

**COUNTY OF RIVERSIDE
AIRPORT LAND USE COMMISSION
STAFF REPORT**

AGENDA ITEM: 3.1

HEARING DATE: January 14, 2021

CASE NUMBER: ZAP1440MA20 – CDRE Holdings 17, LLC (Representative: MIG, Inc.)

APPROVING JURISDICTION: City of Moreno Valley

JURISDICTION CASE NO: PEN20-0118 (General Plan Amendment), PEN20-0119 (Change of Zone), PEN20-0121 (Plot Plan), PEN20-0120 (Tentative Parcel Map)

LAND USE PLAN: 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan

Airport Influence Area: March Air Reserve Base

Land Use Policy: Airport Compatibility Zones D and E

Noise Levels: Below 60 CNEL from aircraft

MAJOR ISSUES: None

RECOMMENDATION: Staff recommends that the Commission find the proposed General Plan Amendment and Change of Zone CONSISTENT with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, and find the proposed Plot Plan and Tentative Parcel Map CONSISTENT, subject to the conditions included herein.

PROJECT DESCRIPTION: The applicant proposes to construct two industrial warehouse buildings with mezzanines totaling 396,275 square feet on 17.67 acres. The applicant also proposes amending the site's General Plan land use designation from Commercial (C) to Business Park/ Light Industrial (BP) and changing its zoning from Community Commercial (CC) to Light Industrial (LI). The applicant also proposes a tentative parcel map to subdivide the site.

PROJECT LOCATION: The site is located northerly of Brodiaea Avenue, southerly of Alessandro Boulevard, easterly of Frederick Street, and westerly of Graham Street, approximately 7,700 feet northeasterly of the northerly end of Runway 14-32 at March Air Reserve Base.

BACKGROUND:

Non-Residential Land Use Intensity: Pursuant to the Airport Land Use Compatibility Plan for the March Air Reserve Base/Inland Port Airport, the site is located within Compatibility Zones D and E. Zones D and E do not limit nonresidential intensity.

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited or discouraged in Compatibility Zones D and E.

Noise: The March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan depicts the site as being outside the 60 CNEL range from aircraft noise. Therefore, no special measures are required to mitigate aircraft-generated noise.

Part 77: The elevation of Runway 14-32 at its northerly terminus is 1,535 feet above mean sea level (1,535 feet AMSL). At a distance of approximately 7,700 feet from the project to the nearest point on the runway, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding 1,612 feet AMSL. The site's elevation is 1,572 feet AMSL, and the proposed maximum building height is 46 feet, resulting in a top point elevation of 1,618 feet AMSL. Therefore, review of buildings by the FAA Obstruction Evaluation Service was required. The applicant submitted Form 7460-1 for FAA OES review. Determinations of No Hazard to Air Navigation letters were issued (Aeronautical Study Nos. 2020-AWP-10718-OE and 2020-AWP-10720-OE), as the FAA OES determined that the project would not result in an impact to air navigation. The FAA OES conditions have been incorporated into ALUC's conditions listed below.

Open Area: None of the Compatibility Zones for the March Air Reserve Base/Inland Port ALUCP require open area specifically.

Hazards to Flight: Land use practices that attract or sustain hazardous wildlife populations on or near airports significantly increase the potential of Bird Aircraft Strike Hazards (BASH). The FAA strongly recommends that storm water management systems located within 5,000 or 10,000 feet of the Airport Operations Area, depending on the type of aircraft, be designed and operated so as not to create above-ground standing water. To facilitate the control of hazardous wildlife, the FAA recommends the use of steep-sided, rip-rap lined, narrow, linearly shaped water detention basins. All vegetation in and around detention basins that provide food or cover for hazardous wildlife should be eliminated. (FAA Advisory Circular 5200-33B).

The nearest portion of the project is located 7,700 feet from the runway, and therefore would be subject to the above requirement. The project would include a bioretention basin, which is to be avoided in Zones D and E due to the potential that such areas could provide food, water, and shelter for hazardous wildlife. Pursuant to the study "Wildlife Hazard Management at Riverside County Airports: Background and Policy", October 2018, by Mead & Hunt, which is the basis of the brochure titled "Airports, Wildlife and Stormwater Management", such basins are to be avoided in Zones D and E, unless they provide for a 48-hour drawdown and propose landscaping that is not attractive to hazardous wildlife. The project has been conditioned to be consistent with these

standards in order to reduce bird attractant: 1) new basins are to be designed so as to provide for a maximum 48-hour detention period following the conclusion of a storm event, and to remain totally dry between rainfalls, and 2) any landscaping proposed in the detention basin shall be in accordance with the ALUC "Landscaping Near Airports" and "Airports, Wildlife and Stormwater Management" brochures.

General Plan Amendment/Change of Zone: The applicant proposes amending the site's General Plan land use designation from Commercial (C) to Business Park/ Light Industrial (BP) and changing its zoning from Community Commercial (CC) to Light Industrial (LI). The proposed amendments would be as, or more, consistent with the Compatibility Plan as the underlying compatibility zone does not restrict intensities.

Tentative Parcel Map: The applicant proposes to divide the 17.67 acres acre site into two commercial lots (12.46 acres and 5.18 acres in size). The subdivision would not result in a significant impact to airport land use compatibility, as nonresidential intensity is not restricted.

CONDITIONS:

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)

- (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Hazards to flight
3. The attached notice shall be provided to all prospective purchasers of the property and tenants of the building, and shall be recorded as a deed notice.
 4. Any proposed detention basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the detention basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the detention basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the detention basin, if not rip-rap, should be in accordance with the guidance provided in ALUC “LANDSCAPING NEAR AIRPORTS” brochure, and the “AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT” brochure available at RCALUC.ORG which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: “There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes”. The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin.

5. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.
6. The Federal Aviation Administration has conducted an aeronautical study of the proposed project (Aeronautical Study Nos. 2020-AWP-10718-OE and 2020-AWP-10720-OE) and has determined that neither marking nor lighting of the structure(s) is necessary for aviation safety. However, if marking and/or lighting for aviation safety are accomplished on a voluntary basis, such marking and/or lighting (if any) shall be installed in accordance with FAA Advisory Circular 70/7460-1 L Change 2 and shall be maintained in accordance therewith for the life of the project.

7. The proposed structures shall not exceed a height of 46 feet above ground level and a maximum elevation at top point of 1,621 feet above mean sea level.
8. The maximum height and top point elevation specified above shall not be amended without further review by the Airport Land Use Commission and the Federal Aviation Administration; provided, however, that reduction in structure height or elevation shall not require further review by the Airport Land Use Commission. The specific coordinates, frequencies, and power shall not be amended without further review by the Federal Aviation Administration
9. Temporary construction equipment used during actual construction of the structure(s) shall not exceed 46 feet in height and a maximum elevation of 1,621 feet above mean sea level, unless separate notice is provided to the Federal Aviation Administration through the Form 7460-1 process.
10. Within five (5) days after construction of the structure reaches its greatest height, FAA Form 7460-2 (Part II), Notice of Actual Construction or Alteration, shall be completed by the project proponent or his/her designee and e-filed with the Federal Aviation Administration. (Go to <https://oeaaa.faa.gov> for instructions.) This requirement is also applicable in the event the project is abandoned or a decision is made not to construct the applicable structure.

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

NOTICE

**THERE IS AN AIRPORT NEARBY.
THIS STORM WATER BASIN IS DESIGNED TO HOLD
STORM WATER FOR ONLY 48 HOURS AND
NOT TO ATTRACT BIRDS**

**PROPER MAINTENANCE IS NECESSARY TO AVOID
BIRD STRIKES**



IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:

Name: _____

Phone: _____



Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2020-AWP-10718-OE

Issued Date: 10/29/2020

Deirdre McCollister
MIG
1500 Iowa Avenue, Suite 110
Riverside, CA 92507

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

| | |
|------------|--|
| Structure: | Building Compass Danbe Centerpointe - Building 1 |
| Location: | Moreno Valley, CA |
| Latitude: | 33-54-56.53N NAD 83 |
| Longitude: | 117-15-31.52W |
| Heights: | 1574 feet site elevation (SE) 46 feet above ground level (AGL) 1620 feet above mean sea level (AMSL) |

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
 Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 2.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 04/29/2022 unless:

- the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- extended, revised, or terminated by the issuing office.
- the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within

6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (847) 294-7575, or vivian.vilaro@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2020-AWP-10718-OE.

Signature Control No: 451038435-455287999

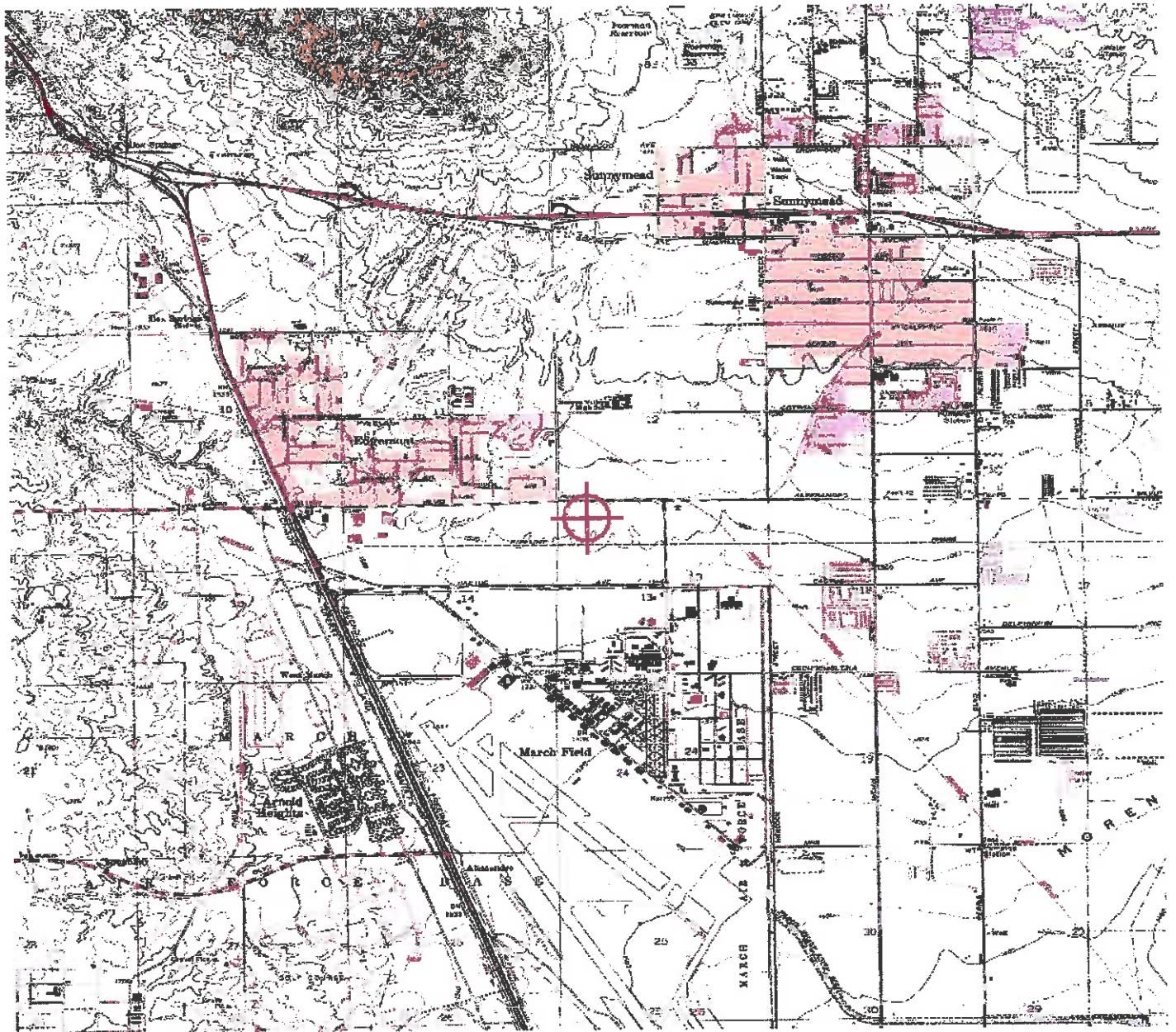
(DNE)

Vivian Vilaro
Specialist

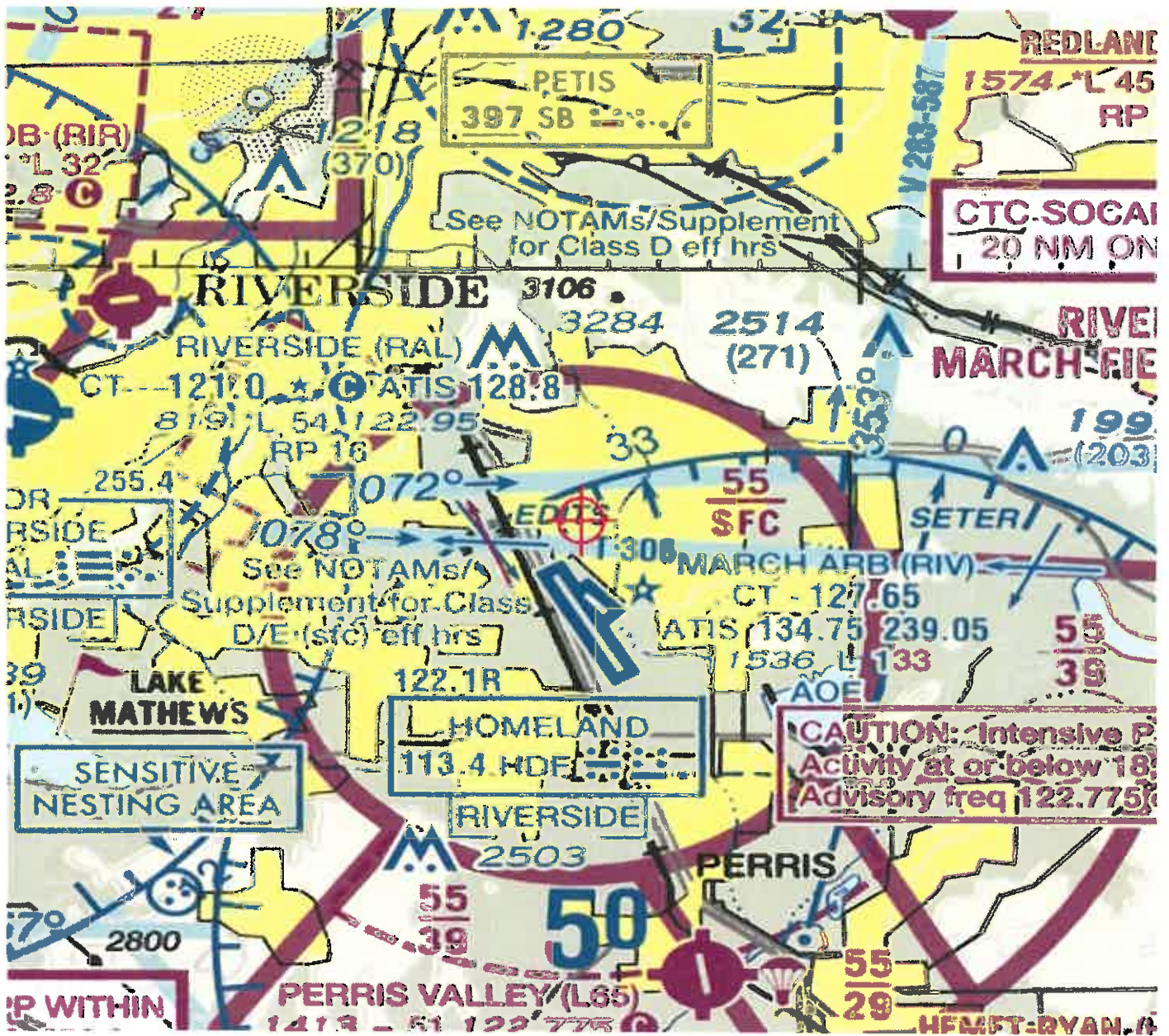
Attachment(s)

Map(s)

TOPO Map for ASN 2020-AWP-10718-OE



Sectional Map for ASN 2020-AWP-10718-OE





Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2020-AWP-10720-OE

Issued Date: 10/29/2020

Deirdre McCollister
MIG
1500 Iowa Avenue, Suite 110
Riverside, CA 92507

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building Compass Danbe Centerpointe - Building 2
Location: Moreno Valley, CA
Latitude: 33-54-57.65N NAD 83
Longitude: 117-15-20.52W
Heights: 1575 feet site elevation (SE)
46 feet above ground level (AGL)
1621 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

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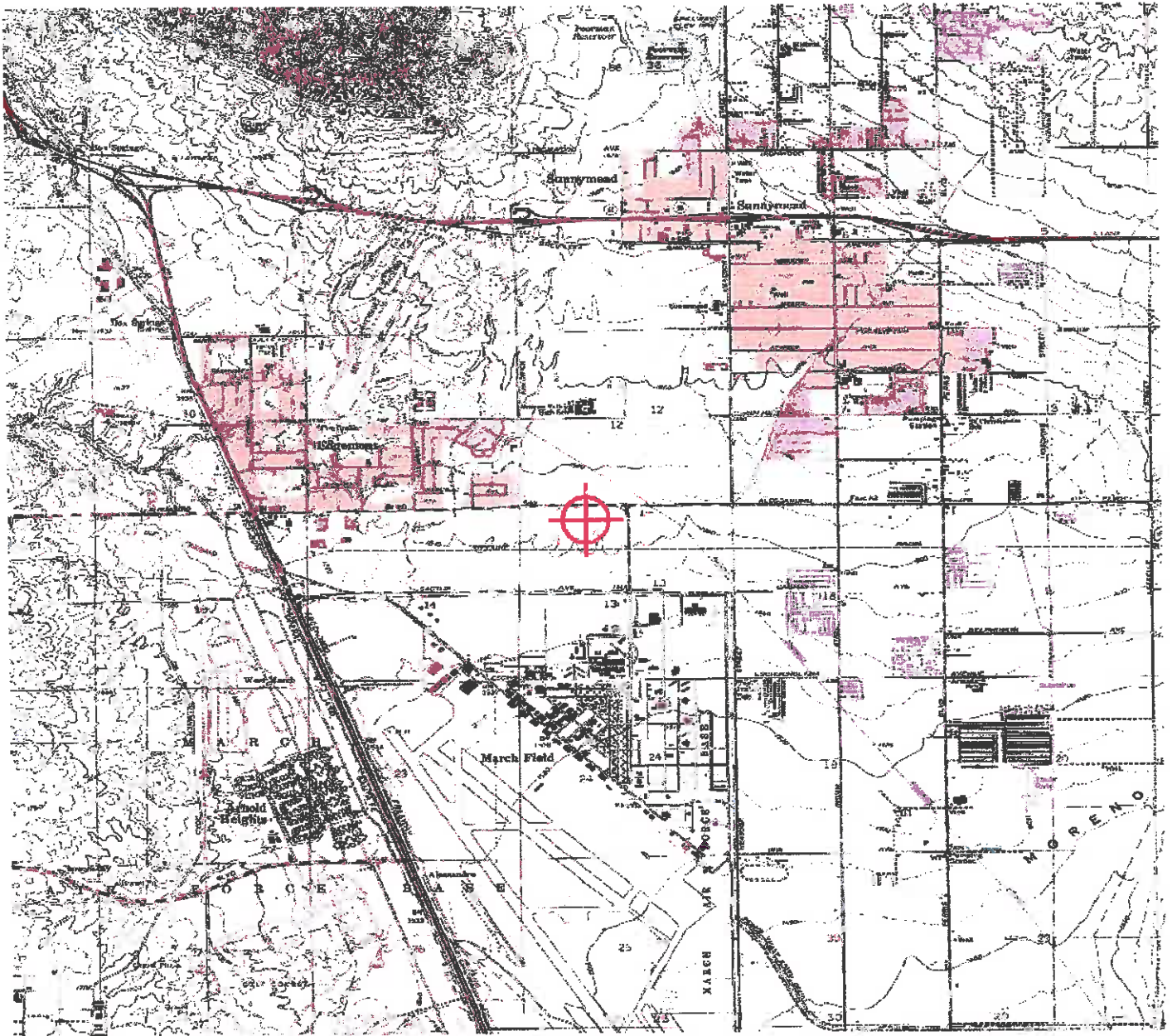
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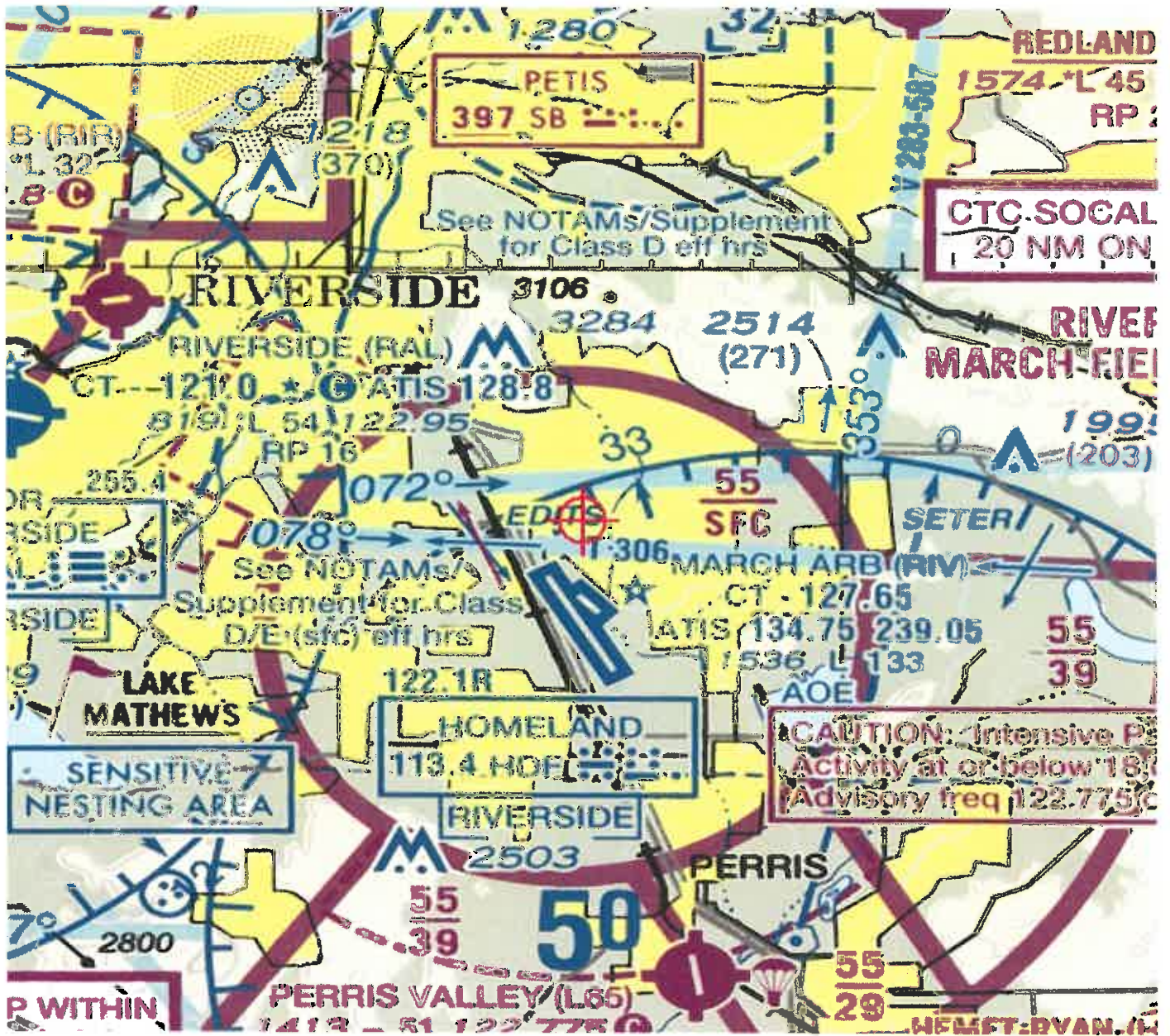
Vivian Vilaro
Specialist

