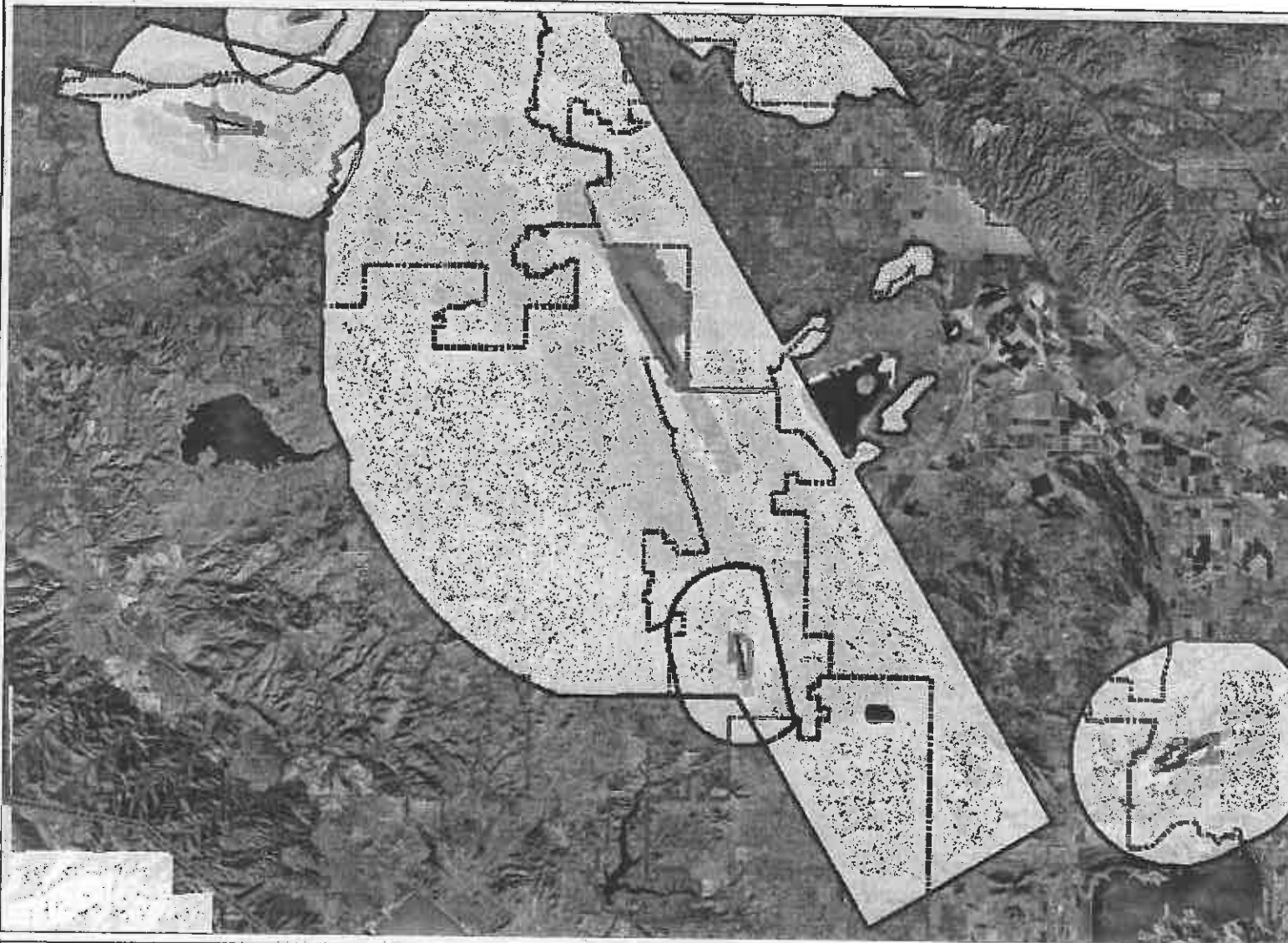
Map MA-1

Compatibility Map
March Air Reserve Base / Inland Port Airport

SEE INSET AT RIGHT



Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones
 - ▨ OTHER COMPATIBILITY ZONE
 - A
 - A-EXC1
 - B1
 - B1-APZ I
 - B1-APZ I-EXC1
 - B1-APZ II
 - B1-APZ II-EXC1
 - B1-EXC1
 - B2
 - B2-EXC1
 - C
 - C1
 - C1-EXC1
 - C1-EXC3
 - C1-EXC4
 - C1-HIGHT
 - C2
 - C2-EXC1
 - C2-EXC2
 - C2-EXC3
 - C2-EXC5
 - C2-EXC6



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

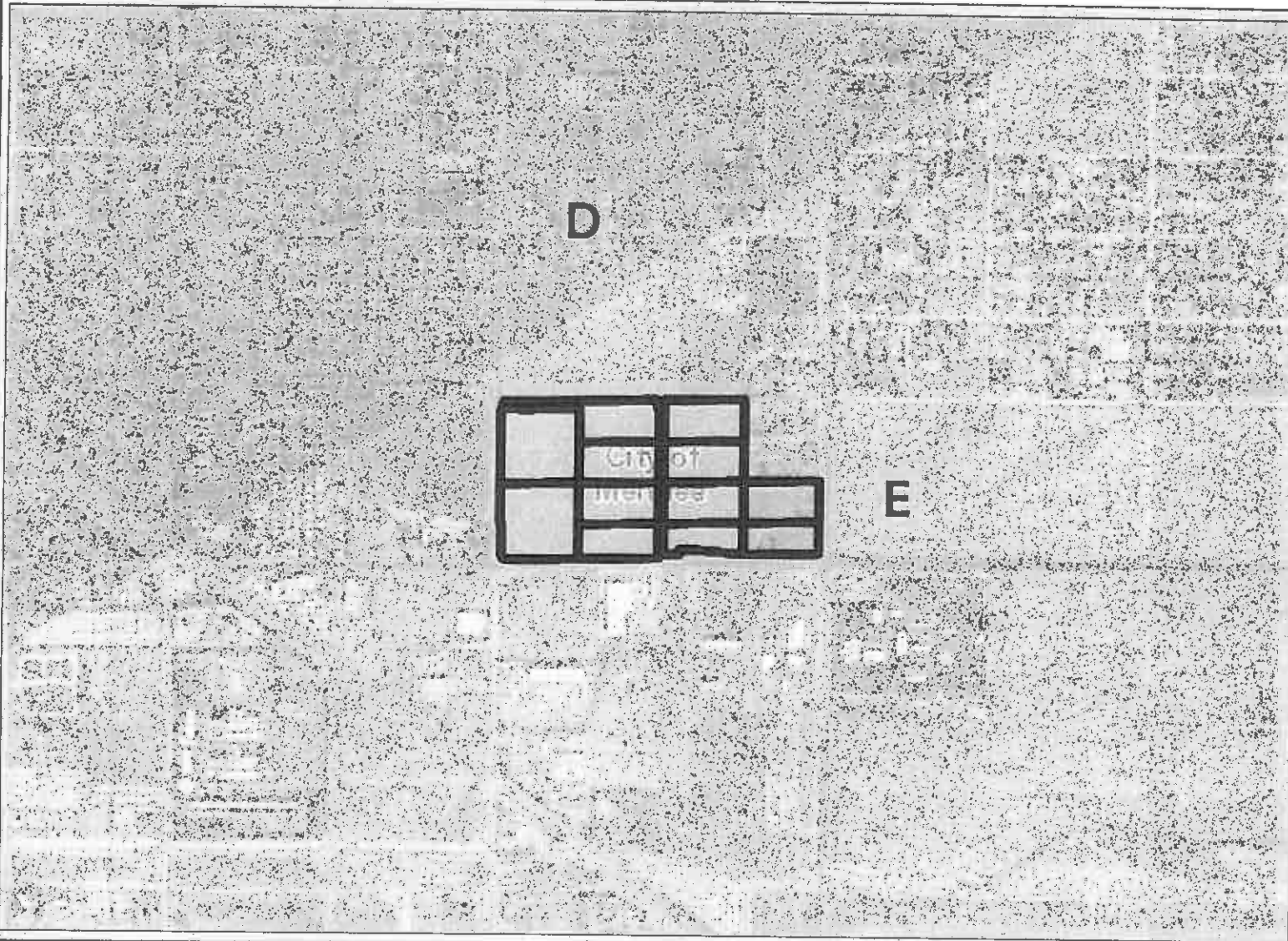
Notes



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Map My County Map



- ### Legend
- Runways
 - ▣ Airports
 - ▣ Airport Influence Areas
 - Airport Compatibility Zones
 - ▨ OTHER COMPATIBILITY ZONE
 - A
 - A-EXC1
 - B1
 - B1-APZ I
 - B1-APZ I-EXC1
 - B1-APZ II
 - B1-APZ II-EXC1
 - B1-EXC1
 - B2
 - B2-EXC1
 - C
 - C1
 - C1-EXC1
 - C1-EXC3
 - C1-EXC4
 - C1-HIGHT
 - C2
 - C2-EXC1
 - C2-EXC2
 - C2-EXC3
 - C2-EXC5
 - C2-EXC6



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Notes





Map My County Map



Legend

- City Areas
- World Street Map

Notes

0 12 24,254 Feet

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Map My County Map



- Legend**
- Blueline Streams
 - ▣ City Areas
 - World Street Map



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Notes

Map My County Map



Legend

- Blueline Streams
- City Areas
- World Street Map



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Notes

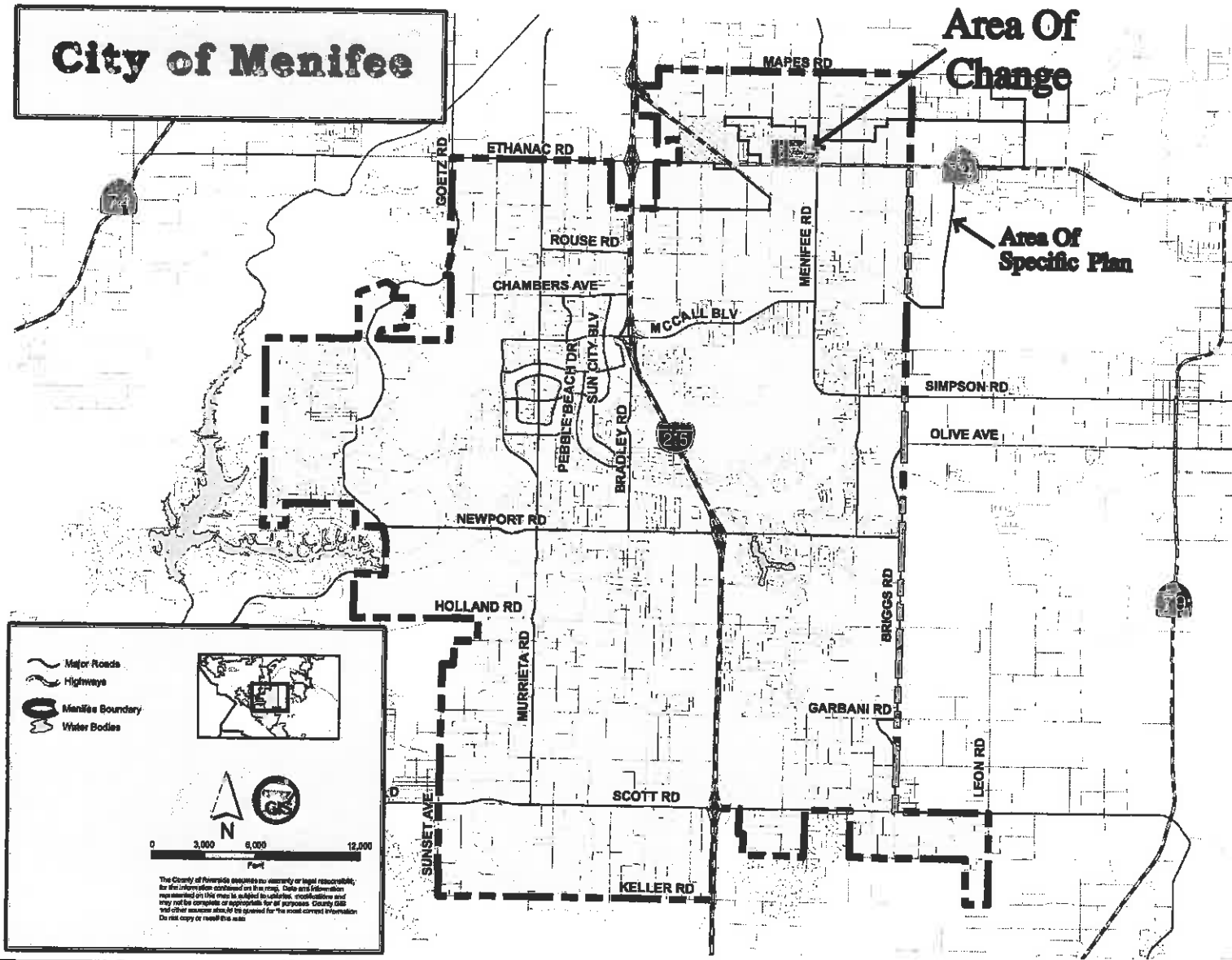


FIGURE III-B
City of
Menifee
Boundary
Map

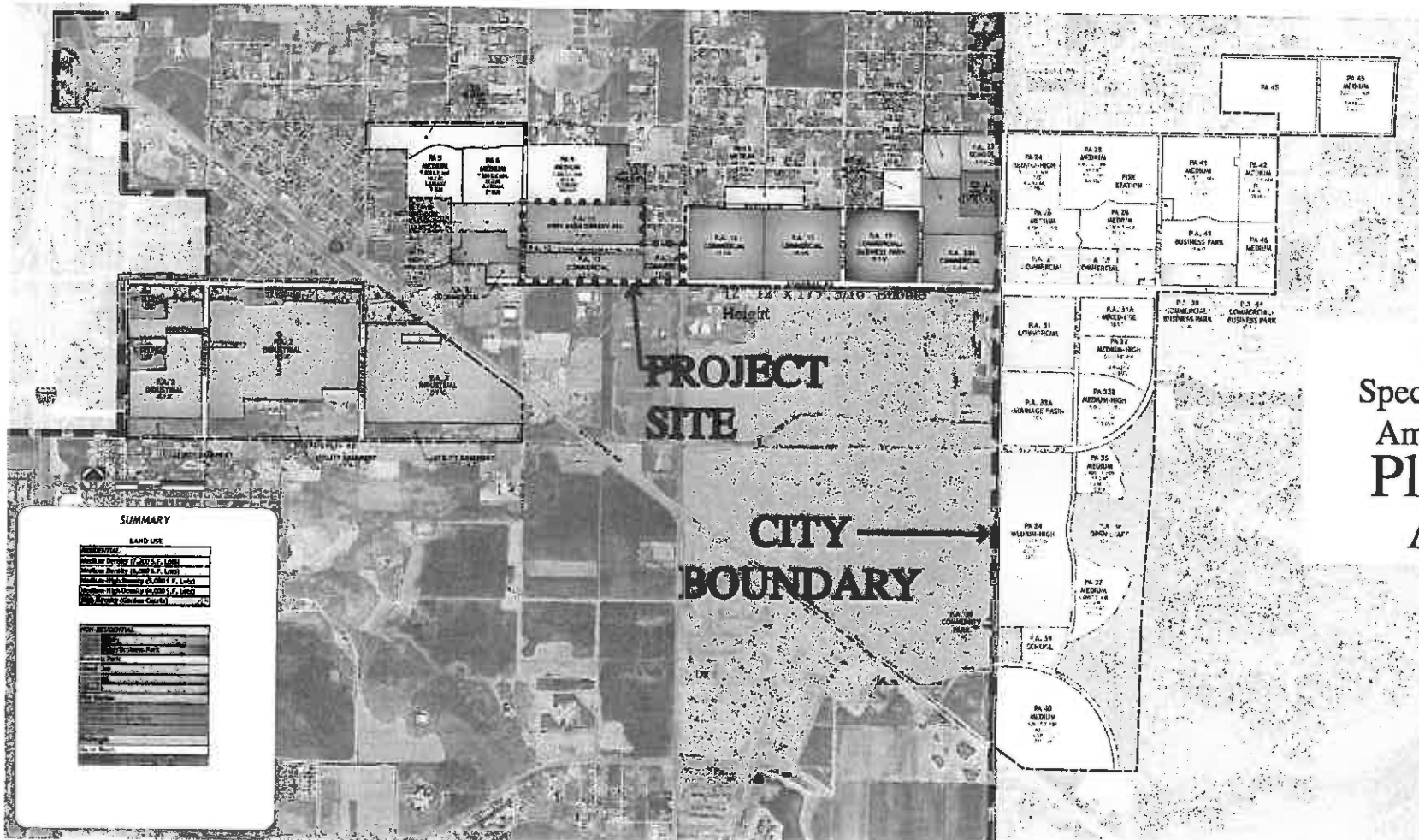


FIGURE III - A
Specific Plan 260
Amendment 3
Planning
Areas

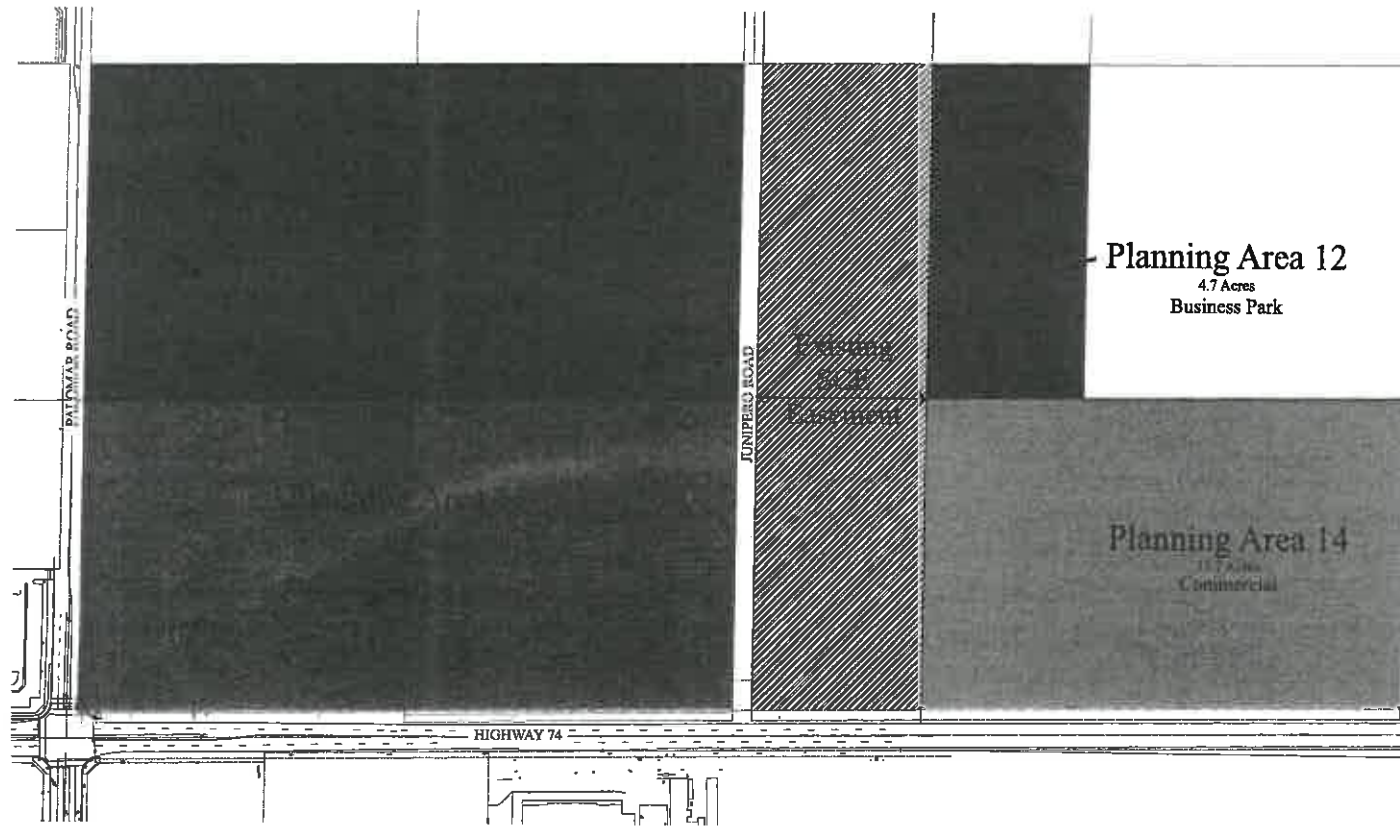
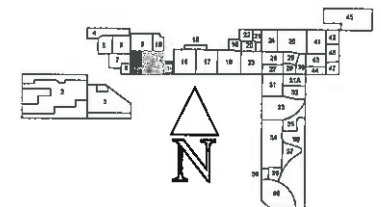


FIGURE III-1

Existing Land Use Plan

- Planning Area 11
- Planning Area 12
- Planning Area 13
- Planning Area 14



8. Project Description:

Specific Plan No. 260, Amendment No. 3 (SP260, A3) proposed the following modifications to the Specific Plan Land Use Plan Planning Areas (PA):

- Planning Area 11 (PA11) would be re-designated from Business Park land uses to Very High Density Residential and would be split into two (2) subareas, 11A and 11B. Subarea 11A has an area of 19.56 acres and is located west of Junipero Road. Subarea 11B has an area of 9.79 acres and is located east of Junipero Road and will include a portion of the existing Southern California Edison (SCE) easement that had not previously been given a specific planning area designation.
- Planning Area 12 (PA12) would be realigned to a newly created area between PA11 and PA13 and re-designated from the current Business Park and Commercial Business Park land use to Commercial / Very High Density Residential land uses. Two (2) subareas are proposed, 12A and 12B. Subarea 12A has an area of 6.14 acres and is located west of Junipero Road. Subarea 12B has an area of 3.06 acres and is located east of Junipero Road and includes a portion of the existing SCE easement that had not previously been given a specific planning area designation.
- Planning Area 13 (PA13) would be re-designated from Commercial Business Park to Commercial and would be split into two (2) subareas, 13A and 13B. Subarea 13A has an area of 10.23 acres and is located west of Junipero Road. Subarea 13B has an area of 5.19 acres and is located east of Junipero Road and includes a portion of the existing SCE easement that had not previously been given a specific planning area designation.
- Planning Area 14 (PA14) would retain a Commercial designation but would be reduced in acreage from 11.7 to 9.27 by redistributing areas into Planning Areas 12B and 13B.

Reference Figure 5a, *Existing Specific Plan Land Use Plan* and Figure 5b, *Proposed Specific Plan Land Use Plan*.

Detailed descriptions of each change that is proposed by SP 260, A3 are provided in Table 1, *SP260, A3 Land Use Summary*, below.

It should be noted that, as a worst-case scenario, 246,312 square feet of commercial uses and 637 multi-family dwelling units were utilized in the analysis of this Initial Study.

The existing SCE easement is being included within Planning Areas 11, 12 and 13 in this amendment. Development will have to conform with all applicable SCE easement restrictions. The easement area shall be allowed to be used in required landscape and open space areas, retention and detention basins, and for passive recreation uses.

Upon approval of SPA 260, A3, total dwelling unit count shall increase by 721 units, based on maximum potential dwelling units in Planning Areas 11 and 12.

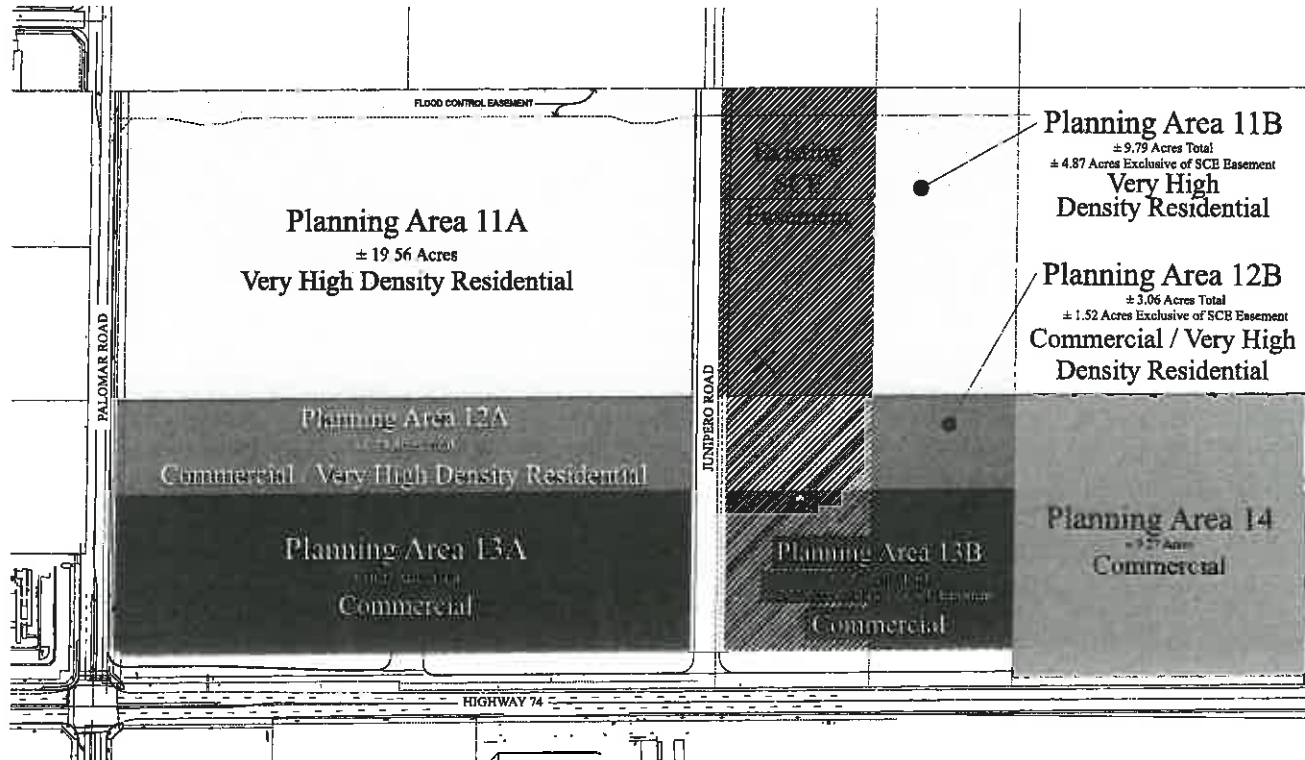
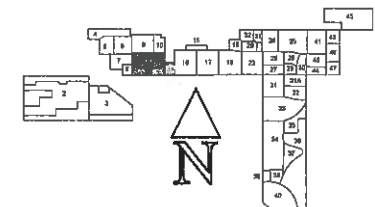


FIGURE III-1A

Proposed Land Use Plan

- Planning Area 11
- Planning Area 12
- Planning Area 13
- Planning Area 14

Area	Acres +/-	Easement Acres +/-	Balance of Usable Acres
PA 11A	19.56	0	19.56
PA 11B	9.79	4.92	4.87
PA 12A	6.14	0	6.14
PA 12B	3.06	1.54	1.52
PA13A	10.23	0	10.23
PA13B	5.19	2.66	2.53
PA 14	9.27	N/A	9.27



Bold Entries are Added or Modified with Amendment 3
~~Strikethru Entries~~ are for Planning Areas outside the City of Menifee

DESIGNATION	PLANNING AREA	ACREAGE WITHIN CITY OF MENIFEE	ACREAGE OUTSIDE CITY OF MENIFEE	TARGET DENSITY WITHIN CITY OF MENIFEE	DWELLING UNITS WITHIN CITY OF MENIFEE
RESIDENTIAL					
Medium – 7,200 SF min.	4	21.8		3.5	76
	5 ⁽¹⁾	18.3		3.7	68
	6 ⁽²⁾	24.6		3.7	91
	9 ⁽¹⁾	30.4		3.5	106
	15	10.6		3.0	32
	18	10.3		3.0	31
	41		34.3	3.5	
	45		74.8	3.5	
	46		20.9	3.7	
SUBTOTAL – MEDIUM - 7,200 SF MIN.		116.0	130.0	3.5	404
Medium – 6,000 SF min.	25		45.1	4.5	
	32 ⁽³⁾		21.8	4.5	
	35		19	4.5	
	37		20.6	4.5	
	40		60.4	4.5	
SUBTOTAL – MEDIUM - 6,000 SF MIN.			166.9	4.5	
Medium High – 5,000 SF min.	22	11.3		5.0	56
	24		22	5.0	
	33 ⁽³⁾⁽⁴⁾		57.7	4.5	
	34 ⁽³⁾⁽⁴⁾		75.2	4.5	
SUBTOTAL – MEDIUM HIGH - 5,000 SF MIN.		11.3	154.9	5.0	56
MEDIUM HIGH – 4,000 SF min.	7A	15.2		5.6	85
HIGH DENSITY – Garden Courts	7B	11.9		7.3	87
	23A	18.1		8.5	153
SUBTOTAL – HIGH DENSITY - Garden Courts		30.0		8.0	240
VERY HIGH DENSITY – 14.1 – 24 DU/AC	11	24.43		24	586
	12 ⁽⁵⁾	5.63		24	135
SUBTOTAL – VERY HIGH DENSITY - 14.1 – 24 DU/AC		30.06		24	721
SUBTOTAL RESIDENTIAL		202.6	451.8	7.4	1506

⁽¹⁾ The maximum density may be increased to 6.0 du/ac with a 5,000 square foot lot minimum in either of the following two circumstances:

- a. The project is designed for and restricted to senior citizen housing, or
- b. The project is a mobile home park or mobile home subdivision

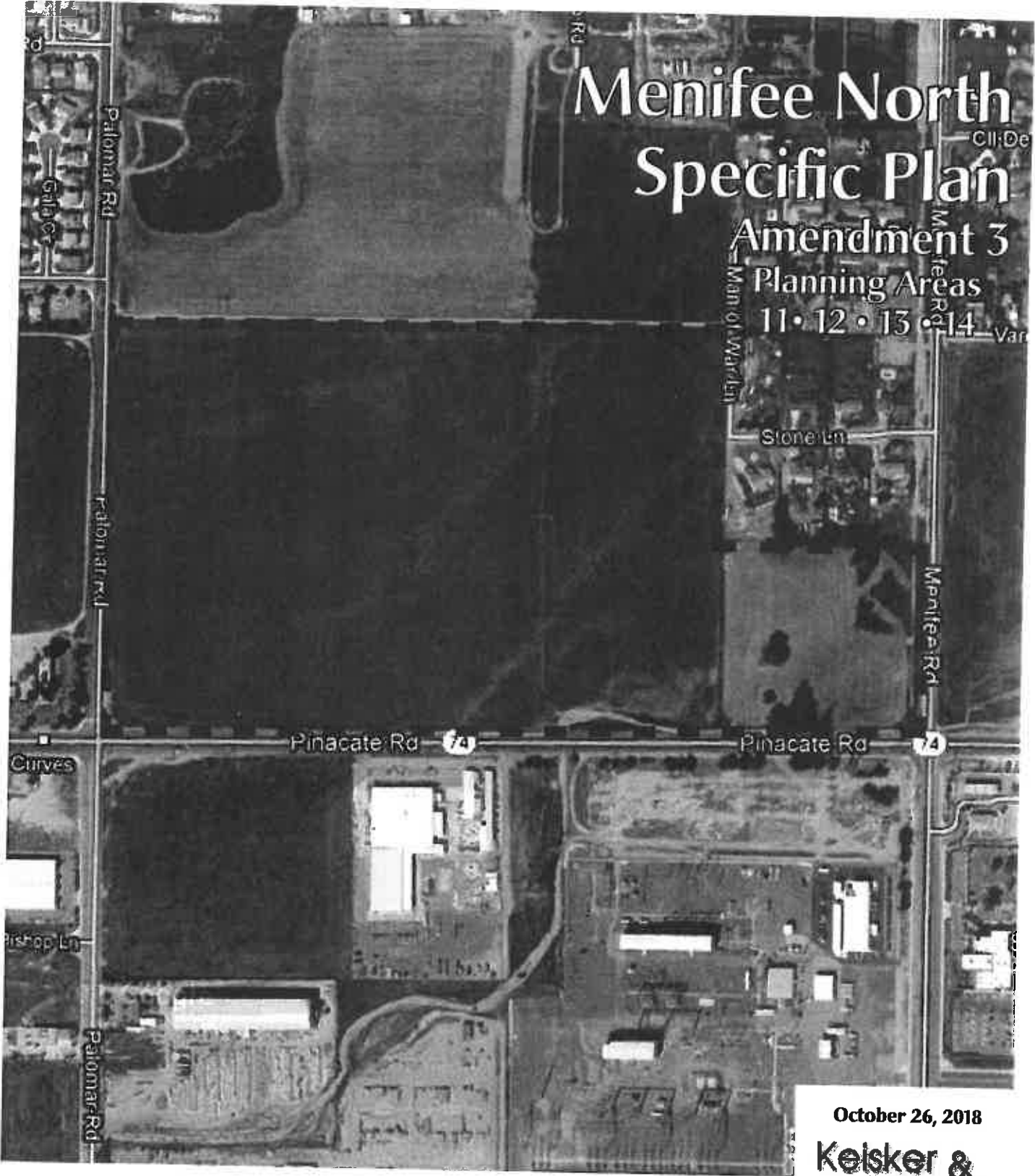
This standard applies to Planning Areas 5, 6, 9, 32, 33 and 34. If this option is elected, the maximum number of dwelling units shown in Table II may be exceeded up to a new maximum which is listed in the Planning Area description for the relevant Planning Area. The overall maximum number of dwelling units for the entire Specific Plan may not exceed 2,815.

⁽²⁾ The density shown for this planning area is 4.5 du/ac even though the minimum lot size is 5,000 square feet. The overall number of units for this planning area is restricted to require that several neighborhoods of differing lot sizes be developed.

⁽³⁾ PA 12A can be either maximum 67% residential or 100% commercial. PA 12B can be either maximum 100% residential or 100% commercial. Density shown here is the maximum allowed (67% of 12A + 100% 12B) not including the area of the SCE Easement.

DESIGNATION	PLANNING AREA	ACREAGE WITHIN CITY OF MENIFEE	ACREAGE OUTSIDE CITY OF MENIFEE	TARGET DENSITY WITHIN CITY OF MENIFEE	DWELLING UNITS WITHIN CITY OF MENIFEE
NONRESIDENTIAL					
Business Park	11	19.0		-	-
	12	4.0		-	-
	26		21.0	-	-
	28		12.6	-	-
	43		17.6	-	-
SUBTOTAL - BUSINESS PARK		0.0	51.2	-	-
Commercial	8	3.3		-	-
	12 ⁽⁵⁾	7.66		-	-
	13 ⁽⁵⁾	12.76		-	-
	14	9.27		-	-
	16	35.3		-	-
	17	34.4		-	-
	23B	23.7		-	-
	27		11.8	-	-
29		8.1	-	-	
31		32.4	-	-	
SUBTOTAL - COMMERCIAL		126.39	52.3	-	-
Mixed Use/Neighborhood Commercial	31A		18.8	-	-
Commercial/Business Park ⁽⁵⁾	13	14.6		-	-
	19	36.0		-	-
	30		13.2	-	-
	44		10.4	-	-
	47		10.9	-	-
SUBTOTAL - COMMERCIAL/BUSINESS PARK		36.0	53.3	-	-
Industrial	2	121.1		-	-
	3	76.4		-	-
SUBTOTAL - INDUSTRIAL		197.5		-	-
Schools	21	8.7		-	-
	39		10.0	-	-
	42		10.0	-	-
SUBTOTAL - SCHOOLS		8.7	20	-	-
Community Parks	10	12.5		-	-
	20	12.0		-	-
	38 ⁽⁵⁾		5 ⁽⁵⁾	-	-
SUBTOTAL - COMMUNITY PARKS		24.5	9	-	-
Open Space	36			-	-
Fire Station	-			-	-
SUBTOTAL NONRESIDENTIAL		393.1	185.8		
PROJECT TOTALS		529.6	751.3		
Drainage Channels	-		15.9	-	-
Utility Easements/Existing Uses	-		111.4	-	-
Major Roadways	-		164.1	-	-

⁽⁵⁾ Not including the area of the SCE Easement



Menifee North Specific Plan

Amendment 3 Planning Areas 11 • 12 • 13 • 14

Prepared for:
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Nuevo, CA 92567

October 26, 2018
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MENIFEE NORTH SPECIFIC PLAN No. 260

AMENDMENT No. 3 Planning Areas 11, 12, 13 & 14

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October 26, 2018

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BOLD ENTRIES INDICATE SECTIONS REVISED IN THIS AMENDMENT 3

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AMENDMENT No. 3 TO MENIFEE NORTH SPECIFIC PLAN No. 260

SUMMARY OF CHANGES

Proposed Amendment No. 3 is required to accommodate modifications to the Land Use Plan for approved MENIFEE NORTH Specific Plan No. 260, Amendments Nos.1 and 2. Refer to the Existing Specific Plan Land Use Plan and Proposed Specific Plan Amendment No. 3 Land Use Plan for a graphic representation of the changes of use proposed for Planning Areas 11, 12, and 13 and the change in size of Planning Area 14.

Since the original Specific Plan was approved by the County of Riverside, the City of Menifee was incorporated on October 1, 2008. This Amendment No. 3 proposes to amend four Planning areas, 11, 12, 13 and 14, that are within the City of Menifee. Additionally, this amendment is also separating those portions of the Specific Plan inside the City and in unincorporated areas. Refer to Table II for totals within and outside the City of Menifee. These amendments do not affect any portion of the Specific Plan that is still within the County of Riverside jurisdiction. Refer to the maps, Figures III –A and III–B.

The Ethanac Corridor Planning Group (ECPG) was formed by multiple stakeholders within the Harvest Valley/Winchester Area Plan of Riverside County to create a guiding vision for the western gateway and urban node connecting Interstate 215 with the inland City of Hemet.

Based on the evaluation and analysis of the existing conditions within the study area, the ECPG has identified numerous issues that will affect the overall development of the corridor. The ECPG Summary of Issues states the following:

Land Uses:

1. Location and designation of land uses
2. Placement of commercial land uses
3. Excess of industrial and commercial/business park land uses
4. Shortage of high-density residential land uses

The *Ethanac Corridor Planning Group Summary Document* provides recommendations that are intended to give guidance for future Land Use policies within this Specific Plan Area. The ECPG Conclusions and Recommendations states the following:

Land Uses:

A1.Create a diverse community with integrated land uses that will result in a balanced, full service, land use plan for a community where people can live, work, shop, secure services, and recreate.

A2.Locate community or neighborhood commercial centers within the Core Area and regional centers adjacent to Interstate 215, thereby providing opportunities for retail, entertainment, and service uses within proximity to residential and office developments, and high volume travel routes.

A3.Create an overlay for industrial land uses in the future to accommodate growth within the corridor, land use compatibility, and expansion of regional transit facilities.

A4.Create a range of housing opportunities and choices by diversifying the residential housing types within the corridor, including traditional single- family homes, courtyard

homes, multi-family attached units, and apartments.

This amendment will create multi family housing opportunities, and eliminate the business park designation in favor of retail and commercial uses. Amendment No.3 requests to make the following changes:

- 1) Redesignate Planning Area 11 from Business Park land uses to Very High Density Residential. PA 11 will also incorporate a portion of the existing Southern California Edison easement that is currently not in a Planning Area. Planning Area 11 increases in size from 23.6 Acres to 29.35 Gross Acres, 24.43 Net Acres (excluding easement).
- 2) Redesignate Planning Area 12 from Business Park To Commercial / Very High Density Residential land uses. Planning Area 12 will be increased in area from 4.7 Acres to 9.2 Acres Gross, 7.66 Net Acres (excluding easement), by reducing the size of Planning Areas 13 and 14, and incorporating a portion of the existing Southern California Edison easement that is currently not in a Planning Area.
- 3) Redesignate Planning Area 13 from Commercial Business Park to Commercial. PA 13 will also incorporate a portion of the existing Southern California Edison easement that is currently not in a Planning Area. Planning Area 13 will increase in size from 14.6 Acres to 15.42 Acres Gross, and 12.76 Net Acres (excluding easement).
- 4) Reduce the size of Planning Area 14 from 11.7 Acres to 9.18 Acres by redistributing areas into Planning Areas 12B and 13B.

As a result, adjustments to planning area boundaries and densities within the Specific Plan area are required. Detailed descriptions of each change that is proposed by MENIFEE NORTH Specific Plan No. 260, Amendment No. 3 are provided on Table I, Table II and Pages III-64 through III-68 of this document. No Planning Areas will remain as Business Park, but there is one Planning Area designated Commercial.

The existing SCE easement is being included within Planning Areas 11, 12 and 13 in this amendment. Development will have to conform with all applicable SCE easement restrictions. However, the easement area shall be allowed to be used in required landscape and open space areas, retention and detention basins, and for passive recreation uses.

Upon approval of Amendment No.3, total dwelling unit count for MENIFEE NORTH Specific Plan No. 260 shall be from 3,035 to 3,465, based on maximum potential dwelling units in Planning Area 12. (refer to Table II, Land Use Summary).

The above-mentioned changes would require that revisions be made to previously-approved MENIFEE NORTH Specific Plan No. 260, Amendment No. 1 and No. 2. The graphic and text pages that would be revised have been included in this abbreviated document.

MENIFEE NORTH SPECIFIC PLAN No. 260 AMENDMENT NO. 3

List of Figures Modified or Created for this Amendment

Figure III-A SP 260 Amendment 3 Planning Areas III - 7A

Figure III-B City of Menifee Map III - 7B

Figure III-1 Existing Land Use Plan III - 8A

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Figure III-14.11A Planning Area 11III - 64D

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Table V-2 Building Standards and Setbacks, Planning Area 11 . III – 64C

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Table V-3 Building Standards and Setbacks, Planning Area 12 . III – 66A

Figure III-14.12A Planning Area 12.....III - 66B

Figure III-14.13 Planning Area 13III - 67B

Table V-4 Building Standards and Setbacks, Planning Area 13 . III – 67C

Figure III-14.13A Planning Area 13III - 67D

Figure III-14.14 Planning Area 14 III - 69

Bold Entries are Added or Modified with Amendment 3
Strikethru Entries are for Planning Areas outside the City of Menifee

DESIGNATION	PLANNING AREA	ACREAGE WITHIN CITY OF MENIFEE	ACREAGE OUTSIDE CITY OF MENIFEE	TARGET DENSITY WITHIN CITY OF MENIFEE	DWELLING UNITS WITHIN CITY OF MENIFEE
RESIDENTIAL					
Medium – 7,200 SF min.	4	21.8		3.5	76
	5 ⁽¹⁾	18.3		3.7	68
	6 ⁽¹⁾	24.6		3.7	91
	9 ⁽¹⁾	30.4		3.5	106
	15	10.6		3.0	32
	18	10.3		3.0	31
	41		34.3	3.5	
	45		74.8	3.5	
SUBTOTAL – MEDIUM - 7,200 SF MIN.		116.0	130.0	3.5	404
Medium – 6,000 SF min.	25		45.1	4.5	
	32 ⁽²⁾		21.8	4.5	
	35		19	4.5	
	37		20.6	4.5	
	40		60.4	4.5	
SUBTOTAL – MEDIUM - 6,000 SF MIN.			166.9	4.5	
Medium High – 5,000 SF min.	22	11.3		5.0	56
	24		22	5.0	
	33 ⁽³⁾⁽²⁾		57.7	4.5	
	34 ⁽³⁾⁽²⁾		75.2	4.5	
SUBTOTAL – MEDIUM HIGH - 5,000 SF MIN.		11.3	154.9	5.0	56
MEDIUM HIGH – 4,000 SF min.	7A	15.2		5.6	85
HIGH DENSITY – Garden Courts	7B	11.9		7.3	87
	23A	18.1		8.5	153
SUBTOTAL – HIGH DENSITY - Garden Courts		30.0		8.0	240
VERY HIGH DENSITY – 14.1 – 24 DU/AC	11	24.43		24	586
	12 ⁽³⁾	7.66		24	184
SUBTOTAL – VERY HIGH DENSITY - 14.1 – 24 DU/AC		32.09		24	770
SUBTOTAL RESIDENTIAL		204.6	451.8	7.6	1555

⁽¹⁾ The maximum density may be increased to 6.0 du/ac with a 5,000 square foot lot minimum in either of the following two circumstances:
a. The project is designed for and restricted to senior citizen housing, or
b. The project is a mobile home park or mobile home subdivision

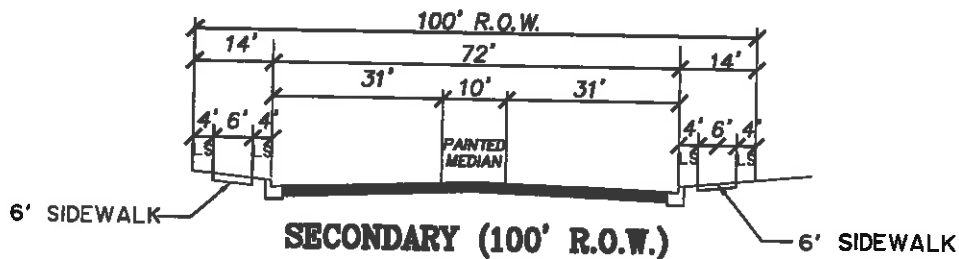
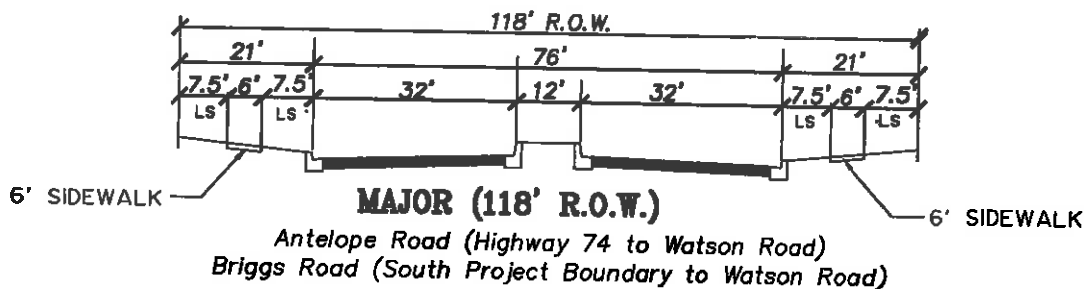
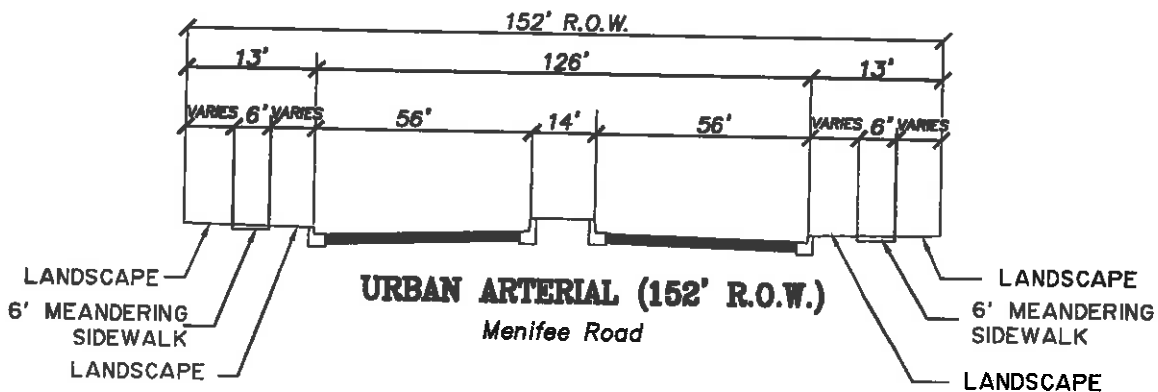
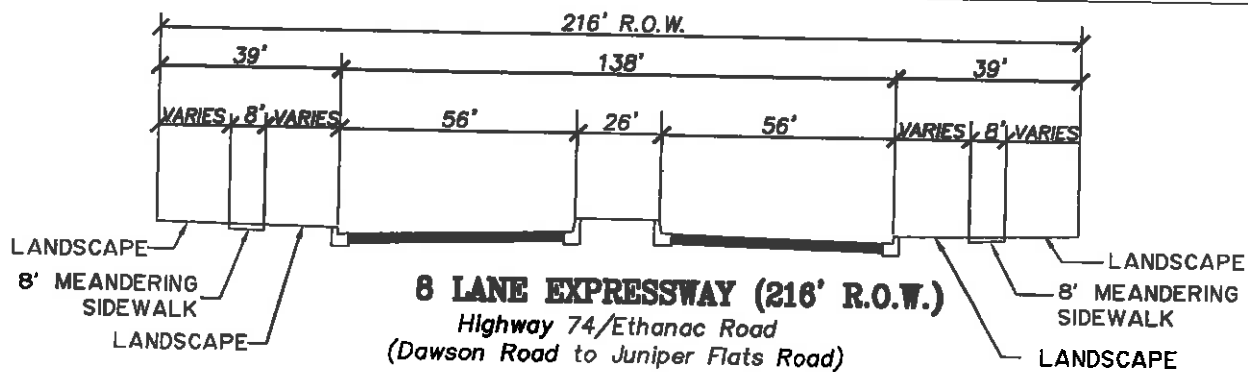
This standard applies to Planning Areas 5, 6, 9, 32, 33 and 34. If this option is elected, the maximum number of dwelling units shown in Table II may be exceeded up to a new maximum which is listed in the Planning Area description for the relevant Planning Area. The overall maximum number of dwelling units for the entire Specific Plan may not exceed 2,815.

⁽²⁾ The density shown for this planning area is 4.5 du/ac even though the minimum lot size is 5,000 square feet. The overall number of units for this planning area is restricted to require that several neighborhoods of differing lot sizes be developed.

⁽³⁾ PA 12 can be either residential or commercial. Density shown here is a maximum if it is all residential, not including the area of the SCE Easement.