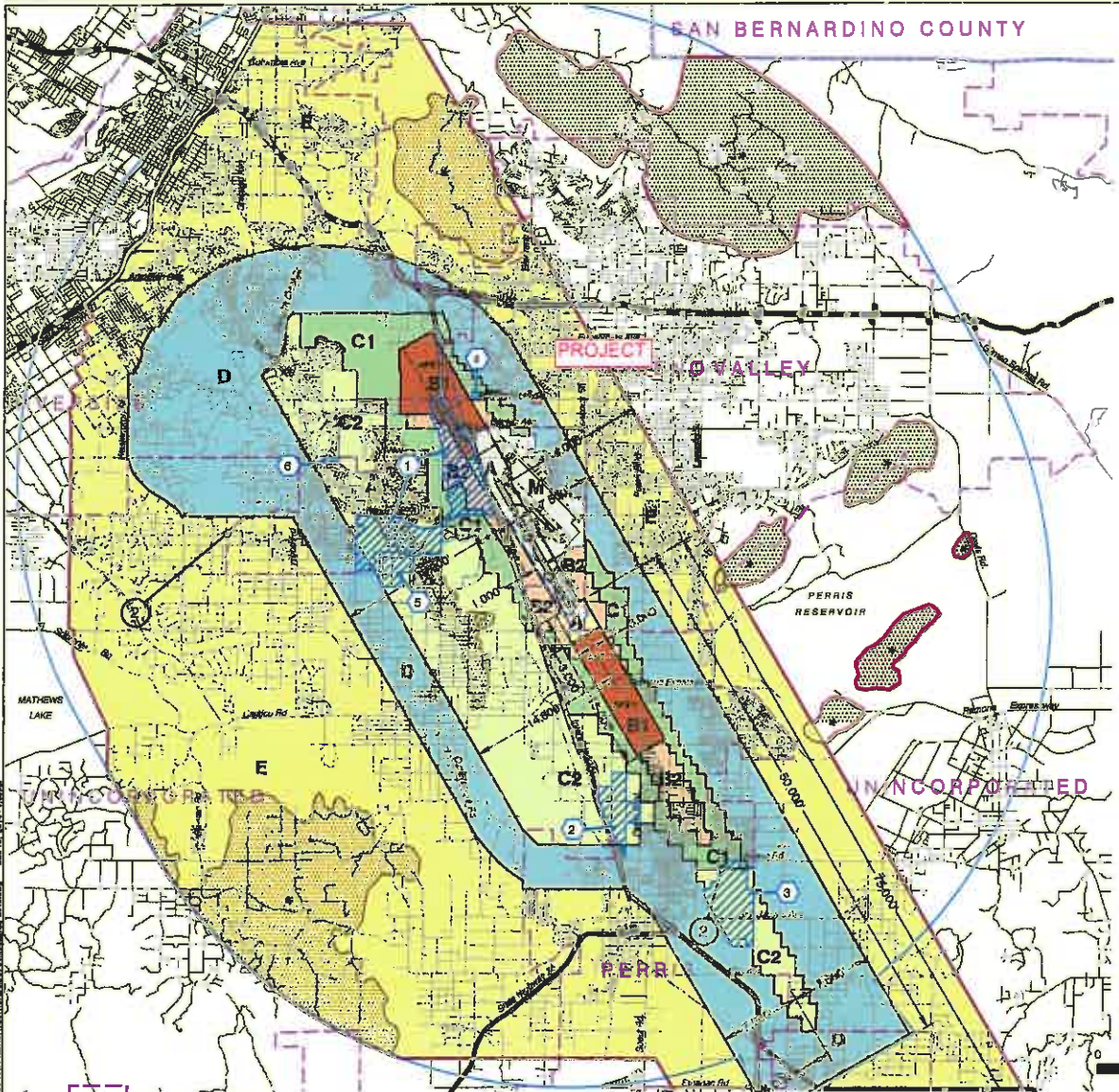
Attachment(s)
Map(s)

TOPO Map for ASN 2020-AWP-10720-OE



Sectional Map for ASN 2020-AWP-10720-OE





LEGEND

Compatibility Zones

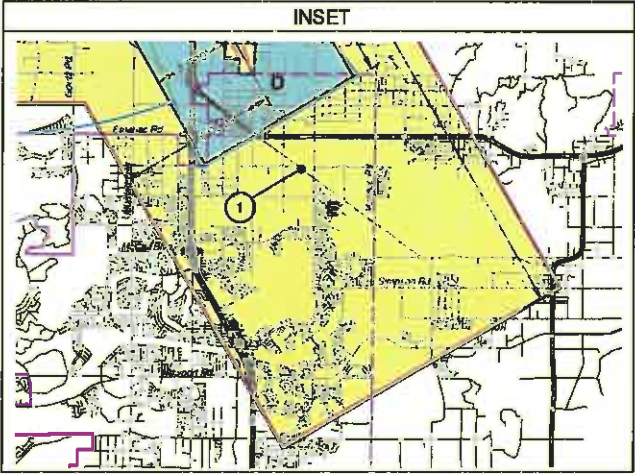
- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

- ① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.

Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- Site-Specific Exceptions (existing local agency commitments to development projects)

- ① March JPA: March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA: Ben Clark Training Center
- ⑥ Riverside: Ridge Crest Subdivision



**Riverside County
Airport Land Use Commission
March Air Reserve Base / Inland Port Airport
Land Use Compatibility Plan
(Adopted November 13, 2014)**

Map MA-1

**Compatibility Map
March Air Reserve Base / Inland Port Airport**

Note:
All dimensions are measured from
runway ends and centerlines.



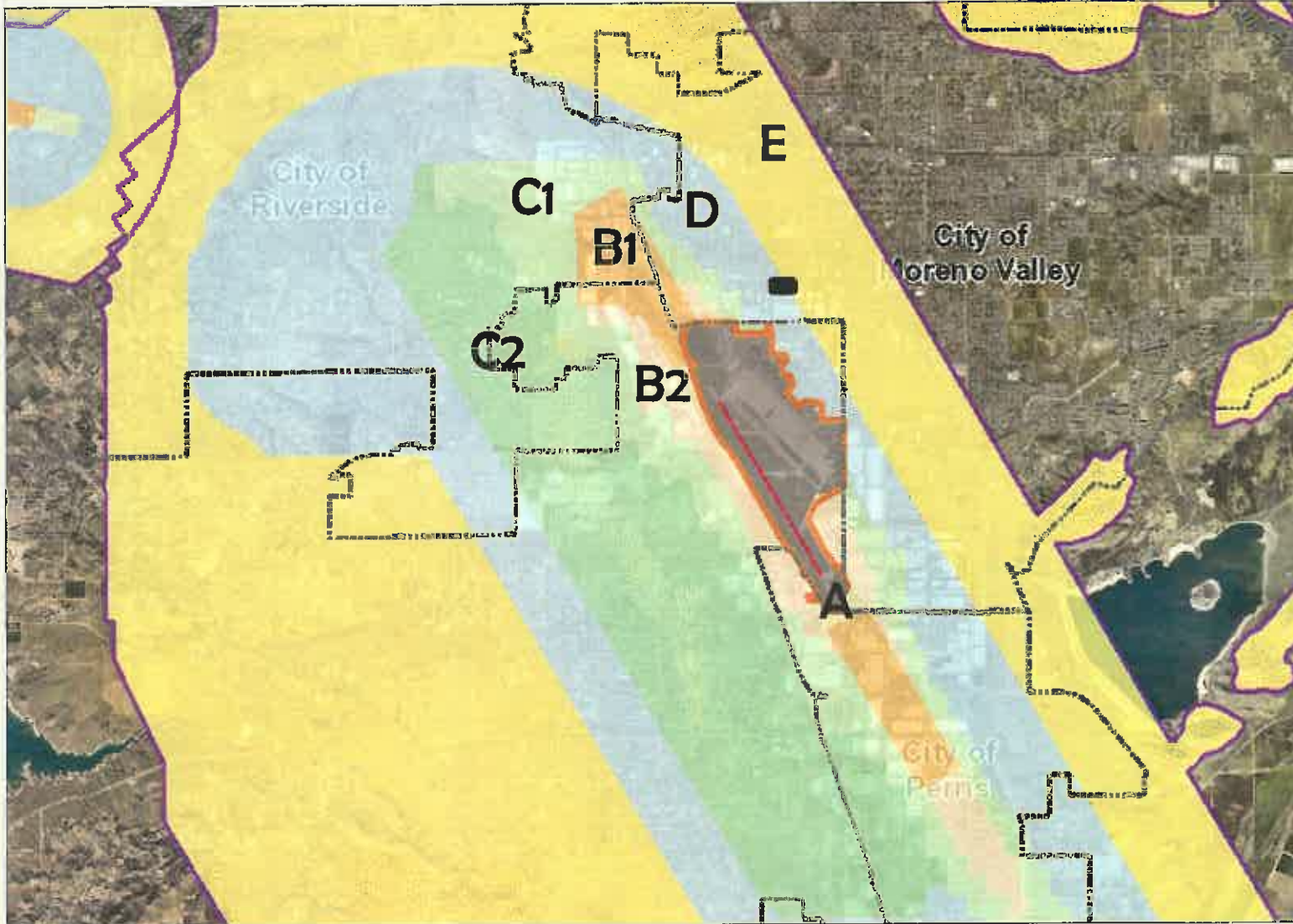
Base map source: County of Riverside 2013

SEE INSET AT RIGHT

Prepared by Mead & Hunt, Inc. (June 2013)

S:\PROJECTS\2013\20130601\20130601_001\20130601_001.dwg, DATE: 11/13/2014, TIME: 10:00:00 AM

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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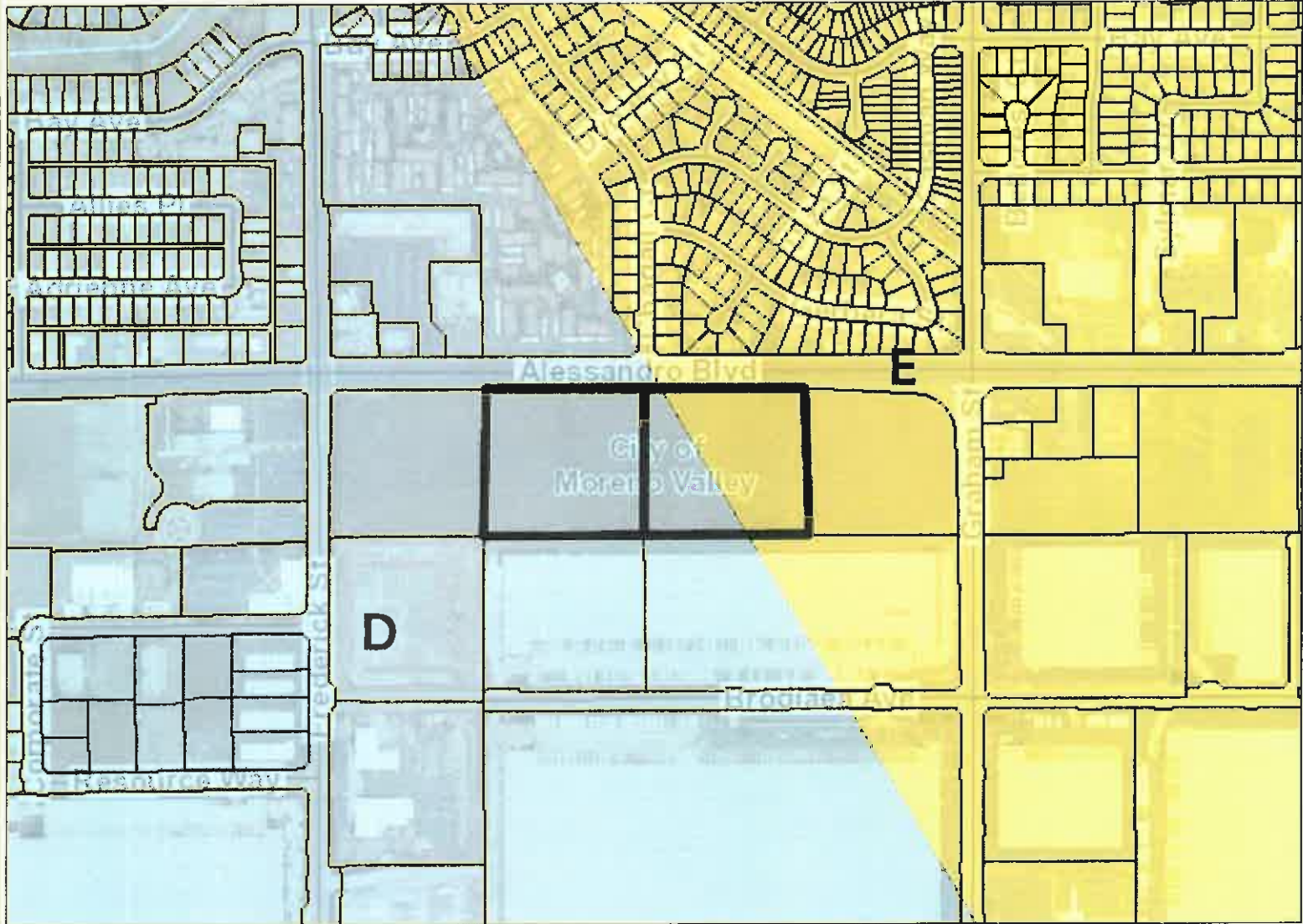


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Notes

Map My County Map



Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5



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0 752 1,505 Feet

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Notes

Map My County Map



- Legend**
- Parcels
 - County Centerline Names
 - County Centerlines
 - Blueline Streams
 - City Areas
 - World Street Map



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0 752 1,505 Feet

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Notes

Map My County Map



Legend

- Blueline Streams
- City Areas
- World Street Map

Notes



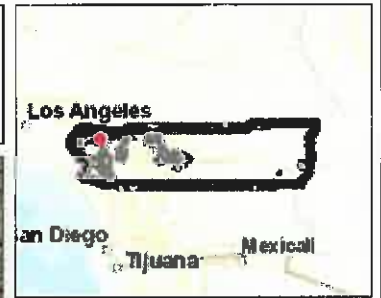
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019

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Map My County Map



Legend

- County Centerlines
- Blue Line Streams
- City Areas
- World Street Map



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0 3,000 6,019 Feet

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Notes

Map My County Map



Legend

- County Centerlines
- Blue line Streams
- City Areas
- World Street Map



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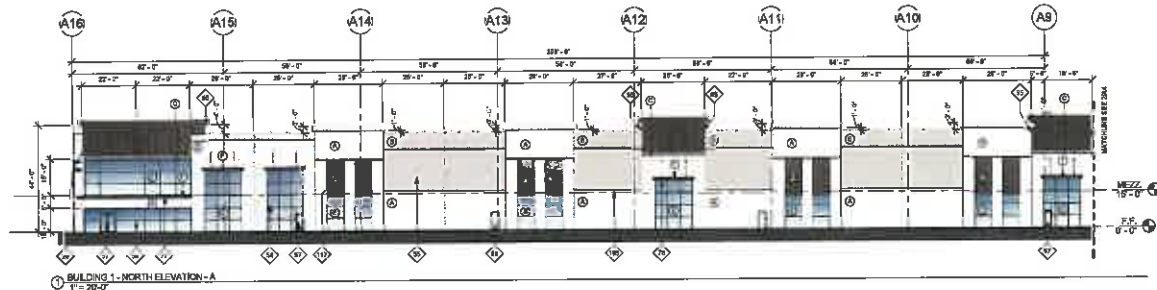
Notes

APPLICATION PARCEL EXHIBIT

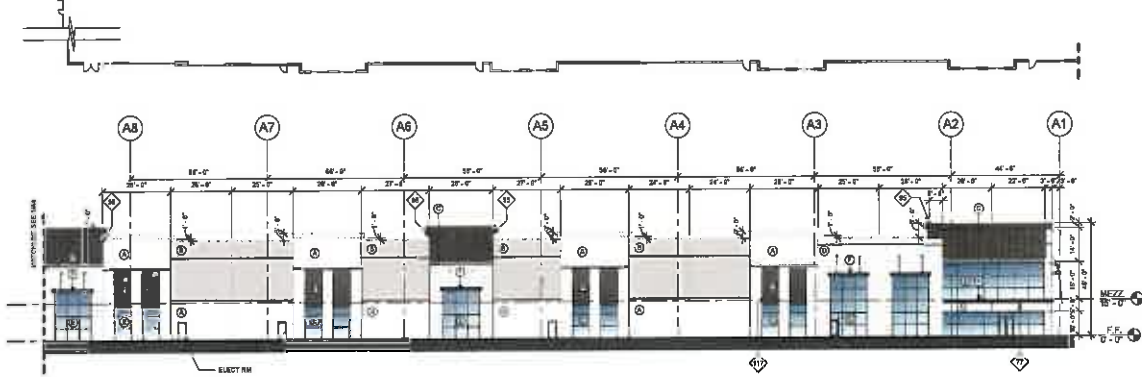


LAND USE AND ZONING TABLE

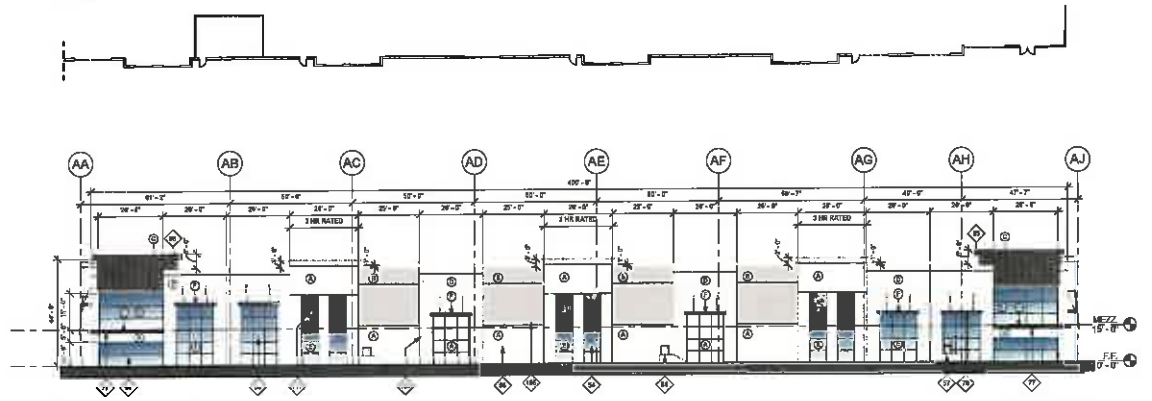
| Building | APN: | Current General Plan Land Use Designation | Proposed General Plan Land Use Designation | Current Zoning | Proposed Zoning |
|----------|-------------|---|--|---------------------------|------------------|
| 1 | 297-170-002 | Commercial | Business Park/ Light Industrial | Community Commercial (CC) | Light Industrial |
| 2 | 297-170-003 | Commercial | Business Park/ Light Industrial | Community Commercial (CC) | Light Industrial |



1 BUILDING 1 - NORTH ELEVATION - A
1" = 20'-0"



2 BUILDING 1 - NORTH ELEVATION - B
1" = 20'-0"



3 BUILDING 1 - WEST ELEVATION
1" = 20'-0"

- NOTES**
- 37 REPAIR BRICKS BACK.
 - 34 STOREFRONT: SEE ELEVATIONS & EXTERIOR COLOR SCHEDULE. BRICK FRONT TO BE ORDERED TO MATCH TYPICAL LOAD AS REQUIRED BY SUBMITTAL. SEE ARCHITECTURAL DETAILS FOR BRICKWORK. BRICKWORK TO BE ORDERED TO MATCH ADJACENT BUILDING WALLS.
 - 64 CONCRETE TYPICAL PANEL TYP. PAINTED. SEE EXTERIOR COLOR SCHEDULE. SEE ARCHITECTURAL DETAILS FOR BRICKWORK. BRICKWORK TO BE ORDERED TO MATCH ADJACENT BUILDING WALLS.
 - 64 EXTERIOR MAIN DOOR 5'X7' HOLLOW METAL PARTED. SEE EXTERIOR COLOR SCHEDULE & DOOR SCHEDULE FOR BRICKWORK. BRICKWORK TO BE ORDERED TO MATCH ADJACENT BUILDING WALLS.
 - 67 EXTERIOR STOREFRONT DOOR. SEE EXTERIOR COLOR SCHEDULE & DOOR SCHEDULE FOR BRICKWORK. BRICKWORK TO BE ORDERED TO MATCH ADJACENT BUILDING WALLS.
 - 74 DECORATIVE METAL BRICK. REFER TO ARCHITECTURAL DETAILS.
 - 86 FINISH: STONE TYP. OR PER PLAN. SEE "T" SYMBOL.
 - 104 PANEL: CORE TYP.
 - 106 2" DECORATIVE CONCRETE REVEAL WITH CHAMFERED EDGES. TYP.
 - 117 DECORATIVE METAL FINISH. REFER TO ARCHITECTURAL ELEVATIONS.