DESIGNATION	PLANNING AREA	ACREAGE WITHIN CITY OF MENIFEE	ACREAGE OUTSIDE CITY OF MENIFEE	TARGET DENSITY WITHIN CITY OF MENIFEE	DWELLING UNITS WITHIN CITY OF MENIFEE
NON-RESIDENTIAL PLANS					
Business Park	11	19.0		-	-
	12	4.0		-	-
	26		21.0	-	-
	28		12.6	-	-
	43		17.6	-	-
SUBTOTAL - BUSINESS PARK		0.0	51.2	-	-
Commercial	8	3.3		-	-
	12 ⁽⁶⁾	7.66		-	-
	13 ⁽⁶⁾	12.76		-	-
	14	9.27		-	-
	16	35.3		-	-
	17	34.4		-	-
	23B	23.7		-	-
	27		11.8	-	-
29		8.1	-	-	
31		32.4	-	-	
SUBTOTAL - COMMERCIAL		126.39	52.3	-	-
Mixed Use/Neighborhood Commercial	31A		18.8	-	-
Commercial/Business Park ⁽⁶⁾	13	14.6		-	-
	19	36.0		-	-
	30		13.2	-	-
	44		10.4	-	-
	47		10.9	-	-
SUBTOTAL - COMMERCIAL/BUSINESS PARK		36.0	53.3	-	-
Industrial	2	121.1		-	-
	3	76.4		-	-
SUBTOTAL - INDUSTRIAL		197.5		-	-
Schools	21	8.7		-	-
	39		10.0	-	-
	42		10.0	-	-
SUBTOTAL - SCHOOLS		8.7	20	-	-
Community Parks	10	12.5		-	-
	20	12.0		-	-
	38 ⁽⁴⁾		5 ⁽⁴⁾	-	-
SUBTOTAL - COMMUNITY PARKS		24.5	9	-	-
Open Space	36			-	-
Fire Station	-			-	-
SUBTOTAL NONRESIDENTIAL		393.1	185.8		
PROJECT TOTALS		529.6	751.3		
Drainage Channels	-		15.9	-	-
Utility Easements/Existing Uses	-		111.4	-	-
Major Roadways	-		164.1	-	-

⁽⁶⁾ Not including the area of the SCE Easement

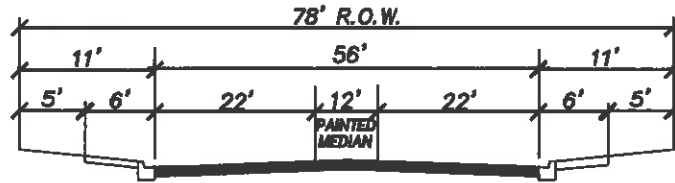


Watson Road (Antelope Road to Menifee Road and Malone Road to Juniper Flats Road) McLaughlin Road (Dawson Road to Antelope Road and Briggs Road to Sultanas Road)
 Antelope Road (McLaughlin Road to Highway 74)
 Sultanas Road (McLaughlin Road to Highway 74)
 Malaga Road (Highway 74 to Varela Lane)
 Juniper Flats Road (Highway 74 to Alicante Road)

NOT TO SCALE

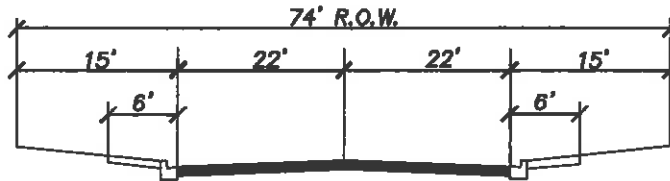
MENIFEE NORTH

Figure III-3
 TYPICAL ROADWAY CROSS SECTION
 AMENDMENT NO. 3



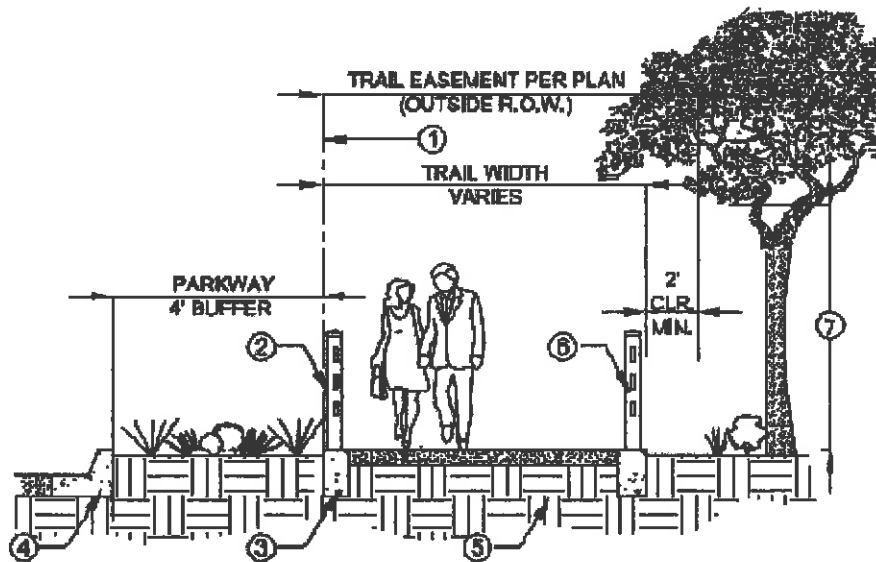
INDUSTRIAL COLLECTOR (78' R.O.W.)

Varela Lane, Pierson Road and McKinley Road
 Sultanas Road (Highway 74 to North Boundary of Planning Area 44)
 Street "B" (Emperor Road to Briggs Road)
 Emperor Road (South Boundary of Planning Area 25 to South Boundary of Planning Area 31)
 Malone Ave. (Highway 74 to Varela Lane)



COLLECTOR (74' R.O.W.)

Leon Road and Alicante Road
 McLaughlin Road (Antelope Road to Palomar Road)
 Emperor Road (South of Planning Area 33 to Rouse Road)
 Sultanas Road (North Boundary of Planning Area 44 to Watson Road)
 Junipero Road (North of Highway 74)
 Palomar Road (North Highway 74 to Watson Road)



TRAIL OPTION

- 1. RIGHT-OF-WAY
- 2. PVC RAIL FENCE ALONG STREET SIDE OF TRAIL
- 3. TRAIL WITH CONCRETE MOM CURB
- 4. STREET CURB / GUTTER
- 5. COMPACTED SUBGRADE
- 6. INSIDE FENCING, IF REQUIRED BY CITY
- 7. MINIMUM OVERHEAD CLEARANCE PER TRAIL STANDARD

NOT TO SCALE

- 4) On-site roads will be constructed as:
 - Expressway (216' ROW)
 - Urban Arterial (152' ROW)
 - Major (118' ROW)
 - Secondary (100' ROW)
 - Industrial Collector (78' ROW)
 - Collector (74' ROW)
 - Local streets (56' ROW)
 - Palomar Rd. (88' ROW North of Hwy 74)
- 4a) The minimum interior local street width may be reduced from the 60-foot standard to no less than 56 feet for the purpose of providing more landscaped area as approved by the Public Works Director/City Engineer.
- 4b) A reduction in the width of a local street may be appropriate to reduce traffic speeds on internal residential streets as determined by a City approved Traffic Impact Analysis. Such reduction in pavement width must be utilized to accomplish other design objectives, e.g., enhancing the buffer treatment of reverse frontage lots, providing for sheltered parking opportunities, or incorporating bus stop turnouts.
- 5) Landscape requirements shall be in accordance with the City Adopted Landscape Design Guidelines as depicted in Section IV, Design Guidelines.
- 6) Major roadway improvements may be be financed through an assessment district or similar financing mechanism.
- 7) The project proponent shall participate in the Traffic Signal Mitigation Program as approved by the City.
- 8) All projects, including subdivisions and plot plans within this Specific Plan, shall be subject to the Mitigation Monitoring Program as described in the Mitigation Monitoring Program (Section II.C) of this Specific Plan.
- 9) Any application for any subdivision within the Specific Plan boundary (including Schedule I Parcel Map) shall cause the design and construction of the Specific Plan master planned infrastructure within the final map boundaries, with the exception of a division of land that has no parcel less than 40 acres or that is not less than a quarter of a quarter section.
- 10) All roadways intersecting four lane facilities or greater shall be a minimum of 66 feet of right-of-way and constructed in accordance with City Standards, adopted City General Plan and City Adopted Ordinance 461.
- 11) All typical sections shall be per City Adopted County Ordinance 461, or as approved by the Public Works – Engineering Department.

**Table IIA
CIRCULATION ELEMENT ROADWAYS**

ROADWAY NAME	Current Amendment No. 2 Circulation Element Classification	Proposed Amendment No. 3 Circulation Element Classification
Highway 74/Ethanac Road	Expressway 220' ROW	Expressway 220' ROW
Watson Road (Antelope to Menifee, and Malone to Juniper Flat)	Secondary 100' ROW	Secondary 100' ROW
McLaughlin Road (Trumble to east Edge of Planning Area 3)	Secondary 100' ROW	Secondary 100' ROW
Sultanas (McLaughlin) (Briggs to Highway 74)	Secondary 100' ROW	Secondary 100' ROW
Sherman Road (McLaughlin to Ethanac)	Major 118' ROW	Major 118' ROW
Antelope Road (McLaughlin to Highway 74)	Secondary 100' ROW	Secondary 100' ROW
Antelope Road (Highway 74 to Watson)	Major 118' ROW	Major 118' ROW
Mathews Road (Highway 74 to Briggs)	Deleted	Deleted
Palomar Road (McLaughlin to Mathews)	Secondary 110' ROW	Secondary 110' ROW
Palomar Road (Mathews to Highway 74)	Secondary 110' ROW	Secondary 110' ROW
Palomar Road (Highway 74 to Cider Street)	Industrial Collector 78' ROW	Secondary 88' ROW
Menifee Road (Highway 74 to Watson)	Arterial 152' ROW	Arterial 152' ROW
Malaga Road (Highway 74 to N. project boundary Street "A")	Secondary 100' ROW	Secondary 100' ROW
Briggs Road	Major 118' ROW	Major 118' ROW
Juniper Flats Road (Highway 74 to Alicante)	Secondary 100' ROW	Secondary 100' ROW

- 2) It is anticipated that major backbone drainage facilities will be maintained by the County Flood Control District. Local drainage devices within street rights-of-way will be maintained by the City of Menifee.
- 3) An erosion, sedimentation and flood control plan should be prepared for all development areas draining into the San Jacinto River and Salt Creek. This plan should include no improvements, and should protect these water courses from erosion and siltation during and after construction. Drainage of urban areas should be controlled prior to reaching either the San Jacinto or Salt Creek courseways through the use of velocity reduction devices at the points where run-off enters the waterways.
- 4) All projects proposing construction activities including: cleaning, grading or excavation that results in the disturbance of at least one acres of total land area, or activity which is part of a larger common plan of development of one acre or greater, shall obtain the appropriate NPDES construction permit and pay the appropriate fees. ~~There is a statewide General Permit applicable to most areas of the state; however, because MENIFEE NORTH is located in the San Jacinto Watershed. The project is subject to a special NPDES permit (No. CAG 618005) per Order No. 01-34 as adopted by the California Regional Water Quality Control Board, Santa Ana Region. On January 19, 2001. The developer or builder shall comply with the requirements of the NPDES construction permit by developing and implementing a Storm Water Pollution Prevention Plan (SWPPP), approved by the Executive Officer of the Regional Water Quality Control Board, that specifies Best Management Practices (BMPs) to minimize pollutants in storm water runoff. During construction, the project would follow the specifications per the site's SWPPP. On January 29, 2010 the Santa Ana Regional Water Quality Control Board (SARWQCB) issued a fourth-term area wide National Pollutant Discharge Elimination System (NPDES) and Municipal Separate Storm Sewer System Permit (MS4 Permit) to the City of Menifee and all Permittees. The Santa Ana MS4 Permit is for the portion of the Santa Ana River watershed located within Riverside County (Order No. R8-2010-0033, NPDES Permit No. CAS618033). The City of Menifee storm water programs are designed to ensure compliance with this permit. All design submittals and construction within Menifee North Specific Plan are required to conform to the permit requirements. All projects are to install Best Management Practices (BMPs) in compliance with the 2010 SARWQCB permit.~~
- 5) The proposed Amendment No. 3 Planning Areas, along with all of the cumulative development projects, will be required to obtain an NPDES permit and to comply with any provisions of that permit, thus reducing their potential for water quality impacts. For post-construction, implementing Project developers would be required to complete a project-specific WQMP containing measures that effectively treat all pollutants of concern and hydrologic conditions of concern, consistent with the approved WQMP developed in compliance with the City's MS4 permit. Project specific site design, source control and treatment control BMPs are included in the project-specific WQMP to protect downstream areas from pollutants after construction. Therefore, Project-related impacts to water quality and hydrology will not be cumulatively considerable.

6) DRAINAGE

The proposed Menifee North Planning Areas 11, 12, 13 and 14 Amendment No. 3 site lies east of Interstate 215, north and of State Highway 74 between

Palomar & Menifee Road. Planning Areas 11, 12, & 13 are not located in located within A Floodplain as mapped by the Federal Emergency Management Agency (FEMA Map No. 06065C2060H August 18, 2014). Planning Area 14 only has a portion of the southeast corner of the property located within Zone "A" Floodplain Area (Zone A – "No base elevation determined"). The Amendment No. 3 site lies within the limits of the Romoland/Homeland Area Drainage Plan (ADP) prepared by Riverside County Flood Control District (R.C.F.C.D.) Amendment No. 2 adopted March 2006. Line A-3 of the Homeland ADP is designed to protect the project from off-site flooding during a 100 year storm event.

11. PLANNING AREA 11: VERY HIGH DENSITY RESIDENTIAL**A. Descriptive Summary**

Planning Area 11, as depicted in Figure III-14.11, is divided into two parts PA 11A west of Junipero Road, and PA 11B east of Junipero Road. Together Planning Areas 11A and 11B provides for development of approximately 24.43 net acres (586 Units) devoted to Multi Family Housing uses. Development of Planning Area 11A may encroach into Planning Area 12A and development of Planning Area 11B may encroach into Planning Area 12B. At that time, the parcels within Planning Area 12 that are included in the Planning Area 11 development will be subject to all standards of Planning Area 11. Refer to Planning Area 12 on Page III-65 for restrictions on PA 11 encroachments onto PA 12.

B. Development Standards**1. Permitted Uses**

Refer to Table V-1 for the list of Permitted Land Uses. Other uses not listed may be permitted provided a Conditional Use Permit has been granted pursuant to the requirements of the City of Menifee Zoning Code.

2. General Development Standards

Refer to Table V-2 for the list of Development Standards.

Residential density includes only the number of residential units and does not consider the square footage of each unit or any non-residential square footage such as freestanding garage facilities, recreation facilities, management office facilities and maintenance facilities. Residential density is calculated by dividing the sum of the number of units proposed on an individual parcel by the total area of the parcel in gross acres.

3. Landscape Standards

Refer to Table V-2 for the required Landscape Coverage Standards. Landscape shall conform to the City Adopted Landscape Guidelines.

C. Planning Standards

1. Primary access to Planning Area 11 shall be provided from Palomar Road and Junipero Road.

2. A roadway landscape treatment, as illustrated in Figure IV-18, shall be established along Junipero Road and Palomar Road.

3. Please refer to Residential Design Guidelines (Section IV.F), for design-related criteria.

4. Common Open Space Standards

a. Common Area Open Space shall be provided at a rate per square foot of usable open space per bedroom or studio unit as required in Table V-2.

b. The Open Space shall:

- i. be located on the same lot as the dwelling unit for which it is required.

- ii. be level and improved, that is, have suitable landscaping and hardscaping.
 - iii. include no obstructions other than structures and amenities such as swimming pools, fountains, benches, sport courts, playground apparatus, and landscaping.
 - iv. be open on at least one side and have a clear vertical height of at least seven (7) feet, with at least 50% of the total required usable open space open to the sky.
- c. Amenities and structures shall only cover up to a maximum of 50% of the total required usable open space area. Other structures that do not relate to enhancing the enjoyment of outdoor open space (carports and storage sheds, for example) are not qualified as usable open space.
- d. The Common Area shall be directly accessible to all units within a multiple-family residential project. An individual space shall be at least 300 square feet in size, with no dimension less than 15 feet. Common Area may include amenities such as swimming pools, cabanas, tennis courts, sport courts, play fields, roof top gardens, etc.
5. Private Open Space shall be provided per the standards within Table V-2. It may include balconies, decks, porches, patios, or yards.
6. A Southern California Edison easement is located within the property boundary for Planning Area 11B. The existing SCE easement is being included within Planning Areas 11B, 12B and 13B in this amendment. Development will have to conform with all applicable SCE easement restrictions. However, the easement area shall be allowed to be used in required landscape and open space areas, retention and detention basins, circulation, parking and for passive recreation uses subject to the approval of SCE. The SCE Easement shall not be allowed to be included in density or intensity calculations.
7. Please refer to Section III for the Development Plans and Standards that apply within PA 11.
- III.A.1: Specific Land Use Plan
 - III.A.2: Circulation Plan
 - III.A.3: Drainage Plan
 - III.A.4: Water and Sewer Plans
 - III.A.5: Open Space and Recreation Plan
 - III.A.6: Grading Plan
 - III.A.7: Landscaping Plan
 - III.A.8: Public Facilities Phasing Plan
 - III.A.9: Phasing Plan

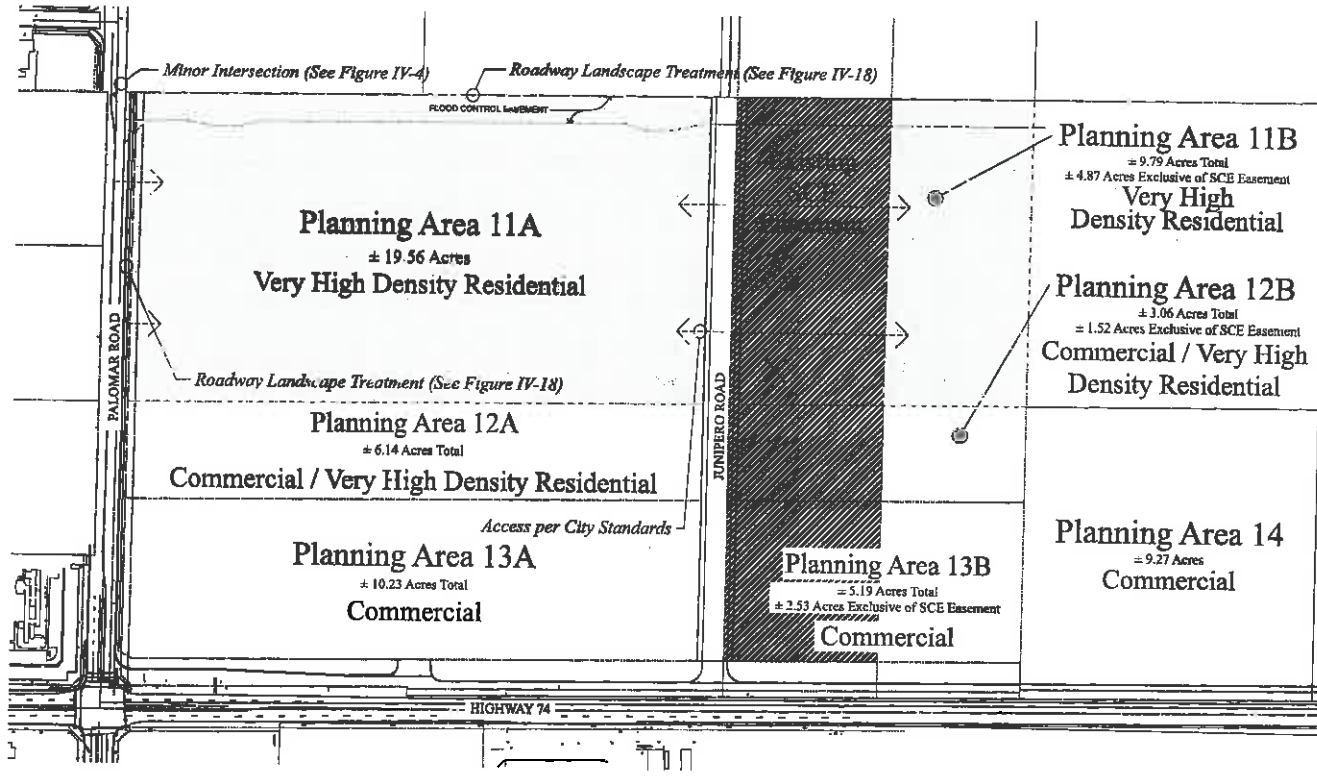
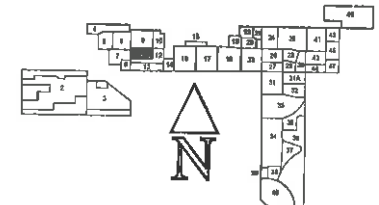


FIGURE III-14.11

Planning Area 11

Area	Acreage +/-	Easement Acreage +/-	Balance of Usable Acreage
PA 11A	19.56	0	19.56
PA 11B	9.79	4.92	4.87
PA 12A	6.14	0	6.14
PA 12B	3.06	1.54	1.52
PA13A	10.23	0	10.23
PA13B	5.19	2.66	2.53
PA 14	9.27	N/A	9.27



**TABLE V-1
Permitted Land Uses
Planning Areas 11, 12 and 13**

USE CATEGORY	TYPICALLY PERMITTED USES	PLANNING AREA 11	PLANNING AREA 12	PLANNING AREA 13
P = Permitted (1) (3) • C = Conditional Use Permit • N = Not Permitted				
AUTOMOBILE/ VEHICLE	Automobile repair garages, not including body and fender shops or spray painting	N	N	C
	Automobile sales and rental agencies.	N	C	C
	Boat and other marine sales, rentals and services.	N	C	C
	Car wash, full service, self-service, drive thru	N	N	C
	Gasoline service stations, liquid petroleum, compressed natural gas (CNG) sales with car wash, convenience stores	N	N	C
	Gasoline service stations, with the concurrent sale of beer and wine for off-premises consumption.	N	N	C
	Liquid petroleum service stations, not including the concurrent sale of beer and wine, provided the total capacity of all tanks shall not exceed 10,000 gallons	N	N	C
	Liquid petroleum, CNG service stations, with the concurrent sale of beer and wine, provided the total capacity of all tanks shall not exceed 10,000 gallons.	N	N	C
	Mobilehome sales, trailer sales and rentals, recreational vehicle rentals	N	N	C(2)
	Parking lots and parking structures	N	P	P
	Rental of trucks and trailers; the rental of trucks not over 19,500 pounds gross weight, with body not to exceed 22 feet in length from the back of the cab to the end of the body; and the rental of trailers not exceeding six feet in width or 22 feet in length.	N	N	C(2)
	Sale, rental, repair, or demonstration of motorcycles, scooters, and motorbikes.	N	C	C
	Tire sales and service, not including recapping	N	N	C(2)
	Trailer and boat storage (for tenant use only, not a primary use)	N	C(2)	C(2)
	Travel trailers, mobilehomes and recreational vehicles sales and service.	N	N	C(2)
Truck sales and service.	N	N	C(2)	
COMMERCIAL/ OFFICE/ PROFESSIONAL	Auditoriums and conference rooms	N	P	P
	Costume design studios	N	P	P
	Employment agencies	N	P	P
	Escort bureaus	N	P	P
	Interior decorating shops	N	P	P
	Mail order businesses	N	P	P
	Manufacturer's agent	N	P	P
	Offices, including business, law, medical, dental, chiropractic, architectural, engineering, community	N	P	P

III. SPECIFIC PLAN

**TABLE V-1
Permitted Land Uses
Planning Areas 11, 12 and 13**

USE CATEGORY	TYPICALLY PERMITTED USES	PLANNING AREA 11	PLANNING AREA 12	PLANNING AREA 13
P = Permitted (1) (3) • C = Conditional Use Permit • N = Not Permitted				
	Printers or publishers	N	P	P
	Radio and television broadcasting studios	N	C(2)	C(2)
	Recording studios	N	C	C
	Travel agencies	N	P	P
	Weight Loss clinic	N	P	P
	Auction houses	N	C	C
ENTERTAINMENT / SPORTS	Billiard and pool halls	N	P	P
	Internet café, including gaming	N	P	P
	Bowling alleys	N	P	P
	Dance halls	N	P	P
	Health club, gym, exercise facility	N	C	C
	Theaters, not including drive-ins	N	P	P
	Sports and recreational facilities, not including motor driven vehicles and riding academies, but including archery ranges, athletic playgrounds, sports arenas, skating rinks, stadiums, and commercial swimming pools	N	P(2)	P(2)
FOOD SERVICE	Bars and cocktail lounges	N	P	P
	Restaurant including on-site brewery	N	P	P
	Delicatessens	N	P	P
	Restaurants and other eating establishments including drive thru/drive-in restaurants	N	P	P
INSTITUTIONAL	Adult Day care centers	P	P	P
	Animal hospitals, Animal Day Care facilities	N	C	C
	Child Day Care Center	P	P	P
	Churches, temples and other places of religious worship	N	P	P
	Educational institutions, public libraries, museums and art galleries not operated for compensation or profit.	N	P(4)	P
	Schools, business and professional, including art, barber, beauty, dance drama, music and swimming	N	P	P
	Wedding chapels	N	P	P
	Multiple family dwellings, bungalow courts and apartment houses	P	P	N
	Home occupations.	P	P	N
	Congregate care residential facilities	C	C	N

**TABLE V-1
Permitted Land Uses
Planning Areas 11, 12 and 13**

USE CATEGORY	TYPICALLY PERMITTED USES	PLANNING AREA 11	PLANNING AREA 12	PLANNING AREA 13
P = Permitted (1) (3) • C = Conditional Use Permit • N = Not Permitted				
RESIDENTIAL	Boarding, rooming and lodging houses for 6 people or less that are not for the providing of substance abuse care, rehabilitation, of substance abuse care, rehabilitation or persons released from prison/jail.	P	P	N
	Temporary real estate tract/project sales and leasing offices located within an approved development, to be used only for and during the original sale or leasing of the project, but not to exceed a period of two years in any event.	P	P	N
RETAIL	Antique shops.	N	P	P
	Appliance stores, household.	N	P	P
	Art supply shops and studios.	N	P	P
	Automobile parts and supply stores.	N	P	P
	Bakery shops, including baking only when incidental to retail sales on the premises.	N	P	P
	Banks and financial institutions.	N	P	P
	Barber and beauty shops.	N	P	P
	Bicycle sales, repairs and rentals.	N	P	P
	Book stores and binders.	N	P	P
	Clothing stores.	N	P	P
	Confectionery or candy stores.	N	P	P
	Convenience stores, including the sale of motor vehicle fuel.	N	N	C
	Convenience stores, not including the sale of motor vehicle fuel.	N	P	P
	Day Spa, Nail salon, Make-up salon	N	P	P
	Department stores.	N	P	P
	Drug store and pharmacy	N	P	P
	Dry goods stores.	N	P	P
	Electronics Hardware and Software, Electronic Gaming, Audio Visual Sales and Service	N	P	P
	Feed and grain sales.	N	N	C
	Fishing and casting pools.	N	P	P
Florist shops.	N	P	P	
Gift shops.	N	P	P	
Golf Cart sales and service	N	P	P	
Grocery Store, Food markets and frozen food lockers.	N	P	P	
Hardware and Home Improvement stores	N	P(2)	P(2)	

III. SPECIFIC PLAN

**TABLE V-1
Permitted Land Uses
Planning Areas 11, 12 and 13**

USE CATEGORY	TYPICALLY PERMITTED USES	PLANNING AREA 11	PLANNING AREA 12	PLANNING AREA 13
P = Permitted (1) (3) • C = Conditional Use Permit • N = Not Permitted				
RETAIL	Hobby shops.	N	P	P
	Ice cream shops, frozen yogurt shops..	N	P	P
	Jewelry stores with incidental repairs.	N	P	P
	Leather goods stores.	N	P	P
	Liquor stores pursuant to the provisions of City of Meniffee Alcoholic Beverage Sales ordinance.	N	C	C
	Locksmith shops.	N	P	P
	Market, food, wholesale or jobber.	N	P	P
	Meat markets, not including slaughtering.	N	P	P
	Music stores.	N	P	P
	News stores.	N	P	P
	Notions or novelty stores.	N	P	P
	Nurseries and garden supply stores.	N	P*	P*
	Paint and wall paper stores, not including paint contractors.	N	P*	P*
	Pawn shops.	N	C	C
	Pet shops and pet supply shops.	N	P	P
	Photography shops and studios and photo engraving.	N	P	P
	Plumbing shops, not including plumbing contractors.	N	P	P
	Poultry markets, not including slaughtering or live sales.	N	P	P
	Produce markets.	N	P	P
	Refreshment kiosks.	N	P	P
	Shoe stores and repair shops.	N	P	P
	Shoeshine stands.	N	P	P
	Sporting goods stores (no firearms)	N	P	P
	Stationery stores.	N	P	P
	Swap meets, Farmers Market	N	C	C
	Tailor shops.	N	P	P
	Telecommunication, cellular service stores.	N	P	P
	Tobacco shops for take away sale. No water pipes sales.	N	P	P
	Toy shops.	N	P	P
	Watch repair shops.	N	P	P
Business machine, computer sales and rental and incidental repairs	N	P	P	

III. SPECIFIC PLAN

**TABLE V-1
Permitted Land Uses
Planning Areas 11, 12 and 13**

USE CATEGORY	TYPICALLY PERMITTED USES	PLANNING AREA 11	PLANNING AREA 12	PLANNING AREA 13
P = Permitted (1) (3) • C = Conditional Use Permit • N = Not Permitted				
SERVICE	Household goods sales and repair, including but not limited to, new and used appliances, furniture, carpets, computers, draperies, lamps, electronics, radios, software, and television sets, including repair thereof.	N	P	P
	Nurseries, horticulture	C	C	P
	Ambulance services.	N	N	P
	Bakery goods distributors.	N	P	P
	Blueprint and duplicating services.	N	P	P
	Mortuaries with onsite crematorium	N	C	C
	Catering services.	N	P	P
	Ceramic sales and manufacturing for on-site sales, provided the total volume of kiln space does not exceed 16 cubic feet.	N	C	C
	Cleaning and dyeing shops.	N	P	P
	Construction offices and caretaker's quarters on construction sites for the duration of a valid building permit.	N	P	P
	Equipment rental services, including rototillers, power mowers, sanders, power saws, cement and plaster mixers not exceeding ten cubic feet in capacity and other similar equipment.	N	P(2)	P(2)
	Ice sales, not including ice plants.	N	P	P
	Laboratories, film, dental, medical, research or testing.	N	P	P
	Laundries and laundromats.	N	P	P
	Mortuaries, with no crematorium	N	N	P
	One on-site operator's residence, which may be located in a commercial building.	N	P	P
	Recycling collection facilities.	N	C	C
Stained glass assembly.	N	P	P	
Taxidermist.	N	P	P	
Wholesale businesses with samples on the premises, but not to include storage.	N	P	P	
SERVICE	Field crops, flower and vegetable gardening, tree crops, and greenhouses used only for purposes of propagation and culture, including the sale thereof from the premises and one unlighted sign that does not exceed two square feet in size pertaining to the sale of products. (Only allowed as a continuation of an existing use.)	P	N	C
	Stations, bus, railroad and taxi.	N	P	P

**TABLE V-1
Permitted Land Uses
Planning Areas 11, 12 and 13**

USE CATEGORY	TYPICALLY PERMITTED USES	PLANNING AREA 11	PLANNING AREA 12	PLANNING AREA 13
P = Permitted (1) (3) • C = Conditional Use Permit • N = Not Permitted				
TOURIST COMMERCIAL	Hotels, resort hotels and motels.	N	P	P
	Tourist information centers.	N	P	P
ACCESSORY USES	<p>An accessory use to a permitted use is allowed provided the accessory use is incidental to, and does not alter the character of, the principal permitted use, including, but not limited to:</p> <p>1) limited manufacturing, fabricating, processing, packaging, treating and incidental storage related thereto, provided any such activity shall be in the same line of merchandise or service as the trade or service business conducted on the premises and provided any such activity does not exceed any of the following restrictions:</p> <p>a. The maximum gross floor area of the building permitted to be devoted to such accessory use shall be 25 percent.</p> <p>b. The maximum total horsepower of all electric motors used in connection with such accessory use shall be five horsepower.</p> <p>2) The accessory use shall be so conducted that noise, vibration, dust, odor, and all other objectionable factors shall be reduced to the extent that there will be no annoyance to persons outside the premises. Such accessory use shall be located not nearer than 50 feet to any residential zone.</p> <p>3) Accessory uses shall be conducted wholly within a completely enclosed building.</p>			
NOTES	<p>1.) Requires Plot Plan Approval per City of Menifee Zoning Code 2.) Permitted, together with screened outside storage and display of materials appurtenant to such use 3.) Screened Outdoor Storage is limited to 200 SF * Public requires a Use Permit</p>			

**TABLE V-2
Building Standards and Setbacks
Planning Area 11**

DEVELOPMENT STANDARD	PLANNING AREA 11 REQUIREMENT
Maximum Building Height	3 Stories / 45 Feet
Building Setbacks	
Front Yard - (Palomar Road, Junipero Road)	25 Feet - Front setback shall be measured from the Curb Line of the ultimate street width as depicted on the Circulation Plan, Table II.A.
Side Yard and Interior Setbacks	Minimum distance between buildings shall be as required in the California Building Code, latest edition, minimum 5 feet
Rear Yard	10 Feet
Building Setbacks from Surface Parking and Driving Aisles	10 Feet
Garage Setbacks from Surface Parking and Driving Aisles	3 Feet
Required Parking	Parking shall be provided as required by City Standards
Maximum Density	24 DU/Acre
Minimum Dwelling Unit Size	500 Square Feet
Lot Coverage	In no case shall more than 60 per cent of any lot be covered by buildings, not including freestanding garage buildings and carports
Landscape Coverage	12% - Landscape Coverage will include all areas within the subject parcel
Minimum Private Storage	72 Cubic Feet per Bedroom with a maximum of 144 Cubic Feet
Common Area Open Space	125 Square Feet of Open Space per Bedroom/Studio Unit
Private Open Space / Dwelling (Deck or Patio)	120 square feet with no dimension less than 10 feet if located at ground level, or at least 60 square feet with no dimension less than 6 feet if located above ground level

III. SPECIFIC PLAN

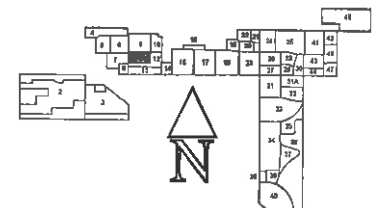
MENIFEE NORTH SPECIFIC PLAN No. 260 Amendment 3



FIGURE III-14.11A

Planning Area 11

This is a conceptual layout of a High Density multi family project incorporating the elements within the Building Standards and Design Guidelines. The actual project plan will be different than this solution, but this gives a clear example of a solution that does conform to the Specific Plan.



12. PLANNING AREA 12: COMMERCIAL / VERY HIGH DENSITY RESIDENTIAL BUSINESS PARK

A. Descriptive Summary

Planning Area 12, as depicted in Figure III-14.12, is divided into two parts PA 12A west of Junipero Road, and PA 12B east of Junipero Road. Together Planning Areas 12A and 12B provides for development of approximately 7.66 net acres devoted to **Commercial** uses or **Very High Density Residential** uses, designed to be integrated with corresponding land uses within the adjacent PA 11A/PA 11B and PA 13A and PA 13B. The uses and development standards for general retail/commercial uses will be the same as those in neighboring Planning Area 13. The uses and development standards for multi-family uses will be the same as those in neighboring Planning Area 11. The purpose of these Planning Areas 12A and 12B is to provide flexibility between the commercial planning areas to the south, and the residential planning areas to the north.

The ultimate use configuration of this Planning Area will be determined by development applications in the neighboring Planning Areas. The first application submitted for either Planning Area 11A/11B or 13A/13B may encroach into Planning Area 12A/12B respectively. Planning Area 12A can be developed as up to 100% Commercial (with PA 13A) or up to a maximum of 67% High Density Residential (with PA 11A) and 33% commercial (with PA13A). These percentages are based on square footage only and may be of any configuration as long as the land uses are contiguous with the PA 11A and PA 13A areas. PA 12 B may be adjusted to a maximum 100% of either High Density Residential (with PA 11B) or Commercial (with PA13B).

Once a development application is approved in either neighboring Planning Area (11A or 11B or 13A or 13B), the Land Use designation will then be determined for the remainder of Planning Area 12A or 12B respectively. If the maximum 67% of Planning Area 12A and all of Area 12B are developed as High Density Residential (24 DU/Ac max) the total number of units would be 135.

B. Development Standards

1. Permitted Uses

Refer to Table V-1 for the list of Allowable Uses. Any use that is not specifically listed within Table V-1 may be considered a permitted or conditionally permitted use provided that the Community Development Director finds that the proposed use is substantially the same in character and intensity as those listed in the designated subsections. Such a use is subject to the permit process, which governs the category in which it falls.

2. General Development Standards.

Refer to Table V-3 for the list of Development Standards.

3. Landscape Standards

Refer to Table V-3 for the required Landscape Coverage Standards. Landscape shall conform to the City Adopted Landscape Guidelines.

C. Planning Standards

1. Primary access to Planning Area 12 shall be provided from Junipero Road and shall be coordinated with adjacent development driveway and intersection spacing and possible signalization and as determined by a City Approved Traffic Impact Analysis.
2. Roadway landscape treatments, as illustrated in Figure IV-18, are planned along Palomar Road and Junipero Road, respectively.
3. For any commercial uses, all roof mounted mechanical equipment shall be screened from the ground elevation view to a minimum sight distance of 1,320 feet.
4. A Southern California Edison easement is located within the property boundary for Planning Area 12B. The existing SCE easement is being included within Planning Areas 11B, 12B and 13B in this amendment. Development will have to conform with all applicable SCE easement restrictions. However, the easement area shall be allowed to be used in required landscape and open space areas, retention and detention basins, circulation, and for passive recreation uses subject to approval of SCE. The SCE Easement shall not be allowed to be included in density or intensity calculations.
5. In conjunction with the first implementing project plan submitted for this planning area, a conceptual site plan for the entire planning area (12A or 12B) shall be provided to the City of Menifee Planning Department for informational purposes. This site plan shall show how the submitted project plan integrates with the remainder of the planning area, how the uses within the Planning Area may be configured to ensure proper design and layout of the circulation, parking, and street access.
6. Please refer to Residential Design guidelines (Section IV.F) and Commercial Design Guidelines (Section IV.G) for design-related criteria.
7. Common Open Space Standards for Residential Developments
 - a. Common Area Open Space shall be provided at a rate per square feet of usable open space per bedroom or studio unit as required in Table V-3.
 - b. The Open Space shall:
 - i. be located on the same lot as the dwelling unit for which it is required.
 - ii. be level and improved, that is, have suitable landscaping and hardscaping.
 - iii. include no obstructions other than structures and amenities such as swimming pools, fountains, benches, sport courts, playground apparatus, and landscaping.
 - iv. be open on at least one side and have a clear vertical height of at least seven (7) feet, with at least 50% of the total required usable open space open to the sky.
 - c. Amenities and structures shall only cover up to a maximum of 50% of the total required usable open space area. Other structures that do not relate to enhancing the enjoyment of outdoor open space (carports and storage sheds, for example) are not qualified as usable open space.
 - d. The Common Area shall be directly accessible to all units within a multiple-family residential project. An individual space shall be at least 300 square feet in size, with no dimension less than 15 feet. Common Area may include amenities such as swimming pools, cabanas, tennis courts, sport courts, play fields, roof top gardens, etc.
8. Private Open Space for Residential Developments shall be provided per the standards within Table V-3. It may include balconies, decks, porches, patios, or yards.

9. Please refer to Section III for the Development Plans and Standards that apply site-wide:

- III.A.1: Specific Land Use Plan
- III.A.2: Circulation Plan
- III.A.3: Drainage Plan
- III.A.4: Water and Sewer Plans
- III.A.5: Open Space and Recreation Plan
- III.A.6: Grading Plan
- III.A.7: Landscaping Plan
- III.A.8: Public Facilities Phasing Plan
- III.A.9: Phasing Plan

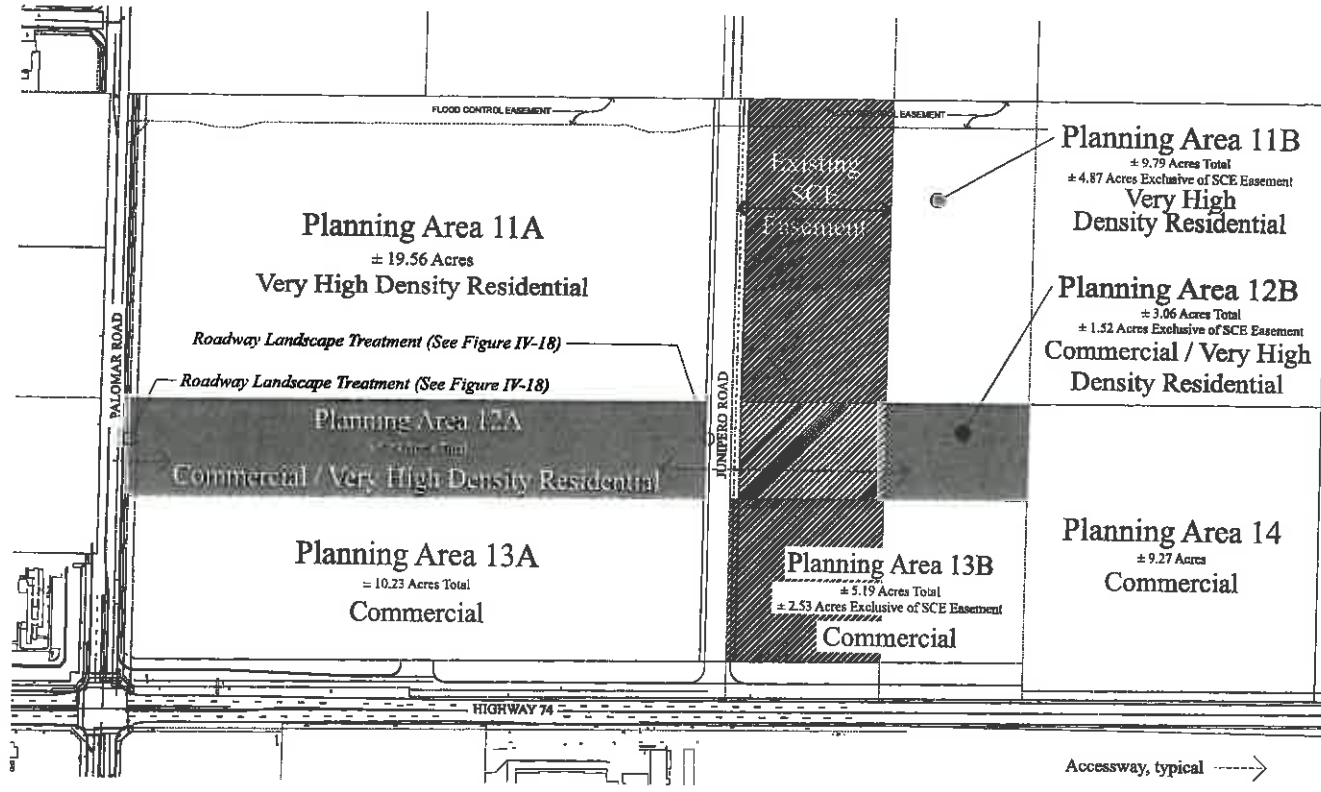


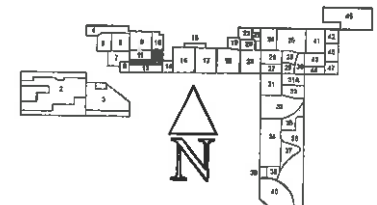
FIGURE III-14.12

Planning Area 12

Note that it is intended that the boundaries of Planning Area 12 will be modified with the development of Planning Areas 11 and 13.

For Planning Areas 12A and 12B it is intended that upon the development of Planning Area 13A/13B and the inclusion of parcels from Planning Area 12A/12B respectively into Planning Area 13A/13B that the balance of Planning Area 12A/12B will meld into Planning Area 11A/11B respectively and cease to be a separate Planning Area.

Area	Acreage +/-	Easement Acreage +/-	Balance of Usable Acreage
PA 11A	19.56	0	19.56
PA 11B	9.79	4.92	4.87
PA 12A	6.14	0	6.14
PA 12B	3.06	1.54	1.52
PA13A	10.23	0	10.23
PA13B	5.19	2.66	2.53
PA 14	9.27	N/A	9.27



**TABLE V-3
Building Standards and Setbacks
Planning Area 12**

DEVELOPMENT STANDARD	PLANNING AREA 12 COMMERCIAL REQUIREMENTS	PLANNING AREA 12 RESIDENTIAL REQUIREMENTS
Maximum Building or Structure Height	50 Feet	3 Stories / 45 Feet
Building Setbacks		
Front Yard – (Palomar Road, Junipero Road)	20 Feet – Front setback shall be measured from the Curb Line of the ultimate street width as depicted on the Circulation Plan, Table II.A.	25 Feet – Front setback shall be measured from the Curb Line of the ultimate street width as depicted on the Circulation Plan, Table II.A.
To Adjacent Planning Area	The setback shall be measured from the PA line. Buildings shall be set back a minimum of 20 feet from the PA line. For any portion of a building which exceeds 35 feet in height that portion of the building shall be set back an additional one foot for each foot by which the height exceeds 35 feet.	10 Feet
Side (Palomar Road and Junipero Road) and Interior Setbacks	Side setbacks (Palomar Road and Junipero Road) shall be a minimum of 25 feet for a building 35 feet or less in height. Each side setback shall be measured from the Curb Line of the ultimate street width as depicted on the Circulation Plan, Figure III-2A. Any portion of a building which exceeds 35 feet in height shall be set back an additional one foot for each foot by which the height exceeds 35 feet	10 Feet
Distance Between Buildings	Minimum distance between buildings shall be as required in the California Building Code, latest edition.	Minimum distance between buildings shall be as required in the California Building Code, latest edition, 5 feet minimum
Building Setbacks from Surface Parking and Drive Aisles	5 Feet	10 Feet
Garage Setbacks from Surface Parking and Drive Aisles	-	3 Feet
Lot Coverage	-	In no case shall more than 60 per cent of any lot be covered by buildings, not including freestanding garage buildings and carports
Maximum Intensity/Density	0.35 Floor Area Ratio	24 DU/ Gross Acre
Minimum Dwelling Unit Size	-	500 Square Feet
Landscape Coverage	12% – Landscape Coverage will include all areas within the subject parcel	12% – Landscape Coverage will include all areas within the subject parcel
Required Parking	Parking shall be provided as required by City Standards or per the requirements of a City approved Shared Parking Analysis	Parking shall be provided as required by City Standards
Minimum Private Storage	-	72 Cubic Feet per Bedroom with a maximum of 144 Cubic Feet
Common Area Open Space	-	125 Square Feet of Open Space per Bedroom/Studio Unit
Private Open Space / Dwelling (Deck or Patio)	-	120 square feet with no dimension less than 10 feet if located at ground level, or at least 60 square feet with no dimension less than 6 feet if located above ground level

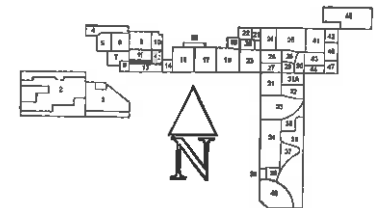
Planning Area 12A shown below as a portion of commercial development consistent with the Planning Area 13A Building Standards and Design Guidelines. This image depicts the pedestrian connection between the Planning Areas as well as the landscape buffer as noted in the Development Standards. The actual project plan will be different than this solution, but this gives a clear example of a solution that does conform to the Specific Plan.



Planning Area 12B shown above with the High Density Residential use and proposed passive development of the SCE easement. This is a conceptual plan showing that a quality project is possible within the Building Standards and High Density Residential Design Guidelines for this Planning Area. This plan is depicting a maximum residential scheme for Planning Area 12B.

FIGURE III-14.12A

Planning Area 12



13. PLANNING AREA 13: COMMERCIAL

A. Descriptive Summary

Planning Area 13, as depicted in Figure III-14.13, is divided into two parts PA 13A west of Junipero Road, and PA 13B east of Junipero Road. Together Planning Areas 13A and 13B provides for development of approximately 12.76 net acres devoted to **Commercial** uses. Development of Planning Area 13A may encroach into Planning Area 12A and development of Planning Area 13B may encroach into Planning Area 12B. At that time, the parcels within Planning Area 12 that are included in the Planning Area 11 development will be subject to all standards of Planning Area 13. Refer to Planning Area 12 on Page III-65 for restrictions on PA 13 encroachments onto PA 12.

B. Development Standards

1. Permitted Uses

Refer to Table V-1 for the list of Allowable Uses. Any use that is not specifically listed within Table V-1 may be considered a permitted or conditionally permitted use provided that the Planning Director finds that the proposed use is substantially the same in character and intensity as those listed in the designated subsections. Such a use is subject to the permit process, which governs the category in which it falls.

2. General Development Standards.

Refer to Table V-4 for the list of Development Standards.

3. Landscape Standards

Refer to Table V-4 for the required Landscape Coverage Standards. Landscape shall conform to the City Adopted Landscape Guidelines.

C. Planning Standards

1. Primary access to Planning Area 13 shall be provided from Highway 74 (as approved by CalTrans), Palomar Road and Junipero Road. Access to Highway 74 east of Junipero Road shall be coordinated with development and access within Planning Area 14 where possible. All proposed driveways, access or improvements off of Highway 74 requires review and approval by CalTrans.
2. A project entry/intersection statement shall be developed at the intersection of Highway 74 and Palomar Road, as shown on Figure IV-3.
3. A minor intersection statement shall be developed at the intersection of Highway 74 and Junipero Road, as shown on Figure IV-4.
4. Roadway landscape treatments, as illustrated in Figures IV-15 and 18, are planned along Highway 74, Palomar Road and Junipero Road, respectively.
5. All roof mounted mechanical equipment shall be screened from the ground elevation view to a minimum sight distance of 1,320 feet.

6. A Southern California Edison easement is located within the property boundary for Planning Area 13B. The existing SCE easement is being included within Planning Areas 11B, 12B and 13B in this amendment. Development will have to conform with all applicable SCE easement restrictions. However, the easement area shall be allowed to be used in required landscape and open space areas, retention and detention basins, circulation, parking and for passive recreation uses subject to the approval of SCE. The SCE Easement shall not be allowed to be included in density or intensity calculations.

7. In conjunction with the first implementing project plan submitted for the planning area (PA 13A or PA 13B), a conceptual site plan for the entire planning area (including 12A or 12B) shall be provided to the City of Menifee Planning Department for informational purposes. This site plan shall show how the submitted project plan integrates with the remainder of the planning area 12A/12B, how the uses within the Planning Area may be configured to ensure proper design and layout of the circulation, parking, and street access.