GLAZING LEGEND

| | |
|--|--------------------|
| | VIEWER GLAZING |
| | NEW VIEWER GLAZING |
| | TEMPERED |

NOTE:
REFER TO ELEVATIONS FOR TEMPERED GLAZING LOCATIONS.

NON-VIEWER GLAZING NOTES:

1. SINGLE PANE GLAZING: PARTIAL FACED OF CONCRETE PANELS. BRICK BACK. NO GLAZING RISERS.
2. DOUBLE PANE GLAZING: BRICK BACK SYSTEM IS REQUIRED IN CONCRETE.
3. FROM BRICK BACK SYSTEM: GLAZING IS AREAS INTENDED TO BE NON-VIEWER. THESE TO BE BRICK BACK. TYP. 2" AIR FILTER PANELS.

TEMPERED GLAZING NOTES:

1. OPERABLE DOORS: WINDOWS AND VENTING 1" OF INSULATION SURFACE TO BE TEMPERED.

EXTERIOR COLOR SCHEDULE

| | |
|--|--|
| | OFF WHITE EXTERIOR PAINT COLOR: SW670 HERON PLUMB |
| | MEDIUM TAUPE EXTERIOR PAINT COLOR: SW670 PERFECT GRASS |
| | WARM GRAY TAUPE |
| | BOARD FORMED OFF WHITE PAINTED CONCRETE |
| | STOREFRONT BLUE REFLECTIVE GLAZING & CHARCOAL ANODIZED ALUMINUM |
| | GLAZED BRONZE BRUSHING |

TYP. PART NOTES:
PART: MAIN DOORS, BRICK WALLS, RAMP WALLS, STAIR WALLS, GLAZED WALLS, ROOF BRICKS, AND LOADING DOORS TO MATCH ADJACENT BUILDING WALLS.

TRUCK DOORS TO BE PRE-FINISHED BY MANUFACTURER IN WHITE PRISH.

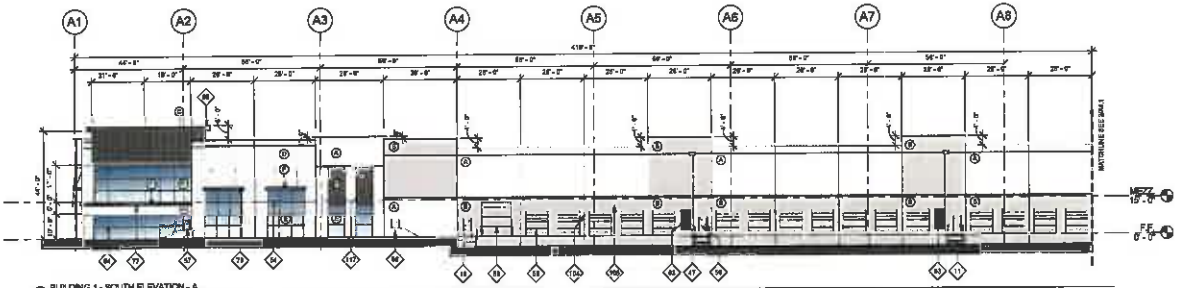




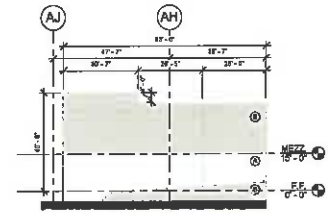
A10-0002
10.03.2020

EXTERIOR
ELEVATIONS - B1

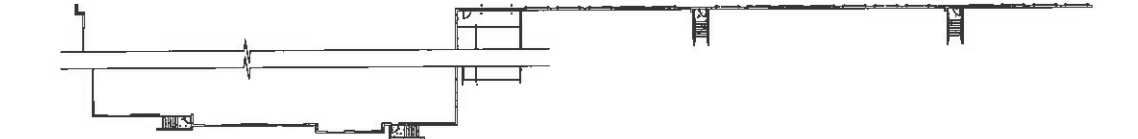
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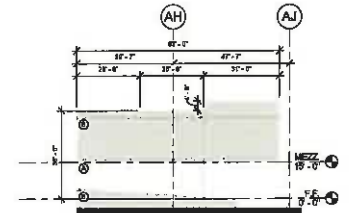
1 BUILDING 1 - SOUTH ELEVATION - A
T = 20'-0"



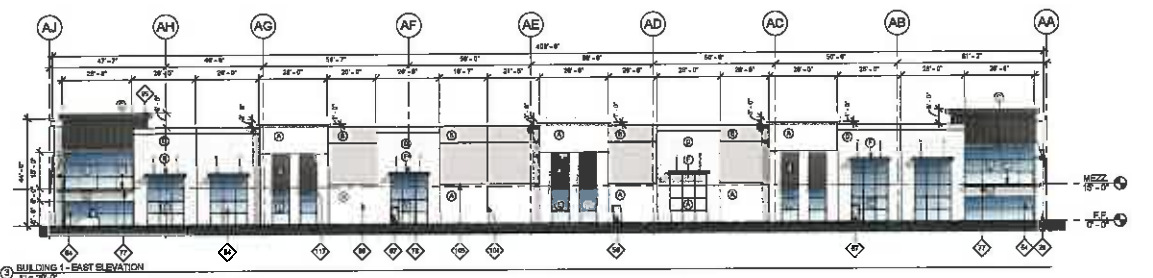
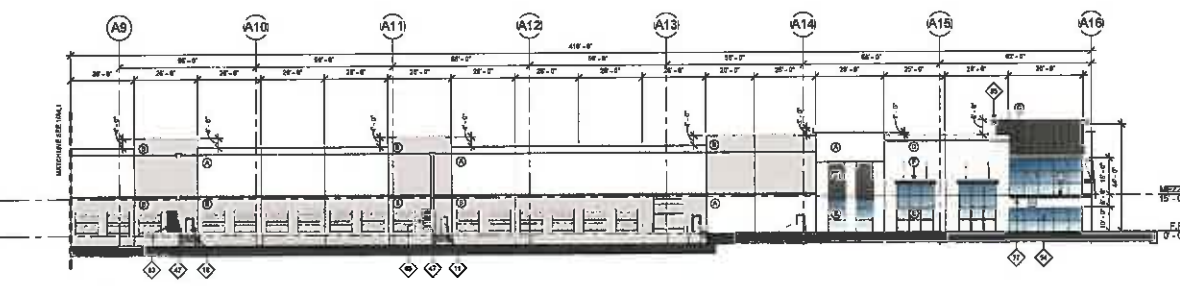
1 BUILDING 1 TRUCK COURT RETURN ELEVATION A
T = 20'-0"



2 BUILDING 1 - SOUTH ELEVATION - B
T = 20'-0"



1 BUILDING 1 TRUCK COURT RETURN ELEVATION B
T = 20'-0"



3 BUILDING 1 - EAST ELEVATION
T = 20'-0"

- REVISIONS
- 11 EXTRINSIC CONCRETE STEEL REINFORCED WALLS, WALLS & MEMBERS PAINTED INTERIOR COLOR SCHEDULES, REFER TO CIVIL AND STRUCTURAL.
 - 18 STRUCTURE METAL BOLLSERS, CONCRETE FILLER, PAINTED TYP.
 - 26 SECURE RECYCLE BAGS.
 - 27 EXTERIOR METAL DOWNSPUT AND OVERLAYS INSULATED PARTS TO MATCH BUILDING, REFER TO PLUMBING PLANS FOR MINIMUM SLOPES OPENINGS ALLOWABLE PER CODE.
 - 34 STRENGTHEN USE ELEVATIONS & EXTERIOR COLOR SCHEDULES, BESE ASSET TO BE EXPENSE TO REPLY HARD LOG AS REQUIRED BY BUILDING CODES AND LOCAL JURISDICTIONS, COLOR OF EXTERIOR PAINT AND SYSTEMS STRUCTURE CALCULATED TO BE WITHIN BUILT BY G.C. AND UNDER DEFERRED SCHEDULE.
 - 36 CONCRETE IS LEFT PAINT, TOP PAINTED, FULL EXTERIOR COLOR FOR FUTURE, REFER TO CIVIL AND STRUCTURAL FOR ADDITIONAL INFORMATION.
 - 38 EXTERIOR MAIN DOOR TYP. INCLUDE METAL PAINTED, SEE EXTERIOR COLOR SCHEDULE & DOOR SCHEDULE FOR ADDITIONAL INFO.
 - 37 EXTERIOR STONEWORK EXTERIOR, SEE EXTERIOR COLOR SCHEDULE & DOOR SCHEDULE FOR ADDITIONAL INFO.
 - 50 DOOR-HI LOCKING DOORS, FILL WITH VISION GLAZING AND FINISH BY MANUFACTURER PER COLOR SCHEDULE.
 - 56 DRIVE THRU LOCKING DOOR EXTERIOR WITH VISION GLAZING, PER FINISH BY MANUFACTURER PER COLOR SCHEDULE.
 - 63 ANCHOR BOLTS, PARTITION BARRIERS WALL, ANCHOR BOLTS, PARTITION BARRIERS WALL, AND REBAR BAR.
 - 77 CANOPY, REFER TO ELEVATIONS - STRUCTURAL DETAILS, CIRCUMFERENCE METAL, REFER TO ARCHITECTURAL-STRUCTURAL DETAILS.
 - 86 PARAPET RETURN 3" MIN. ON PER PLAN, SEE "I" DRAWING.
 - 106 PANEL JOINT TYP.
 - 142 CIRCUMFERENCE CONCRETE REBAR WITH CHAMFERED EDGES, TYP.
 - 117 CIRCUMFERENCE METAL ANCHORS, REFER TO ARCHITECTURAL ELEVATIONS.

| GLAZING LEGEND | |
|--------------------|--|
| VISION GLAZING | |
| NON VISION GLAZING | |
| TEMPERS | |

NOTE:
REFER TO ELEVATIONS FOR TEMPERED GLAZING LOCATIONS.

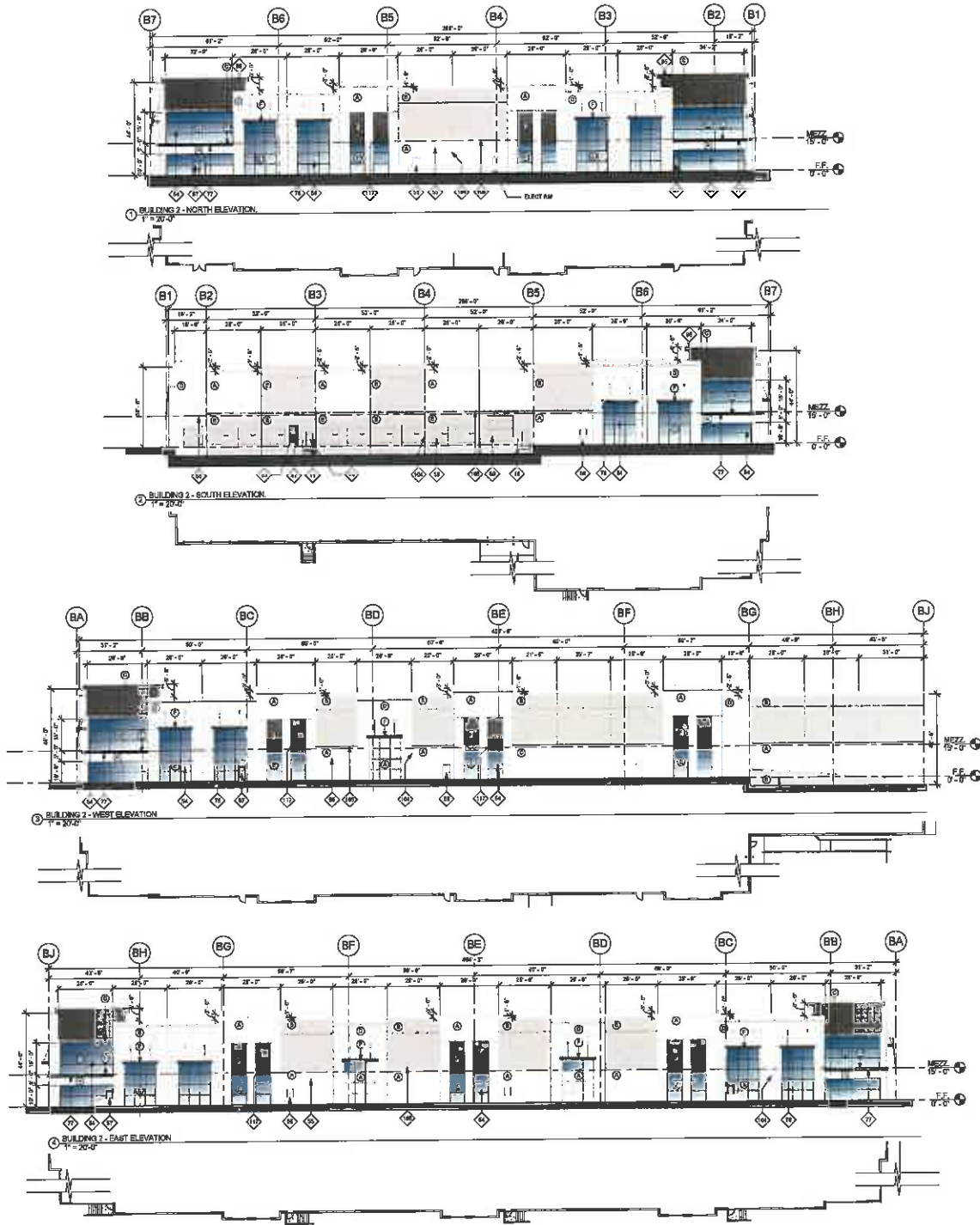
NON VISION GLAZING NOTE:
1. FINISH PAINT GLAZING PAINT FACE OF CONCRETE PANELS, BEHIND GLAZING, NO COATING REQUIRED.
2. PROVIDE GLAZING CHANNEL MULLION SYSTEMS INCLUDING GLAZING SECTIONS, MOVABLE REQUIRED BY CODES.
3. PROVIDE CHARGE GLAZING EDGED GLASS IN GLAZING CHANNELS TO BE COMBINATION WHEN THERE IS NO STANDARD CONCRETE FINISH BEHIND THE GLAZING, USE FINISH PAINT.

TEMPERED GLAZING NOTE:
1. IF CRACKS OCCUR, REPAIR AND REMOVE UP OF MULLION SURFACE TO BE TEMPERED.

| EXTERIOR COLOR SCHEDULE | |
|-------------------------|---|
| | A OFF-WHITE EXTERIOR PAINT COLOR, TRIM TO MATCH PANEL COLOR, MATCH PERFECT GRAB |
| | B WARM GRAY TILE |
| | C BOARD FORMED OFF-WHITE PAINTED CONCRETE |
| | D STOCKWORK BLUE REFLECTIVE GLAZING & CHARCOAL FINISHED BOLLARDS |
| | E DARK BRONZE OR CHARCOAL |

TYP PART NOTE:
PART MAIN LOCKER, GLAZING WALLS, ROOF WALLS, OTHER WALLS, GLAZING WALLS, ROOF WALLS, AND GLAZING TO MATCH ADJACENT BUILDING WALL TAG.

TRUSS COVERS TO BE PER-FINISHED BY MANUFACTURER IN WHITE FINISH.



| KEYNOTES | |
|----------|---|
| 11 | EXTERIOR CONCRETE STAIR WINDWATER WALLS & BALCONY PAINTED PER EXTERIOR COLOR SCHEDULE. REFER TO COLOR AND STRUCTURAL DRAWING. |
| 12 | PROTECTIVE METAL BOLLARDS, CONCRETE FILL. PAINTED, TYP. |
| 47 | EXTERIOR METAL DOWNPOUT AND OVERFLOW SCREWS PAINTED TO MATCH COLOR. REFER TO FINISHING PLAN FOR MINIMUM SCREWS PER DIMENSIONS ALLOWABLE PER CODE. |
| 54 | STONE FINISH SEE ELEVATIONS & SECTION COLOR SCHEDULE. STONE FINISH TO BE ORDERED TO MATCH WINDOW LOOK AS REQUIRED BY ALL DIMENSIONS AND LOCAL REGULATIONS REGARDING ACCESSIBILITY. FINISHING PARTITION AND STRUCTURAL CALCULATIONS TO BE OBTAINED FROM ALL. AND COVER REFERRED APPROPRIATE. |
| 64 | CONCRETE TILT-UP PANELS, TYP. PAINTED SEE EXTERIOR COLOR SCHEDULE. REFER TO ELEVATIONS AND SECTION FOR ADDITIONAL INFORMATION. |
| 66 | EXTERIOR STAIR RAMP DOOR, SEE EXTERIOR COLOR SCHEDULE & DOOR SCHEDULE FOR ADDITIONAL INFO. |
| 67 | EXTERIOR STAIR RAMP DOOR, SEE EXTERIOR COLOR SCHEDULE & DOOR SCHEDULE FOR ADDITIONAL INFO. |
| 68 | DOOR-HULL/CLADDING DOOR, TYP. WITH WINDOW GLAZING SEE SECTION FOR MANUFACTURER PER COLOR SCHEDULE. |
| 69 | DOOR-HULL/CLADDING DOOR, TYP. WITH WINDOW GLAZING SEE SECTION FOR MANUFACTURER PER COLOR SCHEDULE. |
| 76 | IRON MESH LOWER PART TO MATCH CLADDING WALL. TYP. SEE ARCHITECTURAL. PROVIDE INFO BORDERS, TYP. AND DETAILS. |
| 77 | CANOPY, REFER TO ELEVATIONS - STRUCTURAL DETAILS. |
| 78 | IRON MESH LOWER PART TO MATCH CLADDING WALL. TYP. SEE ARCHITECTURAL. PROVIDE INFO BORDERS, TYP. AND DETAILS. |
| 86 | PARAPET RETURN 2" MIN. DR PER PLAN, SEE "C" DRAWING. |
| 106 | PANEL, LIGHT, TYP. |
| 107 | "C" COORDINATE CONCRETE FINISH WITH CHARTERED FIGURE, TYP. |
| 117 | DISCRETE METAL JAMBING, REFER TO ARCHITECTURAL ELEVATIONS. |

| GLAZING LEGEND | |
|---------------------|--|
| VIEWER GLAZING: | |
| NON-VIEWER GLAZING: | |
| TEMPERED: | |

NOTE:
REFER TO ELEVATIONS FOR TEMPERED GLAZING LOCATIONS.

VIEWER GLAZING NOTES:
1. SINGLE FRAME GLAZING PAINT FACE OF GLASS PANEL, WHITE BRASS, NO COATING REQUIRED.
2. PROVIDE INTERMEDIATE MULLION SYSTEM @ NON-VIEWER GLAZING SECTIONS. NO HOLE FINISHES IN CONCRETE.
3. PROVIDE BRASS CLOTHY THERMO GLASS IN AREAS INTENDED TO BE NON-VIEWER. THERE IS NO ADVERTISING, CONCRETE THERMO BRASS 1/8" X 1/8" SET WITH FABRIC.

TEMPERED GLAZING NOTES:
1. IF CRACKS & DISCOLORED WINDOWS AND WITHIN 1" OF WALLING SURFACE TO BE TEMPERED.

| EXTERIOR COLOR SCHEDULE | |
|-------------------------|---|
| | OFF WHITE EXTERIOR PAINT COLOR: WHITE HOUSE FINISH |
| | MEDIUM TRUFFLE EXTERIOR PAINT COLOR: TRUFFLE PER FINISH |
| | WARM BRASS TILE |
| | BOARD FORMED OFF WHITE PAINTED CONCRETE |
| | NON-REFLECTIVE METAL GLAZING & CHARCOAL ANODIZED ALUMINUM |
| | DARK BRONZE BROWNSHINE |

TYP. PAINT NOTE:
PAINT BANK SPEC: GUARD WALLS, RAMP WALLS, STAIR WALLS, GUARD RAILS, BOOF DRAIN, AND LOGGING TO MATCH ALACANTER IRON STAIR WALLS.

TRUCK DOORS TO BE PRE-FINISHED BY MANUFACTURER IN WHITE FINISH



NOTICE OF PUBLIC HEARING
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION
www.rcaluc.org

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. For more information please contact **ALUC Planner Paul Rull at (951) 955-6893**. The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan.

The County of Riverside Planning Department should be contacted on non-ALUC issues. For more information please contact City of Moreno Valley Planner Ms. Julia Descoteaux at (951) 413-3209.

The proposed project application may be viewed by a prescheduled appointment and on the ALUC website www.rcaluc.org. Written comments may be submitted at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Friday from 8:00 a.m. to 3:30 p.m., or by e-mail to prull@rivco.org. Individuals with disabilities requiring reasonable modifications or accommodations, please telephone Barbara Santos at (951) 955-5132.

PLACE OF HEARING: Riverside County Administration Center
4080 Lemon Street, 1st Floor Board Chambers
Riverside California

DATE OF HEARING: January 14, 2021

TIME OF HEARING: 9:30 A.M.

Pursuant to Executive Order N-25-20, this meeting will be conducted by teleconference and at the Place of Hearing, as listed above. Public access to the meeting location will be allowed, but limited to comply with the Executive Order. Information on how to participate in the hearing will be available on the ALUC website at www.rcaluc.org

CASE DESCRIPTION:

ZAP1440MA20 – CDRE Holdings 17, LLC (Representative: MIG, Inc.) – City of Moreno Valley Case Nos. PEN20-0118 (General Plan Amendment), PEN20-0119 (Change of Zone), PEN20-0121 (Plot Plan), PEN20-0120 (Tentative Parcel Map). A proposal to construct two industrial warehouse buildings with mezzanines totaling 396,275 square feet on 17.65 acres located southerly of Alessandro Boulevard, westerly of Graham Street, northerly of Brodiaea Avenue, and easterly of Frederick Street. The applicant also proposes amending the site's General Plan land use designation from Commercial (C) to Business Park/Light Industrial (BP), and changing its zoning from Community Commercial (CC) to Light Industrial (LI). The applicant also proposes a tentative parcel map to subdivide the site (Airport Compatibility Zones D and E of the March Air Reserve Base/Inland Port Airport Influence Area).



RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

APPLICATION FOR MAJOR LAND USE ACTION REVIEW

MEM
D+E

ALUC CASE NUMBER: ZAP1440MA20 DATE SUBMITTED: 12-3-20

APPLICANT / REPRESENTATIVE / PROPERTY OWNER CONTACT INFORMATION

| | | | |
|-----------------|--|--------------|-----------------|
| Applicant | Pamela Steele, MIG, Inc. | Phone Number | 951-733-7240 |
| Mailing Address | 1650 Spruce Street, Suite 102 Riverside, CA 92507 | Email | Pams@migcom.com |

| | | | |
|-----------------|-----------------|--------------|--|
| Representative | (Same as above) | Phone Number | |
| Mailing Address | | Email | |

| | | | |
|-----------------|---|--------------|-------------------|
| Property Owner | Mark Bachli, CDRE Holdings 17, LLC | Phone Number | 310-428-3302 |
| Mailing Address | 523 Main Street El Segundo, CA 90245 | Email | mbachli@danbe.com |

LOCAL JURISDICTION AGENCY

| | | | |
|-------------------------|--|--|------------------|
| Local Agency Name | City of Moreno Valley | Phone Number | 951-413-3209 |
| Staff Contact | Julia Descoteaux, Project Planner | Email | juliad@moval.org |
| Mailing Address | 14177 Frederick Street Moreno Valley, CA 92552 | Case Type | GPA/ZC/PP/TPM |
| Local Agency Project No | Case: PEN20-0121 <u>IF, PEN20-0118 GRA,</u> <u>PEN20-0119 CZ, PEN20-0120 TPM,</u> | <input checked="" type="checkbox"/> General Plan / Specific Plan Amendment | |
| | | <input type="checkbox"/> Zoning Ordinance Amendment | |
| | | <input type="checkbox"/> Subdivision Parcel Map / Tentative Tract | |
| | | <input type="checkbox"/> Use Permit | |
| | | <input checked="" type="checkbox"/> Site Plan Review/Plot Plan | |
| | | <input type="checkbox"/> Other | |

PROJECT LOCATION

Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways

| | | | |
|-----------------------|---|---|-------------------|
| Street Address | XXXXX Alessandro Blvd., Moreno Valley, CA (south side of Alessandro Boulevard between Frederick Street and Graham Street) | | |
| Assessor's Parcel No. | 297-170-002 and -003 | Gross Parcel Size | 17.67 gross acres |
| Subdivision Name | | Nearest Airport and distance from Airport | 7,246.6 feet |
| Lot Number | | | |

PROJECT DESCRIPTION

If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed

| | |
|------------------------------|---|
| Existing Land Use (describe) | Please see the attached Project Description |
|------------------------------|---|

| | | | |
|---|--|--|--|
| Proposed Land Use (describe) | Please see the attached Project Description | | |
| For Residential Uses | Number of Parcels or Units on Site (exclude secondary units) _____ | | |
| For Other Land Uses (See Appendix C) | Hours of Operation | _____ | |
| | Number of People on Site | Maximum Number | _____ |
| | Method of Calculation | _____ | |
| Height Data | Site Elevation (above mean sea level) | Bldg #1 - 1,574 ft and Bldg #2 - 1,575 ft. | ft. |
| | Height of buildings or structures (from the ground) | 46 feet (Both Buildings 1 and 2) | ft. |
| Flight Hazards | Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight? | | <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No |
| | If yes, describe _____ _____ _____ | | |

- A. NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.
- B. REVIEW TIME:** Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.
- C. SUBMISSION PACKAGE:**
1. Completed ALUC Application Form
 1. ALUC fee payment
 1. Plans Package (24x36 folded) (site plans, floor plans, building elevations, grading plans, subdivision maps)
 1. Plans Package (8.5x11) (site plans, floor plans, building elevations, grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
 1. CD with digital files of the plans (pdf)
 1. Vicinity Map (8.5x11)
 1. Detailed project description
 1. Local jurisdiction project transmittal
 3. Gummed address labels for applicant/representative/property owner/local jurisdiction planner
 3. Gummed address labels of all surrounding property owners within a 300 foot radius of the project site. **(Only required if the project is scheduled for a public hearing Commission meeting)**

**COUNTY OF RIVERSIDE
AIRPORT LAND USE COMMISSION**

STAFF REPORT

AGENDA ITEM: 3.2

HEARING DATE: January 14, 2021

CASE NUMBER: ZAP1091PS20 – Mountain View Power Partners, LLC
(Representative: Dudek)

APPROVING JURISDICTION: County of Riverside

JURISDICTION CASE NO.: CZ2000032 (Change of Zone), WCS200003 (WECS Permit)

MAJOR ISSUES: None

RECOMMENDATION: Staff recommends that the proposed Change of Zone and WECS Permit be found CONSISTENT with the 2004 Riverside County Airport Land Use Compatibility Plan.

PROJECT DESCRIPTION: The applicant proposes to decommission and remove 93 existing commercial wind turbines (wind energy conversion systems, abbreviated as “WECS”) and install 16 new commercial wind turbines with a maximum height of 492 feet above ground level on 1,255 acres, as well decommission 3 existing meteorological towers and install 1 new 328 foot tall meteorological tower, turbine pad, safety features, transformers, electrical collection system, access roads, temporary laydown and parking. The applicant also proposes to change the zoning of 281 acres located in the southwest portion of the project from Rural Residential Zone (R-R) to Wind Energy Resource Zone (W-E)

Wind turbine heights are measured at top of blade in the “twelve o’clock position”.

PROJECT LOCATION: The project is located southerly of Interstate 10, easterly of Whitewater Cutoff/Tipton Road, westerly of Indian Canyon Drive, and northerly of State Highway 111. The project site is not located within an existing Airport Influence Area, as it lies approximately 5 miles northwesterly of the northwesterly terminus of the primary runway (Runway 13R-31L) at Palm Springs International Airport and 12 miles easterly of the easterly terminus of the runway at Banning Municipal Airport, but the project comes before the Airport Land Use Commission because of its inclusion of structures exceeding 200 feet in height.

BACKGROUND: As stated in Section 1.5.3.c of the Countywide Policies of the Riverside County Airport Land Use Compatibility Plan, “any proposal for construction or alteration of a structure (including antennas) taller than 200 feet above ground level at the site” requires referral to the Airport Land Use Commission for a determination of consistency with the Commission’s Plan prior to approval by the local jurisdiction. Such facilities also require notification to the FAA pursuant to Code of Federal Regulations Title 14, Chapter 1, Part 77, Paragraph 77.9.

The Riverside County Airport Land Use Compatibility Plan (RCALUCP) Policy Document, adopted on October 14, 2004, does not articulate specific procedures or criteria to guide the Airport Land Use Commission in evaluating such facilities. As such, the determination by the FAA OES (through the Form 7460-1 process) is pivotal in providing a basis for the ALUC's decision regarding such facilities.

On July 16 and October 28, 2020, the FAA OES issued Determinations of No Hazard to Air Navigation letters for Aeronautical Study Nos. 2020-WTW-8073-OE thru 2020-WTW-8082-OE, and 2020-WTW-2215-OE, 2020-WTW-2217-OE thru 2020-WTW-2221-OE for the proposed 16 wind turbines. The studies revealed that the proposed turbine structures does not exceed obstruction standards and would not be a hazard to air navigation provided that obstruction marking and lighting with paint and red lights is required in accordance with FAA Advisory Circular 70/7460-1 L Change 2, Obstruction Marking and Lighting, white paint/synchronized red lights – Chapters 4, 12 & 13 (Turbines).

The FAA OES also issued a Determination of No Hazard to Air Navigation letter for Aeronautical Study Number 2020-WTW-9038-OE for a meteorological tower, and the study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided that obstruction marking and lighting with paint and red lights is required in accordance with FAA Advisory Circular 70/7460-1 M Change 2, Obstruction Marking and Lighting, a med-dual system Chapters 4, 8 (M-Dual), & 15.

Change of Zone: The applicant proposes a change of zone for a 281 acre portion of the site from Rural Residential (R-R) to Wind Energy Resource Zone (W-E). The proposed amendment would be as, or more, consistent with the Compatibility Plan as long as the underlying developments is consistent with the compatibility criteria.

CONDITIONS:

1. The proposed wind turbines (“WECS”) shall not generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
2. Rotor blades shall utilize a flat or matte (non-glossy) finish so as to minimize the reflection of sunlight towards an aircraft engaged in an initial straight climb during takeoff or towards an aircraft engaged in a straight final approach toward a landing at an airport.
3. The WECS and any accessory uses shall not generate smoke or water vapor and shall be designed so as not to attract large concentrations of birds.
4. The combined height of each WECS and its foundation shall not exceed 492 feet above ground level (AGL).
5. This project has been evaluated by Airport Land Use Commission (ALUC) and the Federal Aviation Administration (FAA) for sixteen (16) wind turbines and one (1) meteorological tower. Any increase in number, height, or change in location of the turbines or meteorological tower, or any proposal for new structures taller than 200 feet from ground

level, will require subsequent submittal to, and review by, the ALUC and FAA.

6. The Federal Aviation Administration has conducted aeronautical studies of each proposed wind turbine (Aeronautical Study Nos. 2020-WTW-8073-OE thru 2020-WTW-8082-OE, and 2020-WTW-2215-OE, 2020-WTW-2217-OE thru 2020-WTW-2221-OE) and has specified that each of these structures shall be marked/lighted in accordance with FAA Advisory circular 70/7460-1 L Change 2, Obstruction Marking and Lighting, white paint/synchronized red lights – Chapters 4, 12, & 13 (Turbines), unless superseded by subsequent FAA determination(s) in writing.
7. The Federal Aviation Administration has conducted an aeronautical study for the proposed meteorological tower (Aeronautical Study No. 2020-WTW-9038-OE) and has specified that the structure shall be marked/lighted in accordance with FAA Advisory circular 70/7460-1 M Change 2, Obstruction Marking and Lighting, a med-dual system Chapters 4, 8 (M-Dual), & 15, unless superseded by subsequent FAA determination(s) in writing.
8. In order to ensure proper conspicuity of turbines at night during construction, all turbines should be lit with temporary lighting once they reach a height of 200 feet or greater until such time the permanent lighting configuration is turned on. As the height of the structure continues to increase, the temporary lighting should be relocated to the uppermost part of the structure. The temporary lighting may be turned off for periods when they would interfere with construction personnel. If practical, permanent obstruction lights should be installed and operated at each level as construction progresses. An FAA Type L-810 steady red light fixture shall be used to light the structure during the construction phase. If power is not available, turbines shall be lit with self-contained, solar powered LED steady red light fixture that meets the photometric requirements of an FAA Type L-810 lighting system. The lights should be positioned to ensure that a pilot has an unobstructed view of a least one light at each level. The use of NOTAM (D) to not light turbines within a project until the entire project has been completed is prohibited.
9. Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as normal operation is restored, notify the same number.
10. The maximum top point elevations specified below shall not be amended without further review by the Airport Land Use Commission and the Federal Aviation Administration; provided, however, that reduction in structure height or elevation shall not require further review by the Airport Land Use Commission.

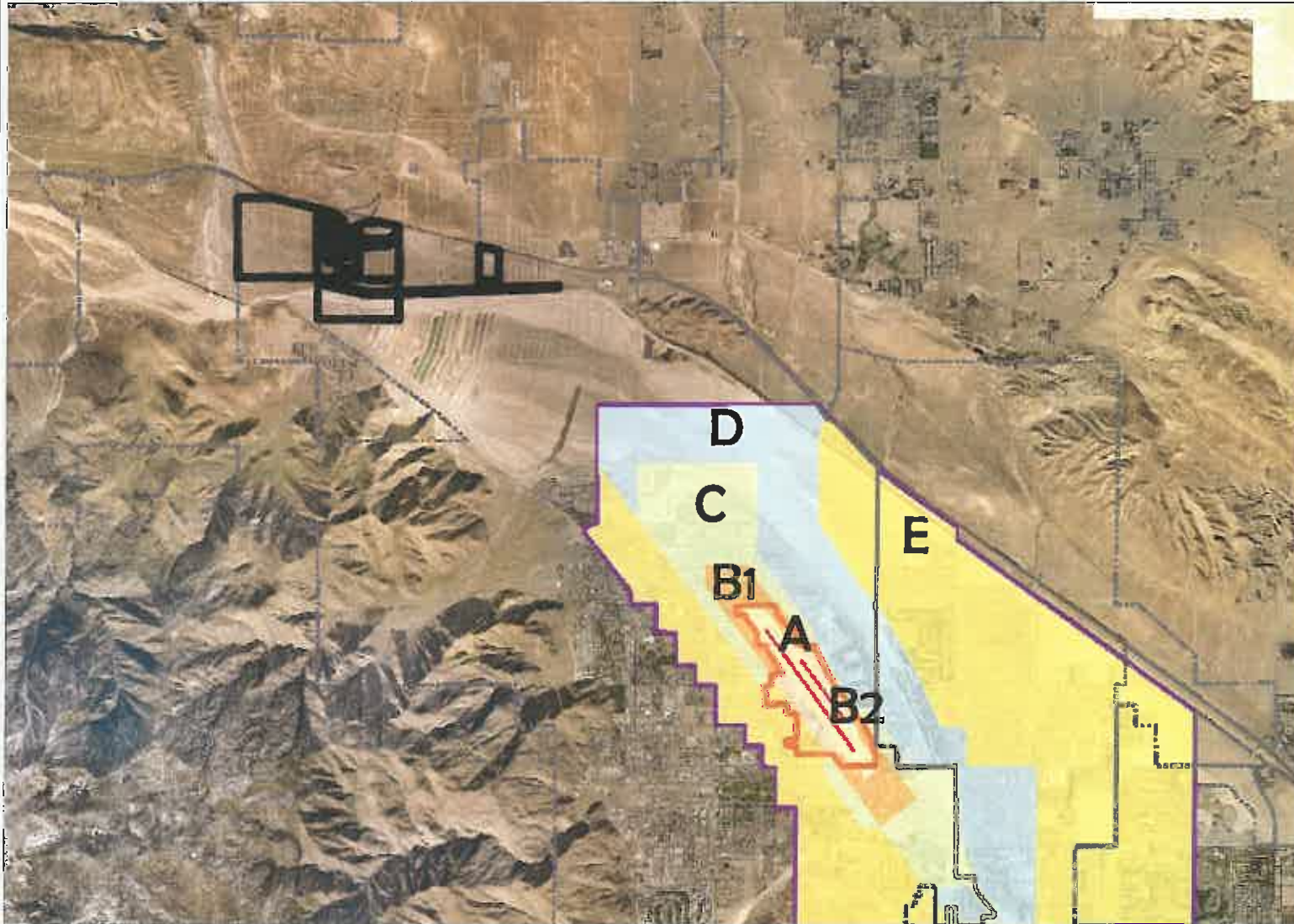
| <u>Turbine Number</u> | <u>Maximum Feet Above Mean Sea Level (AMSL)</u> |
|-----------------------|---|
| Turbine 1 | 1,732 |
| Turbine 2 | 1,712 |
| Turbine 3 | 1,692 |
| Turbine 4 | 1,662 |

| | |
|--------------------|-------|
| Turbine 5 | 1,692 |
| Turbine 6 | 1,672 |
| Turbine 7 | 1,652 |
| Turbine 8 | 1,632 |
| Turbine 9 | 1,637 |
| Turbine 10 | 1,620 |
| Turbine 11 | 1,582 |
| Turbine 12 | 1,502 |
| Turbine 13 | 1,492 |
| Turbine 14 | 1,487 |
| Turbine 15 | 1,482 |
| Turbine 16 | 1,474 |
| Metrological Tower | 1,501 |

11. Temporary construction equipment used during actual construction of the structures shall not exceed 492 feet in height and a maximum elevation (above mean sea level) not to exceed the above turbine table above (and 328 feet in height for the meteorological tower), unless separate notice is provided to the Federal Aviation Administration through the Form 7460-1 process.
12. Within five (5) days after construction reaches its greatest height, FAA Form 7460-2 (Part II), Notice of Actual Construction or Alteration, shall be completed by the project proponent or his/her designee and e-filed with the Federal Aviation Administration. (Go to <https://oeaaa.faa.gov> for instructions.) This requirement is also applicable in the event the project is abandoned or a decision is made not to construct the structure.
13. To the maximum extent possible, in compliance with FAA guidelines regarding lighting, mitigation measures shall be incorporated into the project that would minimize light pollution to the people on the ground.

Map My County Map

Los Angeles



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-FXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.



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Notes

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC8



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

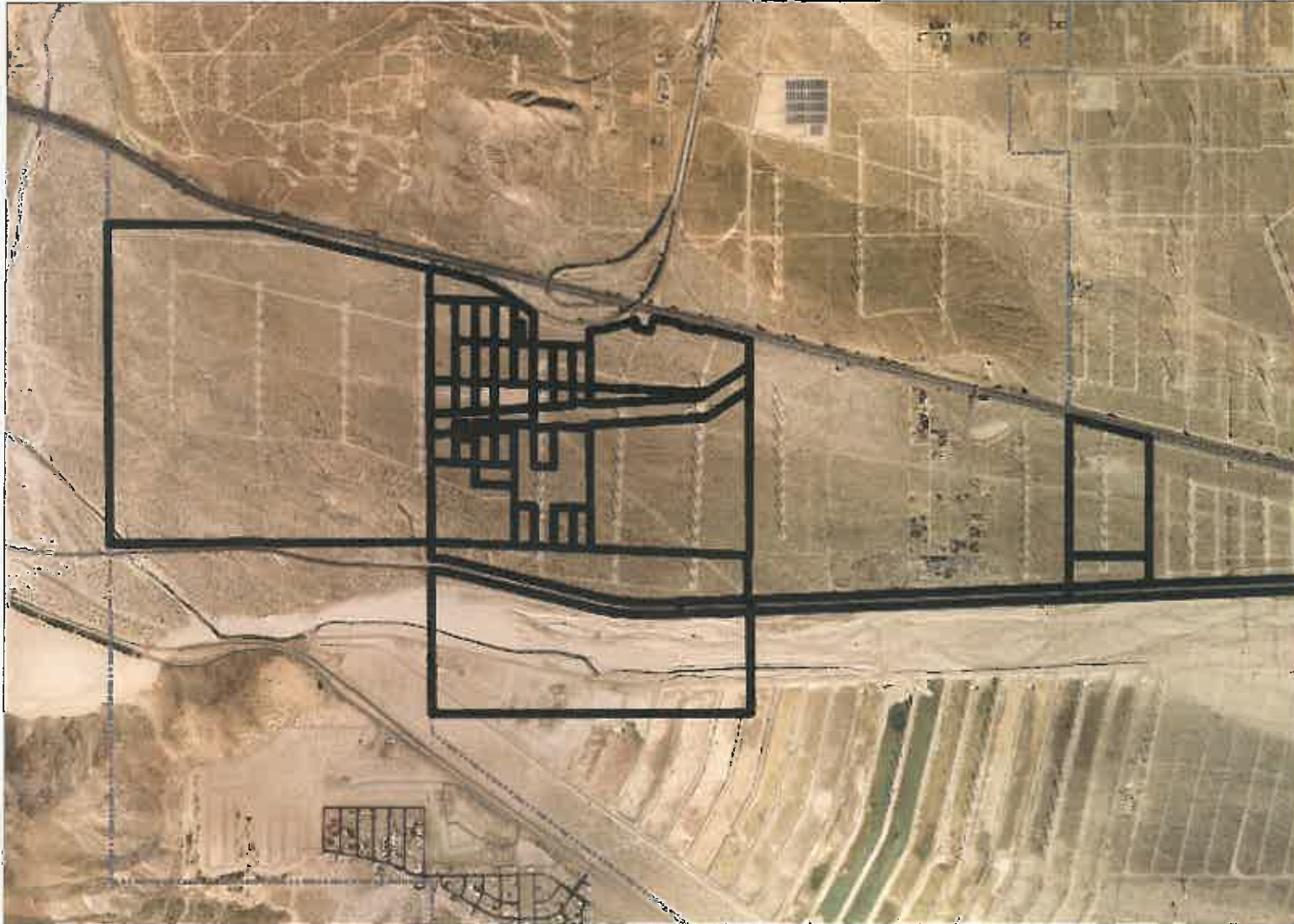


REPORT PRINTED ON... 12/4/2020 9:29:04 AM

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Notes

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.



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Notes



Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2020-WTW-8074-OE
 Prior Study No.
 2020-WTW-2207-OE

Issued Date: 10/28/2020

Mike Hughes
 Mountain View Power Partners, LLC
 AES Southland
 690 N. Studebaker Rd.
 Long Beach, CA 90803

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Wind Turbine WTG1
 Location: Palm Springs, CA
 Latitude: 33-54-58.89N NAD 83
 Longitude: 116-37-42.41W
 Heights: 1240 feet site elevation (SE)
 492 feet above ground level (AGL)
 1732 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

As a condition to this Determination, the structure is to be marked/lighted in accordance with FAA Advisory circular 70/7460-1 L Change 2, Obstruction Marking and Lighting, white paint/synchronized red lights - Chapters 4,12&13(Turbines).

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

See attachment for additional condition(s) or information.

This determination expires on 04/28/2022 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.

(b) extended, revised, or terminated by the issuing office.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates and heights. This determination is valid for coordinates within one (1) second latitude/longitude and up to the approved AMSL height listed above (provided the AGL height does not exceed 499 feet). If a certified 1A or 2C accuracy survey was required to mitigate an adverse effect, any change in coordinates or increase in height will require a new certified accuracy survey and may require a new aeronautical study.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

Additional wind turbines or met towers proposed in the future may cause a cumulative effect on the national airspace system. All information from submission of Supplemental Notice (7460-2 Part 2) will be considered the final data (including heights) for this structure. Any future construction or alteration, including but not limited to changes in heights, requires separate notice to the FAA.

Obstruction marking and lighting recommendations for wind turbine farms are based on the scheme for the entire project. ANY change to the height, location or number of turbines within this project will require a reanalysis of the marking and lighting recommendation for the entire project. In particular, the removal of previously planned or built turbines/turbine locations from the project will often result in a change in the marking/lighting recommendation for other turbines within the project. It is the proponent's responsibility to contact the FAA to discuss the process for developing a revised obstruction marking and lighting plan should this occur.

In order to ensure proper conspicuity of turbines at night during construction, all turbines should be lit with temporary lighting once they reach a height of 200 feet or greater until such time the permanent lighting configuration is turned on. As the height of the structure continues to increase, the temporary lighting should be relocated to the uppermost part of the structure. The temporary lighting may be turned off for periods when they would interfere with construction personnel. If practical, permanent obstruction lights should be installed and operated at each level as construction progresses. An FAA Type L-810 steady red light fixture shall be used to light the structure during the construction phase. If power is not available, turbines shall be lit with self-contained, solar powered LED steady red light fixture that meets the photometric requirements of an FAA Type L-810 lighting system. The lights should be positioned to ensure that a pilot has an unobstructed view of at least one light at each level. The use of a NOTAM (D) to not light turbines within a project until the entire project has been completed is prohibited.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (206) 231-2990, or paul.holmquist@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2020-WTW-8074-OE.

Signature Control No: 451008091-455207564

(DNE -WT)

Paul Holmquist
Specialist

Attachment(s)
Additional Information
Map(s)

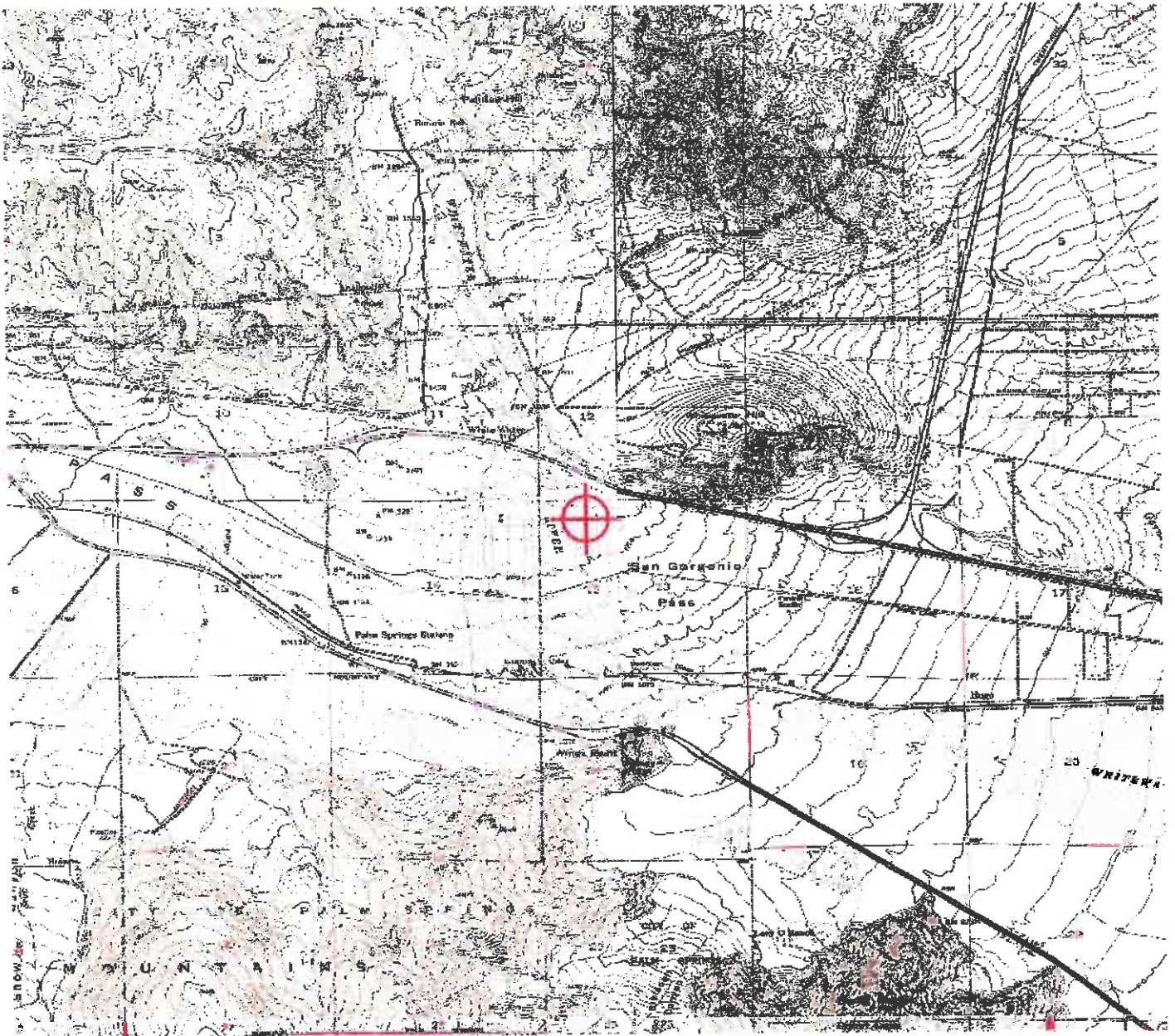
Additional information for ASN 2020-WTW-8074-OE

NO RADAR IMPACT

Analysis indicates that all 10 turbines studied in this project would be in the line of sight for the Palm Springs (PSP) Airport Surveillance Radar-9 (ASR-9); however, Air Traffic has determined this would not create a substantial adverse impact on their operations at this time.

OBSTRUCTION MARKING AND LIGHTING NOTE: A recommendation for white paint/synchronized red lights will be made for all turbines until such time as the proponent confirms that the layout is final (no changes, no additions, no removals) and all turbines can and will be built at their determined location and height. At that time, the proponent may contact this office and request a re-evaluation of the marking and lighting recommendations for the turbines within this project and a portion of the turbines may qualify for the removal of the lighting recommendation.

TOPO Map for ASN 2020-WTW-8074-OE



Sectional Map for ASN 2020-WTW-8074-OE





Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2020-WTW-8075-OE
Prior Study No.
2020-WTW-2208-OE

Issued Date: 10/28/2020

Mike Hughes
Mountain View Power Partners, LLC
AES Southland
690 N. Studebaker Rd.
Long Beach, CA 90803

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

| | |
|------------|---|
| Structure: | Wind Turbine WTG2 |
| Location: | Palm Springs, CA |
| Latitude: | 33-54-50.75N NAD 83 |
| Longitude: | 116-37-41.48W |
| Heights: | 1220 feet site elevation (SE) 492 feet above ground level (AGL) 1712 feet above mean sea level (AMSL) |

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

As a condition to this Determination, the structure is to be marked/lighted in accordance with FAA Advisory circular 70/7460-1 L Change 2, Obstruction Marking and Lighting, white paint/synchronized red lights - Chapters 4,12&13(Turbines).

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
 Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

See attachment for additional condition(s) or information.

This determination expires on 04/28/2022 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.

(b) extended, revised, or terminated by the issuing office.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates and heights. This determination is valid for coordinates within one (1) second latitude/longitude and up to the approved AMSL height listed above (provided the AGL height does not exceed 499 feet). If a certified 1A or 2C accuracy survey was required to mitigate an adverse effect, any change in coordinates or increase in height will require a new certified accuracy survey and may require a new aeronautical study.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

Additional wind turbines or met towers proposed in the future may cause a cumulative effect on the national airspace system. All information from submission of Supplemental Notice (7460-2 Part 2) will be considered the final data (including heights) for this structure. Any future construction or alteration, including but not limited to changes in heights, requires separate notice to the FAA.

Obstruction marking and lighting recommendations for wind turbine farms are based on the scheme for the entire project. ANY change to the height, location or number of turbines within this project will require a reanalysis of the marking and lighting recommendation for the entire project. In particular, the removal of previously planned or built turbines/turbine locations from the project will often result in a change in the marking/lighting recommendation for other turbines within the project. It is the proponent's responsibility to contact the FAA to discuss the process for developing a revised obstruction marking and lighting plan should this occur.

In order to ensure proper conspicuity of turbines at night during construction, all turbines should be lit with temporary lighting once they reach a height of 200 feet or greater until such time the permanent lighting configuration is turned on. As the height of the structure continues to increase, the temporary lighting should be relocated to the uppermost part of the structure. The temporary lighting may be turned off for periods when they would interfere with construction personnel. If practical, permanent obstruction lights should be installed and operated at each level as construction progresses. An FAA Type L-810 steady red light fixture shall be used to light the structure during the construction phase. If power is not available, turbines shall be lit with self-contained, solar powered LED steady red light fixture that meets the photometric requirements of an FAA Type L-810 lighting system. The lights should be positioned to ensure that a pilot has an unobstructed view of at least one light at each level. The use of a NOTAM (D) to not light turbines within a project until the entire project has been completed is prohibited.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (206) 231-2990, or paul.holmquist@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2020-WTW-8075-OE.

Signature Control No: 451008092-455207568

(DNE -WT)

Paul Holmquist
Specialist

Attachment(s)
Additional Information
Map(s)

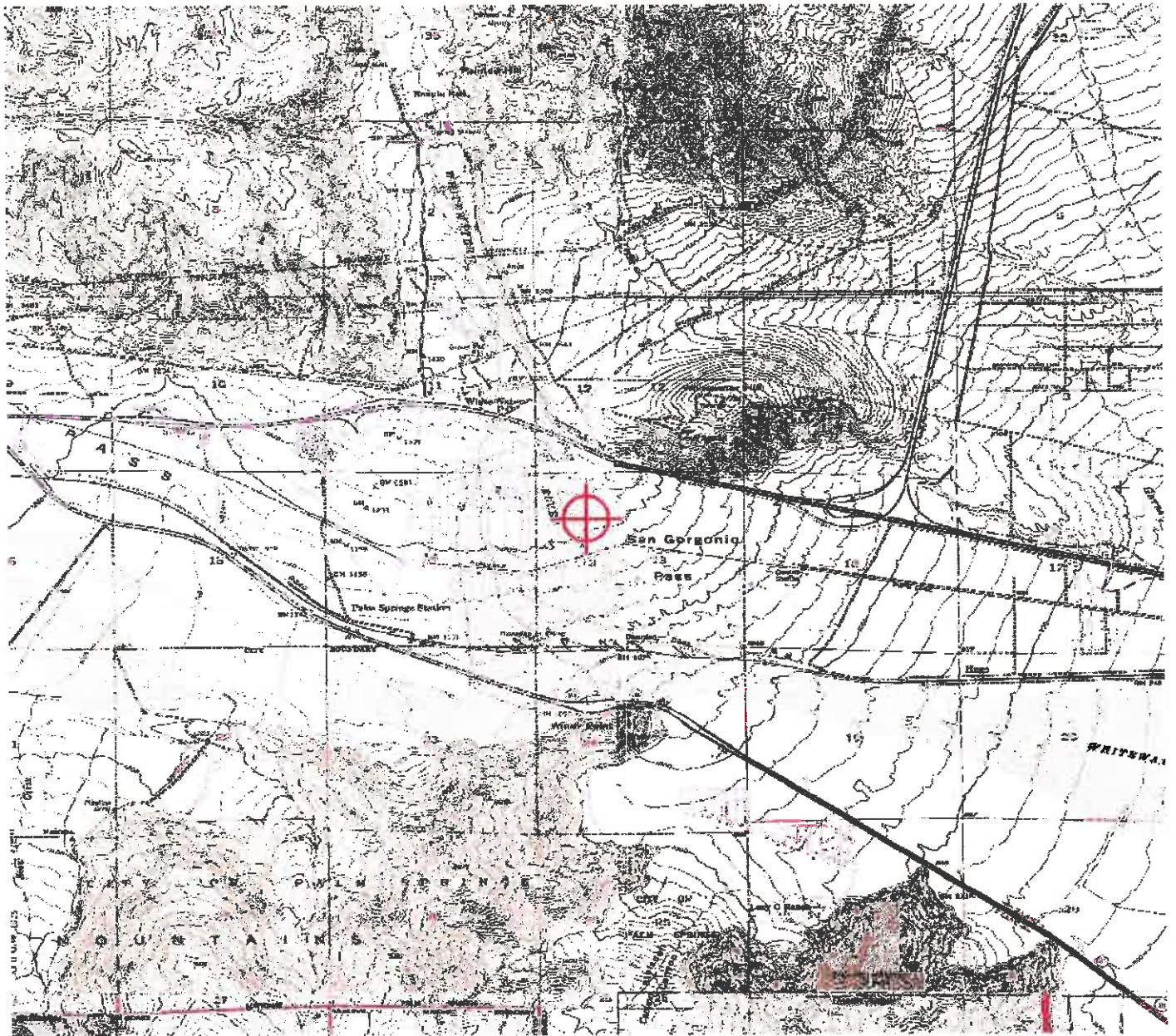
Additional information for ASN 2020-WTW-8075-OE

NO RADAR IMPACT

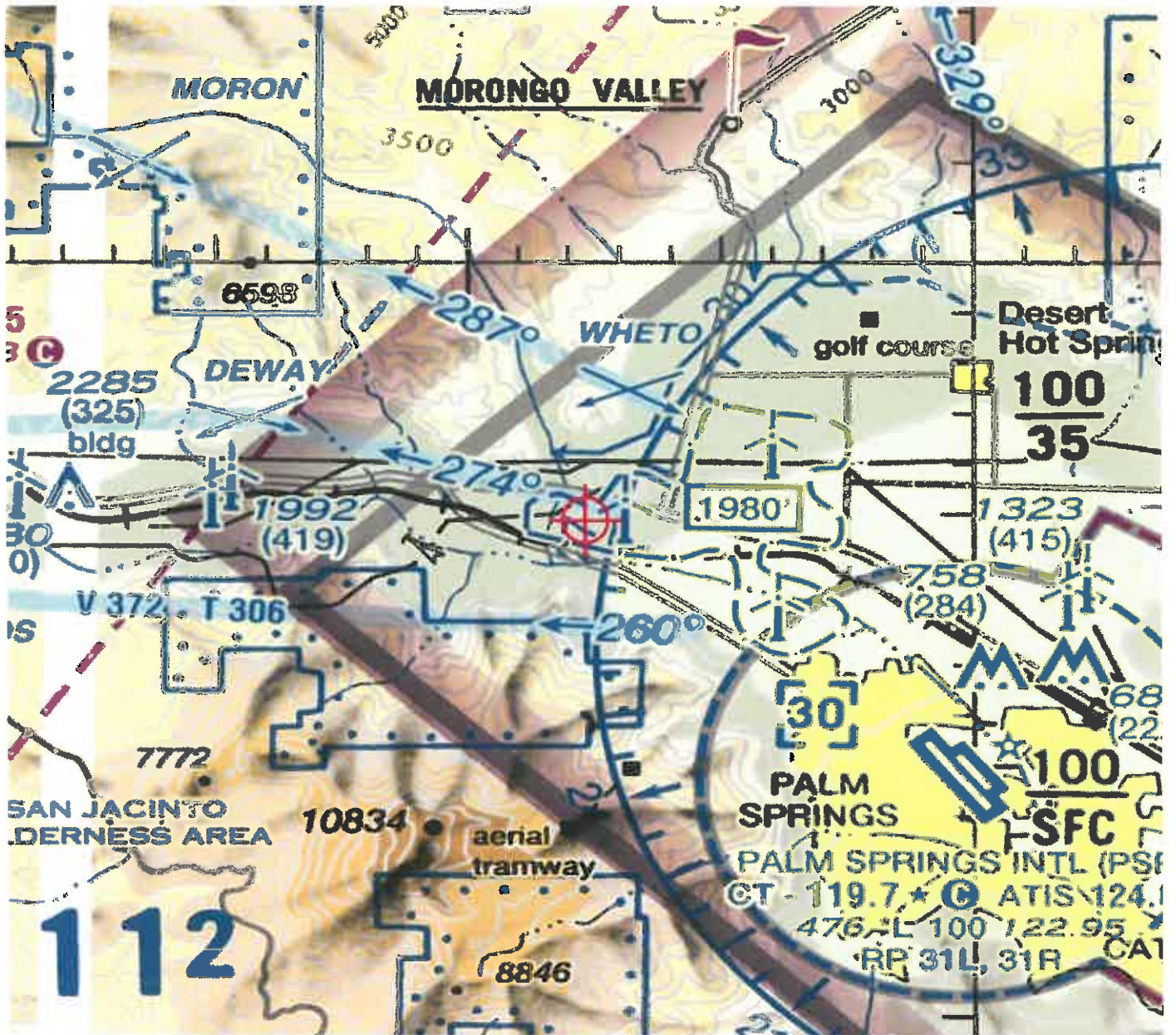
Analysis indicates that all 10 turbines studied in this project would be in the line of sight for the Palm Springs (PSP) Airport Surveillance Radar-9 (ASR-9); however, Air Traffic has determined this would not create a substantial adverse impact on their operations at this time.

OBSTRUCTION MARKING AND LIGHTING NOTE: A recommendation for white paint/synchronized red lights will be made for all turbines until such time as the proponent confirms that the layout is final (no changes, no additions, no removals) and all turbines can and will be built at their determined location and height. At that time, the proponent may contact this office and request a re-evaluation of the marking and lighting recommendations for the turbines within this project and a portion of the turbines may qualify for the removal of the lighting recommendation.

TOPO Map for ASN 2020-WTW-8075-OE



Sectional Map for ASN 2020-WTW-8075-OE





Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2020-WTW-8076-OE
 Prior Study No.
 2020-WTW-2209-OE

Issued Date: 10/28/2020

Mike Hughes
 Mountain View Power Partners, LLC
 AES Southland
 690 N. Studebaker Rd.
 Long Beach, CA 90803

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

| | |
|------------|---------------------------------------|
| Structure: | Wind Turbine WTG3 |
| Location: | Palm Springs, CA |
| Latitude: | 33-54-41.32N NAD 83 |
| Longitude: | 116-37-43.59W |
| Heights: | 1200 feet site elevation (SE) |
| | 492 feet above ground level (AGL) |
| | 1692 feet above mean sea level (AMSL) |

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

As a condition to this Determination, the structure is to be marked/lighted in accordance with FAA Advisory circular 70/7460-1 L Change 2, Obstruction Marking and Lighting, white paint/synchronized red lights - Chapters 4,12&13(Turbines).

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

See attachment for additional condition(s) or information.

This determination expires on 04/28/2022 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.

(b) extended, revised, or terminated by the issuing office.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates and heights. This determination is valid for coordinates within one (1) second latitude/longitude and up to the approved AMSL height listed above (provided the AGL height does not exceed 499 feet). If a certified 1A or 2C accuracy survey was required to mitigate an adverse effect, any change in coordinates or increase in height will require a new certified accuracy survey and may require a new aeronautical study.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

Additional wind turbines or met towers proposed in the future may cause a cumulative effect on the national airspace system. All information from submission of Supplemental Notice (7460-2 Part 2) will be considered the final data (including heights) for this structure. Any future construction or alteration, including but not limited to changes in heights, requires separate notice to the FAA.

Obstruction marking and lighting recommendations for wind turbine farms are based on the scheme for the entire project. ANY change to the height, location or number of turbines within this project will require a reanalysis of the marking and lighting recommendation for the entire project. In particular, the removal of previously planned or built turbines/turbine locations from the project will often result in a change in the marking/lighting recommendation for other turbines within the project. It is the proponent's responsibility to contact the FAA to discuss the process for developing a revised obstruction marking and lighting plan should this occur.

In order to ensure proper conspicuity of turbines at night during construction, all turbines should be lit with temporary lighting once they reach a height of 200 feet or greater until such time the permanent lighting configuration is turned on. As the height of the structure continues to increase, the temporary lighting should be relocated to the uppermost part of the structure. The temporary lighting may be turned off for periods when they would interfere with construction personnel. If practical, permanent obstruction lights should be installed and operated at each level as construction progresses. An FAA Type L-810 steady red light fixture shall be used to light the structure during the construction phase. If power is not available, turbines shall be lit with self-contained, solar powered LED steady red light fixture that meets the photometric requirements of an FAA Type L-810 lighting system. The lights should be positioned to ensure that a pilot has an unobstructed view of at least one light at each level. The use of a NOTAM (D) to not light turbines within a project until the entire project has been completed is prohibited.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (206) 231-2990, or paul.holmquist@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2020-WTW-8076-OE.