8. Please refer to Commercial Design Guidelines (Section IV.G) for design-related criteria.

9. Please refer to Section III for the Development Plans and Standards that apply site-wide.

- III.A.1: Specific Land Use Plan
- III.A.2: Circulation Plan
- III.A.3: Drainage Plan
- III.A.4: Water and Sewer Plans
- III.A.5: Open Space and Recreation Plan
- III.A.6: Grading Plan
- III.A.7: Landscaping Plan
- III.A.8: Public Facilities Phasing Plan
- III.A.9: Phasing Plan

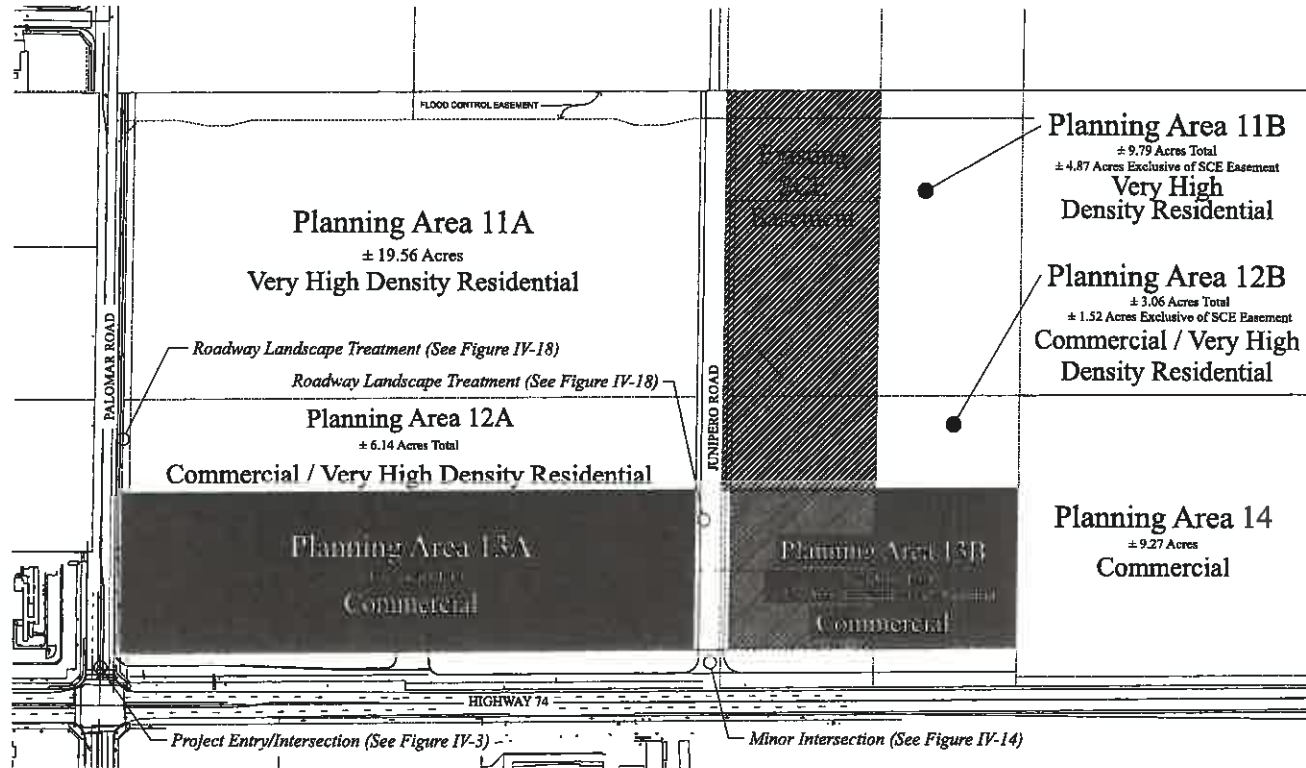
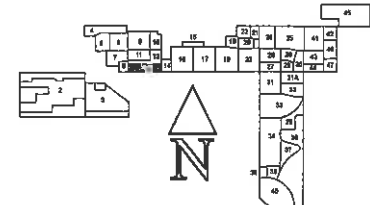


FIGURE III-14.13

Planning Area 13

Area	Acreege +/-	Easement Acreege +/-	Balance of Usable Acreege
PA 11A	19.56	0	19.56
PA 11B	9.79	4.92	4.87
PA 12A	6.14	0	6.14
PA 12B	3.06	1.54	1.52
PA 13A	10.23	0	10.23
PA 13B	5.19	2.66	2.53
PA 14	9.27	N/A	9.27



**TABLE V-4
Building Standards and Setbacks
Planning Area 13**

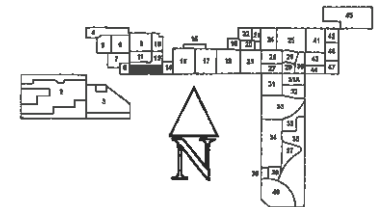
DEVELOPMENT STANDARD	PLANNING AREA 13 REQUIREMENT
Maximum Building or Structure Height	50 Feet
Building Setbacks	
Front Yard (Highway 74)	There are no front yard requirements for buildings which do not exceed 35 feet in height. Any portion of a building which exceeds 35 feet in height shall be set back from the front lot lines not less than one foot for each foot by which the height exceeds 35 feet. The front setback shall be measured from the existing street line (ROW).
To Adjacent Residential Planning Area (PA)	The setback shall be measured from the PA line. Buildings shall be set back a minimum of 20 feet from the PA line. For any portion of a building which exceeds 35 feet in height that portion of the building shall be set back an additional one foot for each foot by which the height exceeds 35 feet.
Side (Palomar Road and Junipero Road)	Side setbacks (Palomar Road and Junipero Road) shall be a minimum of 25 feet for a building 35 feet or less in height. Each side setback shall be measured from the Right of Way Line of the ultimate street width as depicted on the Circulation Plan, Figure III-2A. Any portion of a building which exceeds 35 feet in height shall be set back an additional two feet for each foot by which the height exceeds 35 feet.
Distance Between Buildings	Minimum distance between buildings shall be as required in the California Building Code, latest edition.
Building Setbacks from Surface Parking	5 Feet
Minimum Lot Area	There is no Minimum Lot Area Required
Landscape Coverage	12% - Landscape Coverage will include all areas within the subject parcel.
Required Parking	Parking shall be provided as required by City Standards or per the requirements of a City approved Shared Parking Analysis
Maximum Intensity	0.35 Floor Area Ratio



FIGURE III-14.13A

Planning Area 13

This is a conceptual layout of a Retail Center between Palomar and Junipero fronting on Highway 74 incorporating the elements within the Development Standards, the Building Standards and the Design Guidelines. The actual project plan will be different than this solution, but this gives a clear example of a solution that does conform to the Specific Plan. The plan as shown incorporates all of Planning Area 12 into the commercial designation consistent with the requirements of Planning Area 13.



14. PLANNING AREA 14: COMMERCIAL

A. Descriptive Summary

Planning Area 14, as depicted in Figure III-14.14, provides for development of 9.27 acres devoted to Commercial uses.

B. Land Use and Development Standards

Please refer to Ordinance No. 348. (See Specific Plan Zone Ordinance Tab.)

C. Planning Standards

1) Primary access to Planning Area 14 shall be provided from Highway 74 (as approved by CalTrans) and Menifee Road. Any potential primary access on Highway 74 shall be coordinated with development and access within Planning Area 13.

2) A community entry/intersection statement shall be developed at the intersection of Highway 74 and Menifee Road, as shown on Figure IV-2.

3) A special landscape treatment, as depicted on Figure IV-12, shall be established between the commercial uses in Planning Area 14 and the adjacent residential uses to the north.

4) Roadway landscape treatments, as illustrated in Figures IV-15 and 16, shall be established along Highway 74 and Menifee Road, respectively.

~~5) A Southern California Edison easement forms the western boundary of Planning Area 14. All development applications filed for this planning area must conform with all applicable SCE easement restrictions.~~

5) In conjunction with the first implementing plot plan submitted for this planning area, a conceptual site plan for the entire planning area shall be provided to the Riverside County City of Menifee Planning Department for informational purposes. This site plan shall show how the proposed plot plan integrates into the remainder of the planning area, to ensure proper design and layout of the parking and street access.

6) Please refer to Section IV, Design Guidelines, for design-related criteria.

7) Please refer to Section III.A. for the following Development Plans and Standards that apply site-wide:

- III.A.1: Specific Land Use Plan
- III.A.2: Circulation Plan
- III.A.3: Drainage Plan
- III.A.4: Water and Sewer Plans
- III.A.5: Open Space and Recreation Plan
- III.A.6: Grading Plan
- III.A.7: Landscaping Plan
- III.A.8: Public Facilities Phasing Plan
- III.A.9: Phasing Plan

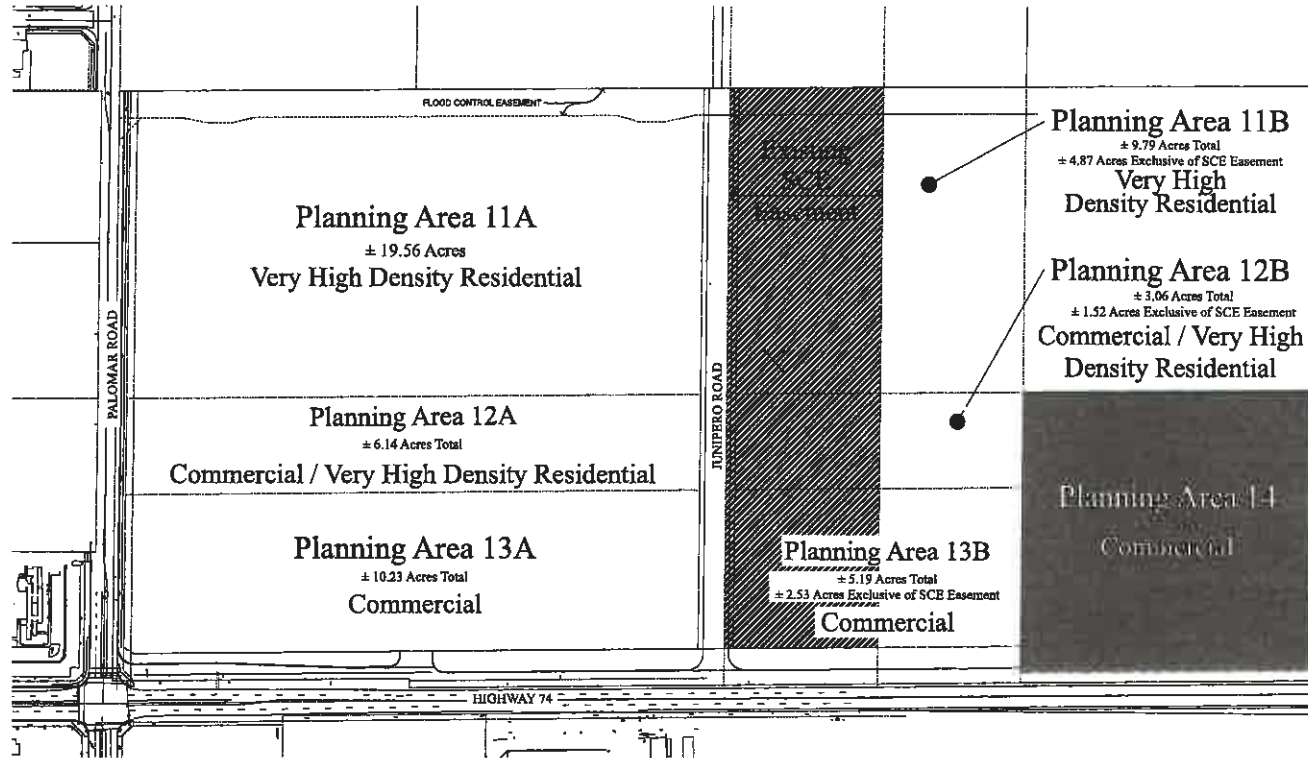
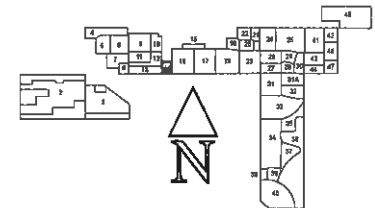


FIGURE III-14.14

Planning Area 14

Area	Acreage +/-	Easement Acreage +/-	Balance of Usable Acreage
PA 11A	19.56	0	19.56
PA 11B	9.79	4.92	4.87
PA 12A	6.14	0	6.14
PA 12B	3.06	1.54	1.52
PA13A	10.23	0	10.23
PA13B	5.19	2.66	2.53
PA 14	9.27	N/A	9.27



F. PLANNING AREAS 11 AND 12 (VERY HIGH DENSITY RESIDENTIAL) Architectural Design Objectives and Guidelines

The multi-family residential developments in Planning Areas 11 and 12 are designed to be neighborhood style communities that offer well designed living units with on-site amenities such as multi purpose rooms, pool and spa areas, open spaces and well defined parking areas offering a mix of covered and uncovered parking spaces.

This section contains Design Objectives and Guidelines that augment the building standards within the Planning Areas. The Objectives are not just the typical goals and minimum expectations for development, but a method to meet and balance the needs of both the City and the developer having an end result in promoting high quality design with flexibility on how to implement the design. However, flexibility shall not mean going to the lowest common denominator that dilutes quality and character.

The Design Objectives present the basis for compliance and the Design Guidelines provide options and examples of how to meet the Objectives. The Design Guideline images used to illustrate the Objectives should be reviewed in reference to the specific topic with which the image is associated and not with all of the guidelines in the document. The intent is for the user to focus on the portions of each photo highlighted with the caption and associated text. While an image may only illustrate a portion of the guideline with which it is associated, the intent of the entire guideline should be met. Although the Design Guidelines include some typical means for successful design, they are not intended to preclude alternative strategies that meet the overall intent and purpose related to a particular concept or feature. Applying the Design Guidelines will allow the design review process to emphasize not just meeting the minimum standards of zoning, but to create high quality places and spaces.

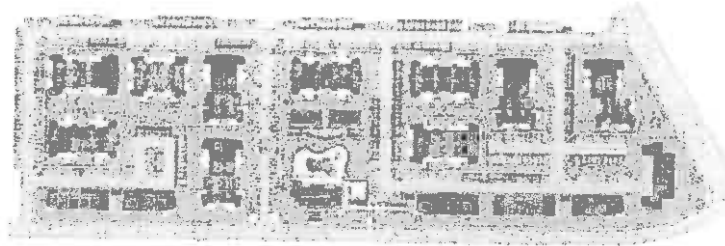
Objectives and Guidelines which include the word 'should' are intended to be applied as stated. Alternative measures may be considered if they meet or exceed the intent of the guideline. If the Objective or Guideline contains the words 'shall' or 'will', they are mandatory and must be included in the project's design. Objectives and Guidelines using the words 'encouraged' or 'discouraged' are desirable but not mandatory.

F1 – General Objectives, Colors and Materials

- F1a. The living units will be a blend of one, two and three story buildings offering a mix of bedroom/bathroom count and varying square footage. Units will have a private outdoor space such as a patio or deck.
- F1b. Strategic orientation and configuration of buildings and shading elements shall be used to passively heat and cool spaces. Careful placement of the shade structures will allow for the creation of shade within the open courtyards and not interfere with the open sky placement of the trees.
- F1c. Quality materials and imaginative design shall establish the theme of the architecture for each project. The character of the buildings shall be residential and not urban.
- F1d. Four-sided architecture shall be provided for all buildings. Designs shall demonstrate a consistent use of colors, materials, and detailing throughout all elevations of the building.
- F1e. Buildings can have a mix of sloping and flat roofs. Sloping roofs should be concrete or clay tile.
- F1f. Horizontal and vertical building offsets and articulations shall be used to create visual relief and promote the residential scale of the project.
- F1g. The residential buildings should be designed with a variety of elements which will help keep a pedestrian scale to the buildings. Building ends and corners should include elements that soften and lower the building, such as stairs and decks.
- F1h. Wall materials, base materials, and accent materials for both buildings and site elements will be a combination of: stucco, textured masonry, wood or composite siding, with accents of stone, tile, and metals.
- F1i. The use of bold plaster masses with articulated openings, accented entryways, and deeper overhangs is encouraged.
- F1j. As one of the unifying materials, plaster will be used on building walls and site walls. It can be both the dominant wall material and an accent material. While a variety of textures may be used on the project, they will not be mixed on a single layer of a building facade.
- F1k. Architectural detailing, plaster, siding, and deep colors shall be included at key places on the buildings and site features.
- F1l. Walls with masonry, stone or tile accents shall be proposed for some of the building finishes. Combinations of textures and colors will be used. Typical locations for these materials will be entries and bases of the buildings.
- F1m. Accent elements such as trellis and shade structures, eyebrows and awnings should be painted steel and other durable material for longevity and ease of maintenance.
- F1n. The colors shall be blended, and should offer subtle definition of neighborhood areas. Choose colors consistent with the chosen design theme. Avoid “trendy” colors which become quickly outdated. Low-key and earthy colors work best for primary colors; use of more vibrant colors should be limited to accents.
- F1o. Care should be taken to not utilize too many materials or colors on the same building.
- F1p. Plaster walls and details should be integral color or painted with deeper tones to minimize the reflective glare and provide a more luxurious ambience.

F2 – Main Project Entry

F2a. Site planning shall be designed to offer a strong entry statement, leading to clear and direct internal circulation.



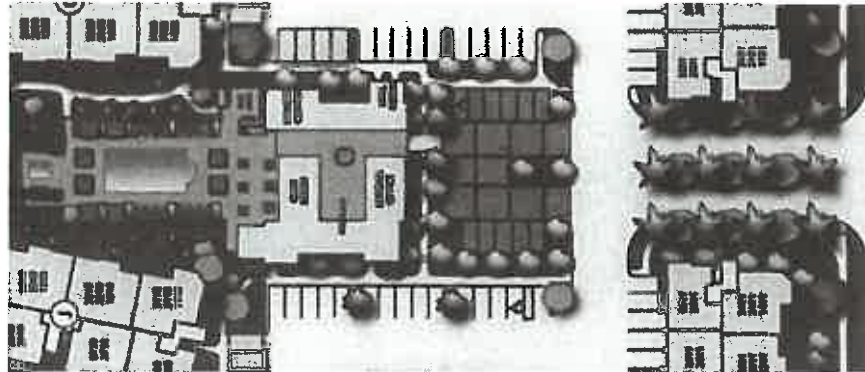
F2b There should be strong project identification with thematic architectural elements, project signage and distinctive landscape.



F2c. A project map shall be integrated into the signage and directories adjacent to the vehicle entry point.



F2d. Visitors/prospective tenants shall have a clear destination upon entering the project.



Direct Visual Access to the Leasing Office

F2e, Entries shall be delineated with pavers or other decorative pavement.



F3 – Entry Security

F3a. Site security is important and needs to be an integral part of the site design.

F3b. Perimeter fencing shall be decorative and located to allow visitors to access the leasing office before passing through vehicle or pedestrian security gates.



Prominent Leasing Office Entry

F3c. Resident parking areas should be behind a gated vehicular access. Landlords are encouraged to provide access control that is easily modified for adding and removing entry credentials



Gated Vehicular Access

F3d. Some guest parking should be provided prior to passing thru secured gates.



F3e. Secondary resident only entries shall have the same security and aesthetic enhancements as the primary Gated Vehicular Access.



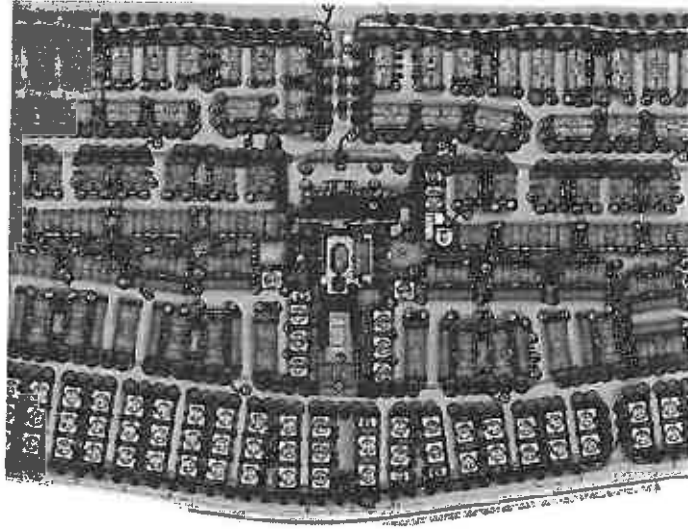
Secondary Entry

F3f. Main Entry design should include a courtesy turnaround prior to the secured gate.



F4 – Leasing Office and Clubhouses

F4a. Leasing offices and clubhouses shall be located so that they can be easily accessed by visitors as well as incorporated into the site plan as a view amenity.



The Leasing Office with direct visual access to the visitor.

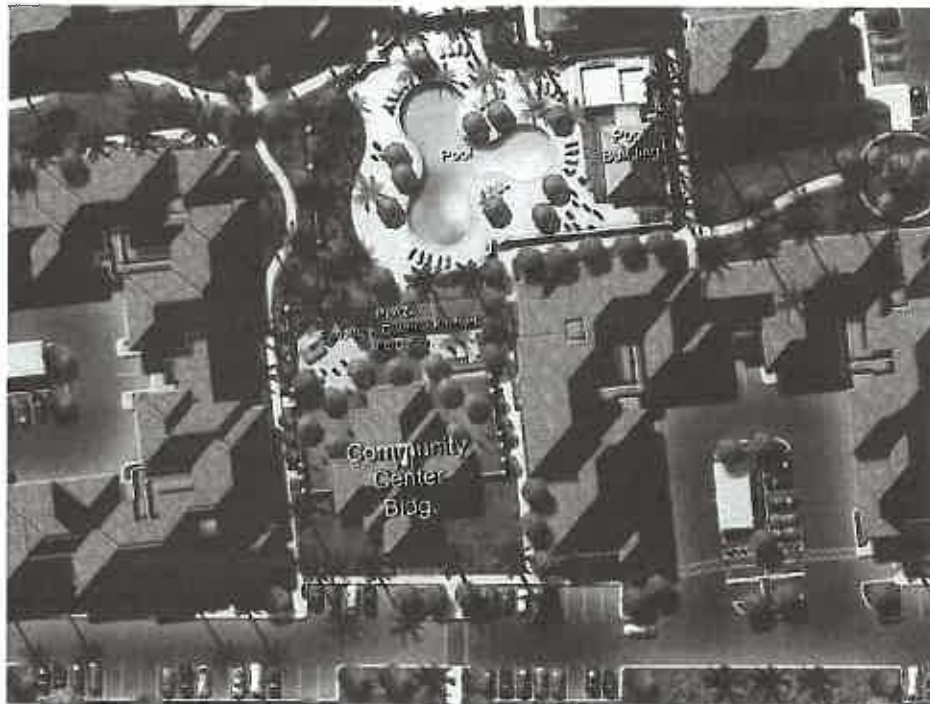
F4b. The architectural design of the Leasing Office entry should be a prominent element on the facade.



F4c. The visitor should be able to access to The Leasing Office outside of the Security Fencing.



F4d. The common facilities should have a central location so that the management services are available easily to residents as well as outside visitors.



F4e. If the clubhouse and leasing offices are adjoining, the design should carefully separate the two uses to discourage non-residents from accessing the amenities.



F4e. The clubhouse design should be stylish, and functional providing gathering places for the residents and their guests.



Comfortable Gathering Places

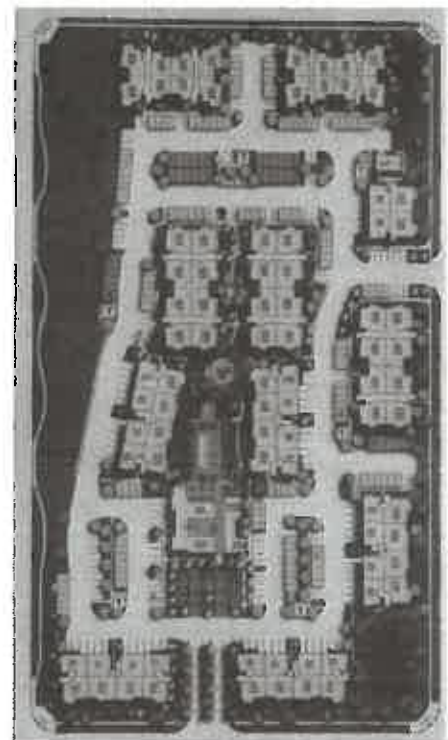
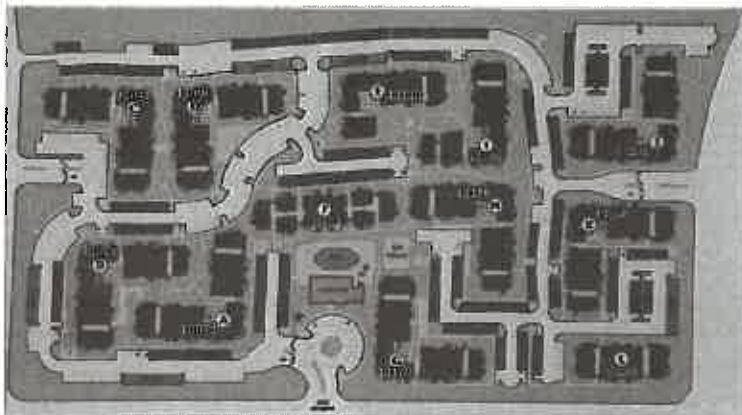


Stylish Decor

F5 – Circulation and Parking

F5a. The multi-family residential developments in Planning Areas 11 and 12 shall provide well defined parking areas offering a mix of covered and uncovered parking spaces.

F5b. Site planning shall be designed to offer a strong entry statement, clear and direct internal circulation and grouping of units and parking in an effort to avoid large expanses of open parking



F5c. Parking lots shall be fully landscaped utilizing a combination of shrubs to screen cars and trees to shade the pavement and vehicles. Plant placement should not inhibit the opening of vehicle doors.



F5d. Coordinate location of lighting fixtures to allow for good illumination at night for safety of both vehicles and pedestrians. Light fixtures shall be decorative and placed clear of trees.



F5e. Decorative signage is encouraged for the labeling of visitor parking and any reserved spaces.



F5f. Trash enclosures should be designed as an integral part of garage buildings to avoid random detached structures within parking areas.

F5g. Defined pedestrian pathways should be planned into the parking lot design to allow for safe and easy passage from parking spaces to unit entry points.



F6 – Covered Parking – General

F6a. Garages may be a part of the residential building (tuck under parking), or in free standing locations within the parking areas.



F6b. Freestanding garage buildings shall be designed in the same architectural style and features as the residential buildings.



F6c. Covered parking spaces should be equitably distributed throughout the project. Both garages and carports should be available.



Flat roofs of carports must have solar panels installed. All other carports shall have sloping roofs in a material that matches the roof of the residences.

F7 – Residential Buildings – Garages & Carports

F7a. Garages designed as part of the residential building should be integrated into the overall design with the site planning providing for easy access and adequate turning spaces.



F7b. Horizontal offsets should be used to break down the height of the buildings.



F7c. Carports in front of a residential building should have a minimalist design as to not detract from the building beyond



F7d. Where space permits, planter areas incorporated adjacent to the garage doors to allow for vertical plantings or accent vine plantings are encouraged.



F7e. The design of some attached garages with direct access to the unit is encouraged.



F7f. Carport vertical supports shall be placed to not interfere with the opening of vehicle doors. Below is an example of the post in the wrong location.



F8 – Residential Buildings – Scale

F8a. The character of the buildings shall be residential and not urban.



F8b. Horizontal and vertical building offsets shall be used to create visual relief and promote the residential scale of the project. Site elements such as patio walls as well as building walls can help achieve this.



F8c. The residential buildings should be designed with a variety of elements which will help keep a pedestrian scale to the buildings.



F8d. Building ends and corners may include elements that soften and lower the building, such as stairs and decks.



F8e. The use of bold plaster masses with articulated openings, accented entryways, and deeper overhangs will all help control the perceived scale of the building.



F8f. Color changes are encouraged to help manage the scale of a building. Random locations of color changes shall be avoided.



F9 – Residential Buildings – Fenestration

Fenestration – the arrangement, proportioning, and design of windows and doors in a building.

F9a. Details on windows and doors such as trim or canopies should be included on visible facades. The detailing should be consistent per building, and thematic per project.



F9b. Fenestration shall be simple and organized. The use of numerous types and styles of openings which causes visual clutter, shall be avoided.



Too many types and styles of openings on a single wall.

F9c. Vertical circulation should be identified visually by vertically scaled openings.



F9d. Visual interest can be created by using strong horizontal or vertical openings



F9f. If arches are used, only a single style should be incorporated. If flat arches are the chosen style, widths should be consistent to avoid too many varying shapes.



F9g. The creation of shade on building facades can be achieved through combinations of recessed opening, deep overhangs, and applied accent features such as awning and eyebrows.



F10 – Residential Buildings – Greenbelts

F10a. The site plan design should include greenbelts throughout the project. These greenbelts buffer the pedestrians from vehicle traffic and noise and provide scenic links between project elements, such as courtyards and recreation areas.



F10b. The entry to the individual units should be oriented towards green belts whenever possible. This encourages the neighborhood style of living



F10c. Perimeter units should have an increased landscape buffer between the front door and the parking lot or vehicular way.



F10d. Courts can be landscape, hardscape or a combination. They should be linked together to create a pedestrian base circulation system through the project that is separated from the vehicles.



F11 – Clubhouse Amenities

F11a. Amenities such as a clubhouse, firepit/fireplace, pool, spa, work-out facilities, half court basketball court, passive exercise courses, or other similar uses shall be provided as determined on a needs assessment. The needs assessment will use the following criteria:

- Size and shape of recreation areas
- Location and placement of buildings
- Diversity of amenities needed based upon anticipated resident demographic and economic mix
- Number of units, lot size and size mixture of units.

F11b. Clubhouses may include additional amenities such as clubhouse kitchen, private dining rooms, common living rooms, patio areas with fire pits, outdoor fireplaces, bar-b-que areas and tot lots.



Shaded Outdoor Living Room



Common Living Room



Firepits



Outdoor BBQ and Dining Area



Clubhouse Kitchen

F12 – Exercise Amenities

F12a. Amenities that encourage an active lifestyle, such as exercise rooms, sport courts, tot lots, and pools large enough for swimming laps are encouraged.



Sports Activity Areas



Tot Lot



Jr. Olympic sized pool



Lounge with Games

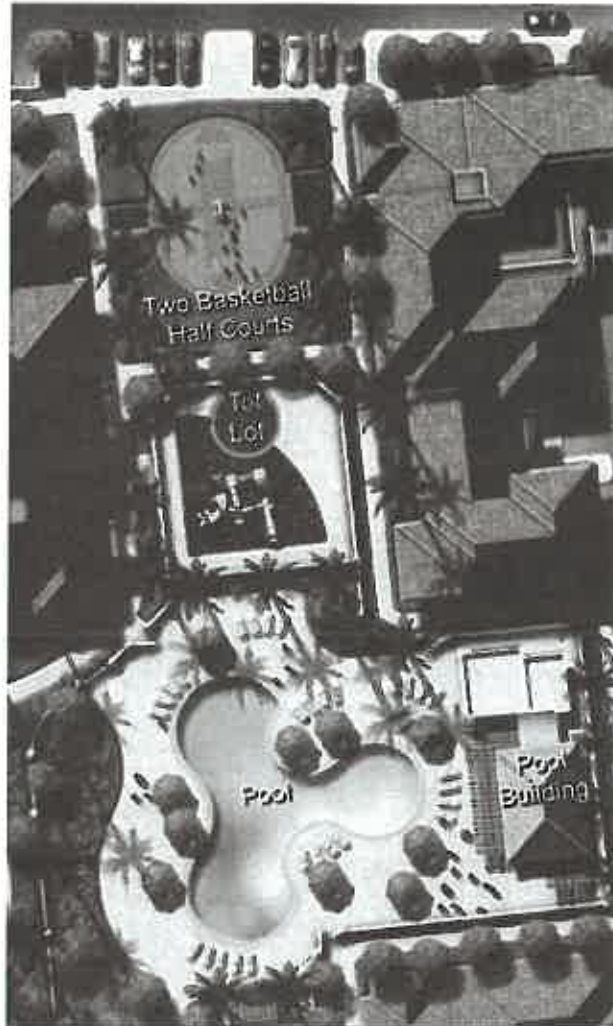


Game Room



Pool Seating

F12b. Based on the site plan, the Sports Activity Areas can be located together or distributed throughout the site.



F12c. Workout Equipment rooms should be of adequate size to provide a nice mix of equipment that encourages residents to utilize the facility.

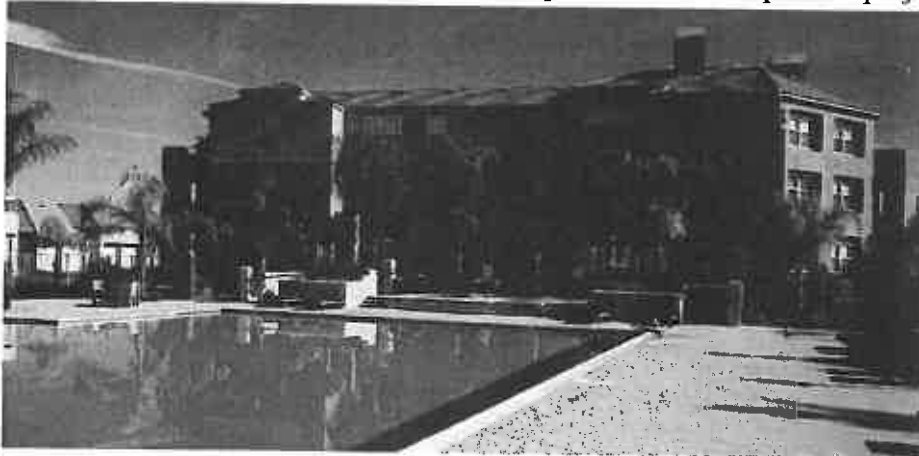


F13 – Pools and Spas

F13a. Pools and spas are encouraged to be the main exterior amenity. Proper planning and design can make a resort like visual amenity instead of of just a pool area.



F13b. Pools and pool decks shall be of adequate size based upon the project density and total number of pools.



Numerous Smaller Pools

F13c. Pool decks shall provide shaded seating areas, areas for sunning and a variety of seating types.



Variety of Seating

F13d. It is encouraged to design pools that function for both exercise style swimming and water play.



F13e. Centrally located pools shall be designed as a visual focal point.



F13f. Bathrooms and outdoor showers should be provided at main pool facilities.



G. PLANNING AREAS 13 and 12 (COMMERCIAL)

Architectural Design Objectives and Guidelines

The retail commercial centers within Planning Areas 12 and 13 shall be designed to be flexible, able to accommodate tenants from a supermarket to small local tenants, restaurants, and service uses such as daycare and gas stations.

This section contains Design Objectives and Guidelines that augment the building standards within the Planning Areas. The Objectives are not just the typical goals and minimum expectations for development, but a method to meet and balance the needs of both the City and the developer having an end result in promoting high quality design with flexibility on how to implement the design. However, flexibility shall not mean going to the lowest common denominator that dilutes quality and character.

The Design Objectives present the basis for compliance and the Design Guidelines provide options and examples of how to meet the Objectives. The Design Guideline images used to illustrate the Objectives should be reviewed in reference to the specific topic with which the image is associated and not with all of the guidelines in the document. The intent is for the user to focus on the portions of each photo highlighted with the caption and associated text. While an image may only illustrate a portion of the guideline with which it is associated, the intent of the entire guideline should be met. Although the Design Guidelines include some typical means for successful design, they are not intended to preclude alternative strategies that meet the overall intent and purpose related to a particular concept or feature. Applying the Design Guidelines will allow the design review process to emphasize not just meeting the minimum standards of zoning, but to create high quality places and spaces.

Objectives and Guidelines which include the word 'should' are intended to be applied as stated. Alternative measures may be considered if they meet or exceed the intent of the guideline. If the Objective or Guideline contains the words 'shall' or 'will', they are mandatory and must be included in the project's design. Objectives and Guidelines using the words 'encouraged' or 'discouraged' are desirable but not mandatory.

G1 – General Objectives

- G1a. The buildings shall maintain a theme carried out by quality materials and imaginative design consistent with these Design Guidelines
- G1b. Building design shall be simple buildings with few or no sloping roofs. These guidelines offer solutions that include overhangs, colonnades and added eyebrow type overhangs to the visible facades while allowing for typical flat roofs on the buildings.
- G1c. Four-sided architecture shall be provided for all buildings. Designs shall demonstrate a consistent use of colors, materials, and detailing throughout all elevations of the building. The rear elevations of the main in-line shop buildings shall have the same finish materials as the rest of the building.
- G1d. Signage locations shall be integrated into the building design and the site elements offering visible signage for all tenants regardless of location. Signage design shall be creative and artistic, promoting the festive atmosphere of a retail center. A sign program is required.
- G1e. Site and building elements can have common detailing to articulate a path of travel and unify the design.
- G1f. Thematic elements per the Ethanac Corridor Planning Group may be incorporated in the landscape and hardscape elements.
- G1g. Main parking lots shall be located in front of the retail buildings so that the parking is visible to the visitor upon entry onto the site, and to minimize car and pedestrian noise on adjacent residential units.
- G1h. Pad buildings should be located close to the front property line, spaced from each other to provide view cones into the major tenants. This location allows entering vehicles to not have to make a turning maneuver until after passing the pad building, helping to alleviate traffic flow into and off of the site. This layout will provide proper stacking distance for exiting vehicles.
- G1i. Major retail buildings and inline tenant buildings shall be oriented towards the highway south of the site. Proper driveways, delivery areas and employee parking shall be provided at the rear of the buildings. Truck delivery areas shall be located to minimize maneuvering and the associated noise adjacent to the residential properties behind the retail center.
- G1j. Onsite vehicle circulation shall include major cross aisles (east-west) located just in front of the major retail buildings and the inline tenants. Any driveways from the highway shall have a direct connection to the cross aisle. Where possible, the cross aisle shall be curved enough to help maintain slow on site speeds, and not curved enough to hamper safe viewing of pedestrians or other maneuvering vehicles. Major intersections shall have enhanced paving and clearly marked cross walks for pedestrian circulation.
- G1k. On site traffic shall be controlled by stop signs at major intersections. The on site vehicle circulation signage shall be designed as a part of the Sign Program for the site. The design package shall include wayfinding signage, signage at all accessible parking spaces, vehicle control signage, and code required signage at each entry. The criteria shall describe the supporting posts, the sign, color and finish of the front sides and rear of the signs, and proper lighting where required.

G2 – Building Massing

G2a. The silhouette of the buildings should be varied, with interplay between flat and curved elements.



Curves create a Soft Silhouette

G2b. The use of bold plaster masses with deep punched openings, accented entryways, and sun control elements is encouraged to control the perceived mass of the buildings.



Deep Set Openings

G2c. Horizontal and vertical building offsets, and curved wall and roof elements to create articulated breaks, visual relief and promote pedestrian circulation shall be included in the design.



Vertical and Horizontal Planes

G2d. Variation in roof lines shall be used to add interest, and reduce the massive scale of the large commercial buildings. Parapet walls, green screens, and roof screens shall be treated as an integral part of the structural design.



GreenWall Panels

G2e. Light and dark building elements can be combined to break down the overall mass of the building.



G2f. Sloping roofs are discouraged, and should be used as small accent roofs only. Sloping roofs shall be metal, such as standing seam roofs.

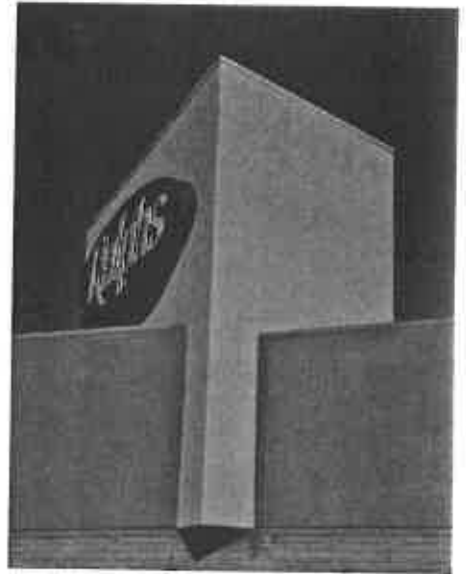


G3 – Vertical Elements

G3a. The base buildings should be simple buildings punctuated with vertical elements such as entry towers and sign pedestals.



Entry Towers



Interplay of Solid Forms

G3b. All roof mounted mechanical equipment shall be screened with building elements such as parapet walls, or within recessed roof wells.

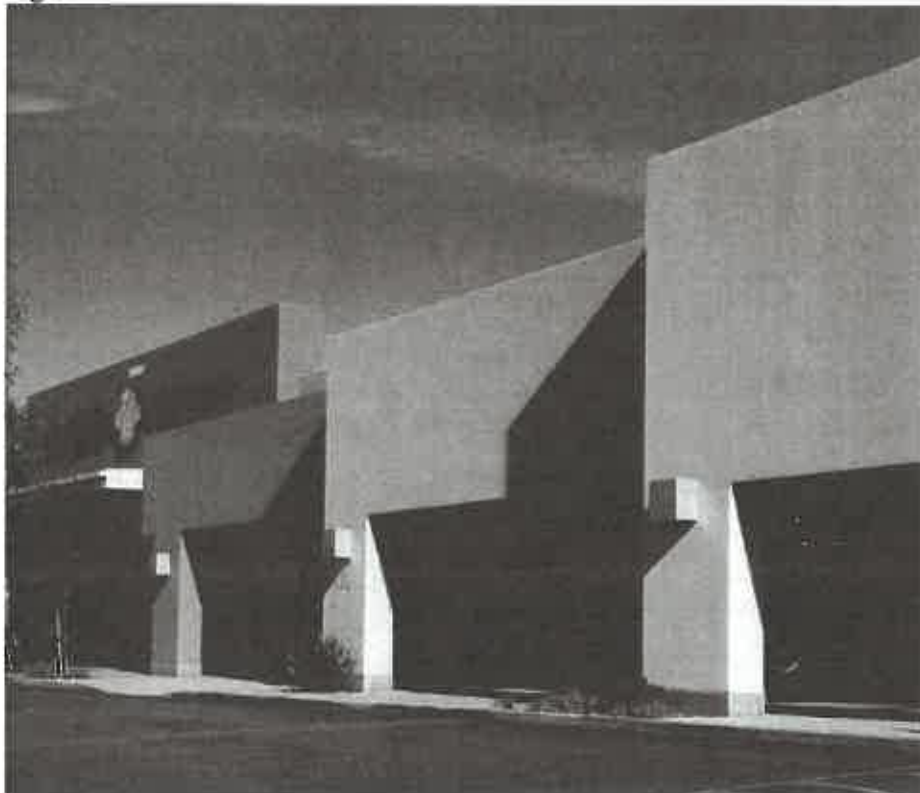


Facade materials blended for walls and roof screens

G3c. Rhythmic elements that wrap back onto the structure give added depth to the facades.



G3d, All buildings should blend stepped vertical parapets and layered building elements to achieve interest through color changes and shadows.



G4 – Facade Colors

G4a. Subtle color changes on the staggered building faces increase the virtual depth of the elevations. While variation in color and texture is encouraged, care should be taken to not incorporate too many materials, colors or details on a single wall or building. Diversity should be achieved in scale with the base building.



G4b. The colors shall be blended, typically with deep tones.



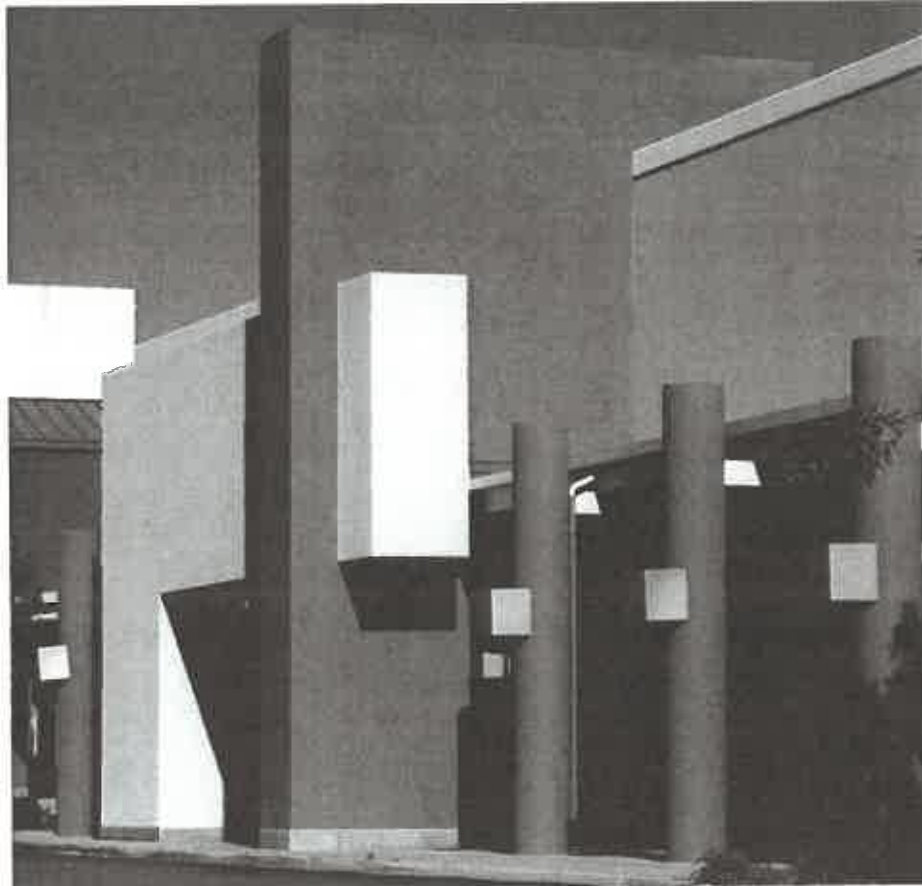
Layering Enhanced by Color

G5 – Interlocking Building Elements

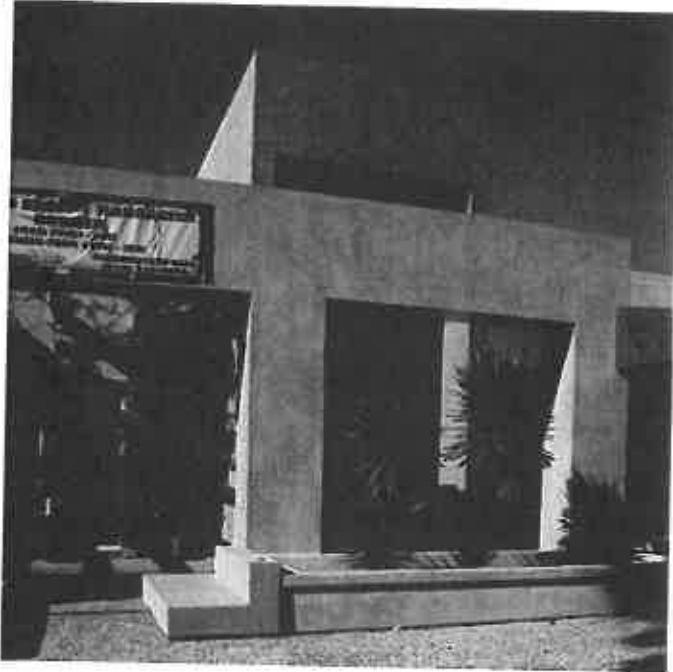
G5a. Large pieces of the building should be interlocked. Interlocking creates strong geometric forms adding to the depth and variety of the façade.



Center Accent



G5b. Articulation of the base, middle and top of the buildings will create visual interest and is encouraged.



G6 – Layering

G6a. Layering the building elements gives individuality to tenants and adds to the visual interest of the overall building. Soft curves in walls or roofs can add to the interest in the building mass. Layering as a way to create sign placement locations should be studied.



Curved Interlocking Forms

G6b. The use of sign panels, or transparent panels such as green wall, is encouraged to add depth while linking different elements together.



G6c. Variation in roof lines shall be used to add interest, and reduce the massive scale of large commercial buildings. Parapet walls and roof screens shall be treated as an integral part of the structural design.



G7 – Building Entries

G7a. The design solution shall provide a strong architectural element that allows easy visibility as well as an ample sign band for each tenant to have separate identity within the center.



Three Dimensional Stepping



Material Accents

G7b. Building entries shall be protected from the elements and articulated by the use of canopies, recesses or arcades.



Subtle Elegance



Deep Recessed Entry



Glass Awning for Accent

G8 – Entry Elements

G8a. The option of vertical glass entries should be considered, giving presence to the main entry. An intermediate metal canopy can provide weather cover for the doors and in some cases a colorful sign band.



Strong Graphics and Bold Colors



Understated with Elegant Materials



Signage/Entry Element Combination

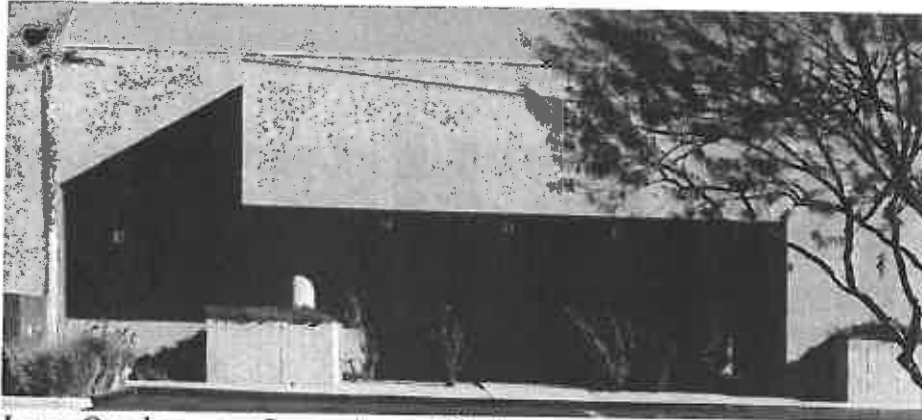
G8b. The entry to the buildings should be punctuated by the use of canopies and structured eyebrows. Entry canopies should be bold in design, readily identifying the entry way. They can be additive to the building or an integral part of the architecture.



Vertical Elements with Strong, Structured Horizontal Accent

G9 – Entry Overhangs

G9a. Overhangs at the entry create a pronounced opening for the entry as well as providing large shaded areas for the pedestrians. Weather and solar protection at the entry points should be included in the design.



Large Overhangs to Create Accent Shadows



Interlocking Color Panels



Overlapping Adjacent Elements With Strong Horizontal Overhang



Three Dimensional Forms



Metal Accents

G10 – Pad Buildings

G10a. Pad buildings require four-sided architecture. Site planning should express the buildings entry. Style may reflect the tenants standard branding and imaging, however there should be a tie into the center architecture through materials and colors.



Corner tower for entry accent



Four sided design



Building details similar to in-line buildings



Corporate identity with tie to colors and materials used in the center. Instead of the corporate white and blue metal canopy, the stucco and soft colors of the center were utilized.