Signature Control No: 451008130-455207563

(DNE -WT)

Paul Holmquist
Specialist

Attachment(s)
Additional Information
Map(s)

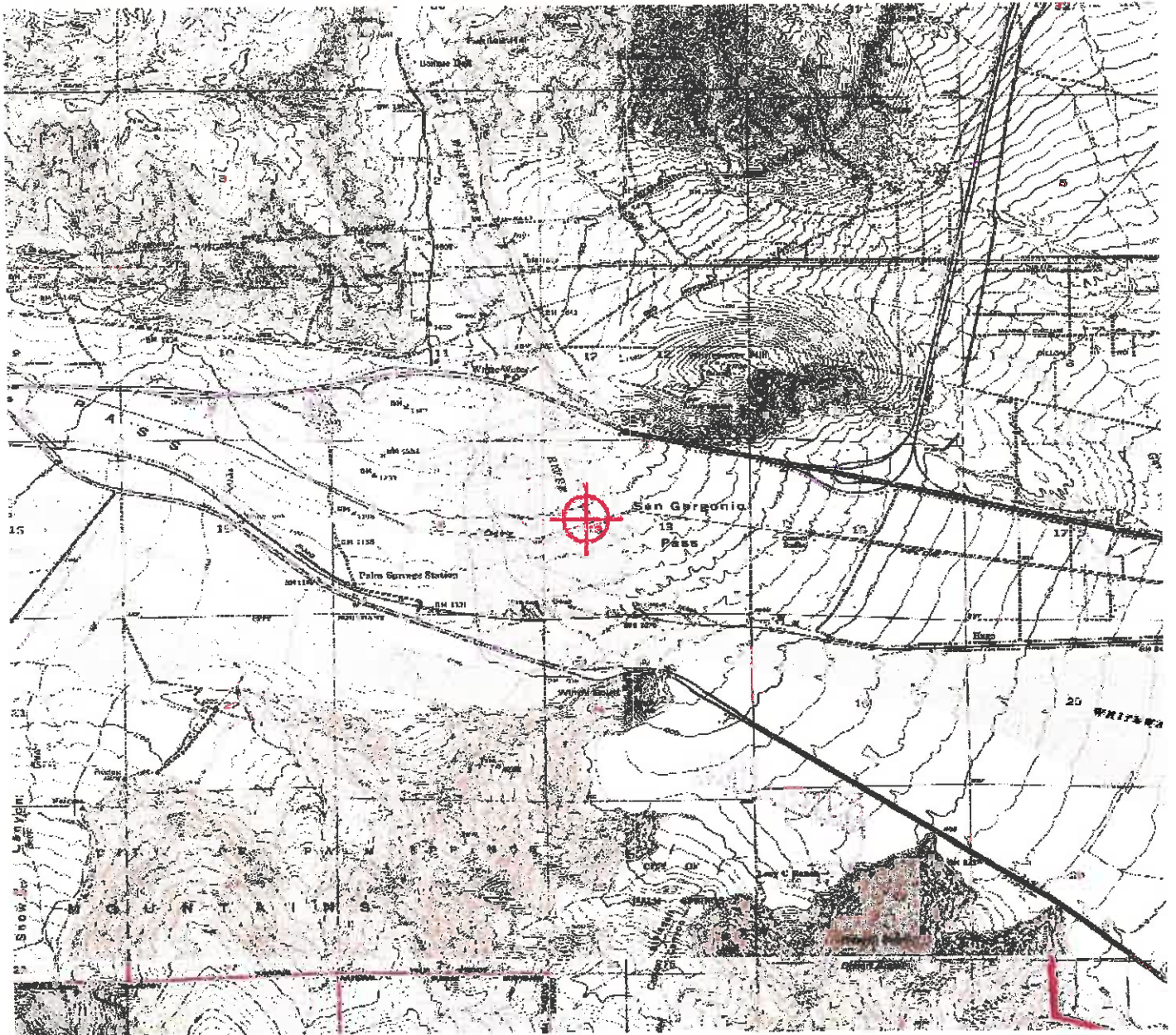
Additional information for ASN 2020-WTW-8076-OE

NO RADAR IMPACT

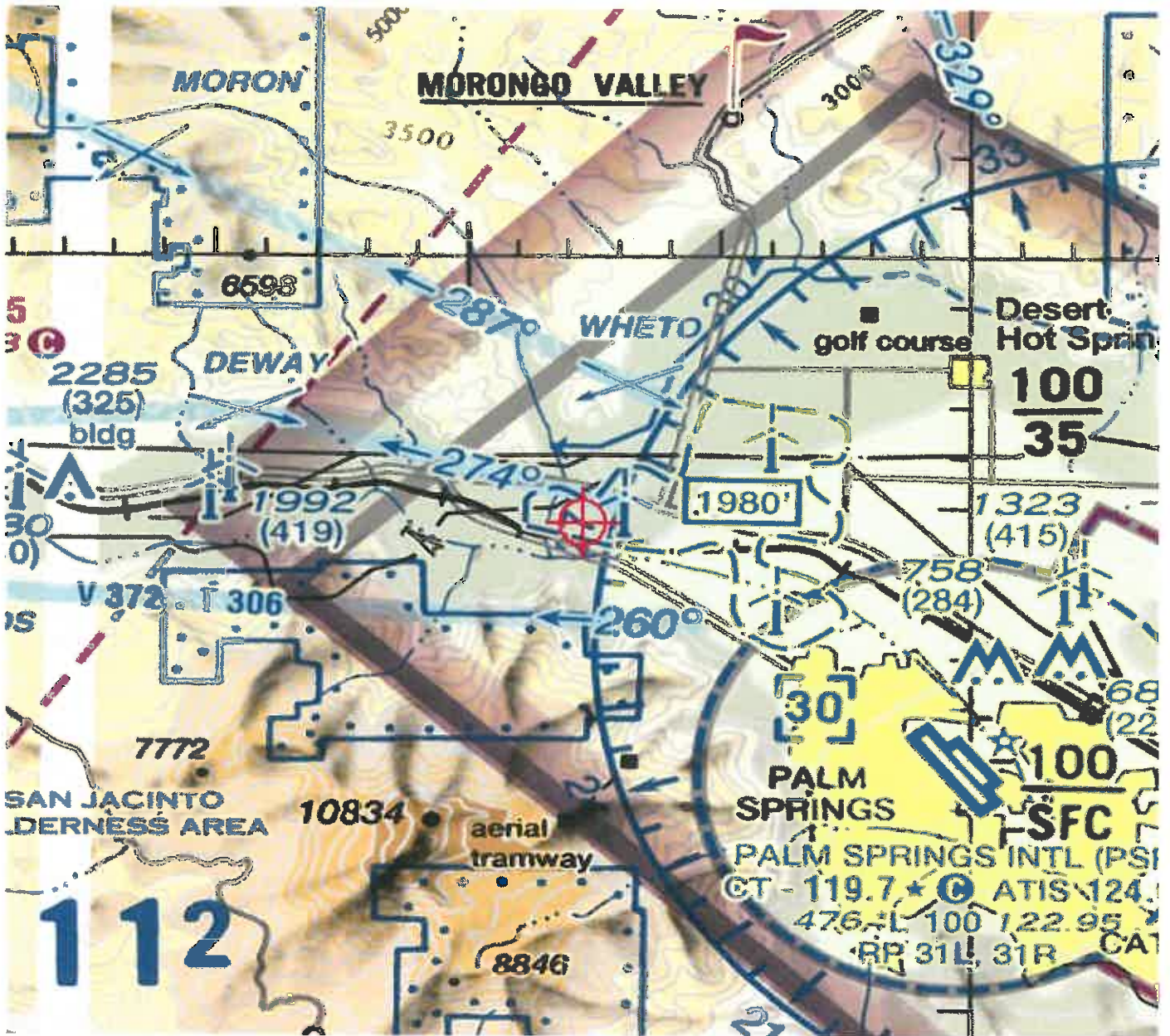
Analysis indicates that all 10 turbines studied in this project would be in the line of sight for the Palm Springs (PSP) Airport Surveillance Radar-9 (ASR-9); however, Air Traffic has determined this would not create a substantial adverse impact on their operations at this time.

OBSTRUCTION MARKING AND LIGHTING NOTE: A recommendation for white paint/synchronized red lights will be made for all turbines until such time as the proponent confirms that the layout is final (no changes, no additions, no removals) and all turbines can and will be built at their determined location and height. At that time, the proponent may contact this office and request a re-evaluation of the marking and lighting recommendations for the turbines within this project and a portion of the turbines may qualify for the removal of the lighting recommendation.

TOPO Map for ASN 2020-WTW-8076-OE



Sectional Map for ASN 2020-WTW-8076-OE





Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2020-WTW-8077-OE
 Prior Study No.
 2020-WTW-2210-OE

Issued Date: 10/28/2020

Mike Hughes
 Mountain View Power Partners, LLC
 AES Southland
 690 N. Studebaker Rd.
 Long Beach, CA 90803

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

| | |
|------------|---------------------------------------|
| Structure: | Wind Turbine WTG4 |
| Location: | Palm Springs, CA |
| Latitude: | 33-54-32.39N NAD 83 |
| Longitude: | 116-37-38.92W |
| Heights: | 1170 feet site elevation (SE) |
| | 492 feet above ground level (AGL) |
| | 1662 feet above mean sea level (AMSL) |

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

As a condition to this Determination, the structure is to be marked/lighted in accordance with FAA Advisory circular 70/7460-1 L Change 2, Obstruction Marking and Lighting, white paint/synchronized red lights - Chapters 4,12&13(Turbines).

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

See attachment for additional condition(s) or information.

This determination expires on 04/28/2022 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.

(b) extended, revised, or terminated by the issuing office.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates and heights. This determination is valid for coordinates within one (1) second latitude/longitude and up to the approved AMSL height listed above (provided the AGL height does not exceed 499 feet). If a certified 1A or 2C accuracy survey was required to mitigate an adverse effect, any change in coordinates or increase in height will require a new certified accuracy survey and may require a new aeronautical study.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

Additional wind turbines or met towers proposed in the future may cause a cumulative effect on the national airspace system. All information from submission of Supplemental Notice (7460-2 Part 2) will be considered the final data (including heights) for this structure. Any future construction or alteration, including but not limited to changes in heights, requires separate notice to the FAA.

Obstruction marking and lighting recommendations for wind turbine farms are based on the scheme for the entire project. ANY change to the height, location or number of turbines within this project will require a reanalysis of the marking and lighting recommendation for the entire project. In particular, the removal of previously planned or built turbines/turbine locations from the project will often result in a change in the marking/lighting recommendation for other turbines within the project. It is the proponent's responsibility to contact the FAA to discuss the process for developing a revised obstruction marking and lighting plan should this occur.

In order to ensure proper conspicuity of turbines at night during construction, all turbines should be lit with temporary lighting once they reach a height of 200 feet or greater until such time the permanent lighting configuration is turned on. As the height of the structure continues to increase, the temporary lighting should be relocated to the uppermost part of the structure. The temporary lighting may be turned off for periods when they would interfere with construction personnel. If practical, permanent obstruction lights should be installed and operated at each level as construction progresses. An FAA Type L-810 steady red light fixture shall be used to light the structure during the construction phase. If power is not available, turbines shall be lit with self-contained, solar powered LED steady red light fixture that meets the photometric requirements of an FAA Type L-810 lighting system. The lights should be positioned to ensure that a pilot has an unobstructed view of at least one light at each level. The use of a NOTAM (D) to not light turbines within a project until the entire project has been completed is prohibited.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (206) 231-2990, or paul.holmquist@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2020-WTW-8077-OE.

Signature Control No: 451008239-455207569

(DNE -WT)

Paul Holmquist
Specialist

Attachment(s)
Additional Information
Map(s)

Additional information for ASN 2020-WTW-8077-OE

NO RADAR IMPACT

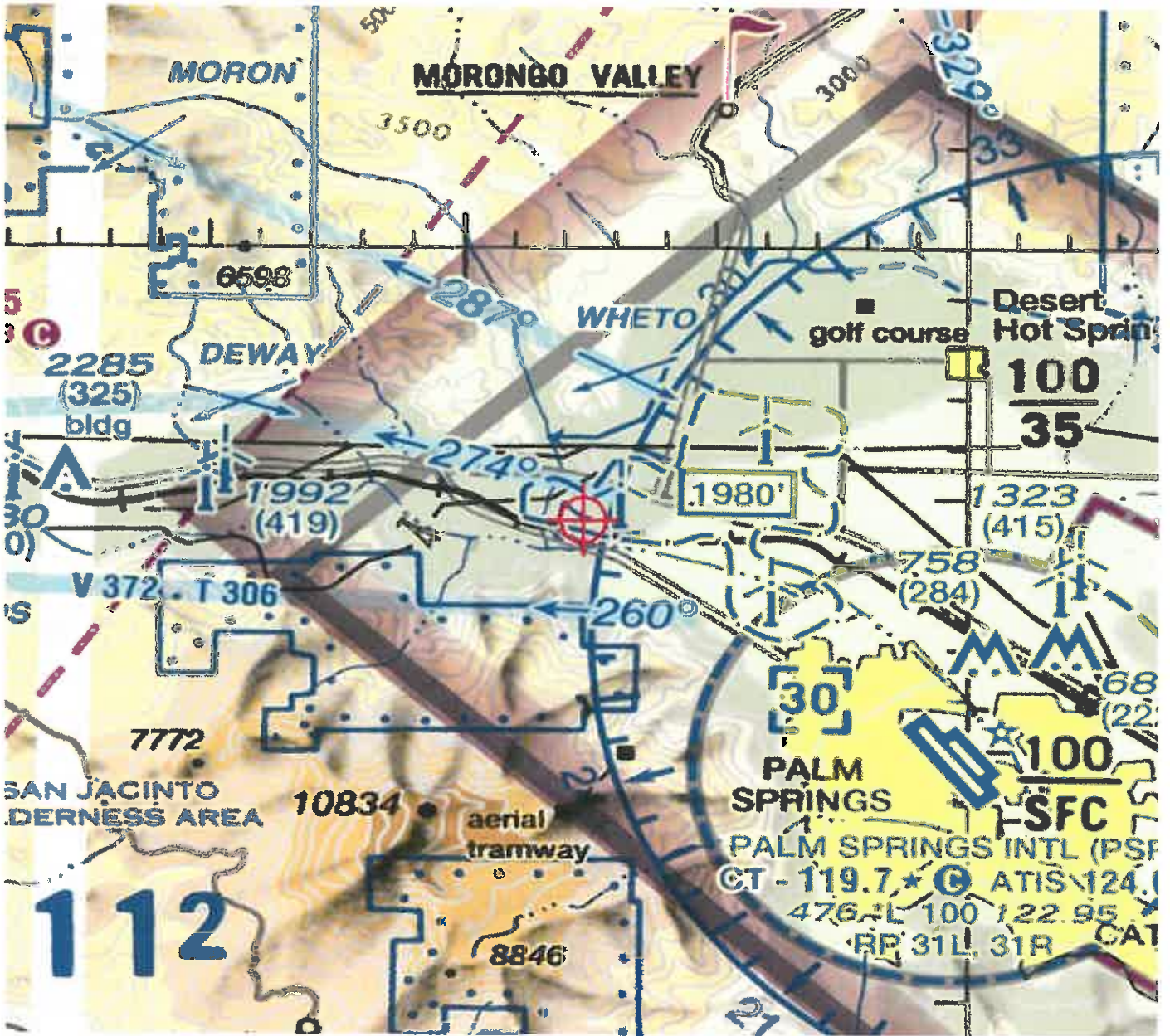
Analysis indicates that all 10 turbines studied in this project would be in the line of sight for the Palm Springs (PSP) Airport Surveillance Radar-9 (ASR-9); however, Air Traffic has determined this would not create a substantial adverse impact on their operations at this time.

OBSTRUCTION MARKING AND LIGHTING NOTE: A recommendation for white paint/synchronized red lights will be made for all turbines until such time as the proponent confirms that the layout is final (no changes, no additions, no removals) and all turbines can and will be built at their determined location and height. At that time, the proponent may contact this office and request a re-evaluation of the marking and lighting recommendations for the turbines within this project and a portion of the turbines may qualify for the removal of the lighting recommendation.

TOPO Map for ASN 2020-WTW-8077-OE



Sectional Map for ASN 2020-WTW-8077-OE





Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2020-WTW-8078-OE
 Prior Study No.
 2020-WTW-2211-OE

Issued Date: 10/28/2020

Mike Hughes
 Mountain View Power Partners, LLC
 AES Southland
 690 N. Studebaker Rd.
 Long Beach, CA 90803

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

| | |
|------------|---------------------------------------|
| Structure: | Wind Turbine WTG5 |
| Location: | Palm Springs, CA |
| Latitude: | 33-54-54.65N NAD 83 |
| Longitude: | 116-37-24.58W |
| Heights: | 1200 feet site elevation (SE) |
| | 492 feet above ground level (AGL) |
| | 1692 feet above mean sea level (AMSL) |

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

As a condition to this Determination, the structure is to be marked/lighted in accordance with FAA Advisory circular 70/7460-1 L Change 2, Obstruction Marking and Lighting, white paint/synchronized red lights - Chapters 4,12&13(Turbines).

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

See attachment for additional condition(s) or information.

This determination expires on 04/28/2022 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.

(b) extended, revised, or terminated by the issuing office.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates and heights. This determination is valid for coordinates within one (1) second latitude/longitude and up to the approved AMSL height listed above (provided the AGL height does not exceed 499 feet). If a certified 1A or 2C accuracy survey was required to mitigate an adverse effect, any change in coordinates or increase in height will require a new certified accuracy survey and may require a new aeronautical study.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

Additional wind turbines or met towers proposed in the future may cause a cumulative effect on the national airspace system. All information from submission of Supplemental Notice (7460-2 Part 2) will be considered the final data (including heights) for this structure. Any future construction or alteration, including but not limited to changes in heights, requires separate notice to the FAA.

Obstruction marking and lighting recommendations for wind turbine farms are based on the scheme for the entire project. ANY change to the height, location or number of turbines within this project will require a reanalysis of the marking and lighting recommendation for the entire project. In particular, the removal of previously planned or built turbines/turbine locations from the project will often result in a change in the marking/lighting recommendation for other turbines within the project. It is the proponent's responsibility to contact the FAA to discuss the process for developing a revised obstruction marking and lighting plan should this occur.

In order to ensure proper conspicuity of turbines at night during construction, all turbines should be lit with temporary lighting once they reach a height of 200 feet or greater until such time the permanent lighting configuration is turned on. As the height of the structure continues to increase, the temporary lighting should be relocated to the uppermost part of the structure. The temporary lighting may be turned off for periods when they would interfere with construction personnel. If practical, permanent obstruction lights should be installed and operated at each level as construction progresses. An FAA Type L-810 steady red light fixture shall be used to light the structure during the construction phase. If power is not available, turbines shall be lit with self-contained, solar powered LED steady red light fixture that meets the photometric requirements of an FAA Type L-810 lighting system. The lights should be positioned to ensure that a pilot has an unobstructed view of at least one light at each level. The use of a NOTAM (D) to not light turbines within a project until the entire project has been completed is prohibited.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (206) 231-2990, or paul.holmquist@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2020-WTW-8078-OE.

Signature Control No: 451008262-455207566

(DNE -WT)

Paul Holmquist
Specialist

Attachment(s)
Additional Information
Map(s)

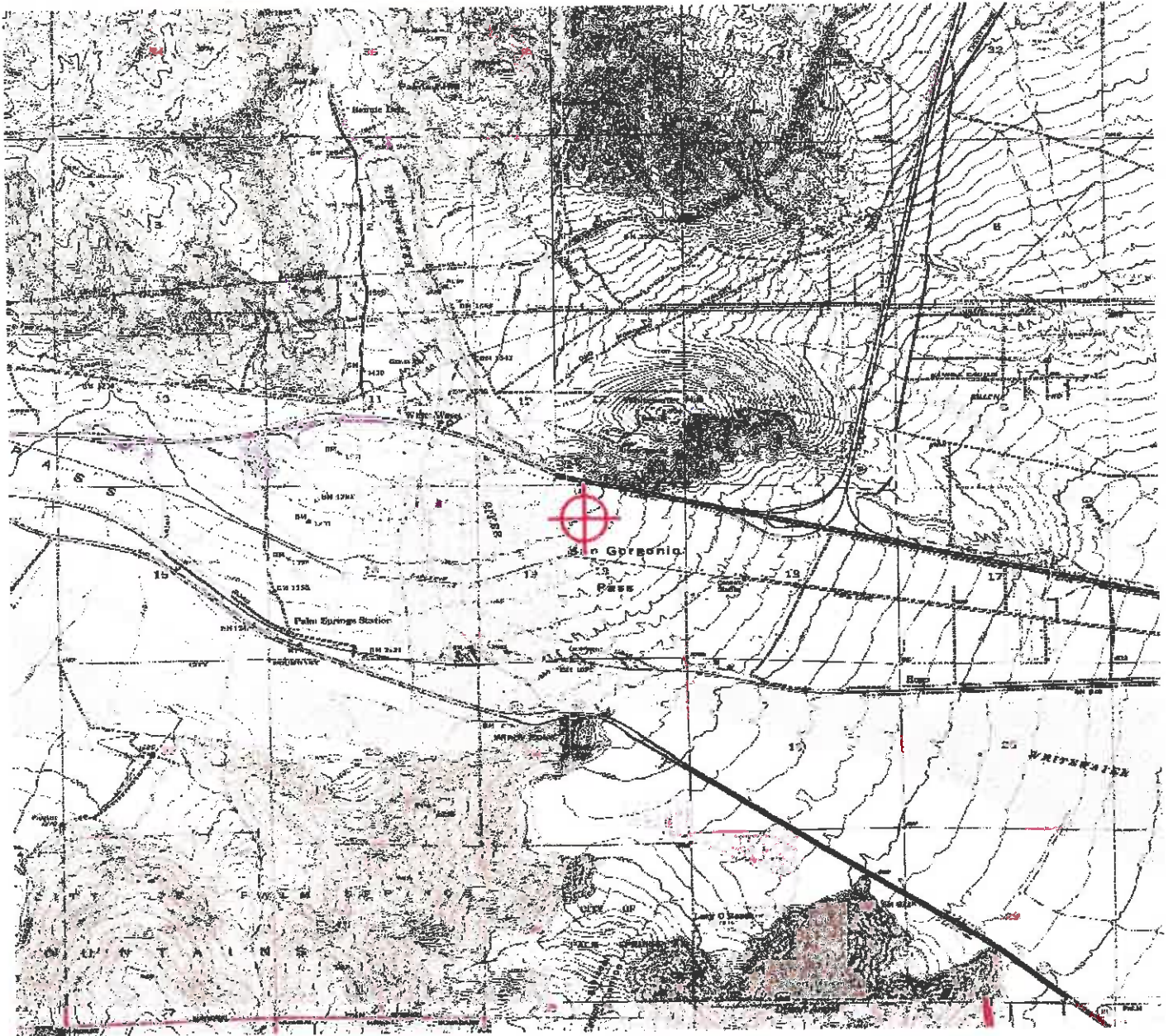
Additional information for ASN 2020-WTW-8078-OE

NO RADAR IMPACT

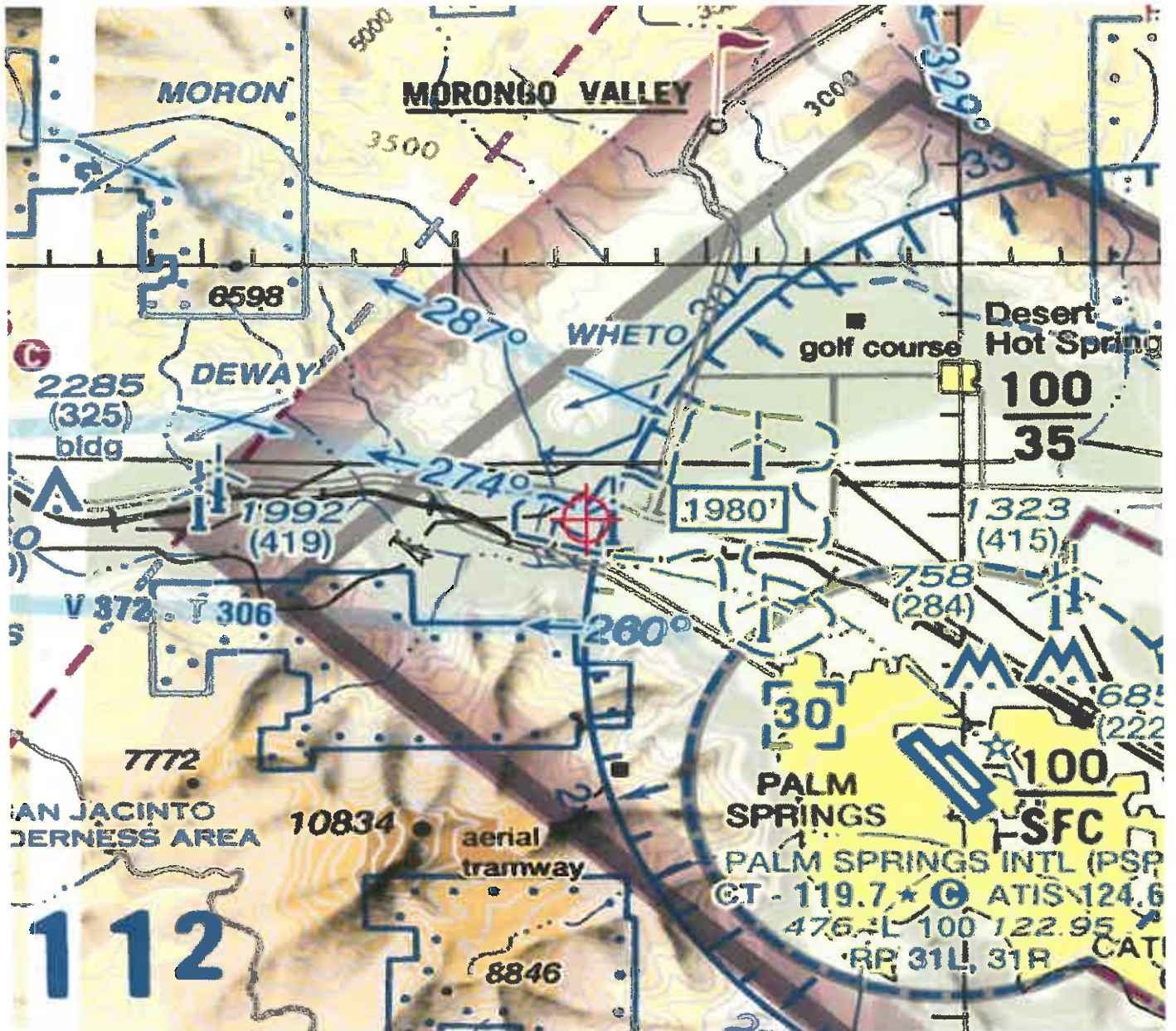
Analysis indicates that all 10 turbines studied in this project would be in the line of sight for the Palm Springs (PSP) Airport Surveillance Radar-9 (ASR-9); however, Air Traffic has determined this would not create a substantial adverse impact on their operations at this time.

OBSTRUCTION MARKING AND LIGHTING NOTE: A recommendation for white paint/synchronized red lights will be made for all turbines until such time as the proponent confirms that the layout is final (no changes, no additions, no removals) and all turbines can and will be built at their determined location and height. At that time, the proponent may contact this office and request a re-evaluation of the marking and lighting recommendations for the turbines within this project and a portion of the turbines may qualify for the removal of the lighting recommendation.

TOPO Map for ASN 2020-WTW-8078-OE



Sectional Map for ASN 2020-WTW-8078-OE





Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2020-WTW-8079-OE
 Prior Study No.
 2020-WTW-2212-OE

Issued Date: 10/28/2020

Mike Hughes
 Mountain View Power Partners, LLC
 AES Southland
 690 N. Studebaker Rd.
 Long Beach, CA 90803

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Wind Turbine WTG6
 Location: Palm Springs, CA
 Latitude: 33-54-46.88N NAD 83
 Longitude: 116-37-22.09W
 Heights: 1180 feet site elevation (SE)
 492 feet above ground level (AGL)
 1672 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

As a condition to this Determination, the structure is to be marked/lighted in accordance with FAA Advisory circular 70/7460-1 L Change 2, Obstruction Marking and Lighting, white paint/synchronized red lights - Chapters 4,12&13(Turbines).

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

See attachment for additional condition(s) or information.

This determination expires on 04/28/2022 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.

(b) extended, revised, or terminated by the issuing office.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates and heights. This determination is valid for coordinates within one (1) second latitude/longitude and up to the approved AMSL height listed above (provided the AGL height does not exceed 499 feet). If a certified 1A or 2C accuracy survey was required to mitigate an adverse effect, any change in coordinates or increase in height will require a new certified accuracy survey and may require a new aeronautical study.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

Additional wind turbines or met towers proposed in the future may cause a cumulative effect on the national airspace system. All information from submission of Supplemental Notice (7460-2 Part 2) will be considered the final data (including heights) for this structure. Any future construction or alteration, including but not limited to changes in heights, requires separate notice to the FAA.

Obstruction marking and lighting recommendations for wind turbine farms are based on the scheme for the entire project. ANY change to the height, location or number of turbines within this project will require a reanalysis of the marking and lighting recommendation for the entire project. In particular, the removal of previously planned or built turbines/turbine locations from the project will often result in a change in the marking/lighting recommendation for other turbines within the project. It is the proponent's responsibility to contact the FAA to discuss the process for developing a revised obstruction marking and lighting plan should this occur.

In order to ensure proper conspicuity of turbines at night during construction, all turbines should be lit with temporary lighting once they reach a height of 200 feet or greater until such time the permanent lighting configuration is turned on. As the height of the structure continues to increase, the temporary lighting should be relocated to the uppermost part of the structure. The temporary lighting may be turned off for periods when they would interfere with construction personnel. If practical, permanent obstruction lights should be installed and operated at each level as construction progresses. An FAA Type L-810 steady red light fixture shall be used to light the structure during the construction phase. If power is not available, turbines shall be lit with self-contained, solar powered LED steady red light fixture that meets the photometric requirements of an FAA Type L-810 lighting system. The lights should be positioned to ensure that a pilot has an unobstructed view of at least one light at each level. The use of a NOTAM (D) to not light turbines within a project until the entire project has been completed is prohibited.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (206) 231-2990, or paul.holmquist@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2020-WTW-8079-OE.

Signature Control No: 451008387-455207567

(DNE -WT)

Paul Holmquist
Specialist

Attachment(s)
Additional Information
Map(s)

Additional information for ASN 2020-WTW-8079-OE

NO RADAR IMPACT

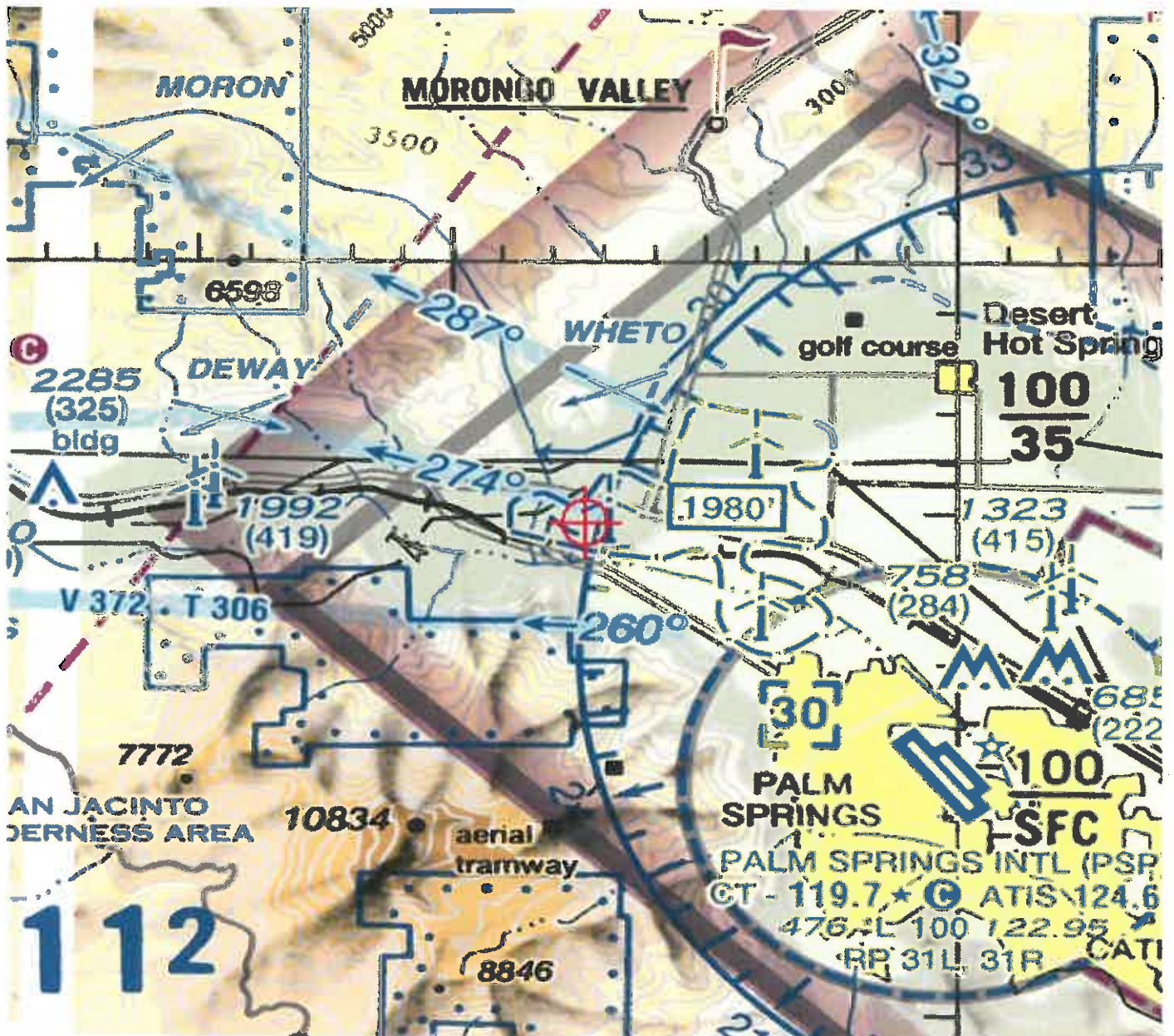
Analysis indicates that all 10 turbines studied in this project would be in the line of sight for the Palm Springs (PSP) Airport Surveillance Radar-9 (ASR-9); however, Air Traffic has determined this would not create a substantial adverse impact on their operations at this time.

OBSTRUCTION MARKING AND LIGHTING NOTE: A recommendation for white paint/synchronized red lights will be made for all turbines until such time as the proponent confirms that the layout is final (no changes, no additions, no removals) and all turbines can and will be built at their determined location and height. At that time, the proponent may contact this office and request a re-evaluation of the marking and lighting recommendations for the turbines within this project and a portion of the turbines may qualify for the removal of the lighting recommendation.

TOPO Map for ASN 2020-WTW-8079-OE



Sectional Map for ASN 2020-WTW-8079-OE





Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2020-WTW-8080-OE
 Prior Study No.
 2020-WTW-2213-OE

Issued Date: 10/28/2020

Mike Hughes
 Mountain View Power Partners, LLC
 AES Southland
 690 N. Studebaker Rd.
 Long Beach, CA 90803

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Wind Turbine WTG7
 Location: Palm Springs, CA
 Latitude: 33-54-38.09N NAD 83
 Longitude: 116-37-24.81W
 Heights: 1160 feet site elevation (SE)
 492 feet above ground level (AGL)
 1652 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

As a condition to this Determination, the structure is to be marked/lighted in accordance with FAA Advisory circular 70/7460-1 L Change 2, Obstruction Marking and Lighting, white paint/synchronized red lights - Chapters 4,12&13(Turbines).

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

See attachment for additional condition(s) or information.

This determination expires on 04/28/2022 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.

(b) extended, revised, or terminated by the issuing office.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates and heights. This determination is valid for coordinates within one (1) second latitude/longitude and up to the approved AMSL height listed above (provided the AGL height does not exceed 499 feet). If a certified 1A or 2C accuracy survey was required to mitigate an adverse effect, any change in coordinates or increase in height will require a new certified accuracy survey and may require a new aeronautical study.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

Additional wind turbines or met towers proposed in the future may cause a cumulative effect on the national airspace system. All information from submission of Supplemental Notice (7460-2 Part 2) will be considered the final data (including heights) for this structure. Any future construction or alteration, including but not limited to changes in heights, requires separate notice to the FAA.

Obstruction marking and lighting recommendations for wind turbine farms are based on the scheme for the entire project. ANY change to the height, location or number of turbines within this project will require a reanalysis of the marking and lighting recommendation for the entire project. In particular, the removal of previously planned or built turbines/turbine locations from the project will often result in a change in the marking/lighting recommendation for other turbines within the project. It is the proponent's responsibility to contact the FAA to discuss the process for developing a revised obstruction marking and lighting plan should this occur.

In order to ensure proper conspicuity of turbines at night during construction, all turbines should be lit with temporary lighting once they reach a height of 200 feet or greater until such time the permanent lighting configuration is turned on. As the height of the structure continues to increase, the temporary lighting should be relocated to the uppermost part of the structure. The temporary lighting may be turned off for periods when they would interfere with construction personnel. If practical, permanent obstruction lights should be installed and operated at each level as construction progresses. An FAA Type L-810 steady red light fixture shall be used to light the structure during the construction phase. If power is not available, turbines shall be lit with self-contained, solar powered LED steady red light fixture that meets the photometric requirements of an FAA Type L-810 lighting system. The lights should be positioned to ensure that a pilot has an unobstructed view of at least one light at each level. The use of a NOTAM (D) to not light turbines within a project until the entire project has been completed is prohibited.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (206) 231-2990, or paul.holmquist@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2020-WTW-8080-OE.

Signature Control No: 451008557-455207560

(DNE -WT)

Paul Holmquist
Specialist

Attachment(s)
Additional Information
Map(s)

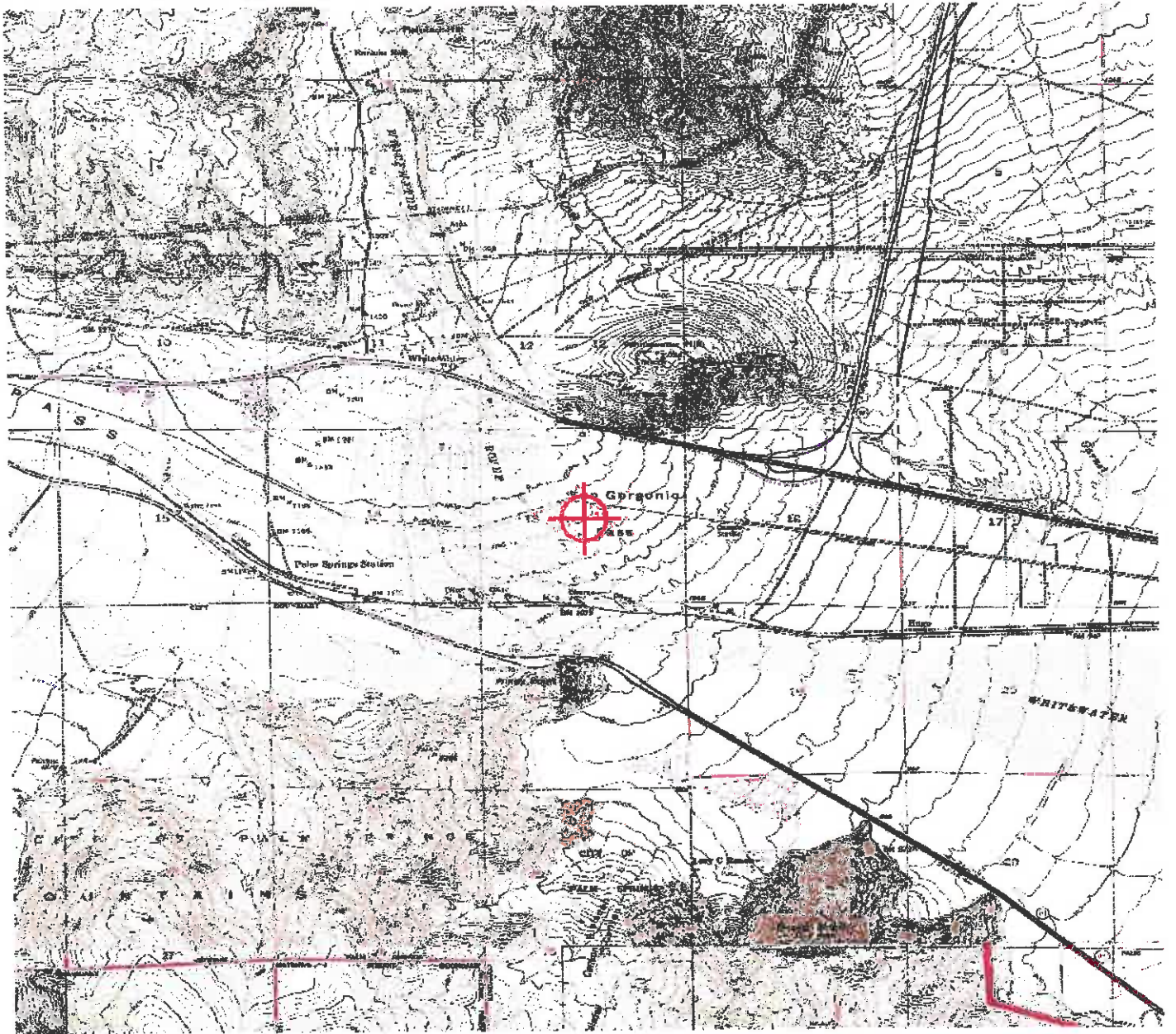
Additional information for ASN 2020-WTW-8080-OE

NO RADAR IMPACT

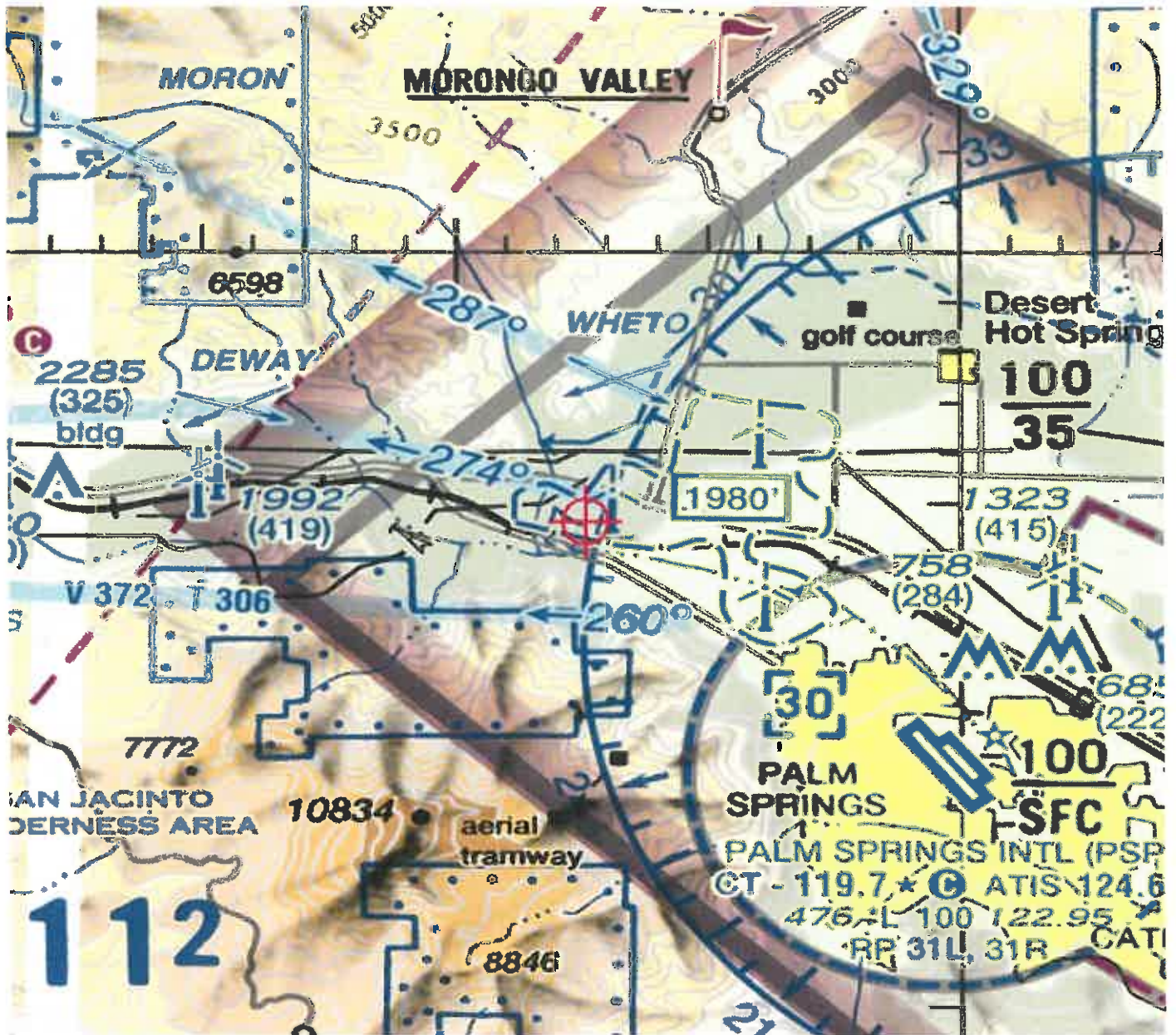
Analysis indicates that all 10 turbines studied in this project would be in the line of sight for the Palm Springs (PSP) Airport Surveillance Radar-9 (ASR-9); however, Air Traffic has determined this would not create a substantial adverse impact on their operations at this time.

OBSTRUCTION MARKING AND LIGHTING NOTE: A recommendation for white paint/synchronized red lights will be made for all turbines until such time as the proponent confirms that the layout is final (no changes, no additions, no removals) and all turbines can and will be built at their determined location and height. At that time, the proponent may contact this office and request a re-evaluation of the marking and lighting recommendations for the turbines within this project and a portion of the turbines may qualify for the removal of the lighting recommendation.

TOPO Map for ASN 2020-WTW-8080-OE



Sectional Map for ASN 2020-WTW-8080-OE





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 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2020-WTW-8081-OE
 Prior Study No.
 2020-WTW-2214-OE

Issued Date: 10/28/2020

Mike Hughes
 Mountain View Power Partners, LLC
 AES Southland
 690 N. Studebaker Rd.
 Long Beach, CA 90803

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Wind Turbine WTG8
 Location: Palm Springs, CA
 Latitude: 33-54-32.50N NAD 83
 Longitude: 116-37-18.47W
 Heights: 1140 feet site elevation (SE)
 492 feet above ground level (AGL)
 1632 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

As a condition to this Determination, the structure is to be marked/lighted in accordance with FAA Advisory circular 70/7460-1 L Change 2, Obstruction Marking and Lighting, white paint/synchronized red lights - Chapters 4,12&13(Turbines).

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

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- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

See attachment for additional condition(s) or information.

This determination expires on 04/28/2022 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.

(b) extended, revised, or terminated by the issuing office.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates and heights. This determination is valid for coordinates within one (1) second latitude/longitude and up to the approved AMSL height listed above (provided the AGL height does not exceed 499 feet). If a certified 1A or 2C accuracy survey was required to mitigate an adverse effect, any change in coordinates or increase in height will require a new certified accuracy survey and may require a new aeronautical study.

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This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (206) 231-2990, or paul.holmquist@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2020-WTW-8081-OE.