Blend of Textures and Colors



Architectural and Color Accents

IV. DESIGN GUIDELINES

MENIFEE NORTH SPECIFIC PLAN No. 260 Amendment 3

G10b. Service areas, trash enclosures and any exterior storage should be architecturally compatible with the

G11 – Sun Control

G11a. Awnings, overhangs, colonnades, and recessed entries all serve to control the sun. This site will have buildings facing predominantly south, so mid-day to later afternoon sun control will be required. A consistent vocabulary of elements should be used, but varied to provide individual tenant identity.

G11b. Combinations of wood and metal should be used for trellis members and eyebrows. Fabric and metal are encouraged for awnings. For all, consistent maintenance is required to keep the center looking fresh.

G11c. Strategic orientation and configuration of buildings and shading elements shall be used to passively heat and cool spaces. Careful placement of a shade structure will allow for the creation of shade within the open courtyard and not interfere with the open sky placement of the trees. The site plan should address this energy saving aspect of the design.



Steel Eyebrow



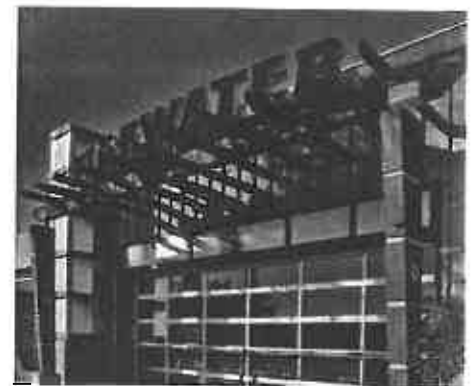
Steel Trellis



Combination of Metal and Wood



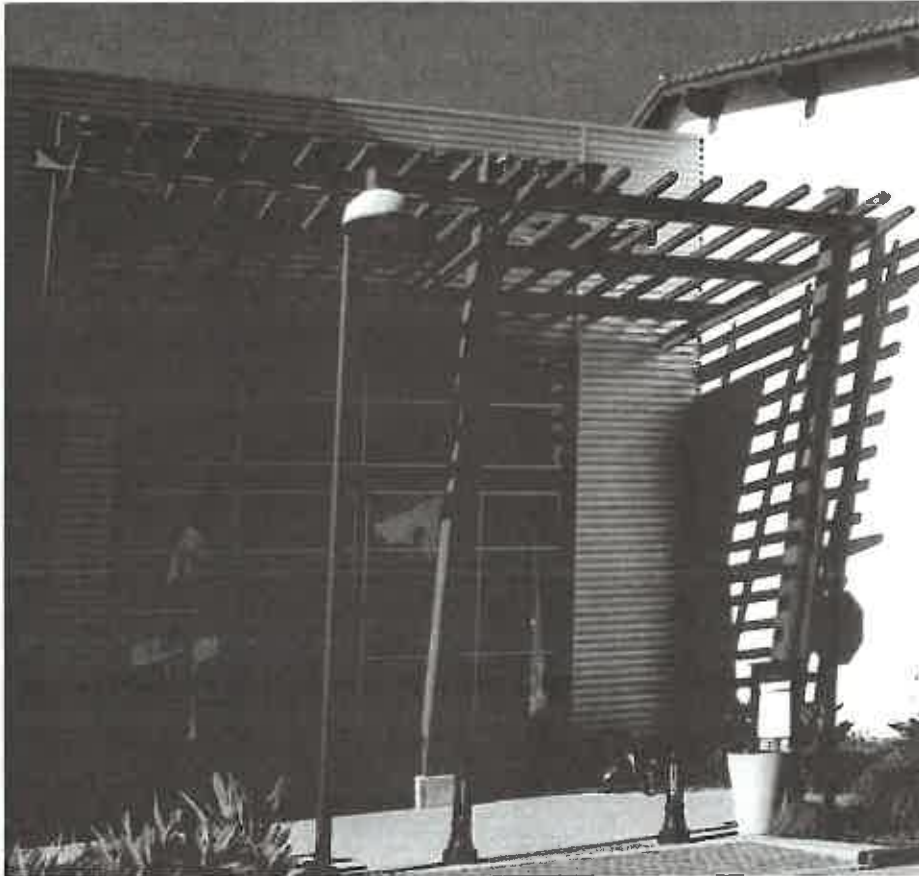
Contemporary Metal Sunshade



Accent Shade Structures

G12 – Trellis Structures

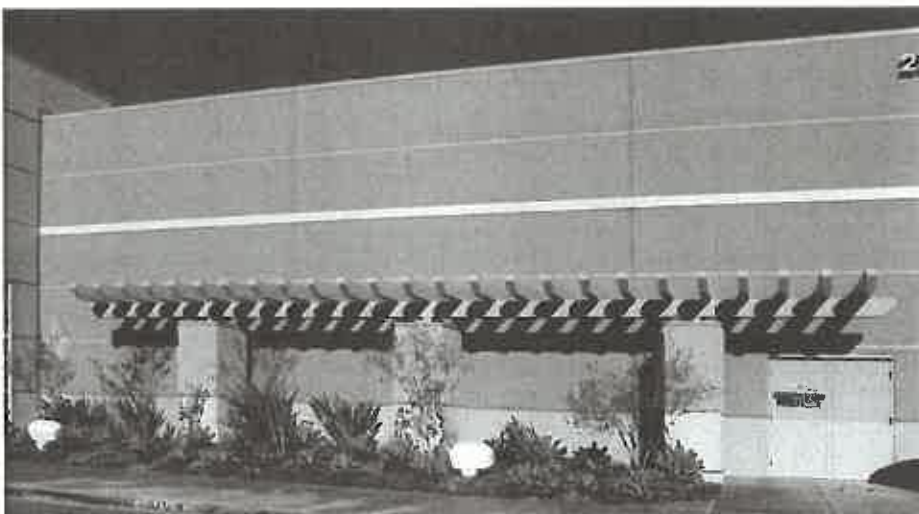
G12a. Trellis overhangs should provide shade and shadow. The shade should be located to benefit the pedestrians while the shadow should be used to enhance and accent architectural features of the building.



Decorative Wood Trellis



Metal Shade Elements Supporting Lighting



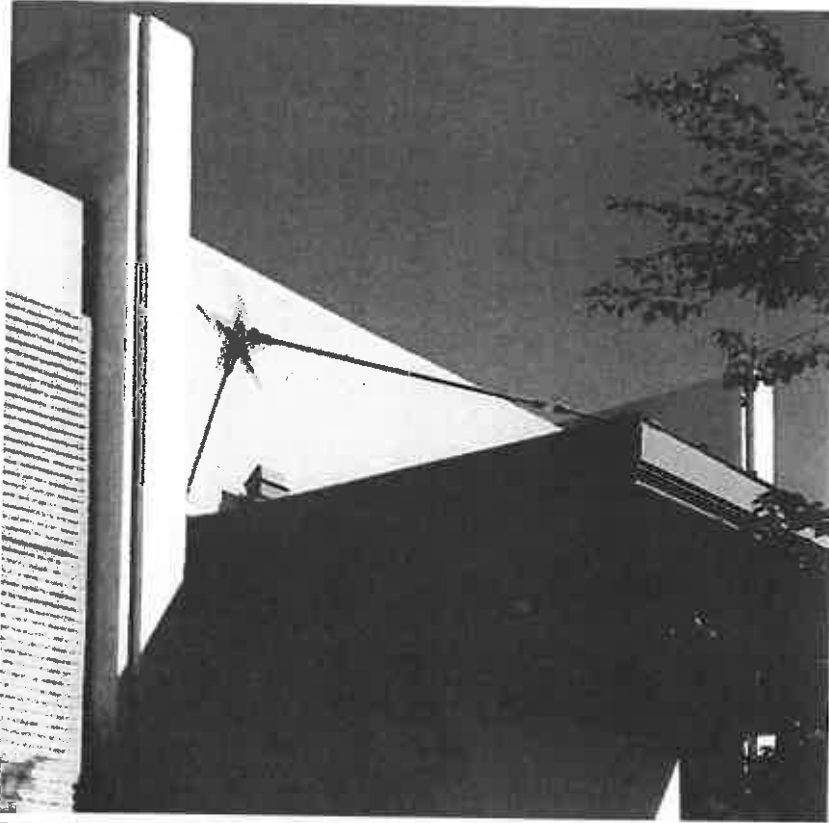
Shallow Trellis Members as an Accent on An Otherwise Blank Wall



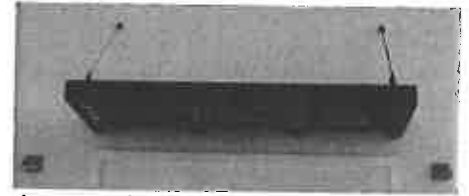
Metal Trellis

G13 – Overhangs

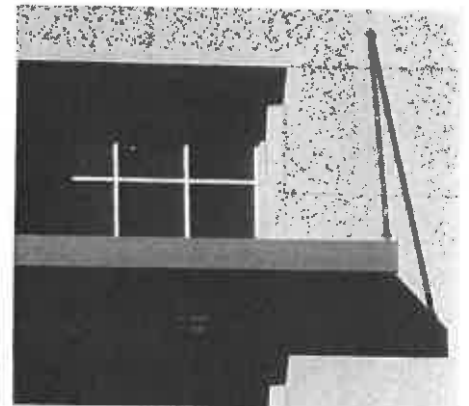
G13a. Eyebrow overhangs can add a level of detail to the building as well as color. A limited vocabulary of styles and materials should be used throughout the center.



Deep Overhangs Create Accent Shading



Accent Shade Canopy



Architectural Overhang



Bold Mix of Vertical and Curves



Curved Forms



Metal Awning Brings Color

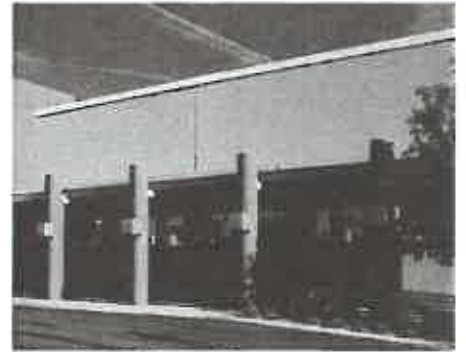
G14 – Dramatic Overhangs and Colonnades

G14a. At major tenants, deep, dramatic overhangs should be utilized to accent entries and shade large portions of the façade.

G14b. Colonnades provide linear shade to the building as well as a covered walkway for pedestrians. Placement of colonnades for pedestrian links throughout the center should be included in the site plan.



Monumental Columns and Overhang



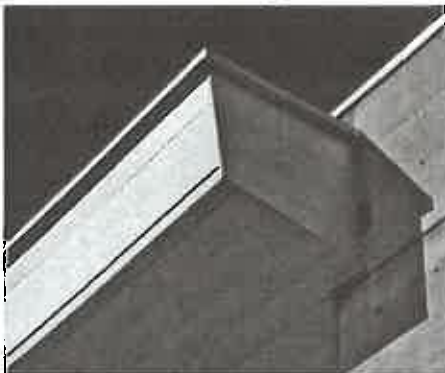
Simple Colonnade



Linear Solid Overhangs



Visually Light Structure



Solid Overhangs



Bold Contemporary Forms

G15 – Sun Control Structures and Awnings

G15a. Independent structures within the parking lots or at building corners can offer way finding cues as well as vertical visual interest. Careful placement of these elements is encouraged to identify main nodes.

G15b. Traditional window awnings can be in fabric or metal. The shape, color and style should match the thematic style of the center.

G15c. The creation of shade along the walkways as well as along the building facades will be accomplished with combinations of:

- Recessed openings
- Awnings
- Trellis structures
- Free standing shade structures
- Deep overhangs
- Eyebrows
- Landscape



Corner Accent Shade Structure



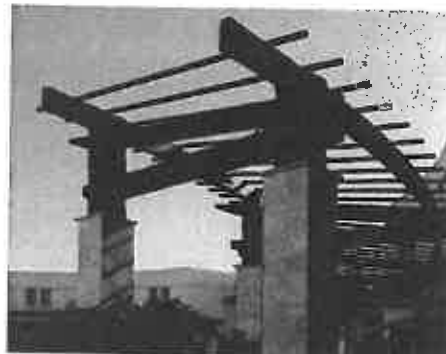
Accent Awnings



Inset Awnings with Signage



Steel and Glass Shading



Curved Metal Arbor



Architectural Elements

G16 – Architectural Metals

G16a. Metal as an accent material should be one of the primary architectural materials. It offers durability and has numerous uses. Metal siding, typically galvanized finish, is a strong counterpoint to smooth plaster walls. Additionally metal detailing in sign bands, green screens and screening shades adds color and detail to the building.

G16b. Metal should be utilized not only as a siding material, but a detail material for eyebrows and awnings, and as the structural members for trellis and shade structures.

G16c. Accent elements such as shade structures will be painted steel and other metals.



Decorative sign band



Horizontal metal siding



Full height accent panels



Metal siding with accent screen panels

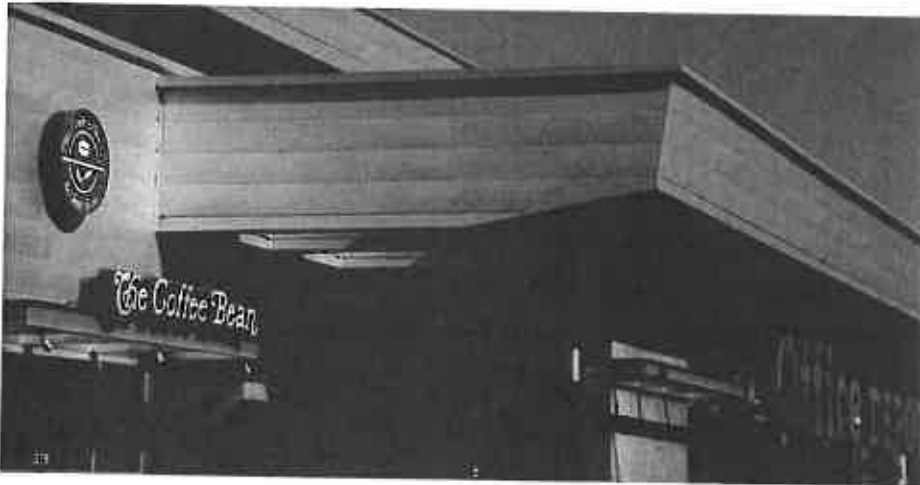


Vertical metal siding

G17 – Wall Surfaces

G17a. As one of the unifying materials, plaster (stucco) will be used on building walls and site walls. It can be both the dominant wall material and an accent material. While a variety of textures may be used on the project, they will not be mixed on a single layer of a building facade. Metal, brick and screening should be integrated into the design.

G17b. Plaster walls and details will be integral color or painted with deeper tones to minimize the reflective glare and provide a more luxurious ambience



Interlocking horizontal metal panels



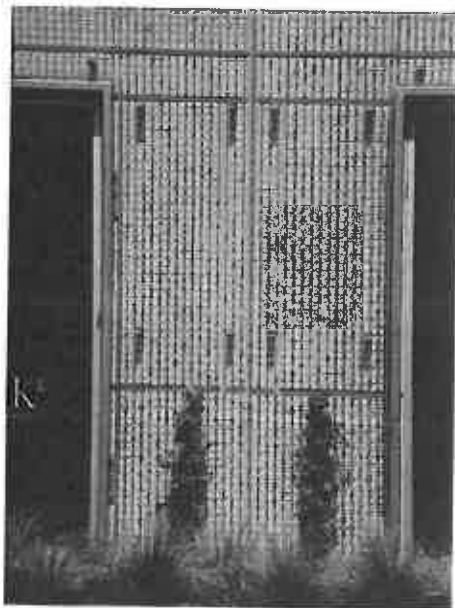
Accent shade screens



Canted and curved wall with vertical metal siding



Horizontal banding in plaster



Greenscreen against a tilt up concrete wall



Brick and Metal Siding with screen accents

G18 – Building Colors

G18a. A variety of colors emphasizes the different building elements and their layers. The project should employ a thoughtful use of color, assisting in tenant branding, a festive atmosphere and visual interest.



Metal and glass accents



Subtle colors enhance the pop out surfaces



Each surface colorized to add depth



Continuity of the fascia color



Combination of plaster colors



Bold color accents

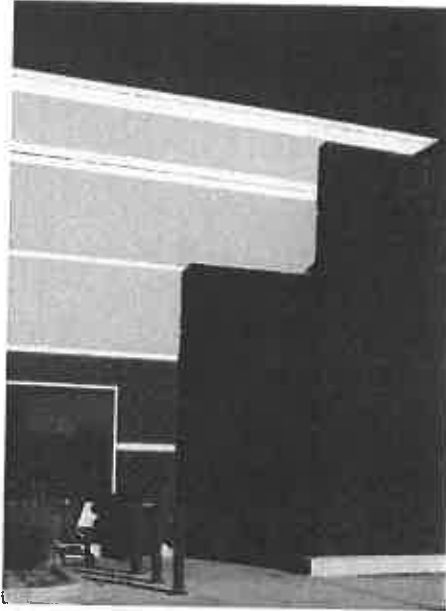
G19 – Wall Materials

G19a. Different wall materials should be selected, allowing for variance in textures and colors.

G19b. The materials proposed should offer a contrast from smooth to textured. Wall materials, base materials, and accent materials for both buildings and site elements should be a combination of: smooth and textured concrete, textured masonry, plaster, metal and composite siding, with accents of stone, tile, and metals.



Horizontal raked finish stucco



Painted tilt up concrete panels



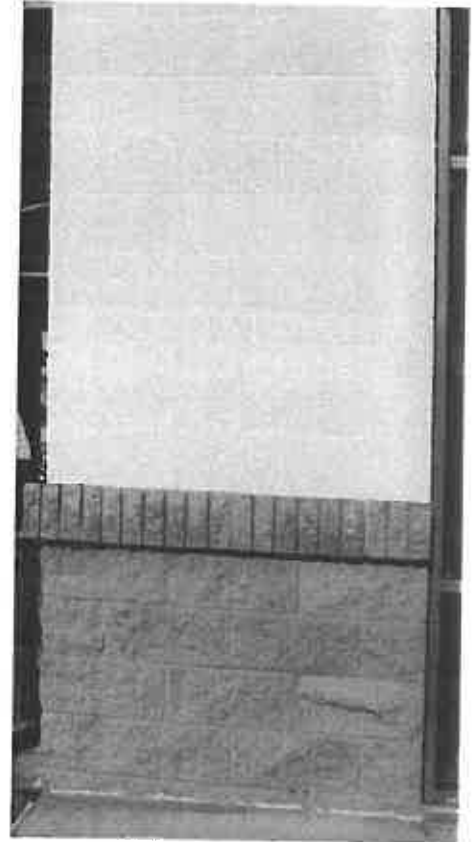
Horizontal siding with stucco



Porcelain tile



Exposed integral color concrete



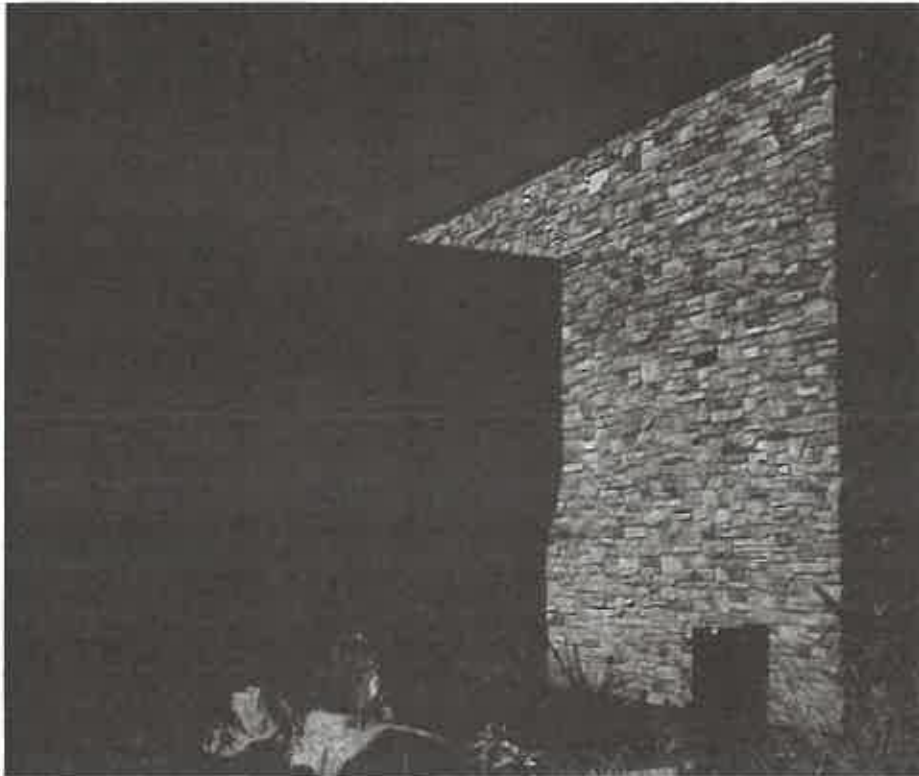
Precision block, fluted block and split faced block column

G20 – Base and Accent Materials

G20a. Natural materials and finishes for accent walls and base materials are encouraged.

G20b. Walls with masonry, natural stone and tile accents should be proposed for some of the building finishes. Combinations of textures and colors should be used. Typical locations for these materials will be entries to buildings, bases and walls of major building elements.

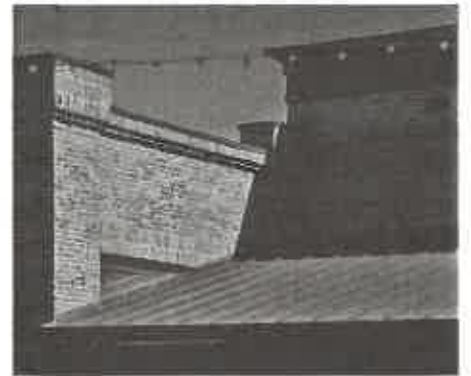
G20c. Architectural detailing utilizing plaster, metal siding, and strong colors shall be included at key places on the buildings and site features.



Stone accent wall



Cantera stone base



Brick walls with standing seam metal roof



Slate tile accent at base



Shot blasted precision concrete block

G21 – Site Furniture

G21a. Outdoor seating areas shall be provided at various locations on the site. Typical locations would be adjacent to major tenants and at corners between two buildings. The spaces should be designed with safety in mind allowing open viewing and be lighted.

G21b. Benches, planters and trash receptacles should be architectural grade and compatible with the colors and style of the architecture and hardscape. The same style furniture should be consistently used throughout the common areas of the site. Private areas such as dining patios may vary from the standard.



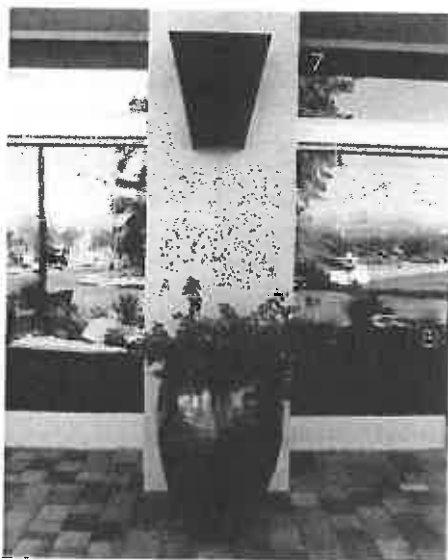
Benches, planters and trash receptacles should be architectural grade and compatible with the colors and style of the architecture and hardscape.



Color Bowl Accents



Hanging Plants



Decorative Pots



Coordinated Site Furnishings

G22 – Landscape

G22a. The landscape design shall be a bold design utilizing plant materials to define the spaces, connect the spaces, and enclose the edges. Required street tree patterns along Highway 74 will be continued and enhanced. Setbacks and edges will have a consistent design palette that includes trees, shrubs, contoured berms, and monumentation for both the entire project and the tenants. Landscape shall conform to the City Adopted Landscape Guidelines.

G22b. Within the Retail Center, the design will encourage pedestrian activity and include shade protection from both structures and landscape.



Integrated Hardscape Elements



Large, Lacy Shade Trees

G22c. Wayfinding will be aided by the use of accent plantings at entry points and thematic plantings along routes. Hardscape elements such as curbs, low walls, seat walls, planters and pilasters will compliment the softscape. These elements will incorporate the building materials, provide seating, and create a foreground for the buildings in some locations.

G22d. Landscaped and lighted passageways through the parking areas will connect to the streetside pad buildings. Pathways will be marked with bold rows of trees, visually linking the street and pad buildings with the main courts and plazas.

G22e. Wayfinding from plaza to plaza will also be enhanced with a landscape edge.

G22f. Edges will include thematic street plantings, shrubs, berms and low walls to screen parking areas, and accent plantings at the major points of access, both vehicle and pedestrian per details within the Specific Plan.

G22g. Transition from landscape to hardscape will be defined with curbs, low walls and pilasters. The landscape will have numerous vertical layers, offering variety in texture and color.



Accent Shadows on the Buildings



Combination of Bold Textures

G23 – Parking Lot Landscape

G23a. The linear promenades through the parking lots will have vertical elements to give prominence and easy visibility while offering a shaded pathway through the use of both structured shade devices and trees.

G23b. Vehicle circulation routes on site shall provide clear and direct paths of travel. Thematic landscape may be used to accent the main circulation aisles.

G23c. Major entries shall include enhanced landscaping at the sides and within any medians.

G23d. The planting within the parking lot will provide shade and offer a canopy that will soften the view over the cars without blocking the signs or views into the buildings.

G23e. Landscape islands will have shrubs and groundcover that is appropriate for the environment, and will enhance the edges of the main driveways.



Trees Surrounding the Signage



End Island Accent Plantings



Shading the Cars and Pavement

G24 – Hardscape

G24a. Hardscape along the storefronts shall be a combination of quality paving, benches, architectural pottery and landscaping. ADA path of travel shall be maintained throughout the site.

G24b. The pedestrian spaces should be enhanced by well designed pavement. The colors, textures and materials of the hardscape will convey direction and quality. The palette may include pavers, colored concrete, natural stone and tile.

G24c. Provide convenient pedestrian circulation between buildings, remote building pads and parking areas.

G24d. Pedestrian circulation and wayfinding shall utilize a strong palette of hardscape and landscape elements and materials.

G24e. Paving details will reflect the use of curves to compliment the architecture. The subtle use of texture as well as the bold use of shape will offer paving variety.

G24f. Architectural pottery should be used to accent building entries, define spaces, separate seating areas, and add color through both the pot and the plantings.



Pavers Provide Texture and Color



Deeper Colors Reflect Less Light



Color, Texture and Pattern

G25 – Lighting

G25a. Lighting fixtures should be decorative, in a style compatible with the architecture. Where applicable, the same family of fixtures should be used for pole and post top locations as well as wall mounting.

G23b. The lighting guidelines listed below are consistent with the City of Menifee and the Mt. Palomar Ordinance. When incorporated into the development they will eliminate sources of light pollution and aesthetic disturbance within the City.

G25c. Exterior lighting will be located and fully shielded so as to direct light away from adjoining lots, buildings, and streets (public or private).

G25d. Any architectural, landscape, or accent lighting (lighting used for decorative effects) will be turned off from one hour after closing until sunrise in order to protect Palomar Observatory from light pollution. This requirement shall exclude outdoor lighting used for illuminating walkways, or other outdoor security lighting as permitted.

G25e. Architectural lighting will include wall mounted decorative fixtures, ground mounted fixtures and in conjunction with architectural details neon and LED lighting may be used. Glare free type opaque fixtures for general task lighting shall be provided. Path lighting shall be accomplished with concealed source post-top fixtures, bollard fixtures and surface mounted building fixtures.

G25f. Parking lot lighting shall be accomplished with pole lighting with a maximum height of 25' or lower depending on a photometric study. Fixtures shall be decorative in a style matching the buildings. Parking lot light fixture locations shall be coordinated with the tree planting plan to avoid conflicts.



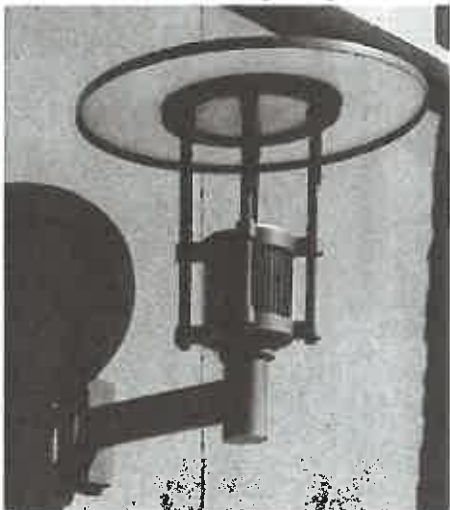
Pedestrian Scaled Lighting



Wall Mounted Accent Lights



Pathway Poletops



Wall Mounted and Pole Top Version of Same Fixture for Continuity



Decorative Wall Sconce

IV. DESIGN GUIDELINES

G26 – Service Areas (Trash Enclosures / Loading Facilities)

G26a. All trash and recycle bins shall be in permanent enclosures as approved by the City. The architectural design of the enclosure, the gates, and the cover if required shall be consistent with the thematic style of the overall center.

G26b. The location of trash enclosures shall provide convenient access for each tenant and be located in discrete areas while still maintaining clear truck access. Loading and trash facilities shall be located and concealed from public view by a combination of walls and landscape.

G26c. Within pedestrian areas all trash receptacles selected shall be in conformity with existing color schemes, materials and site furniture styles.

PAGE BREAK





AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY

August 5, 2019

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Barbara Santos

County Administrative Center
4080 Lemon St., 14th Floor
Riverside, CA 92501
(951) 955-5132

www.aluc.com

Mr. David Alvarez, Project Planner
County of Riverside Planning Division
4080 Lemon Street, 12th Floor
Riverside CA 92501
(VIA HAND DELIVERY)

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW –
DIRECTOR’S DETERMINATION**

File No.: ZAP1378MA19 – Letter 1 of 2
Related File Nos.: SP260SC2 (Specific Plan Substantial Conformance No. 2),
CZ180018 (Change of Zone)
APNs: 457-320-005 through 457-320-007; 457-320-010 through 457-
320-012; 457-320-021; 457-320-023 through 457-320-027; 457-
320-030; 457-320-032; 457-320-035; 457-330-007 through 457-
330-010; 457-330-020; 457-330-021; 457-330-025; 457-330-026.
Airport Zone: Compatibility Zone E

Dear Mr. Alvarez:

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to ALUC Resolution No.15-01 (as adopted on August 13, 2015), staff reviewed County of Riverside Case Nos. SP260SC2 (Specific Plan No. 260, Substantial Conformance No. 2), a proposal to re-allocate the number of dwelling units among Planning Areas 24, 25, 26, and 28 of the Menifee North Specific Plan, as amended through Amendment No. 2, affecting a 112.7-acre area of that Specific Plan located southerly of Watson Road, easterly of Briggs Road, westerly of Sultana Road, and northerly of (but not contiguous to) State Highway Route 74, modify the development standards for these Planning Areas, and correct the land use designations for Planning Areas 26 and 28 to correspond to the approved tentative tract map for that area, and CZ180018 (Change of Zone), a proposal to amend the Specific Plan Zoning Ordinance for Specific Plan No. 260 to modify the minimum lot size requirements and other development standards applicable to Planning Areas 24, 25, 26, and 28 in accordance with the Substantial Conformance proposal. (This proposal does not affect the portions of the Specific Plan that are now included within the boundaries of the City of Menifee.)

Specifically, the Substantial Conformance proposal and the implementing amendments to the Specific Plan Zoning Ordinance affect the Planning Area descriptions as follows:

- Planning Area 24: Increase area from 25.7 to 26.1 acres; increase dwelling unit count from 115 to 125 (+10 DUs); increase residential density from 4.5 to 4.8 dwelling units

AIRPORT LAND USE COMMISSION

per acre (Medium Density Residential designation retained)

- Planning Area 25: Decrease area from 44.7 to 44.6 acres; increase dwelling unit count from 226 to 238 (+12 DUs); increase residential density from 5.1 to 5.3 dwelling units per acre; reduce the minimum lot size from 6,000 square feet to 3,500 square feet; add a 3.2-acre mini-park (Medium High Density Residential designation retained)
- Planning Area 26: Increase area from 18.2 to 19.6 acres; decrease dwelling unit count from 97 to 80 (-17 DUs); decrease residential density from 5.3 to 4.1 dwelling units per acre; amend land use designation from Medium High Density Residential to Medium Density Residential; acknowledge provision of a 0.2-acre mini-park
- Planning Area 28: Increase area from 21.8 to 22.4 acres; decrease dwelling unit count from 113 to 108 (-5 DUs); decrease residential density from 5.2 to 4.8 dwelling units per acre; amend land use designation from Medium High Density Residential to Medium Density Residential
- 2.3 acres are transferred from Major Roadways area to Medium Density Residential.

The site is located within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, residential density and non-residential intensity are not restricted.

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan ("March ALUCP"), provided that the County of Riverside incorporates in the amended text of the Specific Plan an acknowledgement that the Specific Plan is located within Compatibility Zone E of the March Air Reserve Base/Inland Port Airport and that subsequent underlying entitlements will be reviewed in light of the then-applicable Airport Land Use Compatibility Plan.

This finding of consistency relates to airport compatibility issues and does not necessarily constitute an endorsement of the proposed Substantial Conformance determination and change of zone/ordinance amendment. As the site is located within Compatibility Zone E, the Specific Plan and its Zoning Ordinance are consistent with the March ALUCP with or without the proposed changes, in whole or in part.

If you have any questions, please contact Paul Rull, ALUC Principal Planner, at (951) 955-6893.

Sincerely,
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Simon A. Housman, ALUC Director

Attachments: Notice of Airport in Vicinity

AIRPORT LAND USE COMMISSION

cc: WSI Land Holdings, LLC – Attn.: Brian Hardy (applicant/landowner)
Gary Gosliga, Airport Manager, March Inland Port Airport Authority
Sean Feeley or Denise Hauser, March Air Reserve Base
ALUC Case File

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AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY

August 5, 2019

Mr. David Alvarez, Project Planner
County of Riverside Planning Division
4080 Lemon Street, 12th Floor
Riverside CA 92501
(VIA HAND DELIVERY)

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**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW –
DIRECTOR’S DETERMINATION**

File No.: ZAP1378MA19 – Letter 2 of 2
Related File Nos.: TR37553 (Tentative Tract Map), TR29322MC1 (Tentative Tract Map Minor Change No.1), PM37690 (Tentative Parcel Map)
APNs: 457-320-005 through 457-320-007; 457-320-010 through 457-320-012; 457-320-021; 457-320-023 through 457-320-027; 457-320-030; 457-320-032; 457-320-035 (TTM37533)
457-330-007 through 457-330-010; 457-330-020; 457-330-021; 457-330-025; 457-330-026 (TTM29322)
Airport Zone: Compatibility Zone E

STAFF

Director
Simon A. Housman

John Guerin
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Barbara Santos

County Administrative Center
4080 Lemon St., 14th Floor
Riverside, CA 92501
(951) 955-6132

www.rcaluc.org

Dear Mr. Alvarez:

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to Policy 1.5.2(d) of the Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan, staff reviewed County of Riverside Case Nos. TR37553 (Tentative Tract Map No. 37553), a proposal to divide 70.7 gross acres (Planning Areas 24 and 25 of Specific Plan No. 260, as amended by Substantial Conformance No. 2) located southerly of Watson Road, easterly of Briggs Road, and westerly of Sultana Road into 363 residential lots (with a 3.2-acre park), TR29322MC1 (Tentative Tract Map No. 29322, Minor Change No.1), a proposal to modify approved, but as yet unrecorded, Tentative Tract Map No. 29322 so as to reduce the number of residential lots therein from 210 to 188 while increasing its area by 2 acres, and PM37690 (Tentative Parcel Map No. 37690), a proposal to divide the larger 114.14 gross acres constituting the combined areas of the two tracts into five (5) parcels for financing purposes.

The site is located within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, residential density and non-residential is not restricted.

Although the project is located within the March Air Reserve Base/Inland Port AIA, the actual nearest runway is Runway 15-33 at Perris Valley Airport. However, the southerly terminus of this runway is located approximately 24,900 feet from the project site, so the site is beyond the 20,000-foot radius within which notice to the Federal Aviation Administration Obstruction Evaluation Service (FAA OES) could be required. Runway 14-32 at March Air Reserve Base/Inland Port Airport at its southerly terminus is 10 miles from the site. Therefore, FAA OES

AIRPORT LAND USE COMMISSION

review for height/elevation reasons was not required.

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, provided that the County of Riverside applies the following recommended conditions for Tentative Tract Map No. 37553:

CONDITIONS:

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site, in accordance with Note A on Table 4 of the Harvest Valley/Winchester Area Plan:
 - (a) Any use or activity which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use or activity which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use or activity which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area.
 - (d) Any use or activity which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
3. The following uses/activities are specifically prohibited at this location: trash transfer stations that are open on one or more sides; recycling centers containing putrescible wastes; construction and demolition debris facilities; wastewater management facilities; incinerators; and hazards to flight.
4. The attached notice shall be provided to all potential purchasers of the proposed lots and to tenants of the homes thereon.
5. All new aboveground detention or water quality basins on the site shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention/water quality basin(s) that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.

AIRPORT LAND USE COMMISSION

If you have any questions, please contact Paul Rull, ALUC Principal Planner, at (951) 955-6893.

Sincerely,
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Simon A. Housman, ALUC Director

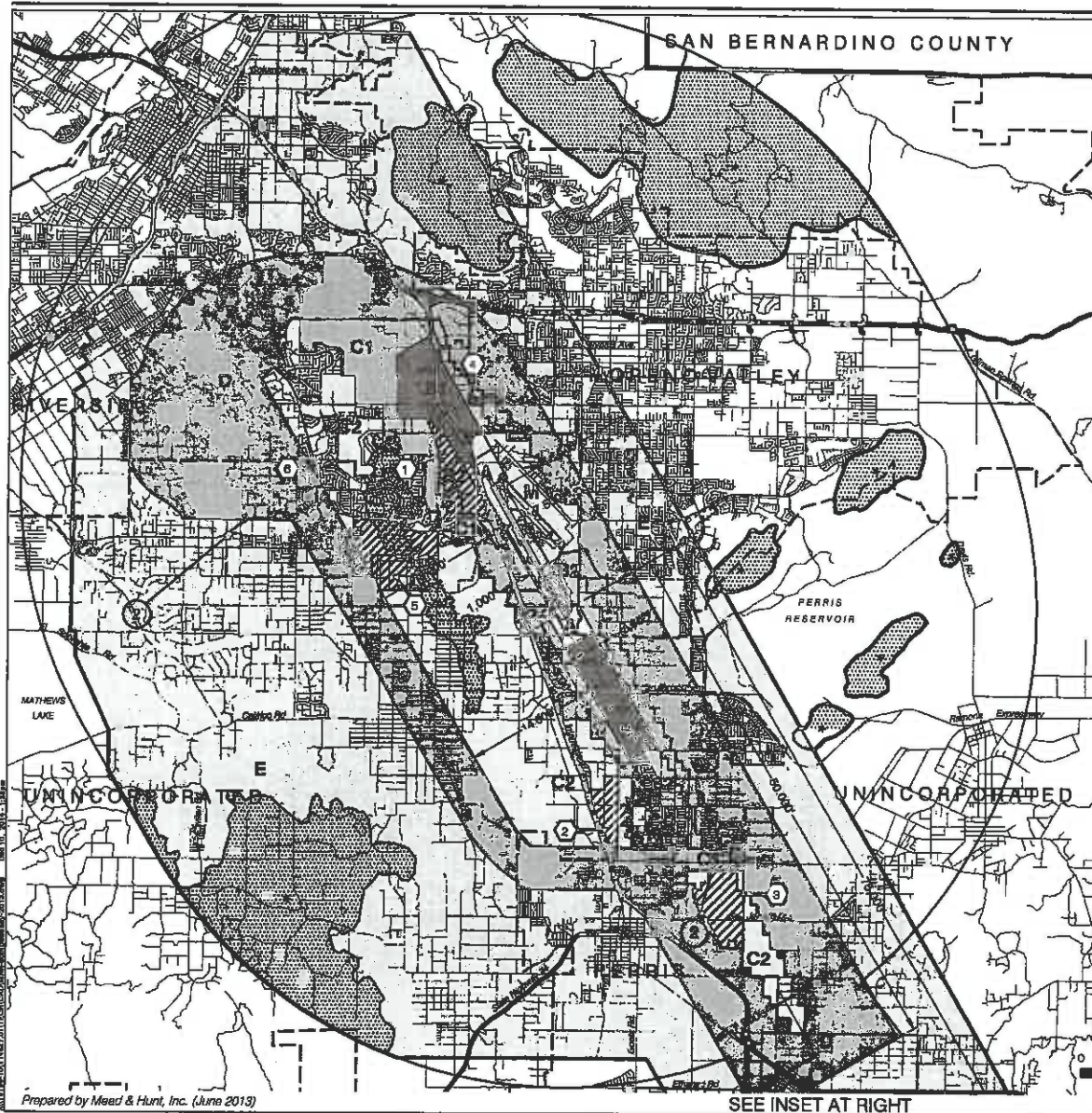
Attachments: Notice of Airport in Vicinity

cc: WSI Land Holdings, LLC (Attn.: Brian Hardy) (applicant/landowner)
Gary Gosliga, Airport Manager, March Inland Port Airport Authority
Sean Feeley or Denise Hauser, March Air Reserve Base
ALUC Case File

Y:\AIRPORT CASE FILES\March\ZAP1378MA19\ZAP1378MA19TTM.LTR.doc

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



LEGEND

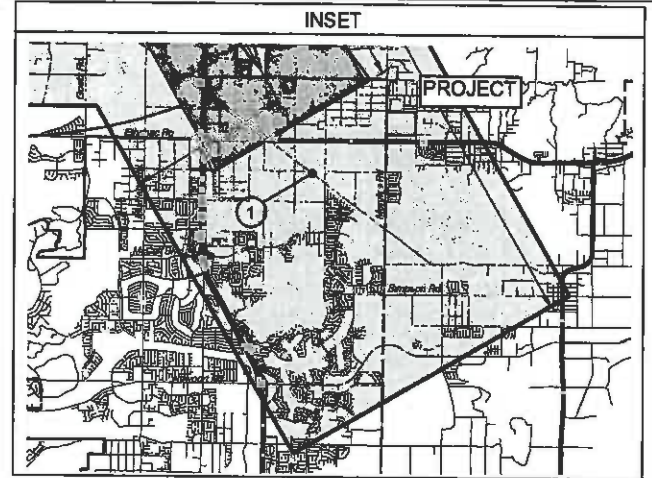
Compatibility Zones

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

Boundary Lines

- March Air Reserve Base / Air Force Property
 - March Joint Powers Authority Property Line
 - County Boundary
 - City Limits
 - ▨ Site-Specific Exceptions (existing local agency commitments to development projects)
- ① March JPA: March Business Center/Meridian
 - ② Perris: Harvest Landing
 - ③ Perris: Park West
 - ④ Moreno Valley: Affordable Housing
 - ⑤ March JPA: Ben Clark Training Center
 - ⑥ Riverside: Ridge Crest Subdivision

- ① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,555 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.



Riverside County
Airport Land Use Commission

March Air Reserve Base / Inland Port Airport
Land Use Compatibility Plan

(Adopted November 13, 2014)

Map MA-1

Compatibility Map
March Air Reserve Base / Inland Port Airport

Note:
All dimensions are measured from
runway ends and centerlines.

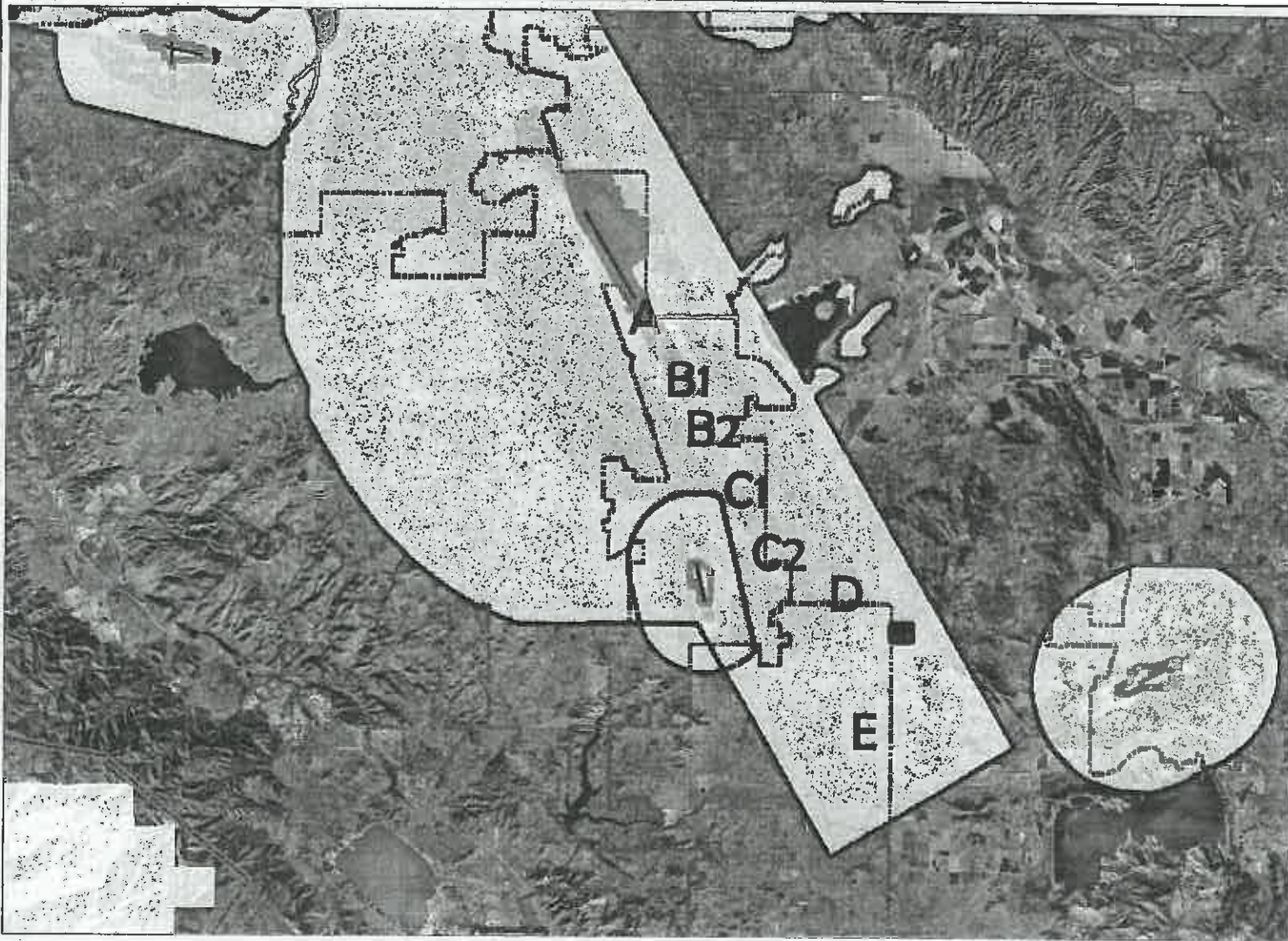
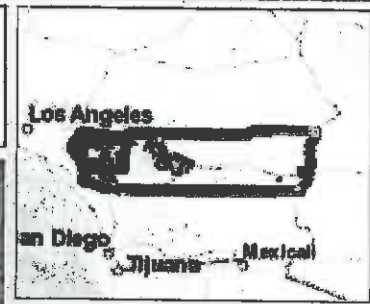


Base map source: County of Riverside 2013

Prepared by Mead & Hunt, Inc. (June 2013)

SEE INSET AT RIGHT

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones
 - ▨ OTHER COMPATIBILITY ZONE
 - A
 - A-EXC1
 - B1
 - B1-APZ I
 - B1-APZ I-EXC1
 - B1-APZ II
 - B1-APZ II-EXC1
 - B1-EXC1
 - B2
 - B2-EXC1
 - C
 - C1
 - C1-EXC1
 - C1-EXC3
 - C1-EXC4
 - C1-HIGHT
 - C2
 - C2-EXC1
 - C2-EXC2
 - C2-EXC3
 - C2-EXC5
 - C2-EXC6



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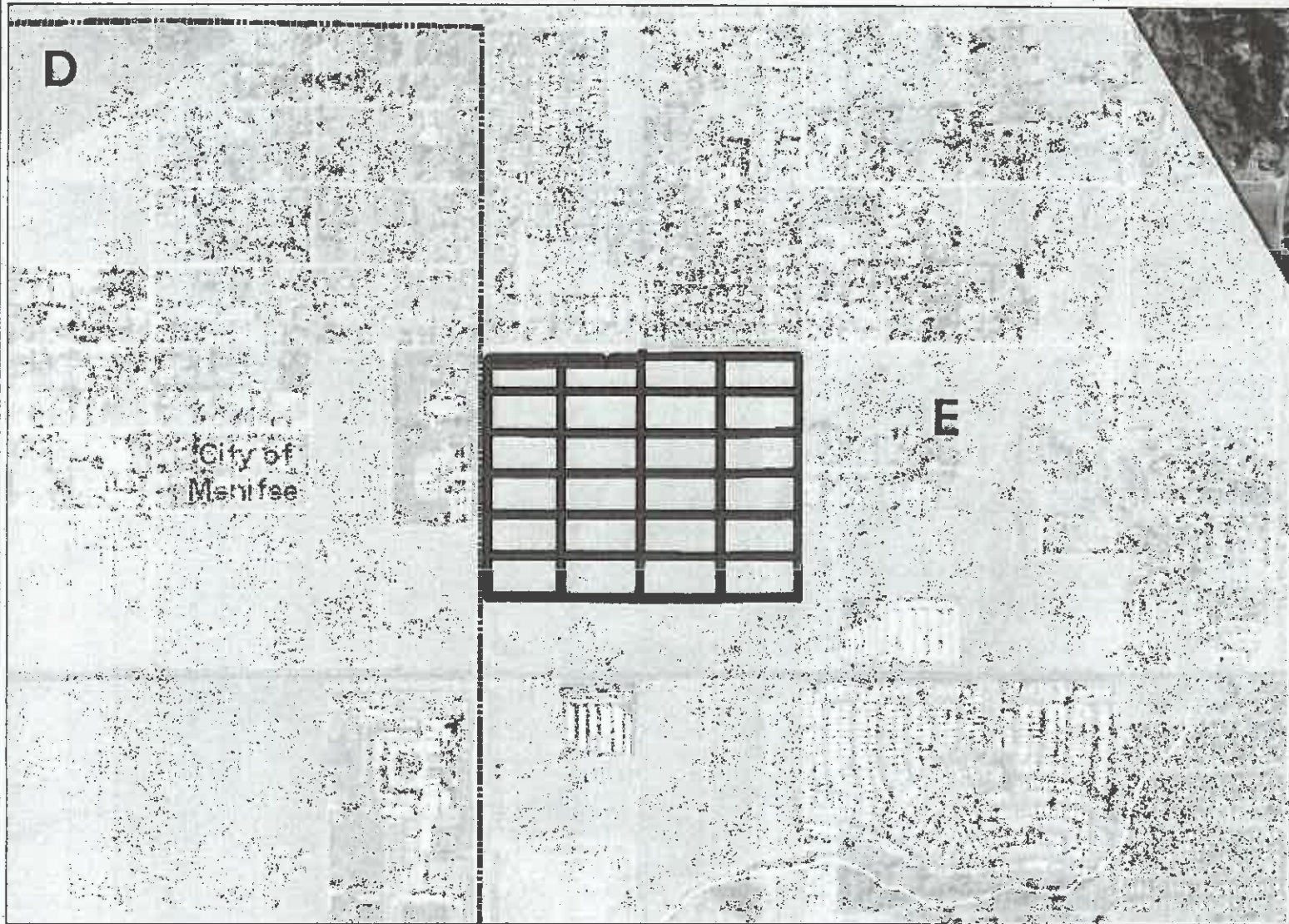


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Notes

Map My County Map



Legend

- Runways
- ◻ Airports
- ◻ Airport Influence Areas
- Airport Compatibility Zones**
- ◻ OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC8

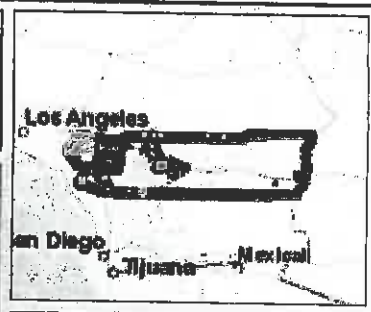
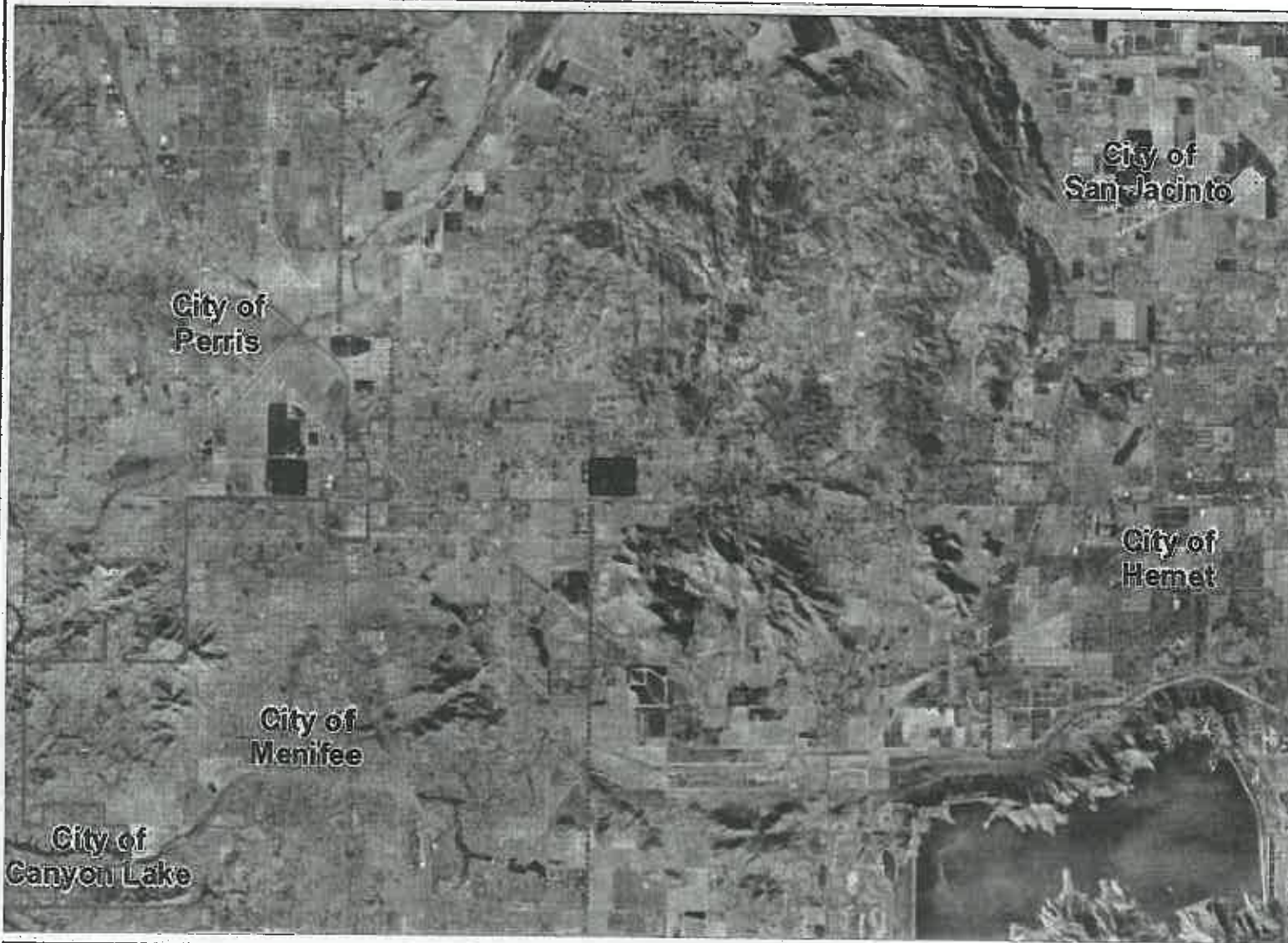


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Notes



Map My County Map



Legend

- City Areas
- World Street Map

Notes



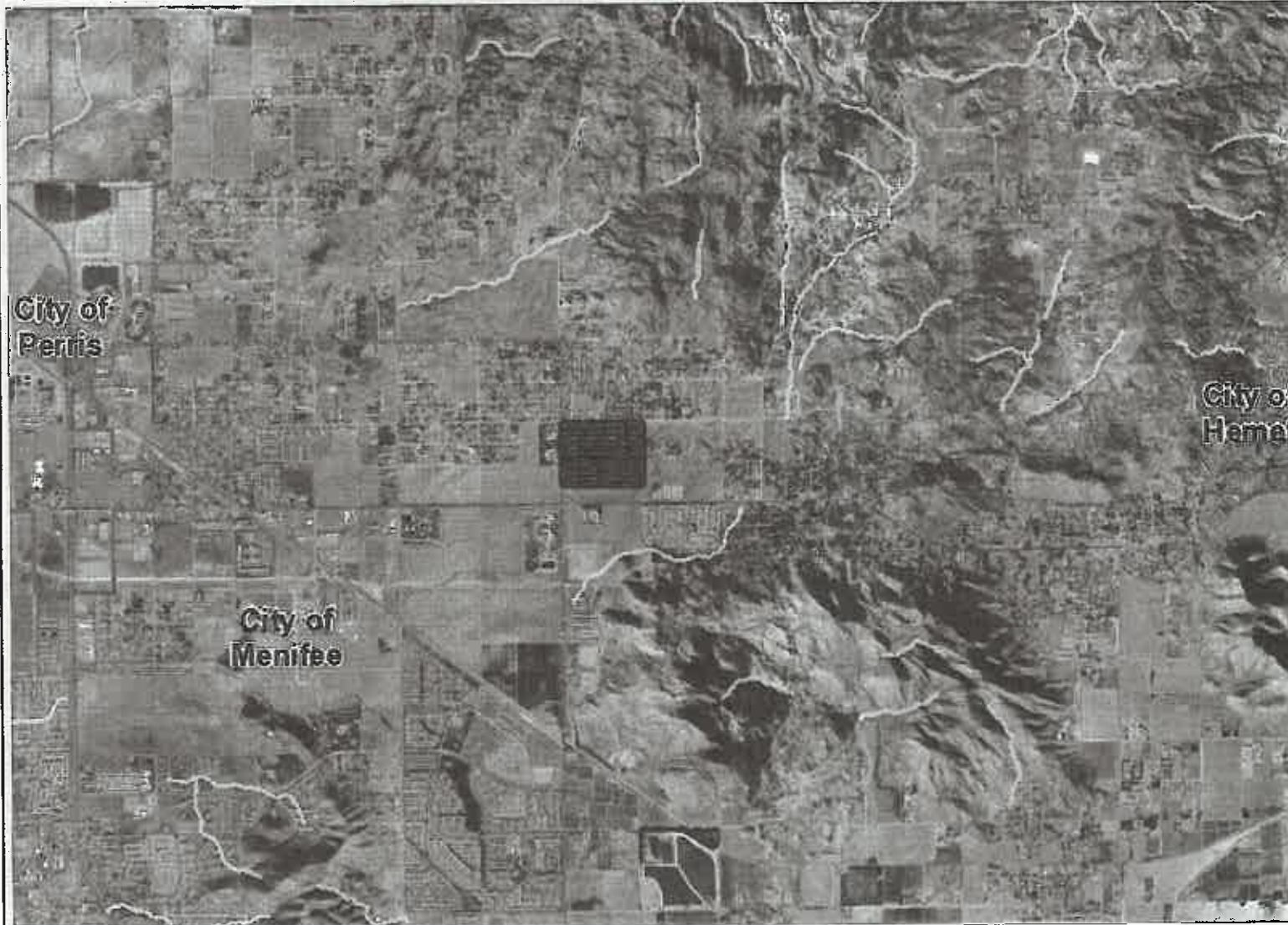
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.






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Map My County Map



Legend

-  Blueline Streams
-  City Areas
-  World Street Map



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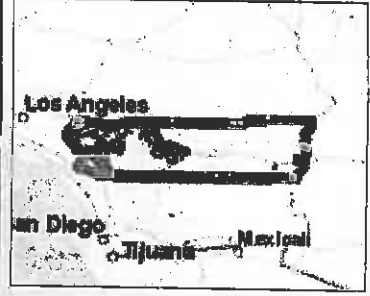
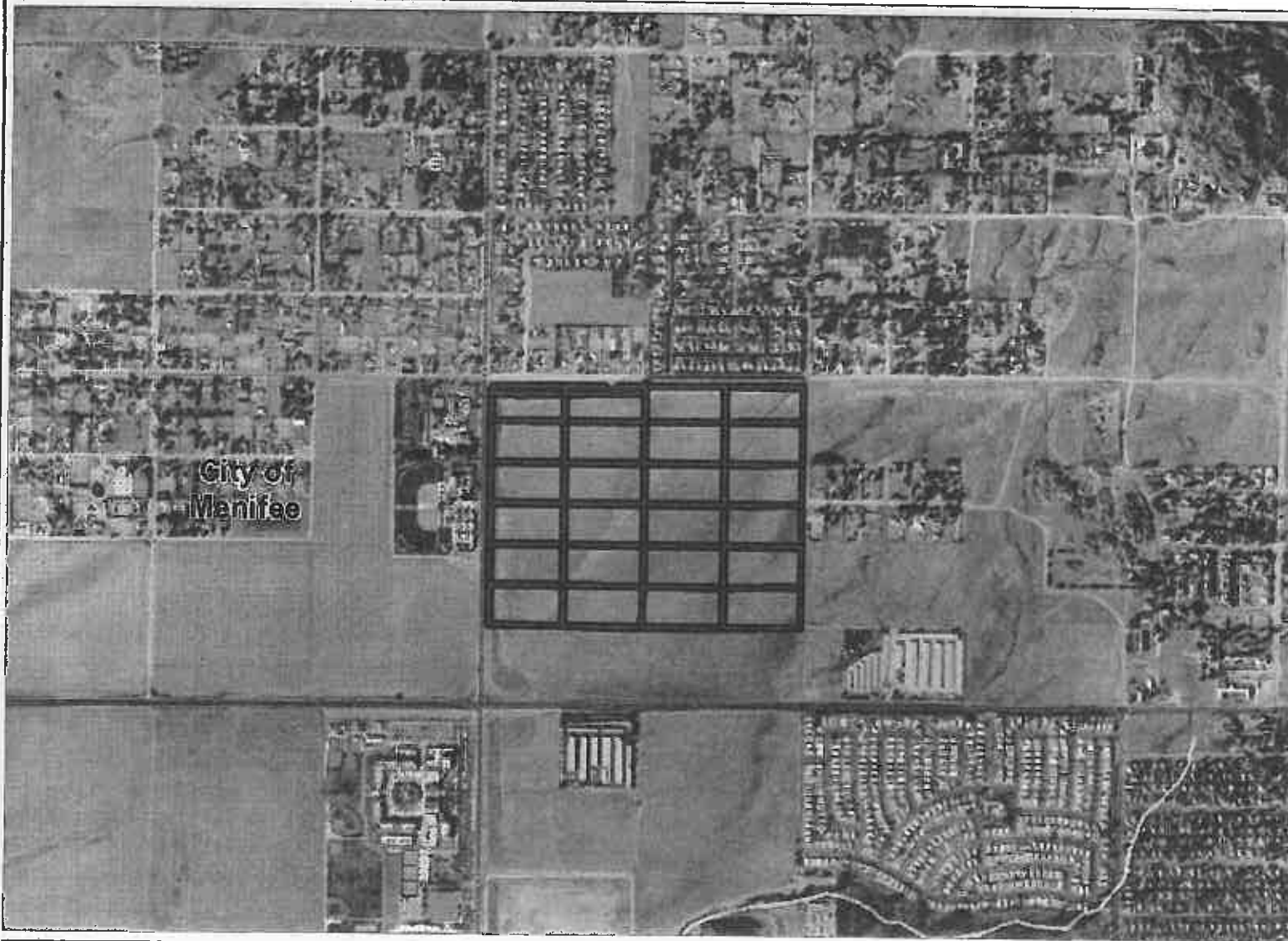
Notes



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Map My County Map



Legend

- Blueline Streams
- ▣ City Areas
- World Street Map

Notes



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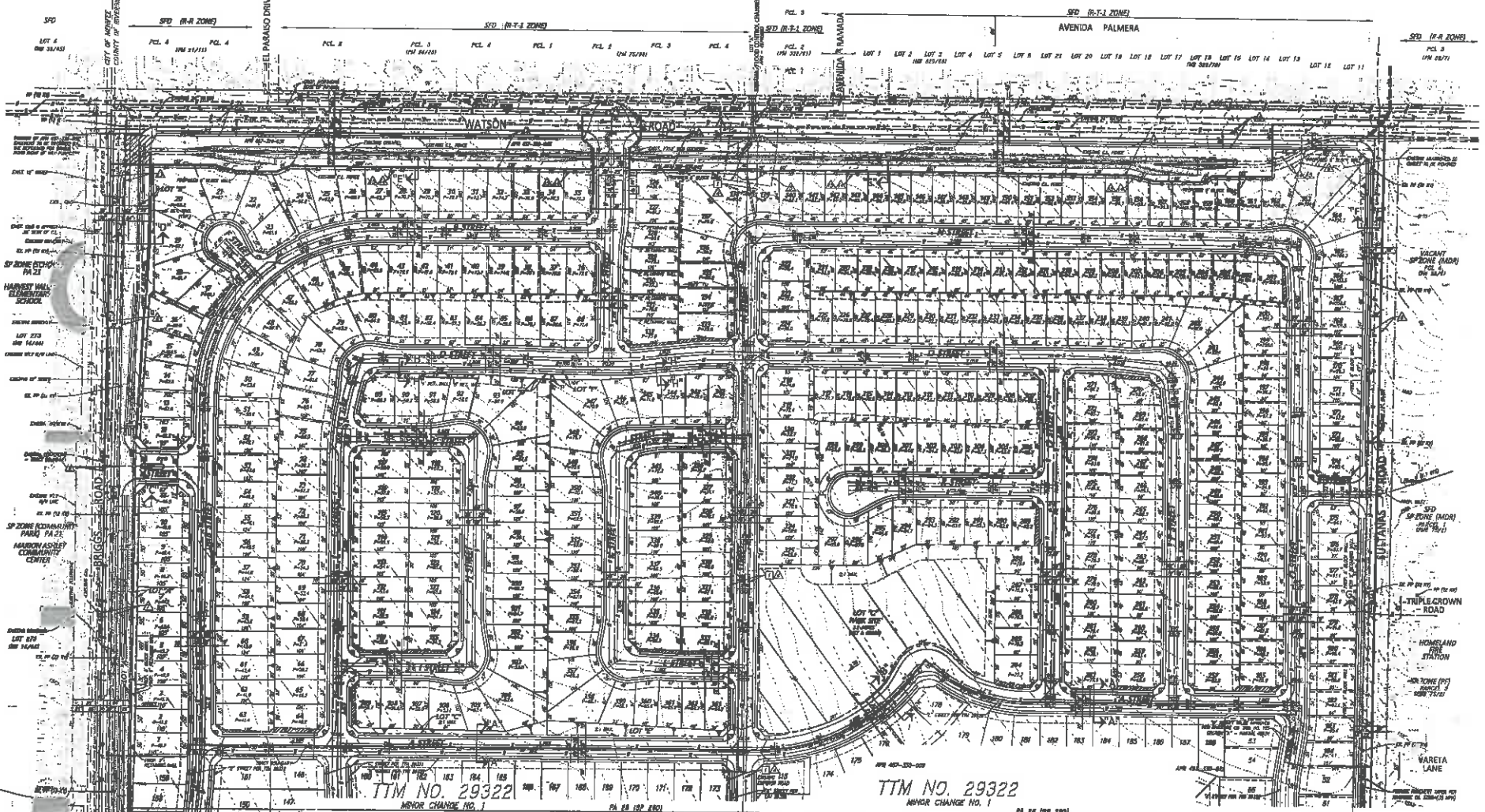
REPORT PRINTED ON... 7/25/2019 7:43:36 AM

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IN THE COUNTY OF RIVERSIDE TENTATIVE TRACT MAP NO. 37533

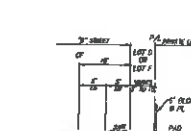
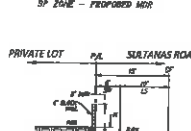
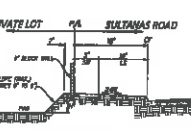
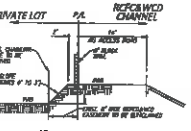
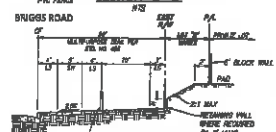
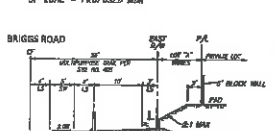
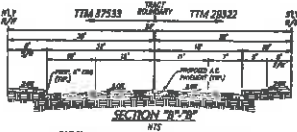
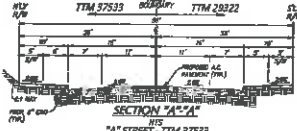
PLANNING AREA 24			PA 24 (CONT)			PA 25 (CONT)			PA 25 (CONT)		
Lot Number	Lot Area (SQ FT)	Prop Area (SQ FT)	Lot Number	Lot Area (SQ FT)	Prop Area (SQ FT)	Lot Number	Lot Area (SQ FT)	Prop Area (SQ FT)	Lot Number	Lot Area (SQ FT)	Prop Area (SQ FT)
1	3320	3520	106	1032	1190	180	4247	4247	283	4960	4880
2	3500	3500	107	6212	6212	181	4247	4247	284	4960	5015
3	3420	3420	108	4230	4230	182	4247	4247	285	4960	5050
4	3400	3400	109	6180	6180	183	4247	4247	286	4960	4930
5	3320	3320	110	3000	3000	184	4247	4247	287	4960	4970
6	3350	3350	111	3000	3161	185	4247	4247	288	4960	4861
7	3220	3220	112	4213	4245	186	4247	4247	289	4960	5002
8	3200	3200	113	3200	3200	187	4247	4247	290	4960	5000
9	3250	3250	114	3200	3200	188	4247	4247	291	4960	5000
10	3250	3250	115	3200	3200	189	4247	4247	292	4960	5000
11	3620	3620	116	3200	3200	200	5012	4768	303	5000	5008
12	3980	3420	117	3200	3200	201	3288	4726	304	5000	4786
13	3724	3242	118	3200	3200	202	4244	4244	305	5000	4631
14	3880	3225	119	4212	4212	203	4247	4247	306	5000	4664
15	4214	4214	120	4212	4212	204	4247	4247	307	4970	4907
16	4072	4091	121	3000	3200	205	4085	3841	308	4452	4452
17	4790	3864	122	3200	3200	206	4162	3849	309	4152	4137
18	4816	4816	123	3200	3200	207	4252	3843	310	4652	4652
19	7676	7676	124	3200	3200	208	4085	3841	311	4452	4273
20	12408	12311	125	3200	3200	209	4085	3849	312	4452	4182
21	8461	8252	126	3200	3200	210	4140	4160	313	4452	4016
22	7772	6233	127	5662	4877	211	4140	4140	314	4452	3525
23	6213	5110	128	2064	-	212	4140	4140	315	4402	3840
24	7828	7828	129	4242	-	213	4140	4140	316	4452	3761
25	3713	3713	130	4242	-	214	4140	4140	317	4742	3881
26	4008	3713	131	4242	-	215	4140	4140	318	4742	3717
27	3465	2463	132	4242	-	216	4140	4140	319	4200	4521
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29	3250	2250	134	4140	4140	218	4140	4140	321	4302	4532
30	3192	3192	135	4140	4140	219	4140	4140	322	4000	4467
31	3290	3290	136	1204	12875	220	4140	4140	323	4000	4467
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IN THE COUNTY OF RIVERSIDE
TENTATIVE TRACT MAP NO. 37533

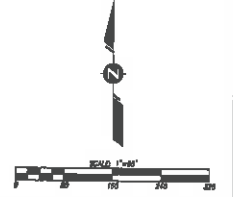


TTM NO. 29322
 MINOR CHANGE NO. 1

TTM NO. 29322
 MINOR CHANGE NO. 1



NOTES
 AND ALSO FIRST TO ALL PROPOSED
 ELEVATIONS SHOWN ON THIS
 TENTATIVE TRACT MAP.
 ANY OVERLAY FROM LOT 136 SHALL BE
 LOCATED ON THE WESTSIDE OF THE
 LOT.



COUNTY OF RIVERSIDE
TENTATIVE TRACT MAP
NO. 37533
 SCHEDULE "M"
TENTATIVE TRACT MAP

KA ENGINEERING
LAND PLANNING
SURVEYING
 257 N. SHERRAN STREET
 SUITE 117
 CORONA, CALIFORNIA 92620
 TEL. (951) 270-1800
 FAX (951) 270-4200

Drawn By: []
 Checked By: []
 Scale: As Shown

IN THE COUNTY OF RIVERSIDE
TENTATIVE TRACT MAP NO. 37533

EXISTING EASEMENT NOTES

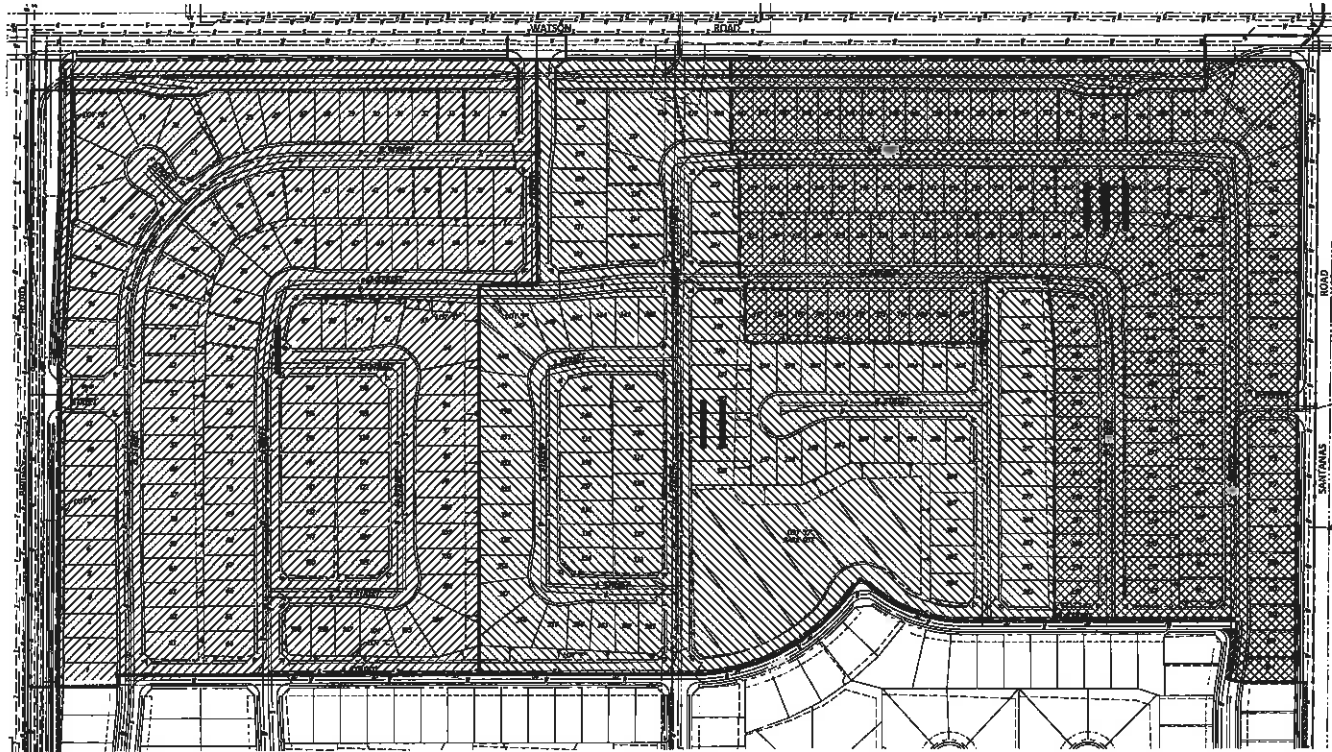
- 1 AN EASEMENT FOR PUBLIC UTILITIES, OTHERS, SERVICES, RIGHTS, BELLS AND INCIDENTAL PURPOSES, RECORDED SEPTEMBER 26, 2007 AS INSTRUMENT NO. 2007-090765 OF OFFICIAL RECORDS, IN FAVOR OF: WESTERN POWER COMPANY
 AFFECTS: AS DESCRIBED THEREIN (PLOTTED HEREIN FROM RECORD)
- 2 AN EASEMENT FOR PUBLIC UTILITIES AND INCIDENTAL PURPOSES, RECORDED APRIL 11, 1988 IN BOOK 708 OF RECORDS, PAGE 188
 IN FAVOR OF: SOUTHERN CALIFORNIA POWER COMPANY
 AFFECTS: AS DESCRIBED THEREIN (PLOTTED HEREIN FROM RECORD)
- 3 AN EASEMENT FOR PUBLIC UTILITIES AND INCIDENTAL PURPOSES, RECORDED OCTOBER 26, 1984 AS INSTRUMENT NO. 2007-090765 OF OFFICIAL RECORDS
 IN FAVOR OF: CALIFORNIA ELECTRIC POWER COMPANY
 AFFECTS: AS DESCRIBED THEREIN (PLOTTED HEREIN FROM RECORD)
- 4 THE EFFECT OF A MAP PURPORTING TO SHOW THE LAND AND OTHER PROPERTY, FILED JANUARY 22, 1975 IN BOOK 546 PAGE 88 OF RECORDS OF SURVEYS (PLOTTED HEREIN) CLOSING THEREON SHOWN TO BE INVALID HEREON ALONG WITH ALL EASEMENTS AND UTILITY RIGHTS
- 5 AN EASEMENT FOR STORM DRAIN AND INCIDENTAL PURPOSES, RECORDED JULY 19, 2006 AS INSTRUMENT NO. 2006-085036 OF OFFICIAL RECORDS
 IN FAVOR OF: RIVERSIDE COUNTY FLOOD CONTROL AND WATER CONSERVATION DISTRICT
 AFFECTS: AS DESCRIBED THEREIN (PLOTTED HEREIN FROM RECORD)
 EASEMENT TO BE ABANDONED BY THE RECORDOR ON THE FINAL MAP
- 6 AN EASEMENT FOR STORM DRAIN AND INCIDENTAL PURPOSES, RECORDED JULY 19, 2006 AS INSTRUMENT NO. 2006-085036 OF OFFICIAL RECORDS
 IN FAVOR OF: RIVERSIDE COUNTY FLOOD CONTROL AND WATER CONSERVATION DISTRICT
 AFFECTS: AS DESCRIBED THEREIN (PLOTTED HEREIN FROM RECORD)
 EASEMENT TO BE ABANDONED BY THE RECORDOR ON THE FINAL MAP
- 7 AN EASEMENT FOR PUBLIC ROAD AND DRAINAGE AND PUBLIC UTILITIES AND INCIDENTAL PURPOSES, RECORDED MAY 14, 2007 AS INSTRUMENT NO. 2007-090765 OF OFFICIAL RECORDS
 IN FAVOR OF: COUNTY OF RIVERSIDE, A POLITICAL SUBDIVISION
 AFFECTS: AS DESCRIBED THEREIN (PLOTTED HEREIN FROM RECORD)
- 8 AN EASEMENT FOR PUBLIC ROAD AND DRAINAGE AND PUBLIC UTILITIES AND INCIDENTAL PURPOSES, RECORDED MAY 14, 2007 AS INSTRUMENT NO. 2007-090765 OF OFFICIAL RECORDS
 IN FAVOR OF: COUNTY OF RIVERSIDE, A POLITICAL SUBDIVISION
 AFFECTS: AS DESCRIBED THEREIN (PLOTTED HEREIN FROM RECORD)
- 9 AN EASEMENT FOR PUBLIC ROAD AND DRAINAGE AND PUBLIC UTILITIES AND INCIDENTAL PURPOSES, RECORDED MAY 14, 2007 AS INSTRUMENT NO. 2007-090765 OF OFFICIAL RECORDS
 IN FAVOR OF: COUNTY OF RIVERSIDE, A POLITICAL SUBDIVISION
 AFFECTS: AS DESCRIBED THEREIN (PLOTTED HEREIN FROM RECORD)
- 10 AN EASEMENT FOR PUBLIC ROAD AND DRAINAGE AND PUBLIC UTILITIES AND INCIDENTAL PURPOSES, RECORDED MAY 14, 2007 AS INSTRUMENT NO. 2007-090765 OF OFFICIAL RECORDS
 IN FAVOR OF: COUNTY OF RIVERSIDE, A POLITICAL SUBDIVISION
 AFFECTS: AS DESCRIBED THEREIN (PLOTTED HEREIN FROM RECORD)

PROPOSED VACATION NOTE

- 1 1' EASEMENT SHALL AS FEET IN WIDTH, (FOR MP 14/14-18) TO BE VACATED BY THE COUNTY OF RIVERSIDE FROM THE COMMENCEMENT WITH THE RECORDED OF TRACT 37533

- 11 AN EASEMENT FOR PUBLIC ROAD AND DRAINAGE AND PUBLIC UTILITIES AND INCIDENTAL PURPOSES, RECORDED SEPTEMBER 26, 2007 AS INSTRUMENT NO. 2007-090765 OF OFFICIAL RECORDS, IN FAVOR OF: COUNTY OF RIVERSIDE, A POLITICAL SUBDIVISION
 AFFECTS: AS DESCRIBED THEREIN (PLOTTED HEREIN FROM RECORD)
- 12 AN EASEMENT FOR PUBLIC ROAD AND DRAINAGE AND PUBLIC UTILITIES AND INCIDENTAL PURPOSES, RECORDED SEPTEMBER 26, 2007 AS INSTRUMENT NO. 2007-090765 OF OFFICIAL RECORDS, IN FAVOR OF: COUNTY OF RIVERSIDE, A POLITICAL SUBDIVISION
 AFFECTS: AS DESCRIBED THEREIN (PLOTTED HEREIN FROM RECORD)
- 13 AN EASEMENT FOR PUBLIC ROAD AND DRAINAGE AND PUBLIC UTILITIES AND INCIDENTAL PURPOSES, RECORDED SEPTEMBER 26, 2007 AS INSTRUMENT NO. 2007-090765 OF OFFICIAL RECORDS, IN FAVOR OF: COUNTY OF RIVERSIDE, A POLITICAL SUBDIVISION
 AFFECTS: AS DESCRIBED THEREIN (PLOTTED HEREIN FROM RECORD)
- 14 AN EASEMENT FOR PUBLIC ROAD AND DRAINAGE AND PUBLIC UTILITIES AND INCIDENTAL PURPOSES, RECORDED SEPTEMBER 26, 2007 AS INSTRUMENT NO. 2007-090765 OF OFFICIAL RECORDS, IN FAVOR OF: COUNTY OF RIVERSIDE, A POLITICAL SUBDIVISION
 AFFECTS: AS DESCRIBED THEREIN (PLOTTED HEREIN FROM RECORD)
- 15 NOTICE OF PENDING OF ACTION RECORDED SEPTEMBER 01, 2006 AS INSTRUMENT NO. 2006-085036 OF OFFICIAL RECORDS
 COURT: SUPERIOR COURT OF THE STATE OF CALIFORNIA, COUNTY OF RIVERSIDE
 CASE NO.: 06-10000
 PLAINTIFF: RIVERSIDE COUNTY FLOOD CONTROL AND WATER CONSERVATION DISTRICT
 DEFENDANT: TWO LAND HOLDING LLC, A DELAWARE LIMITED LIABILITY COMPANY AND FLORENCE MORGAN CLAWSON AN INDIVIDUAL BY THE PROPERTY AND SUEZ S-102
 REFERENCE: TO CORRECTION A PORTION OF REAL PROPERTY (PLOTTED HEREIN FROM RECORD) PORTION OF EASEMENT TO BE ABANDONED BY THE RECORDOR ON THE FINAL MAP
- 16 THE EFFECT OF A MAP PURPORTING TO SHOW THE LAND AND OTHER PROPERTY, FILED MARCH 16, 2007 IN BOOK 546, PAGE 88-87 OF RECORDS OF SURVEYS
 (AFFECTS LOTS 28A, 28B, 300 AND 315)
 WATER RIGHTS, CLAIM FOR FIRE TO WATER, WHETHER OR NOT SHOWN BY THE PUBLIC RECORDS

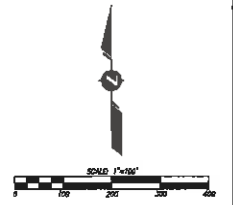
PHASE NO.	PLANNING AREA	LAND USE	MIN. LOT SIZE	AVE. DENSITY	NO. OF RESIDENTIAL LOTS	RESIDENTIAL LOT NOS.	LETTERED LOTS
1	24	MHR	5,000 SF	4.8 DU/AC	125	1-125	LOTS "A" & "B"
2	25A	MHR	4,000 SF	4.4 DU/AC	100	126-225 226-325 326-350 351-363	LOTS "C" & "D"
10	25B	MHR	3,500 SF	6.3 DU/AC	139	341-351 352-375 307-317	



PHASING PLAN
 SCALE: 1"=100'
 TTM NO. 29322
 MINOR CHANGE NO. 1

REVISED	DESIGNED	APPROVED
	BY	BY
	DATE	DATE

KA ENGINEERING
 LAND PLANNING
 SURVEYING
 337 N. SHEDDEN STREET
 SUITE 117
 CORONA, CALIFORNIA 92620
 TEL: (951) 278-1800
 FAX: (951) 278-4300



COUNTY OF RIVERSIDE
TENTATIVE TRACT MAP
NO. 37533
SCHEDULE "A"
EASEMENTS NOTES & PHASING

Project No. 20
 Created By: JLD
 Date: 05/20/09
 Sheet: 43 of 50

DATE: 05/20/09
 SHEET NO. 43
 OF 50 SHEETS

IN THE COUNTY OF RIVERSIDE TENTATIVE TRACT MAP NO. 29322 - MINOR CHANGE NO. 1

PLANNING AREA 26			PLANNING AREA 27			PA 28 (CONT.)		
Lot Number	Lot Area (SQ FT)	Prop Area (SQ FT)	Lot Number	Lot Area (SQ FT)	Prop Area (SQ FT)	Lot Number	Lot Area (SQ FT)	Prop Area (SQ FT)
1	7043	7028	23	4882	4859	103	7161	7048
2	6060	6064	24	6448	6416	104	6461	6449
3	6063	6063	25	7108	7108	105	6336	6336
4	4079	4079	26	6255	6256	106	6918	6846
5	4261	4261	27	6253	6253	107	4998	4988
6	6271	6213	28	6029	6029	108	6248	7066
7	6254	6254	29	6003	6003	109	7540	6713
8	6131	6233	30	6000	6018	110	6260	6254
9	6247	6247	31	6008	6008	111	7564	6830
10	6232	6232	32	6037	6037	112	6277	6258
11	6283	6283	33	6010	6010	113	6300	6027
12	6271	6271	34	6009	6009	114	6288	6286
13	6299	6292	35	6019	6019	115	6282	6282
14	6301	6283	36	6008	6008	116	7222	7222
15	6287	6287	37	6002	6002	117	6061	6061
16	6253	6253	38	6003	6003	118	6061	6114
17	6289	6289	39	6114	6114	119	6062	6037
18	6272	6272	40	6222	6222	120	6053	6087
19	6299	6292	41	6286	6286	121	7013	7023
20	6118	6061	42	6041	6041	122	7013	7023
21	6210	6210	43	6053	6053	123	6362	6362
22	7678	7208	44	10791	10791	124	6250	6250
23	6202	6202	45	6074	6074	125	6020	6020
24	6202	6202	46	6088	6088	126	6220	6220
25	6281	6281	47	6089	6089	127	6282	6282
26	6282	6282	48	6073	6088	128	6282	6282
27	6281	6281	49	6042	6042	129	6282	6282
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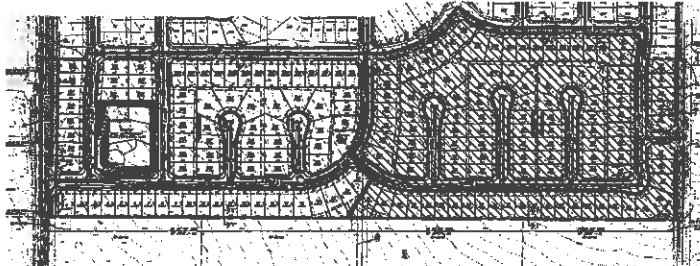
IN THE COUNTY OF RIVERSIDE
TENTATIVE TRACT MAP NO. 29322 - MINOR CHANGE NO. 1

EXISTING EASEMENT NOTES

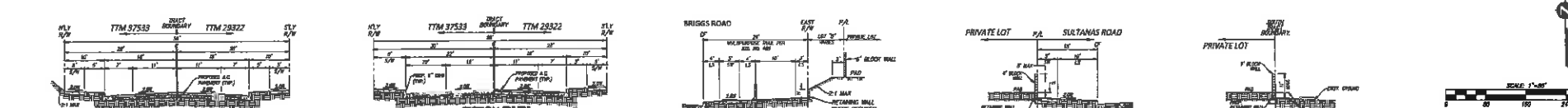
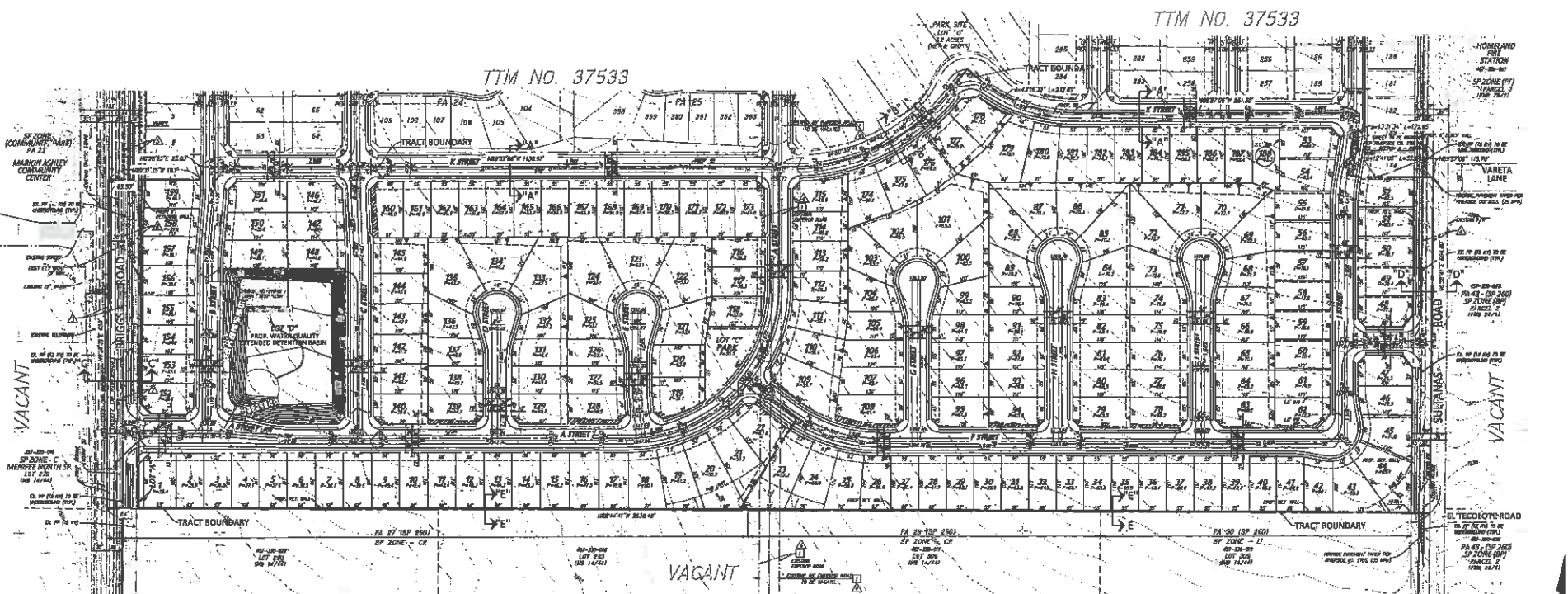
- 1 AN EASEMENT FOR UTILITY, SPECIAL SERVICES, EGRESS, WELLS AND INCIDENTAL PURPOSES, RECORDED JANUARY 26, 1987 IN BOOK 139 OF RECORD, PAGE 446 IN FAVOR OF BRIDGE STREET COMPANY AFFECTS AS DESCRIBED THEREIN (NOT PLEASIBLE FROM RECORD INFORMATION)
- 2 AN EASEMENT FOR PUBLIC UTILITIES AND INCIDENTAL PURPOSES, RECORDED APRIL 11, 1988 IN BOOK 159 OF RECORD, PAGE 102 IN FAVOR OF CALIFORNIA SERVICE POWER COMPANY AFFECTS AS DESCRIBED THEREIN (NOT PLEASIBLE FROM RECORD INFORMATION)
- 3 AN EASEMENT FOR PUBLIC UTILITIES AND INCIDENTAL PURPOSES, RECORDED JULY 14, 1949 IN BOOK 1068, PAGE 243 OF RECORD, IN FAVOR OF CALIFORNIA ELECTRIC POWER COMPANY AFFECTS AS DESCRIBED THEREIN (PLOTTED HEREON FROM RECORD)
- 4 THE EFFECT OF A MAP PURPORTING TO SHOW THE LAND AND OTHER PROPERTY FILED JANUARY 27, 1978 IN BOOK 54, PAGE 88 OF RECORD OF SAFFORD, (PLOTTED HEREON) EXCEPT EXCEPTED ROAD TO BE SCHEDULED HEREON ALONG WITH ALL EASEMENTS AND UTILITY RIGHTS
- 5 AN EASEMENT FOR PUBLIC ROAD AND DRAINAGE AND PUBLIC UTILITIES AND INCIDENTAL PURPOSES, RECORDED MAY 14, 2007 AS INSTRUMENT NO. 3003-ADVERSE OF OFFICIAL RECORD, IN FAVOR OF COUNTY OF RIVERSIDE, A POLITICAL SUBDIVISION AFFECTS AS DESCRIBED THEREIN (PLOTTED HEREON FROM RECORD)
- 6 AN EASEMENT FOR PUBLIC ROAD AND DRAINAGE AND PUBLIC UTILITIES AND INCIDENTAL PURPOSES, RECORDED MAY 14, 2007 AS INSTRUMENT NO. 3003-ADVERSE OF OFFICIAL RECORD, IN FAVOR OF COUNTY OF RIVERSIDE, A POLITICAL SUBDIVISION AFFECTS AS DESCRIBED THEREIN (PLOTTED HEREON FROM RECORD)
- 7 AN EASEMENT FOR PUBLIC ROAD AND DRAINAGE AND PUBLIC UTILITIES AND INCIDENTAL PURPOSES, RECORDED MAY 14, 2007 AS INSTRUMENT NO. 3003-ADVERSE OF OFFICIAL RECORD, IN FAVOR OF COUNTY OF RIVERSIDE, A POLITICAL SUBDIVISION AFFECTS AS DESCRIBED THEREIN (PLOTTED HEREON FROM RECORD)

PROPOSED VACATION NOTE

- 1 CORONAR ROAD, 40 FEET IN WIDTH (PER IAS 14(A)-4) TO BE VACATED BY THE COUNTY OF RIVERSIDE PRIOR TO OR CONCURRENT WITH THE RECORDATION OF TRACT 29322



PHASING SUMMARY							
PHASE NO.	PLANNING AREA	LAND USE	AREA SIZE	AVE DENSITY	NO. OF RESIDENTIAL LOTS	RESIDENTIAL LOT AKA	LETTERED LOTS
1	25	MOR	6,000 SF	3.9 DU/AC	80	5-22 116-373	LOTS "A" & "B" "C" & "D"
2	28	MOR	6,000 SF	4.7 DU/AC	108	23-112 374-388	



COUNTY OF RIVERSIDE
TENTATIVE TRACT MAP NO. 29322
MINOR CHANGE NO. 1
 SCHEDULE "A"
TENTATIVE TRACT MAP

DATE	DESCRIPTION	APPROVED

ENGINEERING
LAND PLANNING
SURVEYING

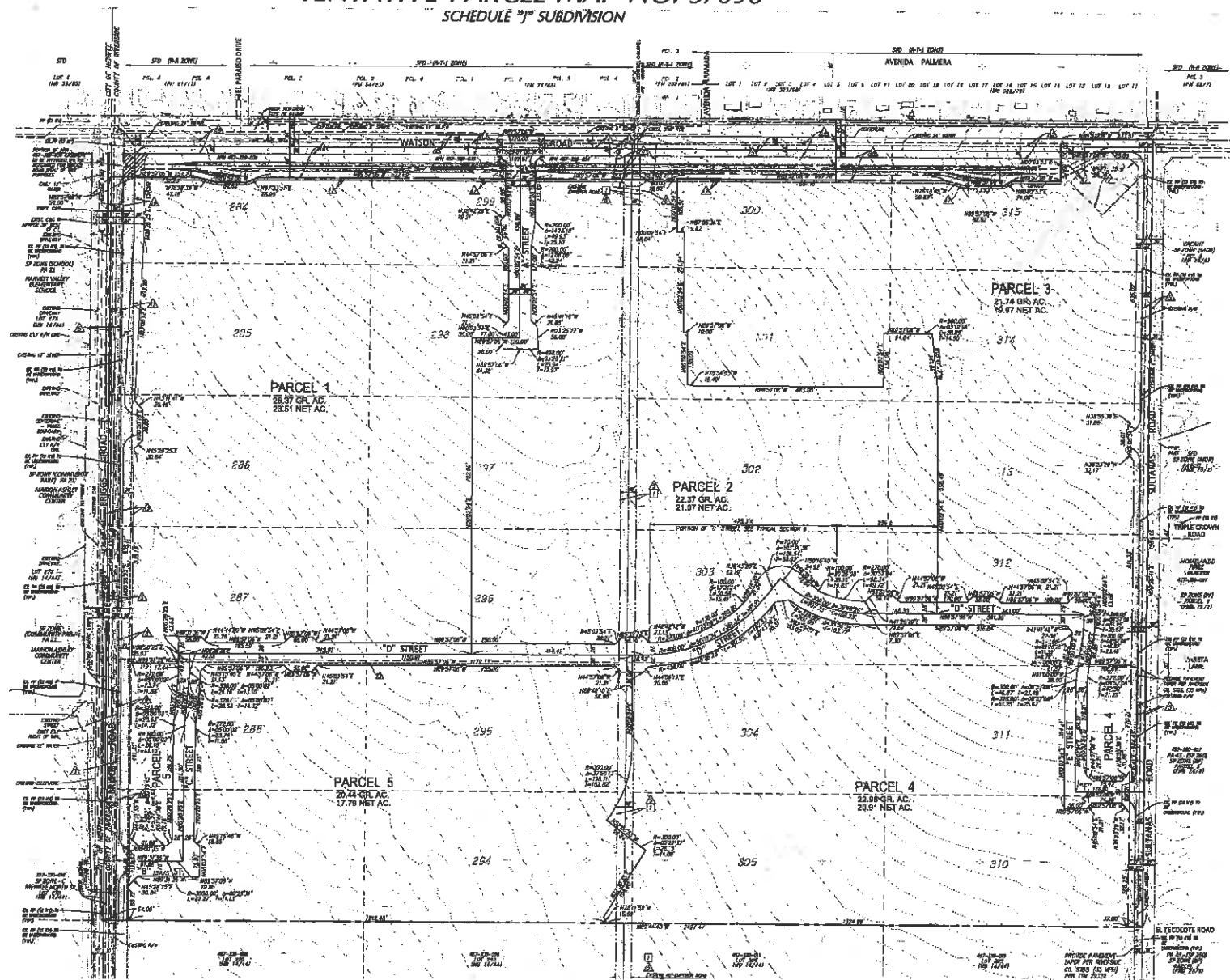
357 N. SHEDDEN STREET
 SUITE 117
 CORONA, CALIFORNIA 92626
 TEL: (951) 270-1800
 FAX: (951) 270-4300



NOTES:
 1. SEE SHEET 2 FOR ALL PROPOSED ELEVATIONS SHOWN ON THIS TENTATIVE TRACT MAP.

DATE: 08/20/2018
 DRAWN BY: JLS
 CHECKED BY: JLS
 SCALE: AS SHOWN

IN THE COUNTY OF RIVERSIDE
TENTATIVE PARCEL MAP NO. 37690
 SCHEDULE "J" SUBDIVISION



FOR FINANCE AND CONVEYANCE PURPOSES ONLY.
 A FUTURE SUBDIVISION MAP OR LAND ENTITLEMENT OR
 PERMIT IS NECESSARY TO DEVELOP THIS PROPERTY.
 THIS MAP DOES NOT REMOVE ANY CONDITIONS OF
 APPROVAL FOR SEPARATE LAND USE ENTITLEMENTS OR
 TENTATIVE MAPS OR USE PERMITS APPROVED FOR THIS
 LAND.

NO.	DATE	DESCRIPTION	APPROVED
1		MADE AND REVIEWER CHECKS INDICATED "1" "1"	
2		REVISION	

KA ENGINEERING
 LAND PLANNING
 SURVEYING
 327 N. SHERIDAN STREET
 SUITE 117
 CORONA, CALIFORNIA 92620
 TEL. (951) 276-1800
 FAX (951) 276-4207

COUNTY OF RIVERSIDE
**TENTATIVE PARCEL MAP
 NO. 37690**
 SCHEDULE "J" SUBDIVISION
 PLAN

Drawn By: [] Date: []
 Checked By: [] Date: []
 Scale: AS SHOWN

PREPARED: FEBRUARY 2019
 SHEET 1 OF 2 SHEETS

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ORDINANCE NO. 348.xxxx

AN ORDINANCE OF THE COUNTY OF RIVERSIDE AMENDING
ORDINANCE NO. 348 RELATING TO ZONING

The Board of Supervisors of the County of Riverside Ordains as Follows:

Section 1. Article XVIIa of Ordinance No. 348 is amended in its entirety to read as follows:

SECTION 17.73 SP ZONE REQUIREMENTS AND STANDARDS FOR SPECIFIC PLAN NO. 260.

a. Planning Areas 2 and 3.

- (1) The uses permitted in Planning Areas 2 and 3 of Specific Plan No. 260 shall be the same as those uses permitted in Article XII, Section 12.2 of Ordinance No 348.
- (2) The development standards for Planning Areas 2 and 3 of Specific Plan No. 260 shall be the same as those standards identified in Article XII, Section 12.4 of Ordinance No. 348.
- (3) Except as provided above, all other zoning requirements shall be the same as those requirements identified in Article XII of Ordinance No. 348.

b. Planning Areas 4, 15, 18, 41, 42, 45 and 46.

- (1) The uses permitted in Planning Areas 4, 15, 18, 41, 42, 45 and 46 of Specific Plan No. 260 shall be the same as those uses permitted in Article VI, Section 6.1 of Ordinance No. 348 except that the uses permitted pursuant to Section 6.1.b(1) and (3) shall not be permitted.
- (2) The development standards for Planning Areas 4, 15, 18, 41, 42, 45 and 46 of Specific Plan No. 260 shall be the same as those standards identified in Article VI, Section 6.2 of Ordinance No. 348, except that the development standards set forth in Article VI, Section 6.2.e (1), (3), and (4) shall be deleted and replaced by the following:

- 1 A. The front yard shall be not less than fifteen feet (15') measured from the
- 2 existing street line or from any future street line as shown on any specific
- 3 plan of highways, whichever is nearer the proposed structure.
- 4 B. Rear yards shall be not less than twenty feet (20').
- 5 C. Chimneys and fireplaces shall be allowed to encroach into side yards a
- 6 maximum of two feet (2'). No other structural encroachments shall be
- 7 permitted in the front, rear or side yard except as provided for in Section
- 8 18.9 of Ordinance No. 348.

- 9 (3) Except as provided above, all other zoning requirements shall be the same as those
- 10 requirements identified in Article VI of Ordinance No. 348.

11 c. Planning Areas 5, 6 and 9.

- 12 (1) The uses permitted in Planning Areas 5, 6 and 9 of Specific Plan No. 260 shall be
- 13 the same as those uses permitted in Article VI, Section 6.1 of Ordinance No. 348
- 14 except that the uses permitted pursuant to Section 6.1.b(1) and (3) shall not be
- 15 permitted.

- 16 (2) The development standards for Planning Areas 5, 6 and 9 of Specific Plan No. 260
- 17 shall be the same as those standards identified in Article VI, Section 6.2 of
- 18 Ordinance No. 348, except that the development standards set forth in Article VI,
- 19 Section 6.2.b.; and e(3) and (4) shall be deleted and replaced by the following:

- 20 A. Lot Area shall be not less than seven thousand two hundred (7,200) square
- 21 feet unless the development proposed is a mobile home park or is restricted
- 22 to senior citizen housing then the lot area shall be not less than five
- 23 thousand (5,000) square feet. The minimum lot area shall be determined by
- 24 excluding that portion of a lot that is used solely for access to the portion of
- 25 a lot used as a building site.
- 26 B. Rear yards shall be not less than twenty feet (20').
- 27 C. Chimneys and fireplaces shall be allowed to encroach into side yards a
- 28 maximum of two (2'). No other structural encroachments shall be

1 permitted in the front, rear or side yard except as provided for in Section
2 18.19 of Ordinance No. 348.

3 (3) Except as provided above, all other zoning requirements shall be the same as those
4 requirements identified in Article VI of Ordinance No. 348.

5 d. Planning Area 7A.

6 (1) The uses permitted in Planning Area 7A of Specific Plan No. 260 shall be the same
7 as those uses permitted in Article VI, Section 6.1 of Ordinance No. 348, except that
8 the uses permitted pursuant to Section 6.1.b(1) and (3) shall not be permitted.

9 (2) The development standards for Planning Area 7A of Specific Plan No. 260 shall be
10 the same as those standards identified in Article VI, Section 6.2 of Ordinance No.
11 348, except that the development standards set forth in Article VI, Section 6.2.b.,
12 c., d., e. shall be deleted and replaced by the following:

13 A. Lot area shall be not less than four thousand (4,000) square feet. The
14 minimum lot area shall be determined by excluding that portion of a lot that
15 is used solely for access to the portion of a lot used as a building site.

16 B. The minimum average width of that portion of a lot to be used as a building
17 site shall be forty feet (40') with a minimum average depth of ninety feet
18 (90').

19 C. The minimum frontage of a lot shall be forty feet (40'), except that lots
20 fronting on knuckles or cul-de-sacs may have a minimum frontage of thirty-
21 five feet (35'). Lot frontage along curvilinear streets may be measured at
22 the building setback in accordance with zone development standards.

23 D. The front yard shall be not less than ten feet (10'), measured from the
24 sidewalk.

25 E. Side yards on interior and through lots shall be not less than five feet (5') in
26 width. Side yards on corner and reversed corner lots shall be not less than
27 five feet (5') from the existing street line or from any future street line as
28 shown on any specific plan of highways.

- 1 F. There is no minimum rear yard area.
- 2 G. The minimum garage setback from the private drive is three feet (3'). The
- 3 minimum distance from garage door to garage door is thirty feet (30'). The
- 4 minimum distance between buildings is ten feet (10').
- 5 H. Chimneys, media niches and fireplaces shall be allowed to encroach into
- 6 side yards a maximum of two and a half feet (2.5').
- 7 I. Front yard porches shall not be allowed to encroach into front yards a
- 8 maximum of five feet (5').
- 9 J. Side yard porches shall be allowed to encroach into side yards a maximum
- 10 of three feet (3').
- 11 K. Balconies and decks shall be allowed to encroach into rear yards a
- 12 maximum of two and a half feet (2'-6").
- 13 L. No other structural encroachments shall be permitted in the front, rear or
- 14 side yard except as provided for in Section 18.19 of Ordinance No. 348.

15 In addition, the following standard shall also apply:

- 16 AA. In no case shall more than sixty-five percent (65%) of any lot be
- 17 covered by buildings and paving for lots with one and two-story
- 18 buildings.

- 19 (3) Except as provided above, all other zoning requirements shall be the same as those
- 20 requirements identified in Article VI of Ordinance No. 348.

21 e. Planning Areas 7B and 23A.

- 22 (1) The uses permitted in Planning Area 7B and 23A of Specific Plan No. 260 shall be
- 23 the same as those uses permitted in Article VIII, Section 8.1 of Ordinance No. 348,
- 24 except that the uses permitted pursuant to Section 8.1.a.(18), (21) and (24) shall not
- 25 be permitted.
- 26 (2) The development standards for Planning Areas 7B and 23A of Specific Plan No.
- 27 260 shall be the same as those standards identified in Article VIII, Section 8.2 of
- 28

1 Ordinance No. 348, except that the development standards set forth in Article VIII,
2 Section 8.2. b., c., d., shall be deleted and replaced by the following:

- 3 A. The front yard shall be not less than ten feet (10'), measured from the
4 sidewalk.
- 5 B. Side yards on interior units shall be not less than five feet (5') in width.
6 Side yards on corner and reversed corner units shall be not less than five
7 feet (5') from the sidewalk.
- 8 C. There shall be no minimum setback for the rear yard.
- 9 D. The minimum garage setback from the private drive is three feet (3'). The
10 minimum distance from garage door to garage door is thirty feet (30'). The
11 minimum distance between buildings is eight feet (8').
- 12 E. Chimneys, media niches and fireplaces shall be allowed to encroach into
13 side yards a maximum of two and a half feet (2.5').
- 14 F. Front yard porches shall not be allowed to encroach into front yards
15 setbacks.
- 16 G. Side yard porches shall be allowed to encroach into side yards a maximum
17 of three feet (3').
- 18 H. Balconies and decks shall be allowed to encroach into rear yards a
19 maximum of two and a half feet (2.5').
- 20 I. No other structural encroachments shall be permitted in the front, rear or
21 side yard except as provided for in Section 18.19 of Ordinance No. 348.
- 22 J. In no case shall more than sixty-five percent (65%) of any lot be covered by
23 buildings and paving for lots with one and two-story buildings.

24 In addition, the following standard shall also apply:

- 25 AA. The minimum unit area within a lot shall be one thousand two
26 hundred (1,200) square feet.
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1 i. A unit area is defined as an exclusive use area, which
2 includes the building footprint and private yard area and
3 excludes common open space area on a lot.

4 BB. The minimum width of a unit area within a lot shall be forty feet
5 (40') with a minimum depth of thirty feet (30').

6 i. A unit area is defined as an exclusive use area, which
7 includes the building footprint and private yard area and
8 excludes common open space area on a lot.

9 (3) Except as provided above, all other zoning requirements shall be the same as those
10 requirements identified in Article VI of Ordinance No. 348.

11 f. Planning Areas 8, 14, 16, 17, 23B, 27 and 29.

12 (1) The uses permitted in Planning Areas 8, 14, 16, 17, 23B, 27 and 29 of Specific Plan
13 No. 260 shall be the same as those uses permitted in Article IXb, Section 9.50 of
14 Ordinance No. 348 except that the uses permitted pursuant to Section 9.50a. (32),
15 (52), and (64) shall not be permitted.

16 (2) The development standards for Planning Areas 8, 14, 16, 17, 23B, 27 and 29 of
17 Specific Plan No. 260 shall be the same as those standards identified in Article IXb,
18 Section 9.53 of Ordinance No. 348.

19 (3) Except as provided above, all other zoning requirements shall be the same as those
20 requirements identified in Article IXb of Ordinance No. 348.

21 g. Planning Areas 10, 20 and 38.

22 (1) The uses permitted in Planning Areas 10, 20 and 38 of Specific Plan No. 260 shall
23 be the same as those uses permitted in Article VIIIe, Section 8.100 of Ordinance
24 No. 348, except that the uses permitted in Section 8.100.a. and 8.100.b. shall not be
25 permitted.

26 (2) The development standards for Planning Areas 10, 20 and 38 of Specific Plan No.
27 260 shall be the same as those standards identified in Article VIIIe, Section 8.101
28 of Ordinance No. 348.

1 (3) Except as provided above, all other zoning requirements shall be the same as those
2 requirements identified in Article VIIIe of Ordinance No. 348.

3 h. Planning Areas 11, 12 and 43.

4 (1) The uses permitted in Planning Areas 11, 12 and 43 of Specific Plan No. 260 shall
5 be the same as those uses permitted in Article X, Sections 10.1 and 10.2 of
6 Ordinance No. 348, except that the uses permitted pursuant to Section 10.1.a.(1.) b
7 and c. shall not be permitted.

8 (2) The development standards for Planning Areas 11, 12 and 43 of Specific Plan No.
9 260 shall be the same as those standards identified in Article X, Section 10.4 of
10 Ordinance No. 348.

11 (3) Except as provided above, all other zoning requirements shall be the same as those
12 requirements identified in Article X of Ordinance No. 348.

13 i. Planning Areas 13, 19, 30 and 44.

14 (1) The uses permitted in Planning Areas 13, 19, 30 and 44 of Specific Plan No. 260
15 shall be the same as those uses permitted in Article XI, Section 11.2 of Ordinance
16 No. 348, except that the uses permitted pursuant to Section 11.2.b.(1) c.(1), (3), (4)
17 and (6), d.(1), g., h.(3), (4), (6) and (8), k., and m.(1), (3), (4) and (9); and
18 11.2.b(2)c., k., and l. shall not be permitted.

19 (2) The development standards for Planning Areas 13, 19, 30 and 44 of Specific Plan
20 No. 260 shall be the same as those standards identified in Article XI, Section 11.4
21 of Ordinance No. 348.

22 (3) Except as provided above, all other zoning requirements shall be the same as those
23 requirements identified in Article XI of Ordinance No. 348.

24 j. Planning Area 21.

25 (1) The uses permitted in Planning Area 21 of Specific Plan No. 260 shall be the same
26 as those uses permitted in Article VIIIe, Section 8.100 of Ordinance No. 348,
27 except that the uses permitted pursuant to Section 8.100.a. and b. shall not be
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1 permitted. In addition, the permitted uses identified under Section 8.100.a. shall
2 include public schools.

3 (2) The development standards for Planning Area 21 of Specific Plan No. 260 shall be
4 the same as those standards identified in Article VIIIe, Section 8.101 of Ordinance
5 No. 348.

6 (3) Except as provided above, all other zoning requirements shall be the same as those
7 requirements identified in Article VIIIe of Ordinance No. 348.

8 k. Planning Areas 22, 24, 32 and 33B.

9 (1) The uses permitted in Planning Areas 22, 24, 32 and 33B of Specific Plan
10 No. 260 shall be the same as those uses permitted in Article VI, Section 6.1 of
11 Ordinance No. 348, except that the uses permitted pursuant to Section 6.1.b.(1) and
12 (3) shall not be permitted.

13 (2) The development standards for Planning Areas 22, 24, 32 and 33B of
14 Specific Plan No. 260 shall be the same as those standards identified in Article VI,
15 Section 6.2 of Ordinance No. 348, except that the development standards set forth
16 in Article VI, Section 6.2.b., c., d., e. (1), (2), and (4) shall be deleted and replaced
17 by the following:

18 A. Lot area shall be not less than five thousand (5,000) square feet. The
19 minimum lot area shall be determined by excluding that portion of a lot that
20 is used solely for access to the portion of a lot used as a building site.

21 B. The minimum average width of that portion of a lot to be used as a building
22 site shall be fifty feet (50') with a minimum average depth of eighty feet
23 (80'). That portion of a lot used for access on "flag" lots shall have a
24 minimum width of twenty feet (20').

25 C. The minimum frontage of a lot shall be forty-five feet (45'), except that lots
26 fronting on knuckles or cul-de-sacs may have a minimum frontage of thirty-
27 five feet (35'). Lot frontage along curvilinear streets may be measured at
28 the building setback in accordance with zone development standards.

1 D. The front yard for living areas and porches shall be not less than twelve feet
2 (12') measured from the existing street line or from any future street line as
3 shown on any specific plan of highways, whichever is nearer the proposed
4 structure. The face of the garage shall be setback eighteen feet (18'). In a
5 side-entry garage condition, the front yard shall not be less than ten feet
6 (10').

7 DE. Side yards on interior and through lots shall be not less than ~~five~~ four feet
8 (45') in width. Side yards on corner and reversed corner lots shall be not
9 less than ~~ten-nine~~ feet (10'9') from the existing street line or from any
10 future street line as shown on any specific plan of highways, whichever is
11 nearer the proposed structure, upon which the main building sides, except
12 that where the lot is less than fifty feet (50') wide, the yard need not exceed
13 twenty percent (20%) of the width of the lot.

14 EF. Chimneys, air conditioning units, and fireplaces shall be allowed to
15 encroach into side yards (on the non-gated side) and rear yards a maximum
16 of two feet (2'). No other structural encroachments shall be permitted in the
17 front, rear or side yard except as provided for in Section 18.19 of Ordinance
18 No. 348.

19 In addition, the following standard shall also apply:

20 AA. In no case shall more than fifty percent (50%) of any lot be covered
21 by buildings for lots with one-story buildings and in no case shall
22 more than forty-five percent (45%) of any lot be covered by
23 buildings for lots with two-story buildings.

24 (3) Except as provided above, all other zoning requirements shall be the same as those
25 requirements identified in Article VI of Ordinance No. 348.

1 I. Planning Area 25.

2 (1) The uses permitted in Planning Area 25 of Specific Plan No. 260 shall be the same
3 as those uses permitted in Article VI, Section 6.1 of Ordinance No. 348, except that
4 the uses permitted pursuant to Section 6.1.b.(1) and (3) shall not be permitted.

5 (2) The development standards for Planning Areas 25 of Specific Plan No. 260
6 shall be the same as those standards identified in Article VI, Section 6.2 of
7 Ordinance No. 348, except that the development standards set forth in Article VI,
8 Section 6.2.b., c., d., e. (1), (2), and (4) shall be deleted and replaced by the
9 following:

10 A. Lot area shall be not less than three thousand five hundred (3,500) square
11 feet. The minimum lot area shall be determined by excluding that portion
12 of a lot that is used solely for access to the portion of a lot used as a
13 building site.

14 B. The minimum average width of that portion of a lot to be used as a building
15 site shall be forty feet (40') with a minimum average depth of seventy-five
16 feet (75'). That portion of a lot used for access on "flag" lots shall have a
17 minimum width of twenty feet (20').

18 C. The minimum frontage of a lot shall be forty feet (40'), except that lots
19 fronting on knuckles or cul-de-sacs may have a minimum frontage of thirty-
20 five feet (35'). Lot frontage along curvilinear streets may be measured at
21 the building setback in accordance with zone development standards.

22 D. The front yard for living areas and porches shall be not less than twelve feet
23 (12') measured from the existing street line or from any future street line as
24 shown on any specific plan of highways, whichever is nearer the proposed
25 structure. The face of the garage shall be setback eighteen feet (18'). In a
26 side-entry garage condition, the front yard shall not be less than ten feet
27 (10').

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1 E. Side yards on interior and through lots shall be not less than four feet (4') in
2 width. Side yards on corner and reversed corner lots shall be not less than
3 nine feet (9') from the existing street line or from any future street line as
4 shown on any specific plan of highways, whichever is nearer the proposed
5 structure, upon which the main building sides, except that where the lot is
6 less than forty feet (40') wide, the yard need not exceed ten percent (10%)
7 of the width of the lot.

8 F. Chimneys, air conditioning units, and fireplaces shall be allowed to
9 encroach into side yards (on the non-gated side) and rear yards, a maximum
10 of two feet (2'). No other structural encroachments shall be permitted in the
11 front, rear or side yard except as provided for in Section 18.19 of Ordinance
12 No. 348.

13 In addition, the following standard shall also apply:

14 AA. In no case shall more than sixty-five percent (65%) of any lot be
15 covered by buildings for lots with one-story buildings and in no case
16 shall more than sixty percent (60%) of any lot be covered by
17 buildings for lots with two-story buildings.

18 (3) Except as provided above, all other zoning requirements shall be the same as those
19 requirements identified in Article VI of Ordinance No. 348.

20 1. Planning Areas 25, 26, 28, 35, 37 and 40.

21 (1) The uses permitted in Planning Areas 25, 26, 28, 35, 37 and 40 of Specific Plan No.
22 260 shall be the same as those uses permitted in Article VI, Section 6.1 of
23 Ordinance No. 348, except that the uses permitted pursuant to Section 6.1.b.(1) and
24 (3) shall not be permitted.

25 (2) The development standards for Planning Areas 25, 26, 28, 35, 37 and 40 of
26 Specific Plan No. 260 shall be the same as those standards identified in Article VI,
27 Section 6.2 of Ordinance No. 348, except that the development standards set forth
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1 in Article VI, Section 6.2.b., c., d., e. (1), (2) and (4) shall be deleted and replaced
2 by the following:

- 3 A. Lot area shall be not less than six thousand (6,000) square feet. The
4 minimum lot area shall be determined by excluding that portion of a lot that
5 is used solely for access to the portion of a lot used as a building site.
- 6 B. The minimum average width of that portion of a lot to be used as a building
7 site shall be fifty feet (50') with a minimum average depth of one hundred
8 feet (100'). That portion of a lot used for access on "flag" lots shall have a
9 minimum width of twenty feet (20').
- 10 C. The minimum frontage of a lot shall be fifty feet (50'), except that lots
11 fronting on knuckles or cul-de-sacs may have a minimum frontage of thirty-
12 five feet (35'). Lot frontage along curvilinear streets may be measured at
13 the building setback in accordance with zone development standards.
- 14 D. The front yard for living areas and porches shall be not less than ~~fifteen~~
15 twelve feet (15'12') measured from the existing street line or from any
16 future street line as shown on any specific plan of highways, whichever is
17 nearer the proposed structure. The face of the garage shall be setback
18 eighteen feet (18'). In a side-entry garage condition, the front yard shall not
19 be less than ten feet (10').
- 20 E. Side yards on interior and through lots shall be not less than ~~five-four~~ feet
21 (5'4') in width. Side yards on corner and reversed corner lots shall be not
22 less than ~~ten-nine~~ feet (10'9') from the existing street line or from any
23 future street line as shown on any specific plan of highways, whichever is
24 nearer the proposed structure, upon which the main building sides, except
25 that where the lot is less than fifty-five feet (55') wide, the yard need not
26 exceed twenty percent (20%) of the width of the lot.
- 27 F. Chimneys, air conditioning units, and fireplaces shall be allowed to
28 encroach into side yards (on the non-gated side) and rear yards, a maximum

1 of two feet (2'). No other structural encroachments shall be permitted in the
2 front, rear or side yard except as provided for in Section 18.19 of Ordinance
3 No. 348.

4 (3) Except as provided above, all other zoning requirements shall be the same as those
5 requirements identified in Article VI of Ordinance No. 348.

6 m. Planning Area 31.

7 (1) The uses permitted in Planning Area 31 of Specific Plan No. 260 shall be the same
8 as those permitted in Article IXb, Section 9.50 of Ordinance No. 348, except that
9 the uses permitted pursuant to Section 9.50.a (32), (52) and (64) shall not be
10 permitted. In addition, the permitted uses identified under Section 9.50b. shall also
11 include self-storage facilities and mini-warehouse structures.

12 (2) The development standards for Planning Area 31 of Specific Plan No. 260 shall be
13 the same as those identified in Article IXb, Section 9.53 of Ordinance No. 348.

14 (3) Except as provided above, all other zoning requirements shall be the same as those
15 requirements identified in Article IXb of Ordinance No. 348.

16 n. Planning Area 31A.

17 (1) The uses permitted in Planning Area 31A shall be the same as those uses permitted
18 in Article IXb, Section 9.50 of Ordinance No. 348, except that the uses permitted
19 pursuant to Section 9.50.a (30), (31), (32), (52), (55), (62), (64), (82), (97), (98),
20 and (99); and b. (1), (2), (3), (5), (7), (8), (10), (13), (14), (15), (16), (17), (18),
21 (19), (22) and (23) shall not be permitted. In addition, the permitted uses identified
22 under Section 9.50.a. shall also include multiple family dwellings, medical and
23 dental offices, real estate offices, and congregate care residential facilities.

24 (2) Any land division application submitted within Planning Area 31A of Specific Plan
25 No. 260 shall be heard concurrently with a comprehensive plot plan application for
26 the entire affected Planning Area by the Planning Commission in accordance with
27 Section 18.30.d.(3) of Ordinance No. 348. The application for a comprehensive
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1 plot plan shall be submitted in accordance with the provisions of Section 18.30 of
2 Ordinance No. 348 and shall also at a minimum include the following:

3 A. A statement indicating how the land division and comprehensive plot plan
4 applications implement Specific Plan No. 260 and comply with the
5 conditions of approval for said Specific Plan.

6 B. A comprehensive plot plan for the entire planning area, a conceptual
7 grading plan and a tentative subdivision map, based upon a contour interval
8 no greater than four feet (4') which in addition to the Requirements of
9 Ordinance No. 460 and Section 18.30 of Ordinance No. 348 include:

- 10 i. The proposed lots including lot lines and proposed easement, if any;
- 11 ii. building footprints;
- 12 iii. floor plan assignments;
- 13 iv. pad elevations, street grades and cut and fill slopes in excess of one
14 foot in vertical height;
- 15 v. the proposed uses, their location and architectural designs;
- 16 vi. the proposed internal circulation system; and,
- 17 vii. buffers, if any.

18 C. A design manual which includes:

- 19 i. A description of residential floor plans and their mix;
- 20 ii. The lot and building calculations for each lot and building as
21 follows:
 - 22 (a) lot area and lot pad area;
 - 23 (b) building footprint area;
 - 24 (c) percentage of lot coverage;
 - 25 (d) front setback;
 - 26 (e) useable rear yard area and depth; and,
 - 27 (f) building square footage for commercial and residential uses.
- 28 iii. a fencing plan including details of proposed materials to be used;

- iv. dimensioned conceptual floor plans and elevations, including details of proposed materials for evaluations, and square footages and heights of individual units; and,
- v. a proposed phasing plan showing the planned sequence of subdivision map recordation and development.

(3) The development standards for commercial uses within Planning Area 31A of Specific Plan No. 260 shall be the same as those standards identified in Article IXb, Section 9.53 of Ordinance No. 348. For purposes of this ordinance amendment, a commercial use shall be defined as development which includes any permitted use other than multiple family dwellings or apartments.

(4) The development standards for residential uses and combined residential and commercial uses within Planning Area 31A of Specific Plan No. 260 shall be as follows:

A. The minimum lot area shall be seven thousand two hundred (7,200) square feet with a minimum average width of sixty feet (60') and a minimum average depth of one hundred feet (100') for all permitted uses, unless different minimums are specifically required in a particular area.

B. The minimum front and rear yards shall be ten feet (10') for buildings that do not exceed thirty-five feet (35') in height. Any portion of a building which exceeds thirty-five feet (35') in height shall be set back from the front and rear lot lines no less than ten feet (10') plus two feet (2') for each foot by which the height exceeds thirty-five feet (35'). The front setback shall be measured from any existing or future street line as shown on any specific street plan of the County. The rear setback shall be measured from the existing rear lot line or from any recorded alley or easement, if the rear line adjoins a street, the rear setback requirement shall be the same as required for a front setback.

- 1 C. The minimum side yard shall be five feet (5') for buildings that do not
2 exceed thirty-five feet (35') in height. Any portion of a building which
3 exceeds thirty-five feet (35') in height shall be set back from each side lot
4 line five feet (5') plus two feet (2') for each foot by which the height
5 exceeds thirty-five feet (35'); if the side yard adjoins a street, the side
6 setback requirement shall be the same as required for a front setback. No
7 structural encroachments shall be permitted in the front, side or rear yard
8 except as provided in Section 18.19 of Ordinance No. 348.
- 9 D. No lot shall have more than fifty percent (50%) of its net area covered with
10 buildings or structures.
- 11 E. The maximum ratio of floor area to lot area shall not be greater than two to
12 one (2:1), not including basement floor area.
- 13 F. All buildings and structures shall not exceed fifty feet (50') in height, unless
14 a height up to seventy-five feet (75') is specifically permitted under the
15 provisions of Section 18.34 of Ordinance No. 348.
- 16 G. Automobile storage space shall be provided as required by Section 18.12 of
17 Ordinance No. 348.
- 18 H. Interior side yards may be reduced to accommodate zero lot line or common
19 wall situations, except that, in no case shall the reduction in side yard areas
20 reduce the required separation between detached structures.
- 21 I. Where the front, side or rear yard adjoins a lot zoned R-R, R-A, R-2, R-3,
22 R-4, R-6, R-T, R-T-R, W-2-M or SP with a residential use, the minimum
23 setback shall be twenty-five feet (25') from the property line.
- 24 J. Setback areas may be used for driveways, parkways and landscaping.
- 25 K. A minimum of fifteen percent (15%) of the site proposed for development
26 shall be landscaped and irrigated.
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L. Trash collection areas shall be screened by landscaping or architectural features in such a manner as not to be visible from a public street or from any adjacent residential area.

M. Outside storage areas are prohibited.

N. Utilities shall be installed underground except that electrical lines rated at 33 kv or greater may be installed above ground.

O. All lighting fixtures, including spot lights, electrical reflectors and other means of illumination for signs, structures, landscaping, parking, loading, unloading and similar areas, shall be focused, directed and arranged to prevent glare or direct illumination on residential uses.

(5) Nonsubstantial adjustments to an approved project's design are permitted subject to the approval of a minor change pursuant to Ordinance No. 460. For purposes of this section, "Nonsubstantial adjustment" shall be defined as changes to setbacks, floor plans and elevations. All other changes including changes in concept and product type shall be submitted for review in accordance with the provisions of Ordinance No. 460 governing minor changes and revised tentative maps.

(6) Except as provided above, all other zoning requirements shall be the same as those requirements identified in Article IXb of Ordinance No. 348.

o. Planning Area 34.

(1) The uses permitted in Planning Area 34 of Specific Plan No. 260 shall be the same as those uses permitted in Article VI, Section 6.1 of Ordinance No. 348, except that the uses permitted pursuant to Section 6.1.b.(1) and (3); and d. shall not be permitted.

(2) The development standards for Planning Area 34 of Specific Plan No. 260 shall be the same as those standards identified in Article VI, Section 6.2 of Ordinance No. 348, except that the development standards set forth in Article VI, Section 6.2.b., c., d., e.(2) and (4) shall be deleted and replaced by the following:

- 1 A. Lot area shall be not less than five thousand (5,000) square feet. The
2 minimum lot area shall be determined by excluding that portion of a lot that
3 is used solely for access to the portion of a lot used as a building site.
- 4 B. The minimum average width of that portion of a lot to be used as a building
5 site shall be fifty feet (50') with a minimum average depth of eighty feet
6 (80'). That portion of a lot used for access on "flag" lots shall have a
7 minimum width of twenty feet (20').
- 8 C. The minimum frontage of a lot shall be forty feet (40'), except that lots
9 fronting on knuckles or cul-de-sacs may have a minimum frontage of thirty-
10 five feet (35'). Lot frontage along curvilinear streets may be measured at
11 the building setback in accordance with zone development standards.
- 12 D. Side yards on interior and through lots shall be not less than five feet (5') in
13 width. Side yards on corner and reversed corner lots shall be not less than
14 ten feet (10') from the existing street line or from any future street line as
15 shown on any specific plan of highways, whichever is nearer the proposed
16 structure, upon which the main building sides, except that where the lot is
17 less than fifty feet (50') wide, the yard need not exceed twenty percent
18 (20%) of the width of the lot.
- 19 E. Chimneys and fireplaces shall be allowed to encroach into side yards a
20 maximum of two feet (2'). No other structural encroachments shall be
21 permitted in the front, rear or side yard except as provided for in Section
22 18.19 of Ordinance No. 348.

23 In addition, the following standards shall also apply:

- 24 AA. In no case shall more than fifty percent (50%) of any lot be covered
25 by buildings for lots with one-story buildings and in no case shall
26 more than forty-five percent (45%) of any lot be covered by
27 buildings for lots with two-story buildings.

1 (3) Except as provided above, all other zoning requirements shall be the same as those
2 requirements identified in Article VI of Ordinance No. 348.

3 p. Planning Area 33A.

4 (1) The uses permitted in Planning Area 33A of Specific Plan No. 260 shall be the
5 same as those uses permitted in Article VIII, Section 8.100 of Ordinance No. 348,
6 except that the uses permitted pursuant to Section 8.100.a. and b. shall not be
7 permitted. In addition, the permitted uses identified under Section 8.100.a. shall
8 also include trails.

9 (2) The development standards for Planning Area 33A of Specific Plan No. 260 shall
10 be the same as those standards identified in Article VIIIe, Section 8.101 of
11 Ordinance No. 348.

12 (3) Except as provided above, all other zoning requirements shall be the same as those
13 requirements identified in Article VIIIe of Ordinance No. 348.

14 q. Planning Area 36.

15 (1) The uses permitted in Planning Areas 36 of Specific Plan No. 260 shall be the same
16 as those uses permitted in Article XVb, Section 15.200 of Ordinance No. 348,
17 except that the uses permitted pursuant to Section 15.200.c(4), (6), (11), (12), and
18 (14) shall not be permitted.

19 (2) The development standards for Planning Area 36 of Specific Plan No. 260 shall be
20 the same as those standards identified in Article XVb, Section 15.201 of Ordinance
21 No. 348, except that the development standards set forth in Article XVb, Section
22 15.201 a. shall be deleted and replaced by the following:

23 A. Lot area shall be not less than ten thousand (10,000) square feet. The
24 minimum lot area shall be determined by excluding that portion of a lot that
25 is used solely for access to the portion of a lot used as a building site.

26 (3) Except as provided above, all other zoning requirements shall be the same as those
27 requirements identified in Article XVb of Ordinance No. 348,

28 r. Planning Area 39.

- 1 (1) The uses permitted in Planning Area 39 of Specific Plan No. 260 shall be the same
2 as those uses permitted in Article VI, Section 6.1 of Ordinance No. 348, except that
3 the uses permitted pursuant to Section 6.1.b.(1) and (3); and d. shall not be
4 permitted. In addition, the permitted uses identified under Section 6.1.a. shall also
5 include public schools.
- 6 (2) The development standards for Planning Area 39 of Specific Plan No. 260 shall be
7 the same as those standards identified in Article VI, Section 6.2 of Ordinance No.
8 348, except that the development standards set forth in Article VI, Section 6.2.b.,
9 c., d., e.(2) and (4) shall be deleted and replaced by the following:
- 10 A. Lot area shall be not less than five thousand (5,000) square feet. The
11 minimum lot area shall be determined by excluding that portion of a lot that
12 is used solely for access to the portion of a lot used as a building site.
- 13 B. The minimum average width of that portion of a lot to be used as a building
14 site shall be fifty feet (50') with a minimum average depth of eighty feet
15 (80'). That portion of a lot used for access on "flag" lots shall have a
16 minimum width of twenty feet (20').
- 17 C. The minimum frontage of a lot shall be forty-five feet (45'), except that lots
18 fronting on knuckles or cul-de-sacs may have a minimum frontage of thirty-
19 five feet (35'). Lot frontage along curvilinear streets may be measured at
20 the building setback in accordance with zone development standards.
- 21 D. Side yards on interior and through lots shall be not less than five feet (5') in
22 width. Side yards on corner and reversed corner lots shall be not less than
23 ten feet (10') from the existing street line or from any future street line as
24 shown on any specific plan of highways, whichever is nearer the proposed
25 structure, upon which the main building sides, except that where the lot is
26 less than fifty feet (50') wide, the yard need not exceed twenty percent
27 (20%) of the width of the lot.
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E. Chimneys and fireplaces shall be allowed to encroach into side yards a maximum of two feet (2'). No other structural encroachments shall be permitted in the front, rear or side yard except as provided for in Section 18.19 of Ordinance No. 348.

In addition, the following standards shall also apply:

AA. In no case shall more than fifty percent (50%) of any lot be covered by buildings for lots with one-story buildings and in no case shall more than forty-five percent (45%) of any lot be covered by buildings for lots with two-story buildings.

(3) Except as provided above, all other zoning requirements shall be the same as those requirements identified in Article VI of Ordinance No. 348.

Section 2. This Ordinance shall take effect 30 days after its date of adoption.

BOARD OF SUPERVISORS OF THE COUNTY
OF RIVERSIDE, STATE OF CALIFORNIA

BY: _____
CHAIRMAN OF THE BOARD OF SUPERVISORS

ATTEST:
Clerk to the Board

By: _____
Deputy

(SEAL)

APPROVED AS TO FORM AND CONTENT:

By: _____

MENIFEE NORTH

SPECIFIC PLAN No. 260, AMENDMENT No. 2

SUBSTANTIAL CONFORMANCE No. 2

(REVISED PAGES ONLY)

PREPARED FOR:

RICHLAND COMMUNITIES

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Irvine, CA 92612

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PREPARED BY:

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IN CONSULTATION WITH:

K&A ENGINEERING – *Civil Engineer*

1ST DRAFT SCREENCHECK: JULY 2018

I. SUMMARY OF CHANGES

Substantial Conformance No. 2 to the MENIFEE NORTH Specific Plan, Amendment No. 2 (SP260-A2-SC2) modifies the acreages, unit counts and lot sizes for Planning Areas 24, 25, 26 and 28 to be consistent with approved Tentative Tract Map No. 29322 and proposed Tentative Tract Map No. 37533, which are being processed concurrently with Substantial Conformance No. 2.

Substantial Conformance No. 2 maintains the approved maximum of 2,025 residential units permitted on 829.2 acres, while modifying the Specific Plan text and graphics to: reduce the minimum lot sizes within Planning Area 25 from 6,000 s.f. to 3,500 s.f.; re-allocate 22 dwelling units from Planning Areas 26 and 28 (TTM 29322) to Planning Areas 24 and 25 (10 dwelling units and 12 dwelling units respectively) to be consistent with Tentative Tract Map No. 37533; revise the locations of the water quality basins and mini-park sites within Planning Areas 24, 25, 26, and 28; and reduce the overall acreage of major roads within the overall SP area from 77.1 acres to 74.8 acres. Additionally, Substantial Conformance No. 2 makes other, non-substantive changes throughout the document to accommodate these modifications.

Substantial Conformance No. 2 also includes a technical correction for the land use designations of Planning Areas 26 and 28, from “Medium-High Density Residential” to “Medium Density Residential”, so that the designations are consistent with previously approved TM 29322. Substantial Conformance No. 1 modified the land use designations of Planning Areas 26 and 28 in error, because no consideration was made of the approved unit count and acreages in approved TTM No. 29322. Substantial Conformance No. 2 re-designates Planning Areas 26 and 28 from MHDR to MDR to resolve this error.

Figure I-1, *Land Use Plan Comparison – Areas of Change*, provides a graphic comparison between the adopted Land Use Plan for MENIFEE NORTH Specific Plan, Amendment No. 2, Substantial Conformance No. 1 and the proposed Land Use Plan for Substantial Conformance No. 2.

Specifically, Substantial Conformance No. 2 provides the following modifications to the Land Use Plan of approved Amendment No. 2, Substantial Conformance No. 1:

- The acreage of Planning Area 24 is increased from 25.7 acres to 26.1 acres, the dwelling unit count is increased from 115 to 125, and the residential density is increased from 4.5 du/ac to 4.8 du/ac to reflect TTM No. 37533. Both the existing and proposed densities for this Planning Area are consistent with the “Medium Density Residential” of the County General Plan Designation; and no change in designation is proposed.
- The acreage of Planning Area 25 is decreased from 44.7 acres to 44.6 acres, the dwelling unit count is increased from 226 to 238, and the residential density is increased from 5.1 du/ac to 5.3 du/ac to reflect TTM No. 37533. Both the existing and proposed densities for this Planning Area are consistent with the County’s “Medium-High Density Residential” General Plan Land Use Designation; and no change in designation is proposed. Planning Area 25 provides a new 3.2-acre Mini-Park site, which is included in Planning Area 25’s overall acreage and does not result in a density range change.

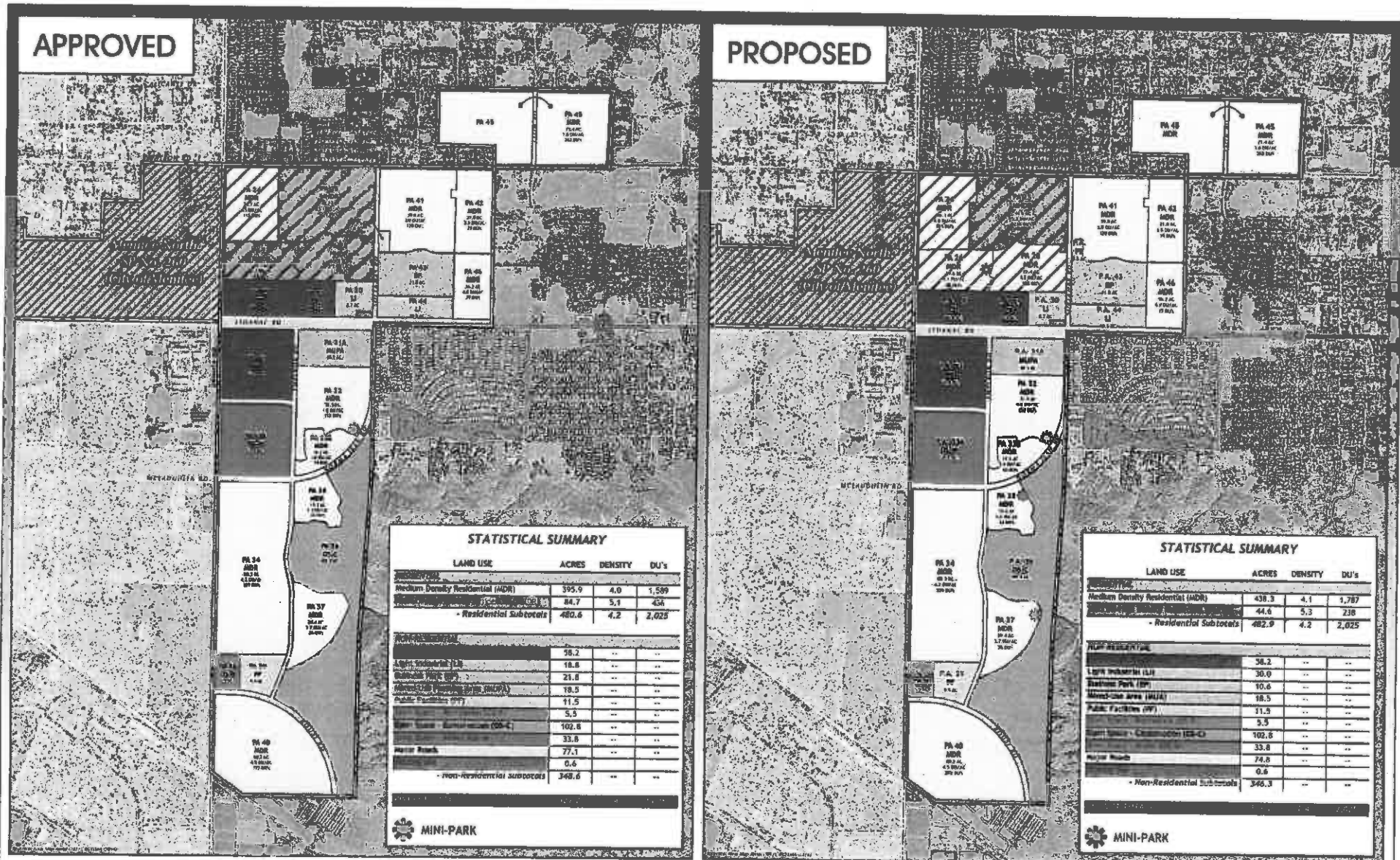
- The acreage of Planning Area 26 is increased from 18.2 acres to 19.6 acres; the dwelling unit count is decreased from 97 to 80; and the residential density is decreased from 5.3 du/ac to 4.1 du/ac to reflect TTM No. 29322. Substantial Conformance No. 1 modified the land use designation of Planning Areas 26 in error, because no consideration was made of the approved unit count and acreages in approved TTM No. 29322. The change from “Medium-High Density Residential” to “Medium Density Residential” represents a technical correction to the General Plan Designation and nomenclature of PA 26 used throughout the document to be consistent with the dwelling unit count and density of approved TTM No. 29322. Both the existing and proposed densities for this Planning Area are consistent with the County’s “Medium Density Residential” General Plan Land Use Designation; and no change in density range is proposed. A 0.2-acre Mini-Park site is provided within Planning Area 26 and is accounted for in Planning Area 26’s total acreage.
- The acreage of Planning Area 28 was increased from 21.8 acres to 22.4 acres; the dwelling unit count was decreased from 113 to 108; and the residential density was decreased from 5.2 du/ac to 4.8 du/ac to reflect TTM No. 29322. Substantial Conformance No. 1 modified the land use designation of Planning Areas 28 in error, because no consideration was made of the approved unit count and acreages in approved TTM No. 29322. The change from “Medium-High Density Residential” to “Medium Density Residential” represents a technical correction to the General Plan Designation and nomenclature of PA 28 used throughout the document to be consistent with the dwelling unit count and density of approved TTM No. 29322. Both the existing and proposed densities for this Planning Area are consistent with the “Medium Density Residential” of the County General Plan Designation; and no change in density range is proposed.
- The acreage reserved for Major Community Roadways has been decreased from 77.1 acres to 74.8 acres to reflect the engineered boundaries and acreages of TTM No. 29322 and TTM No. 37533.

The modifications provided by Substantial Conformance No. 2 are summarized in Table SC-1, *Specific Plan No. 260, Amendment No. 2, Substantial Conformance No. 2*.

Table SC-1 - Specific Plan No. 260, Amendment No. 2, Substantial Conformance No. 2

Approved Menifee North Specific Plan No. 260 Amendment No. 2 Substantial Conformance No. 1					Menifee North Specific Plan No. 260 Amendment No. 2 Substantial Conformance No. 2				
PA	Land Use	Target Units	Acres	Target Density	PA	Land Use	Target Units	Acres	Target Density
24	Medium Density Residential	115	25.7	4.5	24	Medium Density Residential	125	26.1	4.8
25	Medium-High Density Residential	226	44.7	5.1	25	Medium-High Density Residential	238	44.6	5.3
26	Medium-High Density Residential	97	18.2	5.3	26	Medium Density Residential	80	19.6	4.1
28	Medium-High Density Residential	113	21.8	5.2	28	Medium Density Residential	108	22.4	4.8
--	Major Roads	--	77.1	--	--	Major Roads	--	74.8	--
Total		551	187.5		Total		551	187.5	

I. SUMMARY OF CHANGES
**MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 2
SUBSTANTIAL CONFORMANCE NO. 2**



MENIFEЕ NORTH SP 260 - A2, SC2



FIGURE I-1

LAND USE PLAN COMPARISON - AREAS OF CHANGE

II. SUMMARY

A. INTRODUCTION

Section 65450 of the California Government Code grants local government agencies the authority to prepare specific plans of development for any area covered by a General Plan, for the purpose of establishing systematic methods of implementation of the agency's General Plan. A Specific Plan, as the name implies, is specific in nature as compared to a General Plan. It is designed to address site specific issues such as building setbacks and visual appearance, as well as project-wide concerns such as circulation and provisions for utilities. A General Plan does not show local streets, individual parcels or specific land uses. While a General Plan examines an entire City or County, a Specific Plan will concentrate on the individual development issues of a particular project or region. The Riverside County General Plan has established objectives which mandate the preparation of individual Specific Plan documents in order to ensure that new developments meet the basic standards of environmental safety, infrastructure, quality of structural design, site planning and contain the provisions to maintain aesthetic quality and cultural identity. Most importantly, the Specific Plan has strong implications on government budgeting decisions.

In addition to the *MENIFEE NORTH* Specific Plan, the accompanying Environmental Impact Report (EIR), was prepared under the authority of the County of Riverside and is in compliance with the California Environmental Quality Act (CEQA) and County of Riverside Guidelines. The EIR is intended to serve as the project-wide Master Environmental Document for the *MENIFEE NORTH* Specific Plan and all subsequent development projects undertaken pursuant to and within the Specific Plan boundaries. Furthermore, the EIR is an informational document designed to provide decision-makers and members of the general public with a full understanding of the potential environmental effects of the development proposal. Together, the Specific Plan and EIR provide a path to properly develop the site, taking into account all local goals, objectives and environmental considerations.

B. PROJECT SUMMARY

Since the adoption of Amendment No. 2 to the *MENIFEE NORTH* Specific Plan in 2008, the City of Menifee incorporated, resulting in an approximately 775.4 acre portion of the adopted Specific Plan west of Briggs Road being removed from County of Riverside jurisdiction. In January 2016, Substantial Conformance No. 1 to Amendment No. 2 was adopted and modified the Specific Plan boundary, removing all areas west of Briggs Road from the *MENIFEE NORTH* Specific Plan. The revised limits of the *MENIFEE NORTH* Specific Plan were reduced to approximately 829.2 acres east of Briggs Road, west of Juniper Flats Road, south of Alicante Drive, and north of Matthews Road and provides for a wide range of land uses including 2,025 residential units.

Substantial Conformance No. 2 to the *MENIFEE NORTH* Specific Plan, Amendment No. 2 (SP260-A2-SC2) modifies the acreages, unit counts and lot sizes for Planning Areas 24, 25, 26 and 28 to be consistent with approved Tentative Tract Map No. 29322 and proposed Tentative Tract Map No. 37533, which are being processed concurrently with Substantial Conformance No. 2.

Substantial Conformance No. 2 maintains the approved maximum of 2,025 residential units permitted on 829.2 acres, while modifying the Specific Plan text and graphics to: reduce the minimum lot sizes within Planning Area 25 from 6,000 s.f. to 3,500 s.f.; re-allocate 22 dwelling units from Planning Areas 26 and 28

(TTM 29322) to Planning Areas 24 and 25 (10 dwelling units and 12 dwelling units respectively) to be consistent with Tentative Tract Map No. 37533; revise the locations of the water quality basins and mini-park sites within Planning Areas 24, 25, 26, and 28; and reduce the overall acreage of major roads within the overall SP area from 77.1 acres to 74.8 acres. Additionally, Substantial Conformance No. 2 makes other, non-substantive changes throughout the document to accommodate these modifications.

Substantial Conformance No. 2 also includes a technical correction for the land use designations of Planning Areas 26 and 28, from “Medium-High Density Residential” to “Medium Density Residential”, so that the designations are consistent with previously approved TM 29322. Substantial Conformance No. 1 modified the land use designations of Planning Areas 26 and 28 in error, because no consideration was made of the approved unit count and acreages in approved TTM No. 29322. Substantial Conformance No. 2 re-designates Planning Areas 26 and 28 from MHDR to MDR to resolve this error.

The *MENIFEE NORTH* Specific Plan provides the County of Riverside, as well as future developers, community groups and community service districts, with a comprehensive set of plans, regulations, conditions and programs for guiding the systematic development of the project. The Specific Plan also implements each applicable Element of the Riverside County General Plan.

The *MENIFEE NORTH* Specific Plan assures that development of the proposed land use plan will proceed as a coordinated project involving the orderly and systematic construction of residential, commercial, industrial and open space uses and the provisions of public infrastructure and services necessary to ensure that Riverside County standards for orderly growth are implemented.

The project area primarily consists of vacant land with some existing scattered residential and commercial uses surrounded by either fragmented residential, commercial, agricultural or some limited forms of professional office land uses. When fully developed, the *MENIFEE NORTH* Specific Plan will provide a balance of residential land uses varying in density from 3.0 dwelling units per acre to 5.3 dwelling units per acre. The project will also include one elementary school site, a community park, and natural open space, as well as commercial retail, business park, mixed use planning areas, and light industrial uses.

The residential products to be provided will serve to meet a market need in this urbanizing portion of southwest Riverside County, while maintaining a sensitive approach to development relative to existing streets and anticipated traffic volumes that are planned for within the Riverside County Circulation Plan and Master Plan of Highways.

MENIFEE NORTH will provide a community which offers a unique living and working environment, providing a range of residential densities, commercial and industrial square footage and park and open space amenities consistent with the on-going development in the Southern Perris Valley region. Many of the future residents, who are projected to range from first-time to move-up buyers, will be able to take advantage of amenities offered in the *MENIFEE NORTH* Specific Plan project area.

The *MENIFEE NORTH* Specific Plan can be briefly summarized as follows:

Table I Land Use Summary

LAND USE	ACREAGE	DWELLING UNITS / ACRE	MAXIMUM DWELLING UNITS	PERCENTAGE
RESIDENTIAL				
7,200 SF Medium	150.4	3.6	538	18.1%
6,000 SF Medium	93.8 135.8	4.24 .3	391 579	11.3 16.3%
5,000 SF Medium ^{(1),(2)}	151.7 152.1	4.34 .4	660 670	18.3%
6,000 SF Medium High ⁽⁴⁾	84.7	5.1	436	10.2%
3,500 SF Medium High	44.6	5.3	238	5.3%
RESIDENTIAL SUBTOTAL	480.6 482.6	4.2	2,025	58.0 58.2 %
NON-RESIDENTIAL				
Commercial Retail	58.2	-	-	7.0%
Light Industrial	18.8	-	-	2.3%
Mixed Use Planning Area	18.5	-	-	2.2%
Business Park	21.8	-	-	2.6%
Public Facility	9.9	-	-	1.2%
Open Space-Recreation ⁽³⁾	5.5	-	-	0.7%
Open Space-Conservation	102.8	-	-	12.4%
Open Space-Water	33.8	-	-	4.1%
Public Facility Fire Station	1.6	-	-	0.2%
Major Roads	77.1 74.8	-	-	9.3%
Easements/Existing Uses	0.6	-	-	0.1%
NON-RESIDENTIAL SUBTOTAL	348.6 346.3	-	-	42.0%
PROJECT TOTAL	829.2	2.4	2,025	100.0%

(1) The maximum density may be increased to 6.0 du/ac with a 5,000 square foot lot size in Planning Area 34 in either of the following two circumstances:

- (a) The project is designed for and restricted to senior citizen housing; or
- (b) The project is a mobile home park or mobile home subdivision.

If this option is elected, the maximum number of dwelling units shown in Table II for Planning Areas 34 may increase, however, the overall maximum number of dwelling units for the entire Specific Plan may not exceed 2,025.

- (2) The overall number of units for Planning Areas 33B and 34 is restricted to require that several neighborhoods of differing lot sizes be developed.
- (3) Mini-parks are required in Planning Areas 25, 26, 32, 35 and 37.

II. SUMMARY

**MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 2
SUBSTANTIAL CONFORMANCE NO. 2**

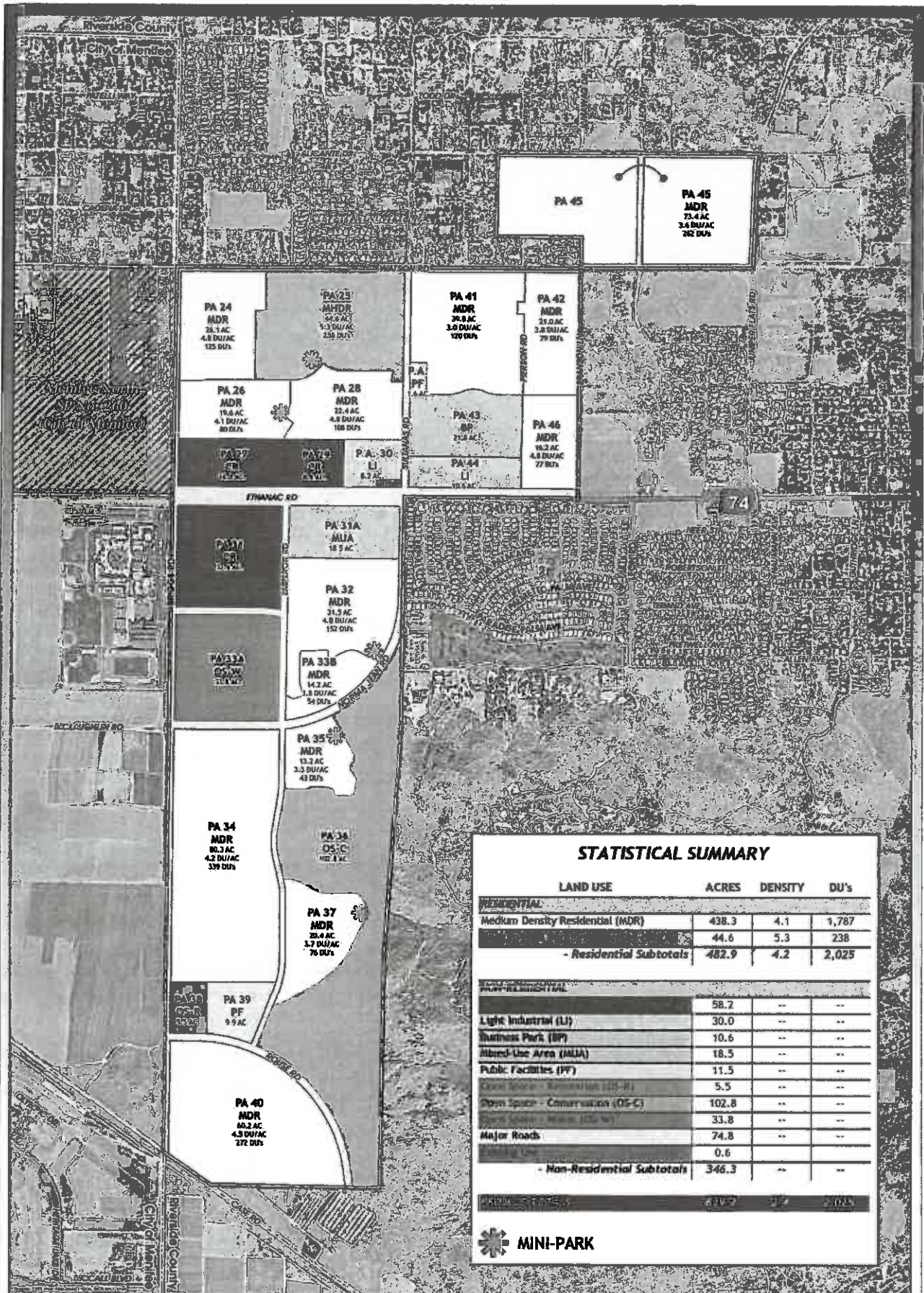
developed, a maximum of 2,025 homes will be constructed in *MENIFEE NORTH*. The units will be developed across a spectrum of lot sizes as depicted in the *Specific Plan Land Use Plan* (Figure III-1). In order to create a full service, balanced community, the project plans for one elementary school site, one community park, natural open space, commercial retail, business park, light industrial, and mixed use planning areas.

The project site has been divided into 26 planning areas on the basis of logical, separate units of development. Specific information on each of the planning areas within *MENIFEE NORTH* is provided within Section III.B; Planning Area Development Standards, in the Detailed Land Use Summary (Table II) and in Figures III-14 through 26).

The proposed land uses within the *MENIFEE NORTH* Specific Plan include:

- RESIDENTIAL - The Specific Plan was formulated based on the goal of providing a variety of housing types, styles, sizes and values to attract a variety of homebuyers. The residential element of the plan accounts for 482.5 acres of the project site, containing 2,025 dwelling units. Residential densities within the project will net 4.2 du/ac and gross 2.4 du/ac overall. The housing mix will be spread over four density ranges varying from Medium to High, as described below.
 - Medium Density Residential (7,200 s.f. lots, 3.6 du/ac) - consists of 538 dwelling units on 150.4 acres in Planning Areas 41, 42, 45, and 46. Lot sizes in these areas will be a minimum of 7,200 square feet.
 - Medium Density Residential (6,000 s.f. lots, 4.2-3 du/ac) - consists of ~~391-579~~ dwelling units on ~~93.8-135.8~~ acres in Planning Areas ~~26, 28,~~ 35, 37, and 40. Lot sizes in these areas will be a minimum of 6,000 square feet.
 - Medium Density Residential (5,000 s.f. lots, 4.3-4 du/ac) - consists of ~~660-670~~ dwelling units on ~~151.7-3~~ acres in Planning Areas 24, 32, 33B and 34. Lot sizes in these areas will be a minimum of 5,000 square feet.
 - Medium High Density Residential (~~6,000-3,500~~ s.f. lots, 5.3+ du/ac) - consists of ~~436-238~~ dwelling units on ~~84.7-44.6~~ acres in Planning Areas ~~25, 26, and 28,~~. Lot sizes in these areas will be a minimum of ~~6,000-3,500~~ square feet.

The maximum density of Planning Area 34 may be increased to 6.0 du/ac with a 5,000 square foot lot minimum if the planning area is designed for and restricted to senior citizen housing or if the planning area is a mobile home park or mobile home subdivision. Additional residential opportunities are also provided by Planning Area 31A which could accommodate high density residential uses within a mixed use/neighborhood commercial environment.



STATISTICAL SUMMARY

LAND USE	ACRES	DENSITY	DU's
RESIDENTIAL			
Medium Density Residential (MDR)	438.3	4.1	1,787
	44.6	5.3	238
- Residential Subtotals	482.9	4.2	2,025
NON-RESIDENTIAL			
Light Industrial (LI)	58.2	--	--
Business Park (BP)	30.0	--	--
Business Park (BP)	10.6	--	--
Mixed-Use Area (MUA)	18.5	--	--
Public Facilities (PF)	11.5	--	--
Open Space - Recreation (OS-R)	5.5	--	--
Open Space - Commercial (OS-C)	102.8	--	--
Major Roads (OS-R)	33.8	--	--
Major Roads	74.8	--	--
Other Use	0.6	--	--
- Non-Residential Subtotals	346.3	--	--

MINI-PARK

MENIFEE NORTH SP 260 - A2, SC2

FIGURE III-1

- **COMMERCIAL RETAIL** - *MENIFEE NORTH* will contain several commercial sites totaling 58.2 acres, proposed for Planning Areas 27, 29 and 31. The commercial uses proposed will serve residents of *MENIFEE NORTH*, off-site users and persons employed in the area. The commercial sites are located to provide easy access for both on-site residents and the regional population. The provision of commercial uses is important not only for convenience, but also to satisfy commercial needs of the future population in this region.
- **LIGHT INDUSTRIAL** - The Light Industrial uses, totaling 18.8 acres of land, are proposed for Planning Areas 30, and 44. This land use is intended to provide areas for wholesale commercial, business/office, supportive commercial, and light industrial uses.
- **BUSINESS PARK** - The Business Park uses, totaling 21.8 acres of land, are proposed for Planning Area 43. This land use is intended to provide development areas for professional office and related supportive commercial uses as well as some limited types of "clean" light industrial uses.
- **MIXED USE PLANNING AREA** - The Mixed Use area in Planning Area 31A encompasses 18.5 acres and provides a mix of limited commercial and high density residential land uses.
- **PUBLIC FACILITY** – One elementary school site is planned within *MENIFEE NORTH*, containing a total of 9.9 acres, located in Planning Area 39. The Specific Plan satisfies the standards of the Perris Union and Romoland School Districts for the reservation of school sites as the project is implemented over the phasing period.
- **OPEN SPACE-RECREATION** - The *MENIFEE NORTH* project offers various sites for active recreation, highlighted by a 5.5-acre community park in Planning Area 38. This park is strategically located adjacent to the elementary school site to provide excellent opportunities for sharing facilities. In addition, mini-parks are required in portions of Planning Areas 25, 26, 32, 35, and 37. Including ~~5.18.5~~ acres of mini-parks, the project provides ~~10.6~~14.0 acres of neighborhood parkland.
- **OPEN SPACE-CONSERVATION** - Approximately 102.8 acres of the Specific Plan area will remain as natural open space, preserved in Planning Area 36. This open space area includes the preservation of a significant hillside in the eastern portion of the site.
- **ROADS** - Major roadways totaling 77.1 acres will be developed in conjunction with the proposed project. The Riverside County Master Plan of Streets and Highways will adequately serve future traffic volumes for the region. On-site traffic will be handled by a hierarchical roadway system consisting of Expressway, Urban Arterial Highway, Major Highway, Secondary Highway, Industrial Collector, Collector and local roadways ranging in right-of-way width from 56 feet to 220 feet.

b. Land Use Development Standards

In order to ensure the orderly and sensitive development of the land proposed for *MENIFEE NORTH*, special techniques or mitigations have been created for each planning area. These area-specific standards, discussed in detail in Section III.B., Planning Area Development Standards, will assist in accommodating the proposed development and provide adequate transitions among neighboring land uses.

**Table II
MENIFEE NORTH
DETAILED LAND USE SUMMARY**

DESIGNATION	PLANNING AREA	ACREAGE	TARGET DENSITY	DWELLING UNITS
RESIDENTIAL				
Medium – 7,200 SF min	41	39.8	3.0	120
	42	21.0	3.8	79
	45	73.4	3.6	262
	46	16.2	4.8	77
SUBTOTAL - MEDIUM - 7,200 SF MIN.		150.4	3.6	538
Medium – 6,000 SF min	26	19.6	4.1	80
	28	22.4	4.8	108
	35	13.2	3.3	43
	37	20.4	3.7	76
	40	60.2	4.5	272
SUBTOTAL - MEDIUM - 6,000 SF MIN.		93.8135.8	4.24.3	391579
Medium - 5,000 SF min.	24	25.726.1	4.54.8	115125
	32	31.5	4.8	152
	33B	14.2	3.8	54
	34 ⁽¹⁾	80.3	4.2	339
SUBTOTAL - MEDIUM - 5,000 SF MIN.		151.7152.1	4.34.4	660670
Medium High – 6,0003,500 SF min.	25	44.744.6	5.15.3	226238
	26	18.2	5.3	97
	28	21.8	5.2	113
SUBTOTAL - MEDIUM HIGH - 6,0003,500 SF MIN.		84.744.6	5.15.3	436238
RESIDENTIAL SUBTOTALS		480.6482.9	4.2	2,025
NON-RESIDENTIAL				
	43	21.8	—	—
SUBTOTAL - BUSINESS PARK		21.8	—	—
	27	16.8	—	—
	29	8.8	—	—
	31	32.6	—	—
SUBTOTAL - COMMERCIAL RETAIL		58.2	—	—
Mixed Use Planning Area	31A	18.5	—	—
SUBTOTAL – MIXED USE PLANNING AREA		18.5	—	—
	30	8.2	—	—
	44	10.6	—	—
SUBTOTAL - LIGHT INDUSTRIAL		18.8	—	—
	39	9.9	—	—

III. SPECIFIC PLAN
MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 2
SUBSTANTIAL CONFORMANCE NO. 2

Table II
MENIFEE NORTH
DETAILED LAND USE SUMMARY

DESIGNATION	PLANNING AREA	ACREAGE	TARGET DENSITY	DWELLING UNITS
SUBTOTAL - PUBLIC FACILITY		9.9	—	—
	38	5.5		
SUBTOTAL - OPEN SPACE-RECREATION		5.5	—	—
Open Space-Conservation	36	102.8	—	—
Open Space-Water	—	33.8	—	—
Utility Easements/Existing Uses	—	0.6	—	—
Major Roadways	—	77.174.8	—	—
Fire Station	—	1.6	—	—
NON-RESIDENTIAL SUBTOTALS		348.6346.3	—	—
PROJECT TOTALS		829.2	2.4	2,025

The maximum density may be increased to 6.0 du/ac with a 5,000 square foot lot size in Planning Area 34 in either of the following two circumstances:

- a. The project is designed for and restricted to senior citizen housing, or
- b. The project is a mobile home park or mobile home subdivision

If this option is elected, the maximum number of dwelling units shown in Table II may be exceeded up to a new maximum which is listed in the Planning Area description for the relevant Planning Area. The overall maximum number of dwelling units for the entire Specific Plan may not exceed 2,025,815.

² Mini-parks are required in Planning Areas 25, 26, 32, 35 and 37.

6. OPEN SPACE AND RECREATION PLAN

a. Open Space and Recreation Plan Description

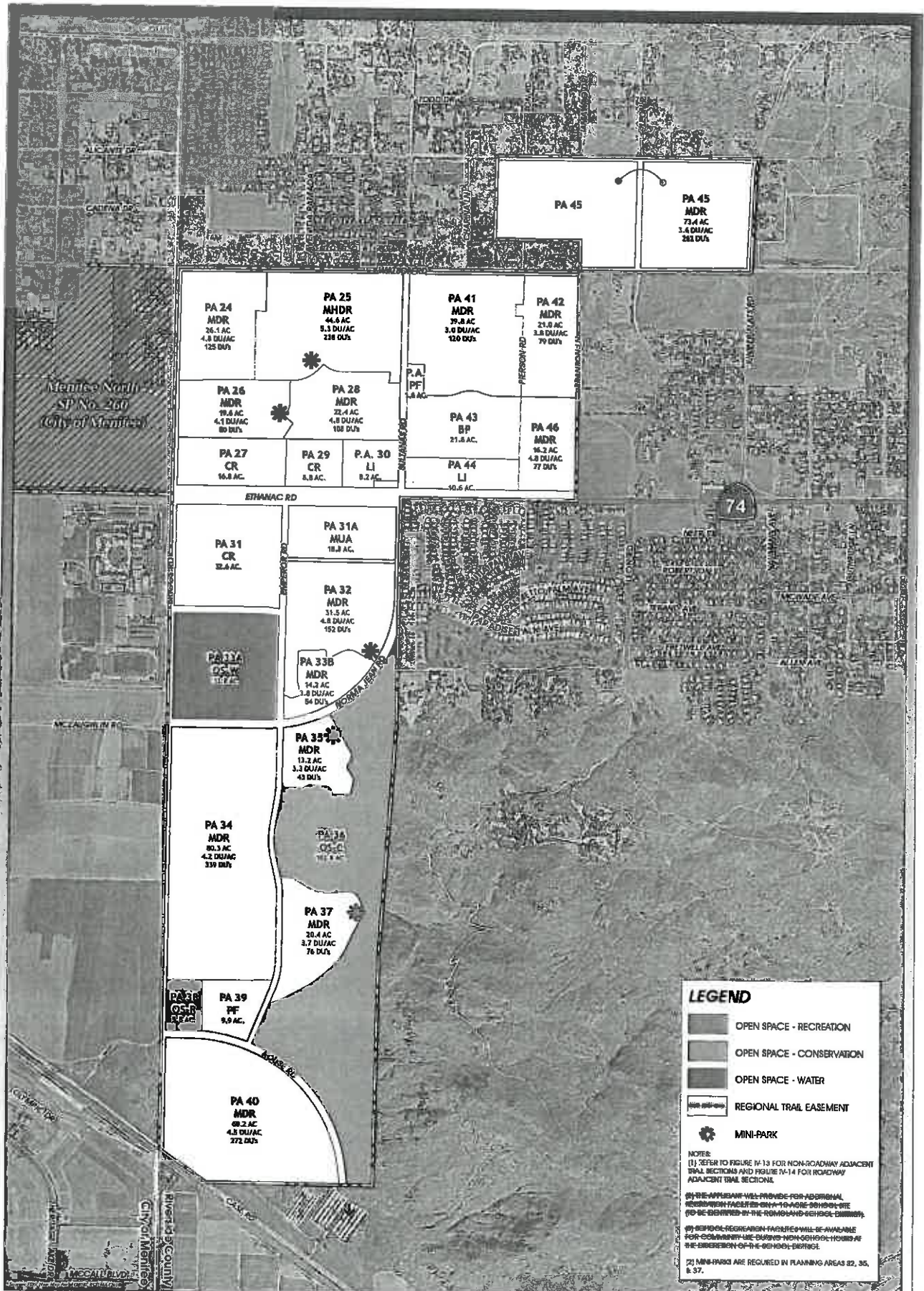
The *MENIFEE NORTH* project provides for an open space and recreation program which offers recreational opportunities in which all members of the community can participate. The program incorporates many diverse elements in a coordinated, cohesive plan that interrelates with and links the various neighborhoods of the community and certain destination points, such as an elementary school, neighborhood parkland, open spaces and shopping facilities. Recreational opportunities vary from passive (i.e. undeveloped open space), to active (i.e. community parks and mini-parks). Varying types and degrees of activities will be available which will provide residents with the opportunity to take quiet "walks in the park," participate in social gatherings, and participate in active outdoor informal recreational activities such as hiking and biking. Figure III-9, *Open Space and Recreation Plan*, depicts the various open spaces and recreational areas that are proposed for *MENIFEE NORTH*. The elements and acreages of the *Open Space and Recreation Plan* are summarized in Table III and discussed below.

The adopted MENIFEE NORTH Specific Plan Amendment No. 2 provided 29.4 acres of neighborhood parkland as well as trails and other open space. However, since the adoption of Amendment No. 2, the City of Menifee incorporated, and annexed the portion of MENIFEE NORTH located west of Briggs Road, which included including 23.9 acres of the Specific Plan's 29.4 acres of recreational facilities, ~~(out of a total of 29.4 acres).~~

**Table III
OPEN SPACE AND RECREATION PLAN SUMMARY**

	ACREAGE
Parks	
• Community Park (Planning Area 38)	5.5
• Mini-Parks (Portions of Planning Areas <u>25, 26, 32, 35, and 37</u>)	5.1 <u>18.5</u>
School Recreation Facilities	—*
SUBTOTAL	10.6 <u>14.0</u>
Natural Open Space	102.8
Community Trail	—
Landscaped Parkways	—
OVERALL OPEN SPACE AND RECREATION OPPORTUNITIES	113.4 <u>117.8</u>

**School recreation facilities will be available for community use during non-school hours at the discretion of the School District.*



MENIFEE NORTH SP 260 - A2, SC2

FIGURE III-9

- **COMMUNITY PARK:** The *MENIFEE NORTH* Specific Plan includes one community park in Planning Area 38, and is designated as Open Space-Recreation. This 5.5-acre park is located in the southern portion of the community and is designed to offer a variety of active recreational opportunities to *MENIFEE NORTH* residents. The park in Planning Area 38 is located adjacent to the elementary school site, providing an excellent opportunity for sharing facilities. Proposed park amenities include picnic areas, tot lots, sport fields and courts, and barbecue facilities, as well as parking areas and restrooms.
- **MINI-PARKS:** Three mini-parks, totaling approximately ~~5-18.5~~ acres are proposed within the *MENIFEE NORTH* community. The mini-parks, varying in size from 0.2 acres to 4 acres are proposed within Planning Areas 25, 26, 32, 35 and 37. These parks will function as central gathering places for the neighborhoods in which they are located, adding to the overall amenity package for *MENIFEE NORTH*. The mini-parks may be either public or private.
- **NATURAL OPEN SPACE:** The *MENIFEE NORTH* community provides for 102.8 acres of natural open space. The open space—represented by Planning Area 36—is located on the southeastern portions of the site. The open space may allow for some passive recreational activities, depending upon the nature of the vegetation and terrain.
- **REGIONAL TRAIL AND LANDSCAPED PARKWAYS:** A regional trail will be provided in Planning Area 36 and along designated project roadways. Landscaped parkways will be incorporated within the streetscapes of major project roads. These elements will provide additional open space for pedestrian use.
- **SCHOOL RECREATION FACILITIES:** One elementary school is proposed for *MENIFEE NORTH*. Recreational facilities developed on the school sites will be available for community use during non-school hours, at the discretion of the School District. Typical recreational equipment and sports fields which may be provided by the School District include playground equipment, basketball courts, tennis courts, volleyball courts, softball fields, soccer fields and football fields.

The revised Specific Plan limits reflected in Substantial Conformance No. ~~1-2~~ provide for ~~10-614.0~~ acres of dedicated, neighborhood parkland, which include a 5.5-acre community park in Planning Area 38 and ~~5-18.5~~-acres of pocket parks in Planning Areas 32, 35, and 37. In addition, recreation facilities at the school in Planning Area 39 will be available to residents, and passive recreation opportunities are provided in Planning Area 36 (which includes a regional trail).

Riverside County's policy implementing the State Quimby Ordinance (regulating parkland requirements) requires that 5.0 acres of parkland be provided for each 1,000 residents. Using the County's standard of ~~3-012.59~~ persons per single-family dwelling unit (attached garage) and 2,025 dwelling units, *MENIFEE NORTH* is estimated to generate a population of ~~6,0955,245~~. As depicted below in Table IIIA - Quimby Act Requirements, the project would require ~~30-526.2~~ acres of parkland. However, the Due to the annexation of the area west of Briggs Road by the City of Menifee that removed 23.9 acres of the Specific Plan's 29.4 acres of recreational facilities, Substantial Conformance No. 1-2 provides increases the park acreage within the County portion of the SP from 10.6 to 10-614.0 acres of parkland.

**Table IIIA
QUIMBY ACT REQUIREMENTS**

LAND USE	SPECIFIC PLAN RESIDENTIAL DENSITY	ORDINANCE 460 HOUSEHOLD GENERATION RATE	NO. OF DWELLING UNITS	TOTAL RESIDENTS	QUIMBY ACREAGE REQUIRED
Single Family Detached	Medium and Medium-High	3.012.59	2,025	6,0955.245	30.526.2
TOTAL				6,0955.245	30.526.2

b. Open Space and Recreation Plan Development Standards

- 1) A community park will be provided for the benefit of all residents within the community.
- 2) At a minimum, the community park will contain the following facilities/equipment: Picnic area(s), tot lot(s), two sport fields or courts, restrooms and parking areas.
- 3) The community park (Planning Area 38), natural open space (Planning Area 36), the regional trail and landscaped parkways will be publicly owned and maintained. Possible maintenance mechanisms include CSA 146, Valley-Wide Recreation and Park District and the formation of a new Community Service District. The maintenance mechanism shall be selected at the time that the first implementing development application is submitted.
- 4) The mini-parks may either be public or private. If they are public, they shall be owned and maintained as described in item 3) above. If they are private, a neighborhood homeowners' association shall be formed for ownership and maintenance.
- 5) All recreational facilities will be landscaped, and where necessary, irrigated in a manner that is conducive to the type of plant material and landscape setting.
- 6) All recreational facilities will provide parking in accordance with Riverside County standards.
- 7) Landscaping within recreation and open space areas will be further governed by the Development Standards contained with this Specific Plan (Section III.B.7.) and the Design Guidelines contained within this Specific Plan (Section IV).

9. PUBLIC FACILITY SITES PHASING PLAN

a. Public Facility Phasing Description

In order to ensure timely development of public facilities, a phasing plan has been prepared for the community parks (Planning Area 38), the elementary schools (Planning Area 39), and the mini-parks within Planning Areas 32, 35, and 37. The public facility sites phasing plan is depicted in Figure III-12.

Public facility construction shall be phased as provided by the Public Facilities Phasing Table (Table IV), below:

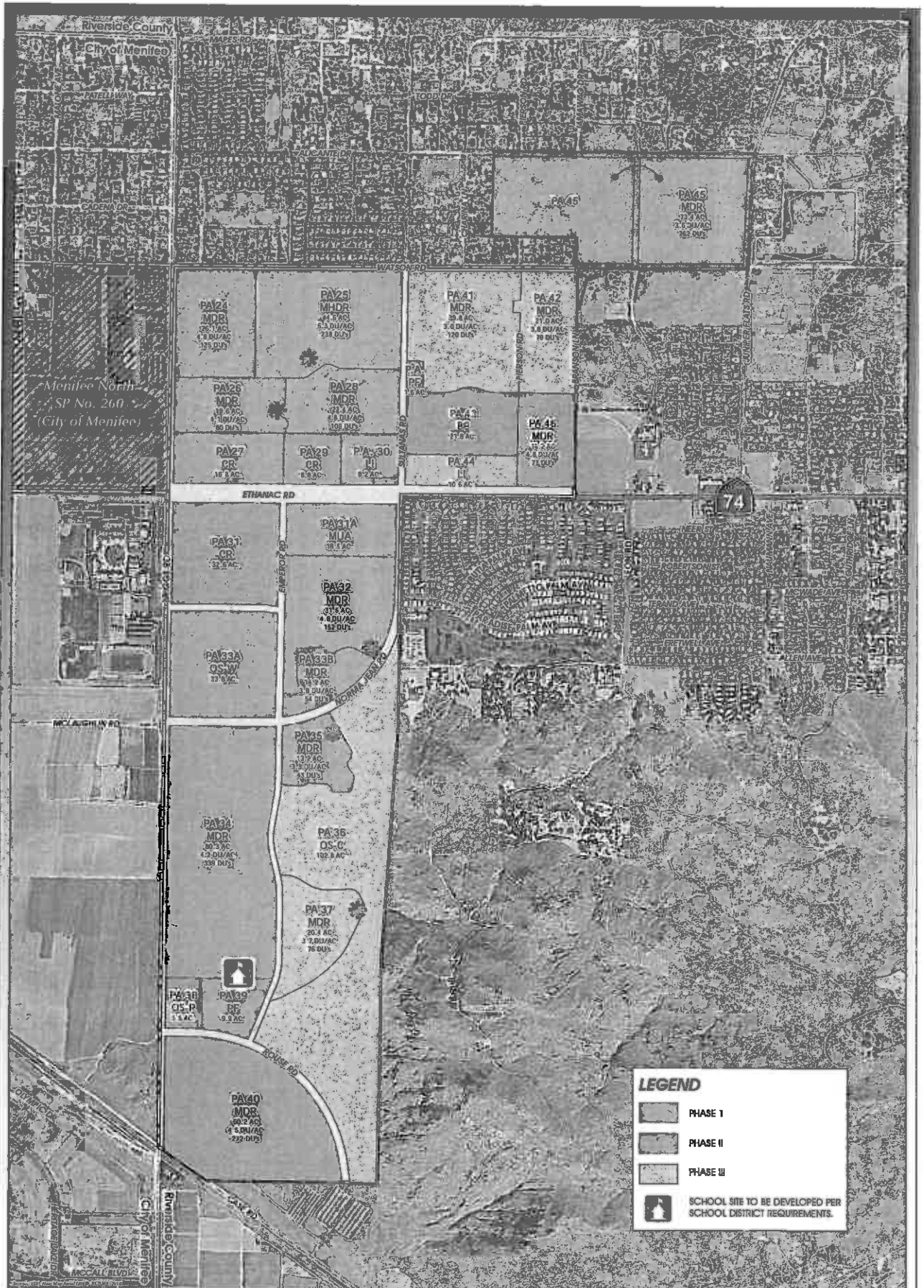
**Table IV
PUBLIC FACILITIES PHASING**

PLANNING AREA	PUBLIC FACILITY	ACREAGE	MILESTONES AND REQUIREMENTS
<u>25 (portion)</u>	<u>Mini-Park</u>	<u>3.2</u>	<u>To be completed during Phase I, prior to issuance of the 180th building permit in Planning Area 25.</u>
<u>26 (portion)</u>	<u>Mini-Park</u>	<u>0.2</u>	<u>To be completed during Phase I, prior to issuance of the 60th building permit in Planning Area 26.</u>
32 (portion)	Mini-Park	4.0 *	To be completed during Phase I, prior to issuance of the 25th building permit in Planning Area 32.
35 (portion)	Mini-Park	0.4	To be completed during Phase I, prior to issuance of the 21st building permit in Planning Area 35.
38	Community Park	5.5	To be completed during Phase I in conjunction with the development of Planning Area 34.
37 (portion)	Mini-Park	0.2	To be completed during Phase III, prior to issuance of the 23rd building permit in Planning Area 37.
39	Elementary School	9.9	To be constructed by the School District in accordance with their pupil demand and funding capabilities.
	Fire Station	1.6	Existing.

** The acreage of the mini-park in Planning Area 32 is an approximation.*

b. Public Facility Phasing Standards

- 1) Improvement plans for mini-park sites shall accompany the first development application for the Planning Area in which the park is contained.
- 2) Improvement plans for the 5.5-acre community park (Planning Area 38) shall accompany the first development application for Planning Area 34 or 40, whichever occurs first.



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FIGURE III-13

**TABLE V
PHASING PLAN**

LAND USE	PLANNING AREA	ACRES	MAXIMUM DWELLING UNITS
PHASE I			
Medium Density Residential	24	25,726.1	115,125
Medium-High Density Residential	25	44,744.6	226,238
Medium-High Density Residential	26	18,219.6	9,780
Commercial Retail	27	16.8	—
Medium Density Residential	28	22.4	108
Light Industrial	30	8.2	—
Mixed Use/Planning Area	31A	18.5	—
Medium Density Residential	32	31.5	152
Medium Density Residential	33B	14.2	54
Medium Density Residential	34	80.3	339
Medium Density Residential	35	13.2	43
Open Space - Recreation	38	5.5	—
Medium Density Residential	45	73.4	262
SUBTOTAL - PHASE I		350,237.6	1,288,139.6
PHASE II			
Commercial Retail	29	8.8	—
Commercial Retail	31	32.6	—
Medium Density Residential	40	60.2	272
Business Park	43	21.8	—
SUBTOTAL - PHASE II		123.4	272
PHASE III			
Medium-High Density Residential	28	21.8	113
Medium Density Residential	37	20.4	76
Medium Density Residential	41	39.8	120
Medium Density Residential	42	21.0	79
Light Industrial	44	10.6	—
Medium Density Residential	46	16.2	77
SUBTOTAL - PHASE III		129,8108.0	465,352
OTHER LAND USES			
Public Facility (School)	39	9.9	—
Major Roads	—	77,174.8	—
Easements/Existing Uses	—	0.6	—
Public Facility (existing Fire Station)	—	1.6	—
Open Space-Conservation	—	102.8	—

III. SPECIFIC PLAN

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 2
SUBSTANTIAL CONFORMANCE NO. 2

24. PLANNING AREA 24: MEDIUM DENSITY RESIDENTIAL

a. Descriptive Summary

Planning Area 24, as depicted in Figure III-14, provides for development of ~~25.7~~26.1 acres of Medium Density Residential uses with minimum lot sizes of 5,000 square feet. A maximum total of ~~115~~125 dwelling units are planned at a target density of ~~4.5~~4.8 du/ac.

b. Land Use and Development Standards

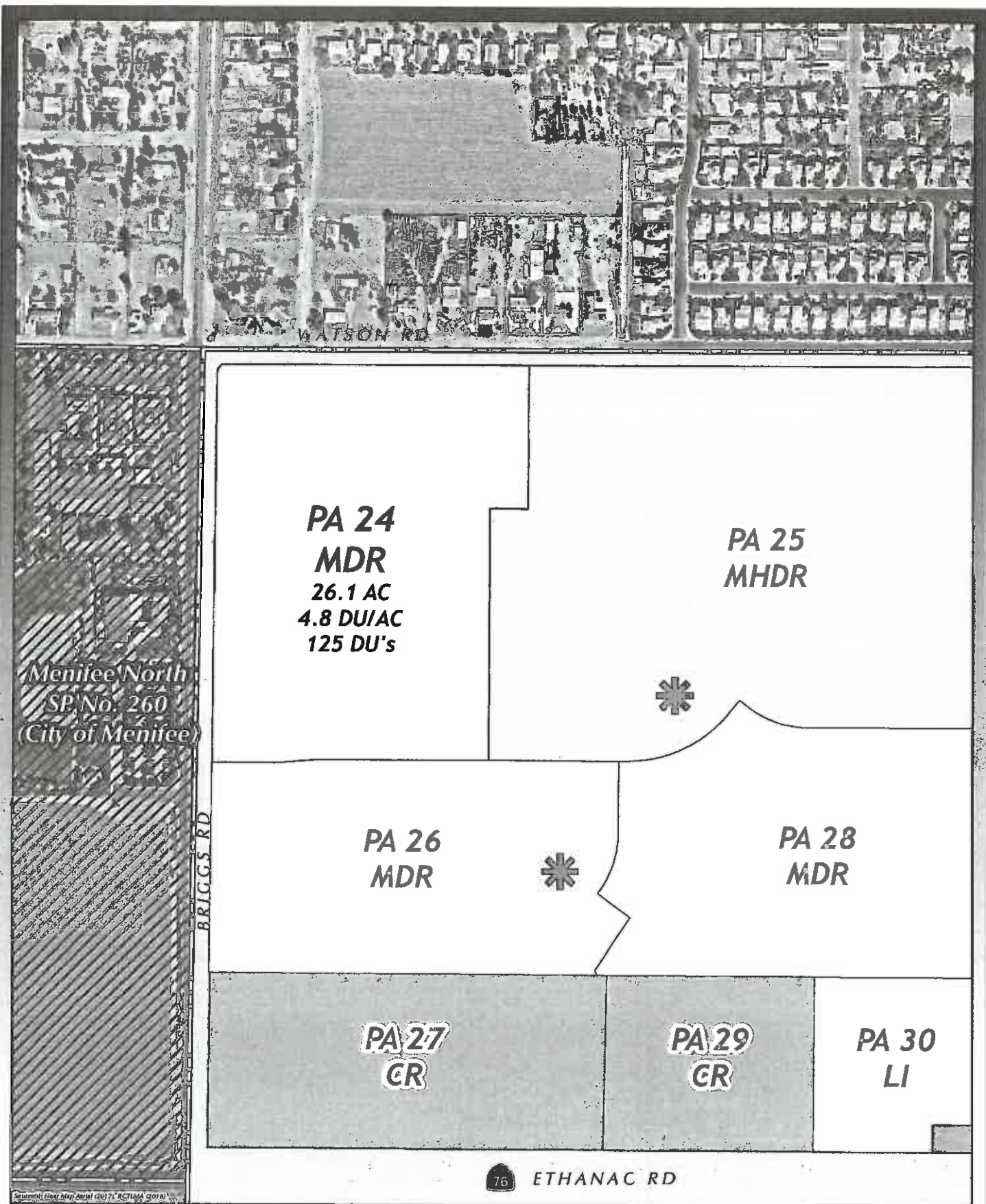
Please refer to Ordinance No. 348. (See Specific Plan Zone Ordinance Tab.)

c. Planning Standards

- 1) Access to Planning Area 24 shall be provided from Briggs Road.
- 2) Roadway landscape treatments, such as those depicted in Figures IV-17 and 18, shall be provided along Briggs Road, and Watson Road.
- 3) A drainage channel edge treatment shall be provided between the residential uses in Planning Area 24 and the adjacent drainage channel proposed along the northern edges of this planning area, as shown on Figure IV-9.
- 4) A special landscape treatment, as shown on Figure IV-8, shall be provided along Watson Road to limit views of this planning area from existing adjacent residential uses.
- ~~5) A project entry/intersection statement, as shown on Figure IV-3, shall be provided at Briggs Road near the southwest corner of Planning Area 24.~~
- 6) A regional trail easement is proposed along Briggs Road, as displayed on Figure IV-14.
- 7) A retaining wall, which results in an increase in the Solid Wall height of up to two (2) feet may be constructed along Planning Area 24's perimeter abutting Briggs Road.
- ~~78)~~ Please refer to Section IV, Design Guidelines, for design-related criteria.
- ~~89)~~ Please refer to Section III.A. for the following Development Plans and Standards that apply site-wide:

III.A.1: Specific Land Use Plan
III.A.2: Circulation Plan
III.A.3: Drainage Plan
III.A.4: Water and Sewer Plans
III.A.5: Open Space and Recreation Plan

III.A.6: Grading Plan
III.A.7: Landscaping Plan
III.A.8: Public Facilities Phasing Plan
III.A.9: Phasing Plan



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FIGURE III-14

25. PLANNING AREA 25: MEDIUM-HIGH DENSITY RESIDENTIAL

a. Descriptive Summary

Planning Area 25, as depicted in Figure III-15, provides for development of 44,744.6 acres of ~~medium~~Medium-high ~~High density~~ Density residential ~~Residential~~ uses with minimum lot sizes of 6,000~~3,500~~ square feet. A maximum total of 226~~238~~ dwelling units are planned at a target density of 5.1~~5.3~~ du/ac.

b. Land Use and Development Standards

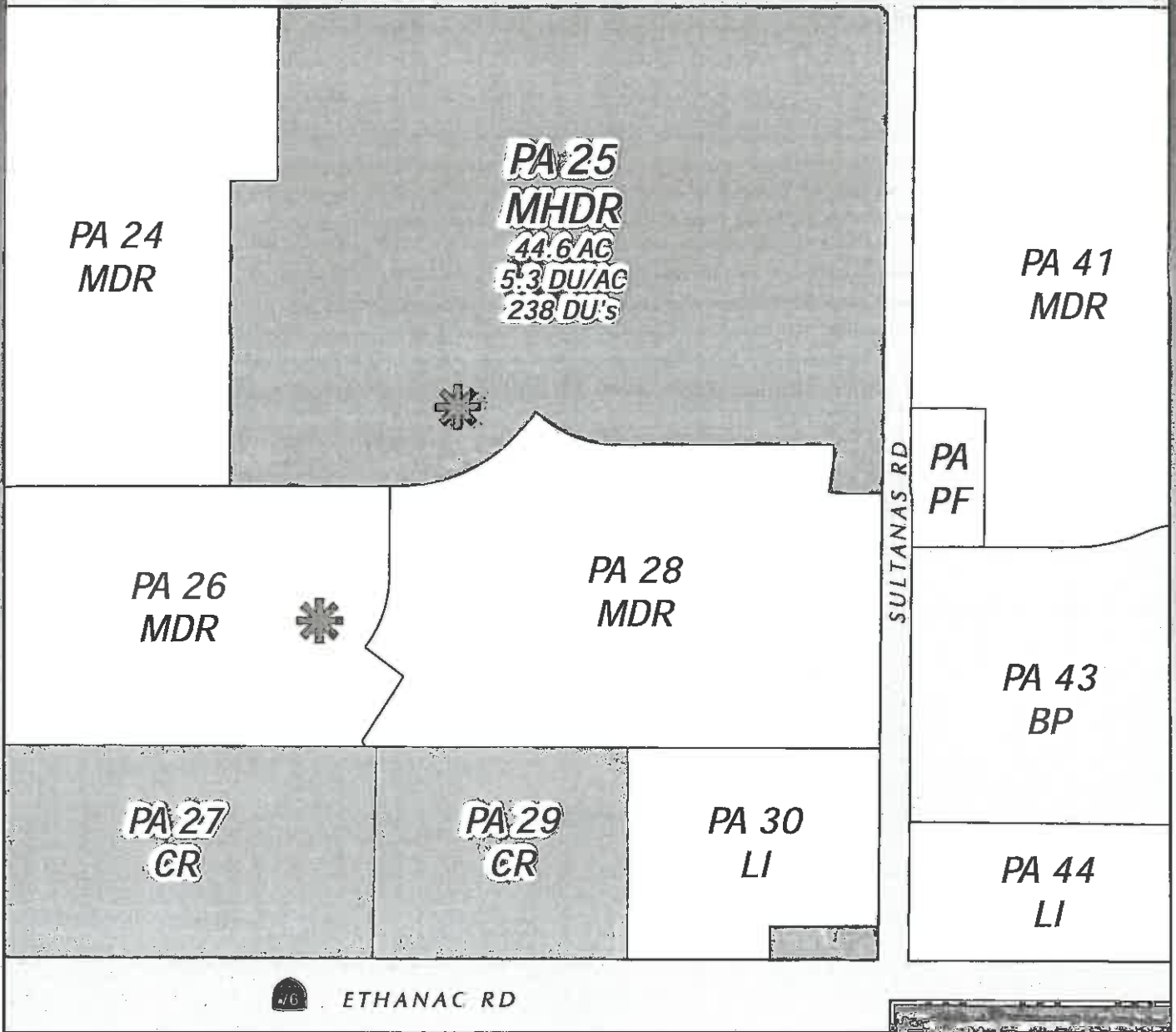
Please refer to Ordinance No. 348. (See Specific Plan Zone Ordinance Tab.)

c. Planning Standards

- 1) Access to Planning Area 25 shall be provided from Sultanas Road, and Watson Road.
- 2) A special landscape treatment, as shown in Figure IV-8, shall be provided along Watson Road to limit views of this planning area from existing adjacent residential uses.
- ~~3) A minor intersection treatment, as shown on Figure IV-4, is planned at the intersection of Sultanas Road and the access road near the southeast corner of Planning Area 25.~~
- 4) A drainage channel edge treatment, as depicted on Figure IV-9, shall be established between the residential uses in Planning Area 25 and the adjacent drainage channel proposed along the northern edge of the planning area.
- 6) A retaining wall, which results in an increase in the Solid Wall height of up to two (2) feet may be constructed along Planning Area 25's perimeter abutting Sultanas Road.
- ~~5) Please refer to Section IV, Design Guidelines, for design-related criteria.~~
- ~~6) Please refer to Section III.A. for the following Development Plans and Standards that apply site-wide:~~

III.A.1: Specific Land Use Plan
III.A.2: Circulation Plan
III.A.3: Drainage Plan
III.A.4: Water and Sewer Plans
III.A.5: Open Space and Recreation Plan

III.A.6: Grading Plan
III.A.7: Landscaping Plan
III.A.8: Public Facilities Phasing Plan
III.A.9: Phasing Plan



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FIGURE III-15



26. PLANNING AREA 26: MEDIUM-HIGH DENSITY RESIDENTIAL

a. Descriptive Summary

Planning Area 26, as depicted in Figure III-16, provides for development of ~~18-219.6~~ acres of ~~Medium-high~~ Density Residential uses with minimum lot sizes of 6,000 square feet. A maximum total of ~~97-80~~ dwelling units are planned at a target density of ~~5-34.1~~ du/ac

b. Land Use and Development Standards

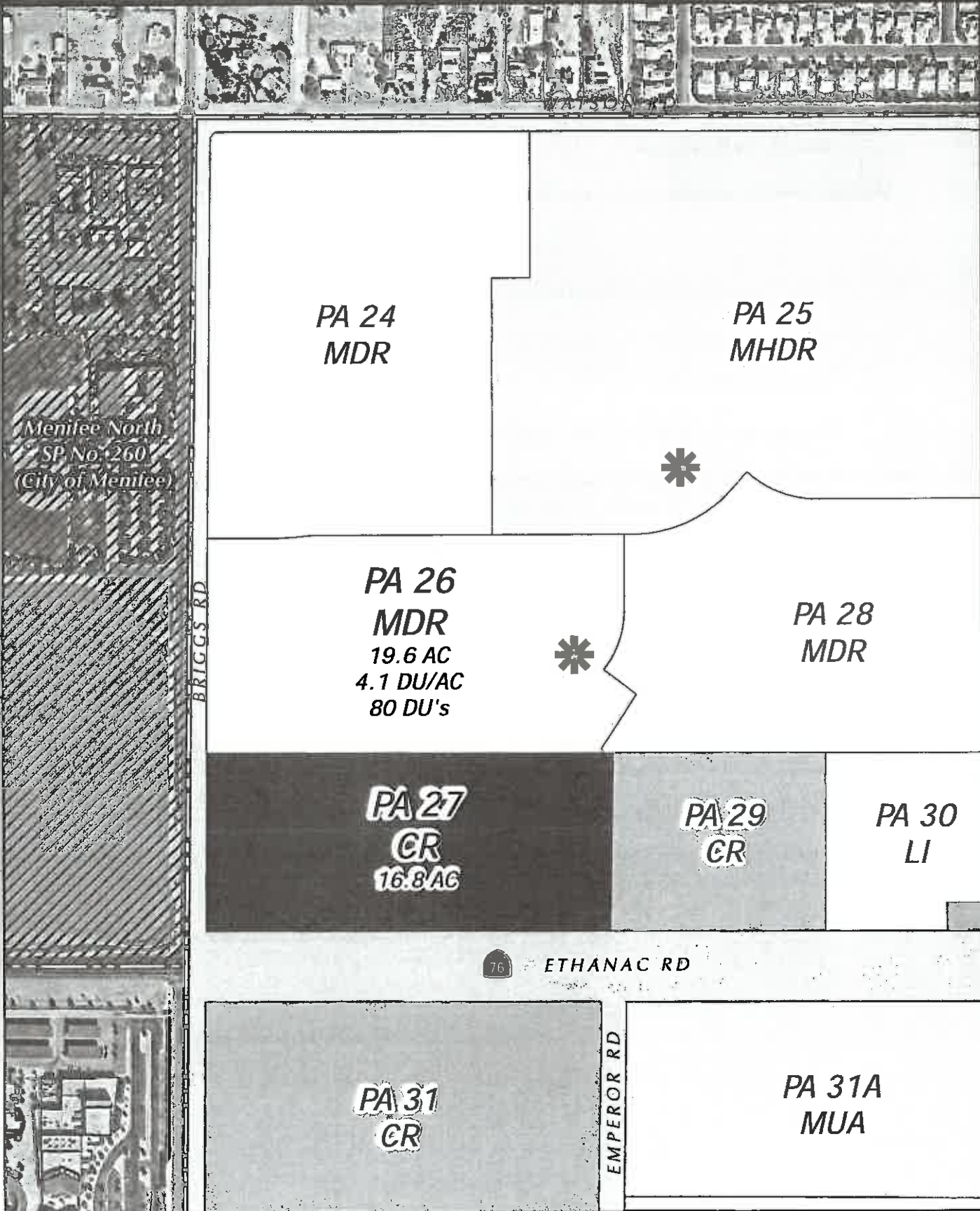
Please refer to Ordinance No. 348. (See Specific Plan Zone Ordinance Tab.)

c. Planning Standards

- 1) Primary access to Planning Area 26 shall be provided from Briggs Road.
- ~~2) A project entry/intersection statement shall be provided at Briggs Road and the northwest corner of Planning Area 26, as shown on Figure IV-3.~~
- 3) A regional trail easement, as shown on Figure IV-14, is proposed along Briggs Road at the planning area's western boundary.
- 4) Roadway landscape treatments, as illustrated in Figures IV-17, shall be provided along Briggs Road.
- 5) A drainage channel edge treatment, as depicted on Figure IV-10, shall be established between the residential uses in Planning Area 26 and the proposed drainage channel along Briggs Road.
- ~~6) A retaining wall, which results in an increase in the Solid Wall height of up to two (2) feet may be constructed along Planning Area 26's perimeter abutting Briggs Road.~~
- ~~67) Please refer to Section IV, Design Guidelines, for design-related criteria.~~
- ~~78) A special landscaped treatment, as depicted on Figure IV-12, shall be established between the residential uses in Planning Area 26 and the adjacent commercial retail uses in Planning Area 27.~~
- ~~89) Please refer to Section III.A. for the following Development Plans and Standards that apply site-wide:~~

III.A.1: Specific Land Use Plan
III.A.2: Circulation Plan
III.A.3: Drainage Plan
III.A.4: Water and Sewer Plans
III.A.5: Open Space and Recreation Plan

III.A.6: Grading Plan
III.A.7: Landscaping Plan
III.A.8: Public Facilities Phasing Plan
III.A.9: Phasing Plan



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FIGURE III-16

28. PLANNING AREA 28: MEDIUM-HIGH DENSITY RESIDENTIAL

a. Descriptive Summary

Planning Area 28, as depicted on Figure III-17, provides for the development of 21.8 acres of ~~medium~~Medium-high Density Residential uses with minimum lot sizes of 6,000 square feet. A maximum total of ~~113-108~~ dwelling units are planned at a target density of ~~5-24.8~~ du/ac.

b. Land Use and Development Standards

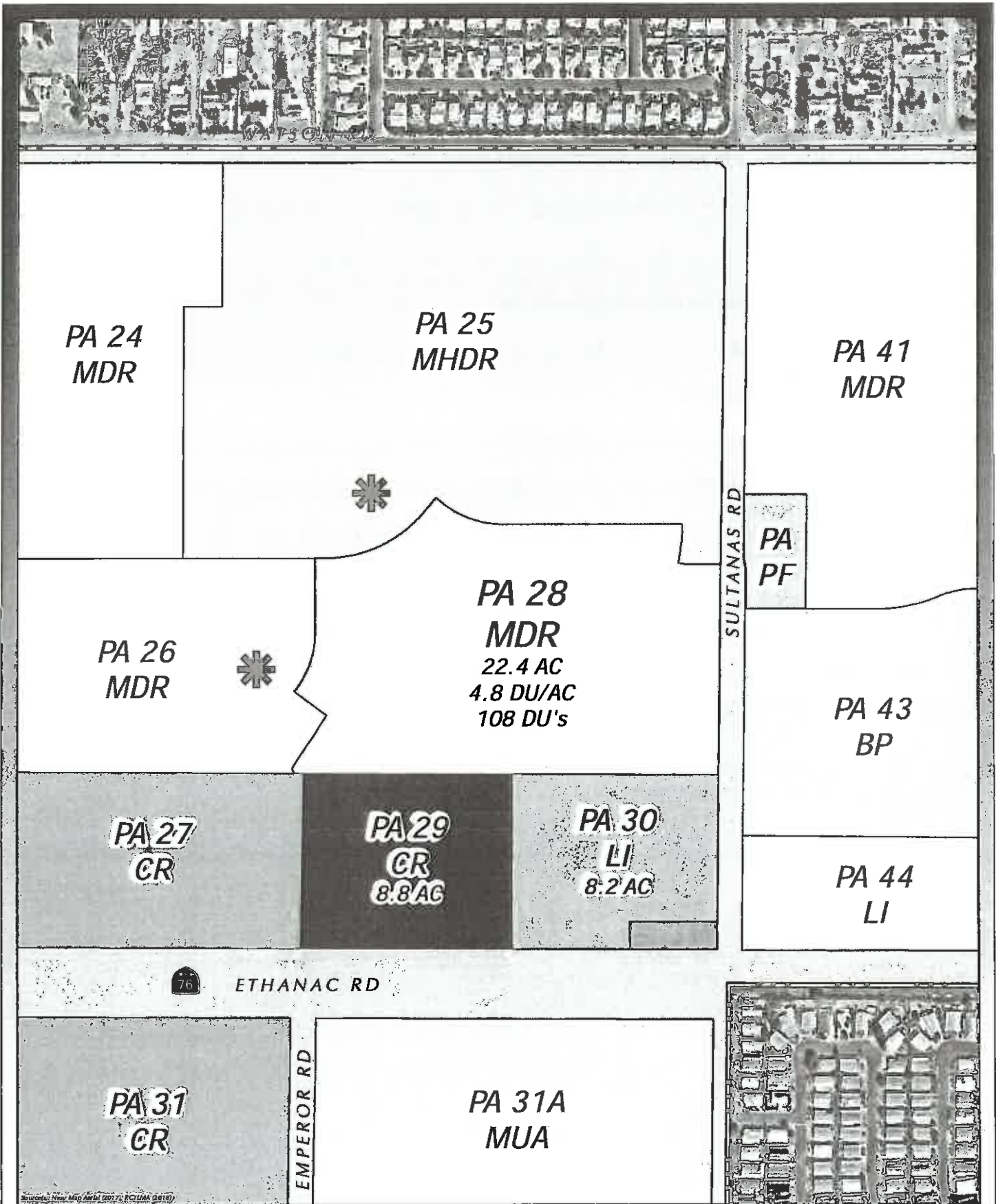
Please refer to Ordinance No. 348. (See Specific Plan Zone Ordinance Tab.)

c. Planning Standards

- 1) Primary access to Planning Area 28 shall be provided from Sultanas Road.
- 2) Please refer to Section IV, Design Guidelines, for design-related criteria.
- 3) Please refer to Section III.A. for the following Development Plans and Standards that apply site-wide:
- 4) A special landscaped treatment, as depicted on Figure IV-12, shall be established between the residential uses in Planning Area 28 and the adjacent non-residential uses in Planning Areas 29 and 30.
- 5) A retaining wall, which results in an increase in the Solid Wall height of up to two (2) feet may be constructed along Planning Area 28's perimeter abutting Sultanas Road.
- ~~5) A minor intersection statement, as shown on Figure IV-4, is planned at the intersection of Sultanas Road and the access road.~~
- 6) Please refer to Section III.A. for the following Development Plans and Standards that apply site-wide:

III.A.1: Specific Land Use Plan
III.A.2: Circulation Plan
III.A.3: Drainage Plan
III.A.4: Water and Sewer Plans
III.A.5: Open Space and Recreation Plan

III.A.6: Grading Plan
III.A.7: Landscaping Plan
III.A.8: Public Facilities Phasing Plan
III.A.9: Phasing Plan



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FIGURE III-17



c. **Mini-Parks (Figure IV-20)**

The land use plan proposes the development of three mini-parks located in Planning Areas 25, 26, 32, 35, and 37. These parks vary in size from 0.2 to approximately 4 acres and are intended serve as gathering places for the residential enclaves in which they are located. Generally these parks might be improved to include a picnic area with tables, benches and barbecues, and a turfed area or sand surfaced tot lot or playground. The park would be planted with a combination of Eucalyptus, Pine or Sycamore tree species to provide shade and to define the individual active or passive use areas. A meandering paved, pedestrian pathway or walkway system could be provided for the park and, where appropriate, would be linked to pedestrian walkway systems to adjacent residential neighborhoods.

5. **COMMUNITY WALLS AND FENCING**

Community walls and fences that can be viewed from the public streets form an integral park of the community design. They can unify the various land uses and reinforce the design themes established at the project entries. A set of standards has been developed for community walls and fences as illustrated in Figure IV-22, *Community Walls and Fence Details*. The location of each wall or fence is conceptually illustrated in Figure IV-21, however, the location will vary depending upon final detail design and grading considerations. In general, solid walls will be located adjacent to residential uses along major highways and secondary roads, where the lot is at approximately the same grade as the street, and as screen elements between incompatible land uses. Concrete, decorative block, or stucco with brick or molded caps are all the accepted materials for these walls. In cases where side and rear yards have desirable views, in which security is a prime consideration but a solid wall is not necessary to inhibit fire or wildlife, high quality wrought iron or tubular steel-type fencing should be employed. Solid walls may be increased by up to two (2) feet with a retaining wall, where required.

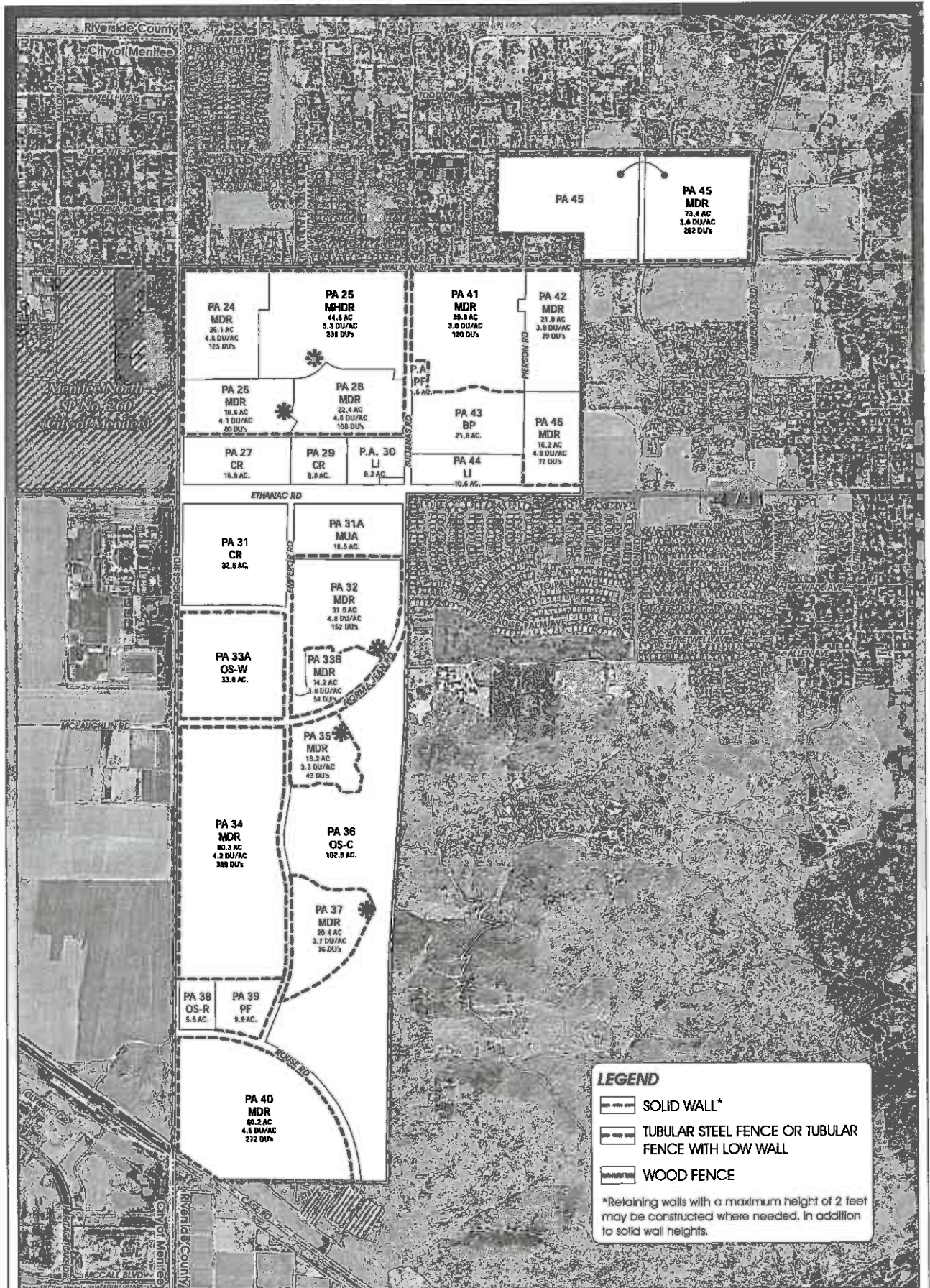
Finally, a solid wood or vinyl fence, as depicted in Figure IV-22, is acceptable for use between residential lots.

6. **SIGNAGE**

The *MENIFEE NORTH* Specific Plan area is intended as a high-quality, mixed-use community. Therefore, only a limited amount of signage shall be permitted in the community.

a. **General Intent**

- Signage shall be attractive and subdued in keeping with the character of a contemporary urban environment.
- Signs shall be limited to community identification, direction and building or company identification only. Signs should not be used for product advertising.
- All business signs shall be compatible with the architecture of the building they identify.
- Retail commercial signage shall be limited to below the roofline and be consistent in color and theme. Monument signs are encouraged and should be of a size only necessary for identification.



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FIGURE IV-21

PAGE BREAK





AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY

August 8, 2019

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VICE CHAIR
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4080 Lemon St., 14th Floor
Riverside, CA 92501
(951) 955-6132

www.rcaluc.org

Mr. John Hildebrand, Administrative Services Manager
County of Riverside Planning Department
4080 Lemon Street, 12th Floor
Riverside CA 92501

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW –
DIRECTOR’S DETERMINATION**

File No.: ZAP1041RG19
Related File No.: CZ1900008 (Ordinance Amendment)
APN: Countywide (unincorporated areas)

Dear Mr. Hildebrand:

As authorized by the Riverside County Airport Land Use Commission (ALUC) pursuant to its Resolution No. 2011-02, as ALUC Director, I have reviewed Change of Zone Case No. 1900008 (County of Riverside Ordinance Amendment No. 348.4911), a proposal to amend County Ordinance No. 348 by amending the zones within which kennels and catteries are permitted, reorganizing Section 18.45 addressing Kennels and Catteries, and updating definitions of catteries (Section 21.20) and kennels (Section 21.40a). Additionally, reference to allowances for kennels and catteries would be deleted from the individual zoning classification lists of permitted land uses, with the zoning classifications specified in Section 18.45.

Presently, the R-R and R-R-O zones allow for Class II (11 to 25 dogs) and Class III (26 to 40 dogs) kennels, subject to plot plan approval, and Class IV kennels (41 or more dogs) and Sentry Dog kennels, subject to conditional use permit approval. Additionally, Class II kennels are permitted in the R-A, W-2, R-T-R, R-D, and W-2-M zones, subject to plot plan approval. These provisions would be deleted, such that the maximum size kennel allowed in residential zones (other than the agricultural zones A-1, A-2, A-P, and A-D) would be Class I kennels (5 to 10 dogs). The amendment also would add the I-P, MU, C-1/C-P, C-P-S, and C-R zones to the list of zones that would permit Class I, Class II, and Class III kennels, subject to plot plan approval, and Class IV kennels, subject to conditional use permit approval. Sentry Dog kennels would also be permitted in the I-P zone, subject to conditional use permit approval. Finally, the I-P, MU, C-1/C-P, C-P-S, and C-R zones would be added to the list of zones permitting Class I (10 to 25 cats) and Class II (26 or more cats) catteries, subject to plot plan approval. The definitions of kennels and catteries would be amended to be consistent with the provisions of County Ordinance No. 630.

There are no development standard changes or changes to zoning land uses that would increase residential density or non-residential intensity within the proposed amendment. Therefore, this amendment has no possibility for having an impact on the safety of air navigation within airport influence areas located within the unincorporated areas of Riverside County.

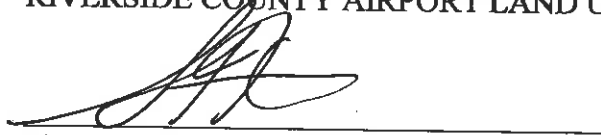
AIRPORT LAND USE COMMISSION

As ALUC Director, I hereby find the above-referenced project CONSISTENT with all Riverside County Airport Land Use Compatibility Plans.

This determination of consistency relates to airport compatibility issues and does not necessarily constitute an endorsement of the proposed amendment.

If you have any questions, please contact Paul Rull, ALUC Principal Planner, at (951) 955-6893.

Sincerely,
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Simon A. Housman, ALUC Director

Attachment: Proposed Ordinance Amendment Text

Y:\AIRPORT CASE FILES\Regional\ZAP1041RG19\ZAP1041RG19.LTR.doc

1 Section 7. Subsection a.(11) of Section 8.1. of Ordinance No. 348 is amended to read as
2 follows:

3 “(11) (Deleted)”

4 Section 8. Subsection e. of Section 8.25. of Ordinance No. 348 is amended to read as
5 follows:

6 “e. (Deleted)”

7 Section 9. Subsection d. of Section 8.60. of Ordinance No. 348 is amended to read as
8 follows:

9 “d. (Deleted)”

10 Section 10. Subsection e. of Section 11.2. of Ordinance No. 348 is amended to read as
11 follows:

12 “e. (Deleted)”

13 Section 11. Subsection e. of Section 11.26. of Ordinance No. 348 is amended to read as
14 follows:

15 “e. (Deleted)”

16 Section 12. Subsection d. of Section 12.2. of Ordinance No. 348 is amended to read as
17 follows:

18 “d. (Deleted)”

19 Section 13. Subsection a.(14) of Section 13.1. of Ordinance No. 348 is amended to read
20 as follows:

21 “(14) (Deleted)”

22 Section 14. Subsection i. of Section 13.51. of Ordinance No. 348 is amended to read as
23 follows:

24 “i. (Deleted)”

25 Section 15. Subsection a.(14) of Section 14.1. of Ordinance No. 348 is amended to read
26 as follows:

27 “(14) (Deleted)”

28

1 Section 16. Subsection a.(21) of Section 14.1. of Ordinance No. 348 is amended to read
2 as follows:

3 “(21) (Deleted)”

4 Section 17. Subsection e. of Section 14.1. of Ordinance No. 348 is amended to read as
5 follows:

6 “e. (Deleted)”

7 Section 18. Subsection d. of Section 14.52. of Ordinance No. 348 is amended to read as
8 follows:

9 “d. (Deleted)”

10 Section 19. Subsection g. of Section 15.1. of Ordinance No. 348 is amended to read as
11 follows:

12 “g. (Deleted)”

13 Section 20. Subsection a.(14) of Section 15.101. of Ordinance No. 348 is amended to
14 read as follows:

15 “(14) (Deleted)”

16 Section 21. Subsection e. of Section 15.200. of Ordinance No. 348 is amended to read as
17 follows:

18 “e. (Deleted)”

19 Section 22. Subsection b. of Section 18.45. of Ordinance No. 348 is amended to read as
20 follows:

21 “b. PERMITTED ZONING. Kennels and catteries as defined within this
22 ordinance shall be permitted in the following zones:

- 23 (1) A Class I Kennel (5 to 10 dogs) is permitted in the following zones,
24 provided a plot plan has been approved under the provisions of this
25 ordinance: A-1, A-2, A-D, A-P, C-1/C-P, C-R, C-P-S, C/V, I-P, M-
26 H, M-M, M-SC, MU, N-A, R-1, R-1A, R-2, R-2A, R-3, R-3A, R-A,
27 R-D, R-R, R-R-O, R-T-R, W-2, and W-2-M.

28

- (2) A Class II Kennel (11 to 25 dogs) is permitted in the following zones, provided a plot plan has been approved under the provisions of this ordinance: A-1, A-P, A-2, A-D, C-1/C-P, C-P-S, C-R, I-P, M-H, M-M, M-SC, MU, and N-A.
- (3) A Class III Kennel (26 to 40 dogs) is permitted in the following zones, provided a plot plan has been approved under the provisions of this ordinance: A-1, A-2, C-1/C-P, C-P-S, C-R, I-P, M-H, M-M, M-SC, and MU.
- (4) A Class IV Kennel (41 or more dogs) is permitted in the following zones, provided a conditional use permit has been approved under the provisions of this ordinance: A-1, A-2, C-1/C-P, C-P-S, C-R, I-P, M-H, M-M, M-SC, and MU.
- (5) A Sentry Dog Kennel is permitted in the following zones, provided a conditional use permit has been approved under the provisions of this ordinance: A-1, A-2, I-P, M-H, M-M, and M-SC.
- (6) A Class I Cattery (10 to 25 cats) is permitted in the following zones, provided a plot plan has been approved under the provisions of this ordinance: A-1, A-2, A-D, A-P, C-1/C-P, C-P-S, C-R, C/V, I-P, M-H, M-M, M-SC, MU, N-A, R-A, R-D, R-R, R-R-O, R-T-R, W-2, and W-2-M.
- (7) A Class II Cattery (26 or more cats) is permitted in the following zones, provided a plot plan has been approved under the provisions of this ordinance: A-1, A-2, C-1/C-P, C-P-S, C-R, I-P, M-H, M-M, M-SC, MU, R-R, R-R-O.”

Section 23. Subsection c. of Section 18.45. of Ordinance No. 348 is amended to read as follows:

1 "c. DEVELOPMENT STANDARDS. The following development standards
2 shall apply to kennels and catteries and are in addition to the development
3 standards for the applicable zoning classification. In the case of a conflict
4 between these development standards and the development standards of the
5 applicable zone, the more restrictive development standard shall apply.

6 1. LOCATION:

7 (a) Kennels or catteries shall not be placed on any lot with a
8 multiple family dwelling.

9 (b) All kennels and catteries located in the C-1/C-P, C-P-S, C-R,
10 or MU zones shall be located within a fully enclosed
11 building.

12 2. LOT SIZE:

13 Subject to permitted zoning, a kennel or Class II Cattery shall have a
14 minimum lot size of one gross acre in the following zones: A-1, A-
15 2, A-D, A-P, C/V, N-A, R-1, R-1A, R-2, R-2A, R-3, R-3A, R-A, R-
16 D, R-R, R-R-O, R-T-R, W-2, and W-2-M. There is no minimum lot
17 size for a kennel or cattery in the C-1/C-P, C-P-S, C-R, I-P, M-H,
18 M-M, M-SC, or MU zones other than what is required by the
19 existing zoning classification for the lot.

20 3. LICENSE:

21 The applicant shall obtain and continuously maintain all necessary
22 licenses from the Riverside County Department of Animal Services.

23 4. ORDINANCE NO. 630:

24 All kennels and catteries are subject to the provisions of Ordinance
25 No. 630.

1 5. CARETAKER:

2 All kennels and catteries shall have an onsite caretaker.”

3 Section 24. Subsection e. of Section 18.45. of Ordinance No. 348 is amended to read as
4 follows:

5 “e. PROCESSING OF APPLICATION. Upon acceptance of an application as
6 complete, the Planning Director shall transmit a copy of the application to
7 the Department of Animal Services and such additional public and private
8 agencies as the Planning Director deems appropriate.”

9 Section 25. Subsection g. of Section 18.45. of Ordinance No. 348 is amended to read as
10 follows:

11 “g. APPEAL. The applicant or any interested person may appeal from the
12 decision of the Planning Director pursuant to the appeal procedures
13 provided in this ordinance.”

14 Section 26. Section 21.20. of Ordinance No. 348 is amended to read as follows:

15 “SECTION 21.20. CATTERY. Any building, structure, enclosure or premises
16 whereupon, or within which ten or more cats, four months of age or older, are kept
17 or maintained.

18 a. CLASS I CATTERY. Any building, structure, enclosure or
19 premises whereupon, or within which, ten to twenty-five cats, four
20 months of age or older, are kept or maintained.

21 b. CLASS II CATTERY. Any building, structure, enclosure or
22 premises whereupon, or within which, twenty-six or more cats, four
23 months of age or older, are kept or maintained.”

24 Section 27. Section 21.40a. of Ordinance No. 348 is amended to read as follows:

25 “SECTION 21.40a. KENNEL. Any building, structure, enclosure or premises
26 whereupon, or within which, five or more dogs, four months of age or older, are
27 kept or maintained.
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- a. CLASS I KENNEL. Any building, structure, enclosure, or premises whereupon, or within which, five to ten dogs, four months of age or older, are kept or maintained. A Class I Kennel shall not include an animal rescue operation that meets the definition and requirements set forth in Ordinance No. 630.
- b. CLASS II KENNEL. Any building, structure, enclosure, or premises, whereupon, or within which, eleven to twenty-five dogs, four months of age or older, are kept or maintained.
- c. CLASS III KENNEL. Any building, structure, enclosure, or premises whereupon, or within which, twenty-six to forty dogs, four months of age or older, are kept or maintained.
- d. CLASS IV KENNEL. Any building, structure, enclosure, or premises whereupon, or within which, forty-one or more dogs, four months of age or older, are kept or maintained.
- e. SENTRY DOG. Any dog trained to work without supervision in a fenced facility and to deter or detain unauthorized persons found within the facility. The term "guard dog" shall also mean "sentry dog."
- f. SENTRY DOG KENNEL. Any building structure, enclosure, or premises whereupon, or within which, five or more guard or sentry dogs are kept or maintained."

1 Section 28. This ordinance shall take effect thirty (30) days after its adoption.

2
3 BOARD OF SUPERVISORS OF THE COUNTY
4 OF RIVERSIDE, STATE OF CALIFORNIA

5
6 By: _____

7 Kevin Jeffries, Chairman

8 ATTEST:
9 CLERK OF THE BOARD
10 Kecia Harper

11 By: _____
12 Deputy

13
14
15 (SEAL)

16
17
18 APPROVED AS TO FORM
19 August __, 2019

20
21 By: _____
22 LEILA MOSHREF-DANESH
23 Deputy County Counsel

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AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY

August 15, 2019

CHAIR

Steve Manos
Lake Elsinore

Mr. John Hildebrand, Administrative Services Manager
County of Riverside Planning Department
4080 Lemon Street, 12th Floor
Riverside CA 92501

VICE CHAIR

Russell Betts
Desert Hot Springs

COMMISSIONERS

Arthur Butler
Riverside

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW –
DIRECTOR'S DETERMINATION**

John Lyon
Riverside

File No.: ZAP1042RG19
Related File No.: CZ1900010 (Ordinance Amendment)
APN: Countywide (unincorporated areas)

Steven Stewart
Palm Springs

Richard Stewart
Moreno Valley

Dear Mr. Hildebrand:

Gary Youmans
Temecula

As authorized by the Riverside County Airport Land Use Commission (ALUC) pursuant to its Resolution No. 2011-02, as ALUC Director, I have reviewed County of Riverside Case No. CZ1900010 (Ordinance Amendment), a proposal to amend County Ordinance No. 348 by adding Section 18.54 relating to Mobile Food Trucks. The purpose of this amendment is to establish regulations related to Mobile Food Truck operations on private property in the unincorporated areas of the County. The new section includes permitting requirements, a list of zones in which they would be prohibited, and specified location, operational, and approval requirements for Mobile Food Truck operations. Mobile Food Truck operations would not be permitted in Compatibility Zone A of any airport. Mobile Food Trucks operating in conjunction with and on the same site as events consistent with a legally permitted land use or temporary event are exempted from these provisions, as are those operating (1) on the property of a legally established school, college, university, hospital, or place of religious worship, (2) in the public right-of-way pursuant to Ordinance No. 853, or (3) in conjunction with an active construction site.

STAFF

Director

Simon A. Houman

John Guerin
Paul Rull
Barbara Santos

County Administrative Center
4080 Lemon St., 14th Floor
Riverside, CA 92501
(951) 955-5132

www.rcaluc.org

There are no development standard changes or changes to zoning land uses that would increase residential density or non-residential intensity within the proposed amendment. Therefore, this amendment has no possibility for having an impact on the safety of air navigation within airport influence areas located within the unincorporated areas of Riverside County.

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with all Riverside County Airport Land Use Compatibility Plans.

This determination of consistency relates to airport compatibility issues and does not necessarily constitute an endorsement of the proposed amendment.

If you have any questions, please contact Paul Rull, ALUC Principal Planner, at (951) 955-6893.

AIRPORT LAND USE COMMISSION

Sincerely,
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Simon A. Housman, ALUC Director

Attachment: Proposed Ordinance Amendment Text

Y:\AIRPORT CASE FILES\Regional\ZAP1042RG19\ZAP1042RG19.LTR.doc

1 operate without a Mobile Food Truck permit approved in accordance with this Article and
2 Section 18.30 of this ordinance. All procedural provisions of Section 18.30 shall apply to
3 the permit application, except Subsection C relating to approval requirements, Subsection
4 D relating to action on applications, and Subsections E and F relating to appeals and
5 Subsection G relating to use of the permit after the application is approved.

6 A. CLASSIFICATION. Action on a Mobile Food Truck permit shall be ministerial
7 and not subject to the California Environmental Quality Act.

8 B. EXPIRATION. An approved permit for a Mobile Food Truck shall expire one
9 year from the date of approval, unless a renewal application and applicable filing
10 fee is timely submitted and approved in accordance with Section 19.1006 of this
11 Article.

12 C. RENEWAL. An approved permit for a Mobile Food Truck shall be renewed on an
13 annual basis based on the anniversary date of the original approved permit. An
14 application for renewal shall be submitted to the Planning Director or designee no
15 later than thirty (30) days prior to the expiration of the approved permit on the
16 form provided by the Riverside County Planning Department and accompanied by
17 the applicable filing fee set forth in Ordinance No. 670.

18 **SECTION 19.1003 ZONES.** Subject to the requirements provided in this Article, Mobile
19 Food Trucks with an approved Mobile Food Truck Permit are allowed in all zone
20 classifications, except the following: R-1, R-R-O, R-2, R-2A, R-3, R-T, R-T-R, R-4, R-5,
21 R-6, R-7, C/V, N-A, W-2-M, W-1, WC-W, WC-WE, WC-E, W-R and W-E.

22 **SECTION 19.1004 LOCATION REQUIREMENTS.** A permit for a Mobile Food
23 Truck shall not be approved unless the following are met:

24 A. VACANT LOT. The Mobile Food Truck shall operate on a vacant lot that is
25 either paved or surfaced with gravel or decomposed granite.

26 B. LOT SIZE. The vacant lot is at least one-half acre in size.

27 C. ACCESS. The lot includes adequate ingress and egress in accordance with
28 Ordinance No. 787.

1 D. NUMBER OF MOBILE FOOD TRUCKS. The number of Mobile Food Trucks
2 permitted on a lot shall be as follows:

- 3 1. No more than three (3) Mobile Food Truck are allowed on lots half (1/2) an
4 acre or greater but less than one (1) gross acre.
5 2. No more than four (4) Mobile Food Trucks are allowed on lots one (1)
6 gross acre or greater but less than two (2) gross acres.
7 3. No more than five (5) Mobile Food Trucks are allowed on lots two (2)
8 gross acres or greater but less than six (6) gross acres.
9 4. There is no limit on the number of Mobile Food Trucks on lots greater than
10 six (6) gross acres.

11 **SECTION 19.1005 OPERATIONAL REQUIREMENTS.** In addition to the location
12 requirements set forth in this Article and requirements provided in Ordinance No. 580, a
13 Mobile Food Truck shall comply with the following:

- 14 A. AREA MAINTAINENCE. The lot used by the Mobile Food Truck shall be kept
15 free of any debris or trash.
16 B. NO ONSITE CONSUMPTION AREA. Except for shade covers or awnings that
17 provide shade for customers waiting for food, no tables or chairs shall be placed on
18 the lot.
19 C. AMPLIFIED SOUND. No loudspeaker systems, or other device(s) designed to
20 amplify sound are allowed on the lot.
21 D. SIGNAGE. All signage shall comply with Article XIX Advertising Regulations of
22 this ordinance. Additionally, air flyers and sign twirlers are prohibited on the Lot.
23 E. LIGHTS. All temporary outdoor lighting shall be shielded or downward facing.
24 F. LOCATION SAFETY.
25 1. Mobile Food Trucks shall be located at least 15 feet from any fire hydrant,
26 utility box or vault, public right of way, emergency vehicle route, internal
27 drive aisle, tree canopy or pedestrian pathway.
28 2. Mobile Food Trucks shall not operate within a five-foot radius of another

1 Mobile Food Truck.

2 3. Mobile Food Trucks shall not operate within Compatibility Zone A of any
3 County of Riverside airport, as designated and defined by the Riverside
4 County Airport Land Use Compatibility Plan.

5 4. Mobile Food Trucks shall not otherwise be located within any area of the
6 lot that obstructs pedestrian or vehicle traffic.

7 G. NO ALCOHOL OR SALE OF CONTROLLED SUBSTANCES. The sale or
8 soliciting the sale of any alcoholic beverages or controlled substances from a
9 Mobile Food Truck is strictly prohibited.

10 H. WASTE DISPOSAL.

11 1. No Mobile Food Truck shall cause or dispose of oil, grease, or waste on the
12 Lot.

13 2. Wastewater generated on-site shall not be released on-site or into the storm
14 drainage system.

15 I. HOURS OF OPERATION. Operations shall be limited to Sunday through
16 Saturday from 8:00AM to 12:00AM.

17 J. PARKING. There shall be no parking on the Lot after 12:00AM, and overnight
18 parking or storage of the Mobile Food Truck on the Lot is prohibited.

19 K. DISPLAY OF PERMIT. A Mobile Food Truck shall possess and at all times
20 display in clear view a certified copy of its current Environmental Health Decal
21 and grade card issued by the Department of Environmental Health, as required by
22 Riverside County Ordinance No. 580, and approved Mobile Food Truck permit.

23 L. TRASH RECEPTACLES.

24 1. At least one (1) visible trash receptacle for use by customers shall be placed
25 within twenty (20) feet of the Mobile Food Truck operation.

26 2. Unless the property owner has provided for a permitted dumpster, all trash
27 receptacles shall be removed once the Mobile Food Truck operation has
28 ceased each day.

1 M. RESTROOMS. In accordance with Section 114315 of the California Retail Food
2 Code, Mobile Food Trucks stopped to conduct business for more than one hour,
3 shall operate within 200 feet of an approved, readily available and functioning
4 restroom facility. If the restroom is within a business, the Mobile Food Truck
5 operator must submit with the plot plan application, written permission from the
6 business owner providing the hours the restroom facilities are being made
7 available for use by the food service workers. Executive portable toilet facilities
8 with handwashing stations may satisfy this requirement.

9 N. FEDERAL, STATE, AND LOCAL LAWS AND REGULATIONS. Mobile Food
10 Trucks shall comply with all other applicable federal, state and local laws and
11 regulations.

12 **SECTION 19.1006 APPROVAL REQUIREMENTS.**

13 The Planning Director shall approve a Mobile Food Truck permit if all of the following
14 are met:

- 15 A. The Mobile Food Truck operation complies with the requirements set forth in this
16 Article;
- 17 B. Written consent from the Lot owner allowing the Mobile Food Truck to operate on
18 the Lot has been provided to the Planning Director;
- 19 C. An Environmental Health Permit has been issued for the Mobile Food Truck by
20 the Department of Environmental Health, pursuant to Ordinance No. 580;
- 21 D. Inspection of the Mobile Food Truck has been completed by the County Fire
22 Marshall; and
- 23 E. There is no pending Code Enforcement action on the lot.

24 **SECTION 19.1007 EXEMPTIONS.** This Article does not apply to any of the following:

- 25 A. Mobile Food Trucks operating in the public right-of-way pursuant to Ordinance
26 No. 853.
- 27 B. Mobile Food Trucks operating in conjunction with an active construction site.
- 28 C. Mobile Food Trucks operating on the property of a legally existing school, college,

1 university, hospital, or places of religious worship.

2 D. Mobile Food Trucks operating in conjunction with and on the same site as events
3 consistent with a legally permitted land use or temporary event.

4 **SECTION 19.1008. REVOCATION.** A Mobile Food Truck permit may be revoked in
5 accordance with section 18.31 of this ordinance.

6 **SECTION 19.1009. PUBLIC NUISANCE.** Any violation of this Article is hereby
7 declared to be unlawful and a public nuisance.”

8 Section 2. If any provision, clause, sentence or paragraph of this ordinance or the
9 application thereof to any person or circumstances shall be held invalid, such invalidity shall not affect
10 the other provisions of this ordinance which can be given effect without the invalid provision or
11 application, and to this end, the provisions of this ordinance are hereby declared to be severable.

12 Section 3. This ordinance shall take effect thirty (30) days after its adoption.

13 **BOARD OF SUPERVISORS OF THE COUNTY**
14 **OF RIVERSIDE, STATE OF CALIFORNIA**

15 By: _____
16 Chairman, Board of Supervisors

17 ATTEST:
18 CLERK OF THE BOARD

19
20 By: _____
21 Deputy

22 (SEAL)

23
24 APPROVED AS TO FORM
25 August __, 2019

26
27 By: _____
28 Deputy County Counsel

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**AIRPORT LAND USE COMMISSION
STRATEGIC VISION PLAN**

LAND USE COMPATIBILITY PLAN PROCESSING PRIORITIES

4.2

2019 PRIORITY RECOMMENDED	PRIOR RANK	AIRPORT	YEAR OF LAST PLAN ADOPTION	YEAR OF LAST PLAN AMENDMENT	CASES SINCE 2006	JURISDICTIONS	MAJOR ISSUES/OTHER COMMENTS
A1	A6	RIVERSIDE MUNICIPAL (RAL)	2005		96	CITY OF RIVERSIDE CITY OF JURUPA VALLEY	Staff proposes an amendment to bring nonresidential intensities up-to-date with State Handbook allowances. There is potential for a stand-alone Plan. It should be noted that the new Master Plan adopted in 2009 provides for an eastward runway extension with declared distances, so as not to extend the Runway Protection Zone further into the residential neighborhood to the east.
A2	B4	PALM SPRINGS (PSP)	2005	2006	78	CITY OF PALM SPRINGS CITY OF CATHEDRAL CITY COUNTY OF RIVERSIDE	Staff proposes an amendment to bring nonresidential intensities up-to-date with State Handbook allowances. There is potential for a stand-alone Plan.
A3	A4	MARCH ARB/INLAND PORT (RIV)	2014		378	CITY OF PERRIS CITY OF MORENO VALLEY CITY OF RIVERSIDE MARCH JPA COUNTY OF RIVERSIDE CITY OF MENIFEE	There is a need to amend single-acre intensities to comply with the military understanding of Department of Defense and Air Force instructions. Staff efforts are currently suspended pending a Joint Land Use Study being pursued by the Joint Powers Authority.
A4	A3	BERMUDA DUNES (UDD)	2004	2005	79	COUNTY OF RIVERSIDE CITY OF INDIO CITY OF PALM DESERT CITY OF LA QUINTA CITY OF INDIAN WELLS	Staff proposes an amendment to bring nonresidential intensities up-to-date with State Handbook allowances. There is potential for a stand-alone Plan. However, a parcel-by-parcel site inventory (12 to 18 month process) would likely be required.
A5	C4	CHINO (CNO)	2008		27	CITY OF EASTVALE	Staff proposes an amendment to bring nonresidential intensities up-to-date with State Handbook allowances. Since the Chino AIA was previously evaluated in an Environmental Assessment, it is likely that a site inventory would not be required.
B1	C1	FRENCH VALLEY (F70)	2007	2011	91	COUNTY OF RIVERSIDE CITY OF MURRIETA CITY OF TEMECULA	Staff proposes an amendment to bring nonresidential intensities up-to-date with State Handbook allowances. Since the French Valley AIA was previously evaluated in an Environmental Assessment, it is likely that a site inventory would not be required.
B2	B2	FLABOB (RIR)	2004	2005	33	CITY OF JURUPA VALLEY CITY OF RIVERSIDE	Staff proposes an amendment to bring nonresidential intensities up-to-date with State Handbook allowances. There is potential for a stand-alone Plan. However, a parcel-by-parcel site inventory (12 to 18 month process) would likely be required.
B3	A5	JACQUELINE COCHRAN (TRM)	2005	2006	45	COUNTY OF RIVERSIDE CITY OF COACHELLA	Staff proposes an amendment to incorporate Vista Santa Rosa policies and to bring nonresidential intensities up-to-date with State Handbook. There are currently inconsistencies between the County General Plan, including some Specific Plans, and the ALUCP.
B4	B3	BLYTHE (BLH)	2004	2005	13	COUNTY OF RIVERSIDE CITY OF BLYTHE	Solar and other energy projects have been the dominant concern in this AIA. Also, the Zone D prohibition of intermediate densities is problematic, due to lack of sewer service in Mesa Verde.
B5	B1	ONTARIO INTERNATIONAL (ONT)	2011 (City of Ontario)			CITY OF EASTVALE CITY OF JURUPA VALLEY	The City of Ontario adopted an ALUCP in 2011 that included portions of Riverside County now within the cities of Eastvale and Jurupa Valley; however, Riverside County ALUC has not to date adopted an ALUCP for the portion of Riverside County affected by this airport. Delineation and formal adoption of an AIA should be considered.
C1	C3	CORONA MUNICIPAL (AJO)	2004	2005	10	CITY OF CORONA CITY OF NORCO COUNTY OF RIVERSIDE	An amendment to bring nonresidential intensities up-to-date with State Handbook recommendations is possible, and there is potential for a stand-alone Plan. However, a parcel-by-parcel site inventory (12 to 18 month process) would likely be required. Prioritization reflects the low level of case submittal/activity.
C2	C2	PERRIS VALLEY (L65)	2011		16	CITY OF PERRIS COUNTY OF RIVERSIDE CITY OF MENIFEE	The 2011 Plan was customized to reflect the vision of the City of Perris for the future of the community.
C3	A2	BANNING MUNICIPAL (BNG)	2004	2016	34	CITY OF BANNING COUNTY OF RIVERSIDE	Nonresidential intensity criteria for Zone D were already increased in the 2016 amendment.
C4	A1	HEMET-RYAN (HMT)	2017		60	CITY OF HEMET COUNTY OF RIVERSIDE	This Plan already includes the new procedure for calculation of retail intensities and increased nonresidential intensity allowances pursuant to the State Handbook.
C5	C5	CHIRIACO SUMMIT (L77)	2004	2005	0	COUNTY OF RIVERSIDE	No projects have been submitted to ALUC in the last 12 years. There is a possibility for solar energy in the future.



**AIRPORT LAND USE COMMISSION
MINUTE ORDER AUGUST 8, 2019
RIVERSIDE MEETING**

A regular scheduled meeting of the Airport Land Use Commission was held on August 8, 2019 at the Riverside County Administrative Center, Board Chambers.

COMMISSIONERS PRESENT: Steve Manos, Chair
Russell Betts, Vice Chair
Arthur Butler
John Lyon
Steven Stewart
Richard Stewart

COMMISSIONERS ABSENT: Gary Youmans

STAFF PRESENT: Simon Housman, ALUC Director
John Guerin, Principal Planner
Paul Rull, Principal Planner
Barbara Santos, ALUC Commission Secretary
Raymond Mistica, ALUC Counsel

OTHERS PRESENT: None

**AIRPORT LAND USE COMMISSION
MINUTE ORDER AUGUST 8, 2019
RIVERSIDE MEETING**

I. **AGENDA ITEM 3.1:** ZAP1034BA19 – AT&T Wireless (Representative: Smartlink, LLC) – City of Banning Case Nos. CUP19-8004 (Conditional Use Permit), DR19-7005 (Design Review). A proposal to establish a 70 foot tall “monopine” wireless communications facility with a 960 square foot equipment shelter area on 2.18 acres located northerly of Ramsey Street, easterly of Phillips Street, southerly of Williams Street and westerly of Hathaway Street (Airport Compatibility Zones C & D of the Banning Municipal Airport Influence Area).

II. **MAJOR ISSUES**
None

III. **STAFF RECOMMENDATION**
Staff recommends that the Conditional Use Permit and Design Review be found CONDITIONALLY CONSISTENT, subject to the conditions included herein, and such additional conditions as may be required by the Federal Aviation Administration Obstruction Evaluation Service.

STAFF RECOMMENDED AT HEARING:
CONSISTENT, subject to the updated conditions provided at this meeting which incorporates Federal Aviation Administration conditions.

IV. **PROJECT DESCRIPTION**
The applicant proposes to establish a 70 foot tall monopine wireless communications facility with a 960 square foot equipment shelter on 2.18 acres.

CONDITIONS: (added conditions shown in bold type)

1. Any outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
 - (a) Any use or activity which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use or activity which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use or activity which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, composting operations, production of cereal grains, sunflower, and row crops, trash transfer stations that are open on one or more sides, recycling centers containing

**AIRPORT LAND USE COMMISSION
MINUTE ORDER AUGUST 8, 2019
RIVERSIDE MEETING**

putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)

- (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Children's schools, day care centers, libraries, hospitals, and nursing homes.
3. The attached notice shall be given to all prospective purchasers and/or tenants of the property, and shall be recorded as a deed notice.
 4. **The Federal Aviation Administration has conducted an aeronautical study of the proposed project (Aeronautical Study No. 2019-AWP-7842-OE) and has determined that neither marking nor lighting of the structure(s) is necessary for aviation safety. However, if marking and/or lighting for aviation safety are accomplished on a voluntary basis, such marking and/or lighting (if any) shall be installed in accordance with FAA Advisory Circular 70/7460-1 L Change 2 and shall be maintained in accordance therewith for the life of the project.**
 5. **The proposed structure shall not exceed a height of 70 feet above ground level and a maximum elevation at top point of 2,352 feet above mean sea level.**
 6. **The maximum height and top point elevation specified above shall not be amended without further review by the Airport Land Use Commission and the Federal Aviation Administration; provided, however, that reduction in structure height or elevation shall not require further review by the Airport Land Use Commission.**
 7. **The coordinates, frequencies, and power specified in the Determination of No Hazard to Air Navigation letter dated August 6, 2019 shall not be amended without further review by the Federal Aviation Administration Obstruction Evaluation Service.**
 8. **Temporary construction equipment used during actual construction of the structure(s) shall not exceed 70 feet in height and a maximum elevation of 2,352 feet above mean sea level, unless separate notice is provided to the Federal Aviation Administration through the Form 7460-1 process.**
 9. **Within five (5) days after construction of the proposed structure reaches its greatest height, FAA Form 7460-2 (Part II), Notice of Actual Construction or Alteration, shall be completed by the project proponent or his/her designee and e-filed with the Federal Aviation Administration. (Go to <https://oeaaa.faa.gov> for instructions.) This requirement is also applicable in the event the project is abandoned or a decision is made not to construct the applicable structures(s).**

**AIRPORT LAND USE COMMISSION
MINUTE ORDER AUGUST 8, 2019
RIVERSIDE MEETING**

V. MEETING SUMMARY

The following staff presented the subject proposal:

Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rivco.org

No one spoke in favor, neutral or opposition to the project.

VI. ALUC COMMISSION ACTION

The ALUC by a unanimous vote of 6-0 found the project **CONSISTENT**, subject to the updated conditions provided at the meeting which incorporates Federal Aviation Administration conditions.

Absent: Commissioner Youmans

VII. VIDEO

The entire discussion of this agenda item is on video and live streamed on the day of the meeting. If you have any questions please contact Barbara Santos, ALUC Commission Secretary, at (951) 955-5132 or e-mail at basantos@rivco.org.

ITEM 3.1: TIME: 9:33 A.M.

**AIRPORT LAND USE COMMISSION
MINUTE ORDER AUGUST 8, 2019
RIVERSIDE MEETING**

I. **AGENDA ITEM 3.2:** ZAP1076PS19 – Desert Springs Finance, LLC (Representative: William Spencer) – City of Cathedral City Case Nos. GPA18-003 (General Plan Amendment), CZ18-002 (Change of Zone), CUP18-024 (Conditional Use Permit), TTM37745 (Tentative Tract Map). A proposal to establish a 110 unit multi-family condominium complex with amenities on 7.06 acres, involving a Conditional Use Permit and a Tentative Tract Map, located at the southerly terminus of Landau Boulevard, southerly of Ramon Road, easterly of the Whitewater River/Coachella Valley Stormwater Channel, westerly of Banff Way, and northwesterly of Creekside Apartments. The applicant also proposes amending the City’s General Plan designation of the site from Medium Density Residential to High Density Residential and changing the site’s zoning from R2 Multiple Family Residential District to R3 Multiple Family Residential District (Airport Compatibility Zone D of the Palm Springs International Airport Influence Area).

II. **MAJOR ISSUES**
None

III. **STAFF RECOMMENDATION**
Staff recommends that the Commission find the proposed General Plan Amendment and Change of Zone CONSISTENT with the 2005 Palm Springs International Airport Land Use Compatibility Plan, and find the proposed Conditional Use Permit, and Tentative Tract Map CONSISTENT, subject to the conditions included herein.

IV. **PROJECT DESCRIPTION**
A proposal to establish a 110 dwelling unit multi-family condominium complex with amenities on 7.06 acres, involving a Conditional Use Permit and a Tentative Tract Map. The applicant also proposes amending the City’s General Plan designation of the site from Medium Density Residential to High Density Residential and changing the site’s zoning from R2 Multiple Family Residential District to R3 Multiple Family Residential District.

CONDITIONS:

1. Any new outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
 - a. Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - b. Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.

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- c. Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - d. Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - e. Highly noise-sensitive outdoor nonresidential uses.
- 3. The attached notice shall be given to all prospective purchasers, lessees, and/or tenants of the property, and shall be recorded as a deed notice.
 - 4. Any new detention basin(s) on the site shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention basin(s) that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.
 - 5. The evaluated project amenities consists of a 2,435 square foot pool, a 1,330 square foot outdoor barbeque area, a 3,600 square foot gym, a 1,080 square foot party room, and a 2,880 square foot of storage area. Any increase in building area or change in use to assembly occupancies will require an amended review by the Airport Land Use Commission.

V. MEETING SUMMARY

The following staff presented the subject proposal:

Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rivco.org

No one spoke in favor, neutral or opposition to the project.

VI. ALUC COMMISSION ACTION

The ALUC by a unanimous vote of 6-0 found the project CONSISTENT. Absent: Commissioner Youmans

VII. VIDEO

The entire discussion of this agenda item is on video and live streamed on the day of the meeting. If you have any questions please contact Barbara Santos, ALUC Commission Secretary, at (951) 955-5132 or e-mail at basantos@rivco.org.

ITEM 3.2: TIME: 9:38 A.M.

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I. 4.0 ADMINISTRATIVE ITEMS

4.1 Director's Approvals – Information Only

4.2 ALUC Commissioner Recusals

Ray Mistica, ALUC Counsel provided an oral briefing to the Commission as a reminder regarding the Conflict of Interest Laws pertaining to recusals.

4.3 Commission Request for Notification of Off-Field Landings

Ray Mistica, ALUC Counsel provided an oral briefing to the Commission regarding the Brown Act and how it applies for notification of off-field landings, serial meetings, and email communication between ALUC staff.

4.4 The FAA Glare Policy

Simon Housman, ALUC Director provided a Power Point slide presentation regarding the use of the FAA's Interim Glare Policy on ALUC solar cases.

II. 5.0 APPROVAL OF MINUTES

The ALUC by a unanimous vote of 6-0 approved the July 11, 2019 minutes. Absent: Youmans

III. 6.0 ORAL COMMUNICATION ON ANY MATTER NOT ON THE AGENDA

None

IV. 7.0 COMMISSIONER'S COMMENTS

None

V. 8.0 ADJOURNMENT

Steve Manos, Chairman adjourned the meeting at 10:14 a.m.

VI. VIDEO

The entire discussion of this agenda item is on video and live streamed on the day of the meeting. If you have any questions please contact Barbara Santos, ALUC Commission Secretary, at (951) 955-5132 or e-mail at basantos@rivco.org.

ITEM 4.0: TIME:9:44 A.M.