Signature Control No: 451008584-455207562

(DNE -WT)

Paul Holmquist
Specialist

Attachment(s)
Additional Information
Map(s)

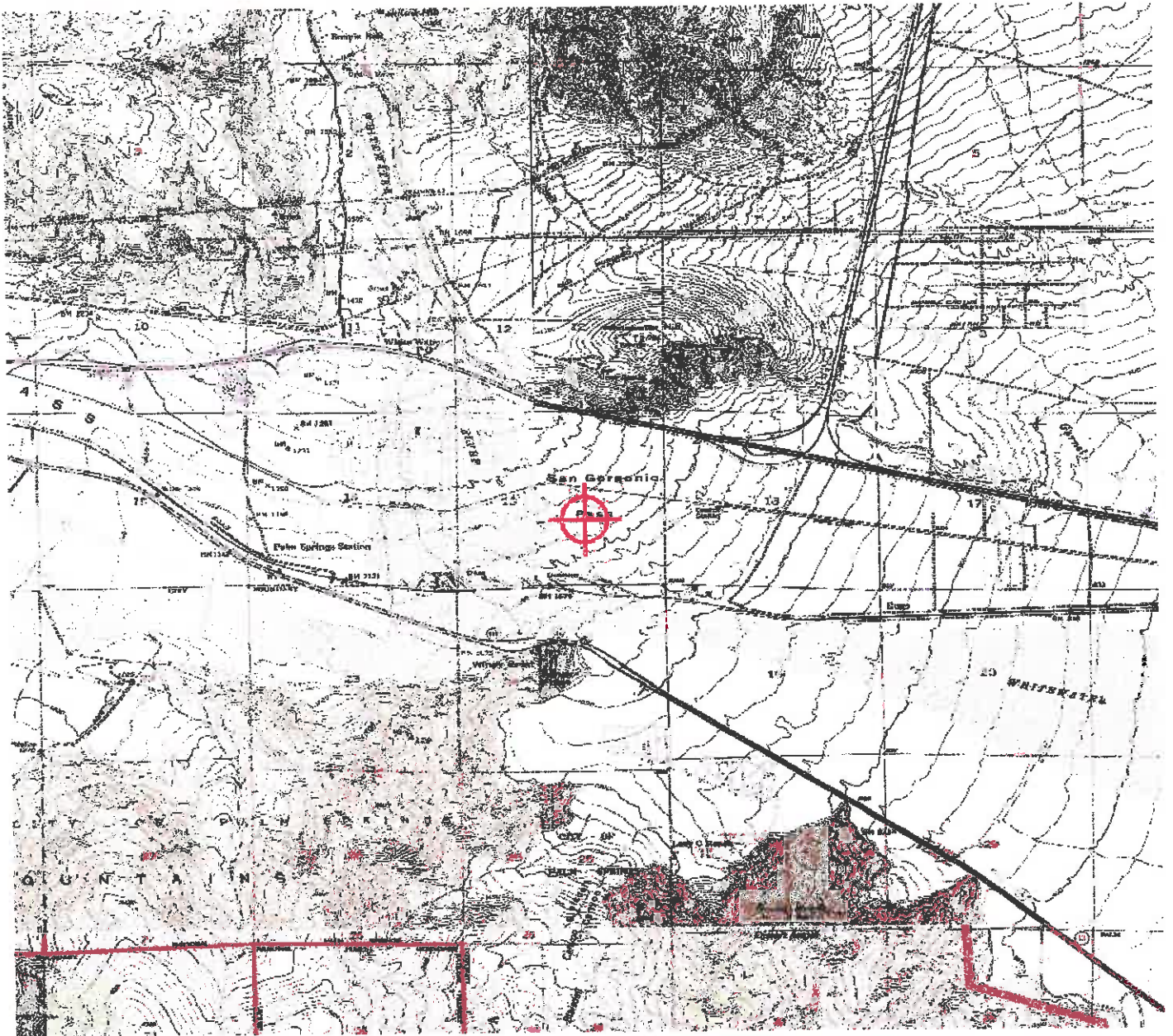
Additional information for ASN 2020-WTW-8081-OE

NO RADAR IMPACT

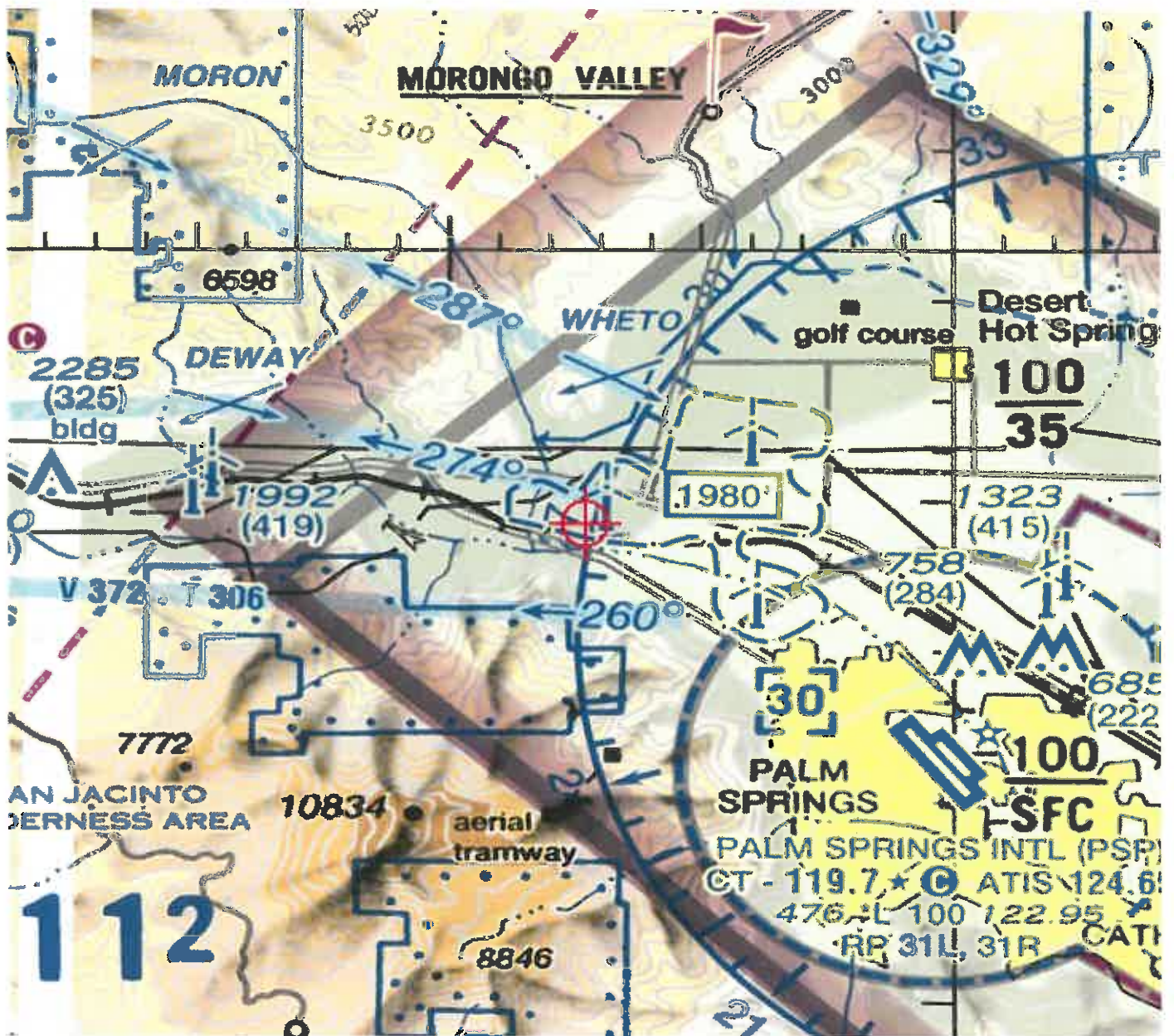
Analysis indicates that all 10 turbines studied in this project would be in the line of sight for the Palm Springs (PSP) Airport Surveillance Radar-9 (ASR-9); however, Air Traffic has determined this would not create a substantial adverse impact on their operations at this time.

OBSTRUCTION MARKING AND LIGHTING NOTE: A recommendation for white paint/synchronized red lights will be made for all turbines until such time as the proponent confirms that the layout is final (no changes, no additions, no removals) and all turbines can and will be built at their determined location and height. At that time, the proponent may contact this office and request a re-evaluation of the marking and lighting recommendations for the turbines within this project and a portion of the turbines may qualify for the removal of the lighting recommendation.

TOPO Map for ASN 2020-WTW-8081-OE



Sectional Map for ASN 2020-WTW-8081-OE





Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2020-WTW-2215-OE

Issued Date: 07/16/2020

Mike Hughes
 Mountain View Power Partners, LLC
 AES Southland
 690 N. Studebaker Rd.
 Long Beach, CA 90803

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Wind Turbine WTG9
 Location: Palm Springs, CA
 Latitude: 33-54-49.82N NAD 83
 Longitude: 116-37-01.56W
 Heights: 1145 feet site elevation (SE)
 492 feet above ground level (AGL)
 1637 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

As a condition to this Determination, the structure is to be marked/lighted in accordance with FAA Advisory circular 70/7460-1 L Change 2, Obstruction Marking and Lighting, white paint/synchronized red lights - Chapters 4,12&13(Turbines).

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

See attachment for additional condition(s) or information.

This determination expires on 01/16/2022 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.

(b) extended, revised, or terminated by the issuing office.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates and heights. This determination is valid for coordinates within one (1) second latitude/longitude and up to the approved AMSL height listed above (provided the AGL height does not exceed 499 feet). If a certified 1A or 2C accuracy survey was required to mitigate an adverse effect, any change in coordinates or increase in height will require a new certified accuracy survey and may require a new aeronautical study.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

Additional wind turbines or met towers proposed in the future may cause a cumulative effect on the national airspace system. All information from submission of Supplemental Notice (7460-2 Part 2) will be considered the final data (including heights) for this structure. Any future construction or alteration, including but not limited to changes in heights, requires separate notice to the FAA.

Obstruction marking and lighting recommendations for wind turbine farms are based on the scheme for the entire project. ANY change to the height, location or number of turbines within this project will require a reanalysis of the marking and lighting recommendation for the entire project. In particular, the removal of previously planned or built turbines/turbine locations from the project will often result in a change in the marking/lighting recommendation for other turbines within the project. It is the proponent's responsibility to contact the FAA to discuss the process for developing a revised obstruction marking and lighting plan should this occur.

In order to ensure proper conspicuity of turbines at night during construction, all turbines should be lit with temporary lighting once they reach a height of 200 feet or greater until such time the permanent lighting configuration is turned on. As the height of the structure continues to increase, the temporary lighting should be relocated to the uppermost part of the structure. The temporary lighting may be turned off for periods when they would interfere with construction personnel. If practical, permanent obstruction lights should be installed and operated at each level as construction progresses. An FAA Type L-810 steady red light fixture shall be used to light the structure during the construction phase. If power is not available, turbines shall be lit with self-contained, solar powered LED steady red light fixture that meets the photometric requirements of an FAA Type L-810 lighting system. The lights should be positioned to ensure that a pilot has an unobstructed view of at least one light at each level. The use of a NOTAM (D) to not light turbines within a project until the entire project has been completed is prohibited.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (206) 231-2990, or paul.holmquist@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2020-WTW-2215-OE.

Signature Control No: 435204277-445645780

(DNE -WT)

Paul Holmquist
Specialist

Attachment(s)
Additional Information
Map(s)

Additional information for ASN 2020-WTW-2215-OE

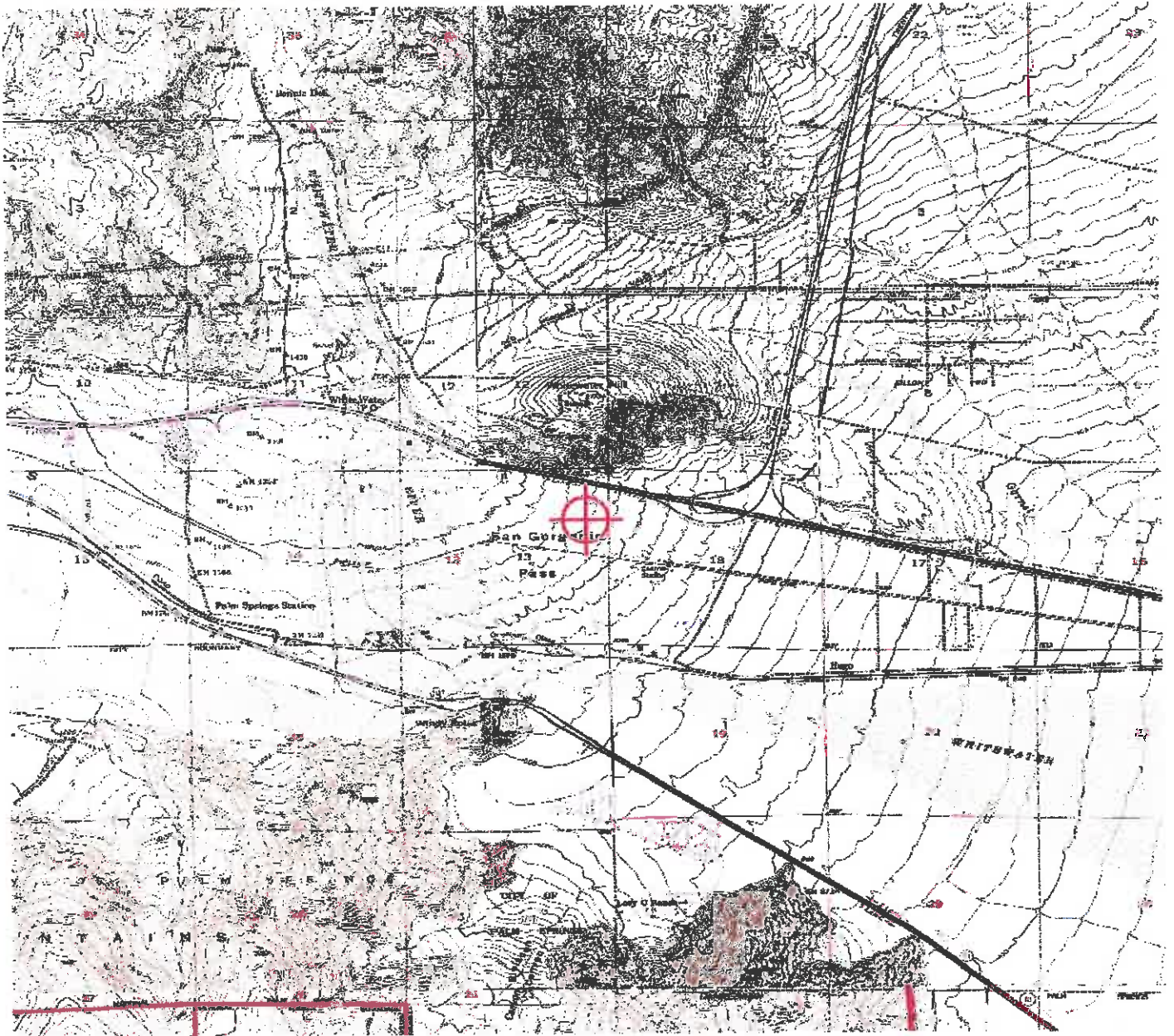
NO RADAR IMPACT

Analysis indicates that all 23 turbines studied in this project would be in the line of sight for the Palm Springs (PSP) Airport Surveillance Radar-9 (ASR-9); however, Air Traffic has determined this would not create a substantial adverse impact on their operations at this time.

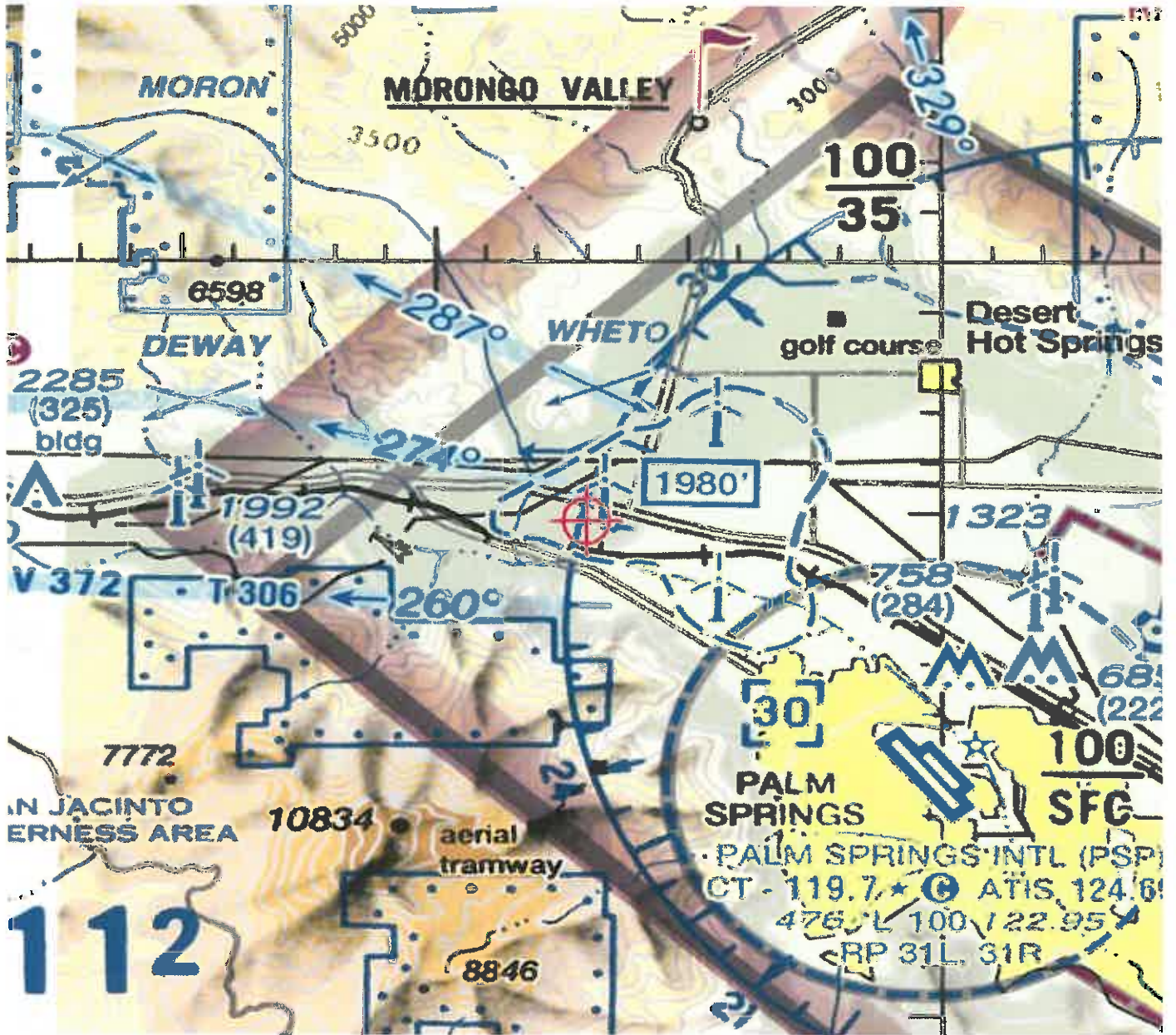
OBSTRUCTION MARKING AND LIGHTING NOTE:

A recommendation for white paint/synchronized red lights will be made for all turbines until such time as the proponent confirms that the layout is final (no changes, no additions, no removals) and all turbines can and will be built at their determined location and height. At that time, the proponent may contact this office and request a re-evaluation of the marking and lighting recommendations for the turbines within this project and a portion of the turbines may qualify for the removal of the lighting recommendation.

TOPO Map for ASN 2020-WTW-2215-OE



Sectional Map for ASN 2020-WTW-2215-OE





Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2020-WTW-8082-OE
 Prior Study No.
 2020-WTW-2216-OE

Issued Date: 10/28/2020

Mike Hughes
 Mountain View Power Partners, LLC
 AES Southland
 690 N. Studebaker Rd.
 Long Beach, CA 90803

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Wind Turbine WTG10
 Location: Palm Springs, CA
 Latitude: 33-54-41.54N NAD 83
 Longitude: 116-37-01.28W
 Heights: 1128 feet site elevation (SE)
 492 feet above ground level (AGL)
 1620 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

As a condition to this Determination, the structure is to be marked/lighted in accordance with FAA Advisory circular 70/7460-1 L Change 2, Obstruction Marking and Lighting, white paint/synchronized red lights - Chapters 4,12&13(Turbines).

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

See attachment for additional condition(s) or information.

This determination expires on 04/28/2022 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.

(b) extended, revised, or terminated by the issuing office.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates and heights. This determination is valid for coordinates within one (1) second latitude/longitude and up to the approved AMSL height listed above (provided the AGL height does not exceed 499 feet). If a certified 1A or 2C accuracy survey was required to mitigate an adverse effect, any change in coordinates or increase in height will require a new certified accuracy survey and may require a new aeronautical study.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

Additional wind turbines or met towers proposed in the future may cause a cumulative effect on the national airspace system. All information from submission of Supplemental Notice (7460-2 Part 2) will be considered the final data (including heights) for this structure. Any future construction or alteration, including but not limited to changes in heights, requires separate notice to the FAA.

Obstruction marking and lighting recommendations for wind turbine farms are based on the scheme for the entire project. ANY change to the height, location or number of turbines within this project will require a reanalysis of the marking and lighting recommendation for the entire project. In particular, the removal of previously planned or built turbines/turbine locations from the project will often result in a change in the marking/lighting recommendation for other turbines within the project. It is the proponent's responsibility to contact the FAA to discuss the process for developing a revised obstruction marking and lighting plan should this occur.

In order to ensure proper conspicuity of turbines at night during construction, all turbines should be lit with temporary lighting once they reach a height of 200 feet or greater until such time the permanent lighting configuration is turned on. As the height of the structure continues to increase, the temporary lighting should be relocated to the uppermost part of the structure. The temporary lighting may be turned off for periods when they would interfere with construction personnel. If practical, permanent obstruction lights should be installed and operated at each level as construction progresses. An FAA Type L-810 steady red light fixture shall be used to light the structure during the construction phase. If power is not available, turbines shall be lit with self-contained, solar powered LED steady red light fixture that meets the photometric requirements of an FAA Type L-810 lighting system. The lights should be positioned to ensure that a pilot has an unobstructed view of at least one light at each level. The use of a NOTAM (D) to not light turbines within a project until the entire project has been completed is prohibited.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (206) 231-2990, or paul.holmquist@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2020-WTW-8082-OE.

Signature Control No: 451008596-455207561

(DNE -WT)

Paul Holmquist
Specialist

Attachment(s)
Additional Information
Map(s)

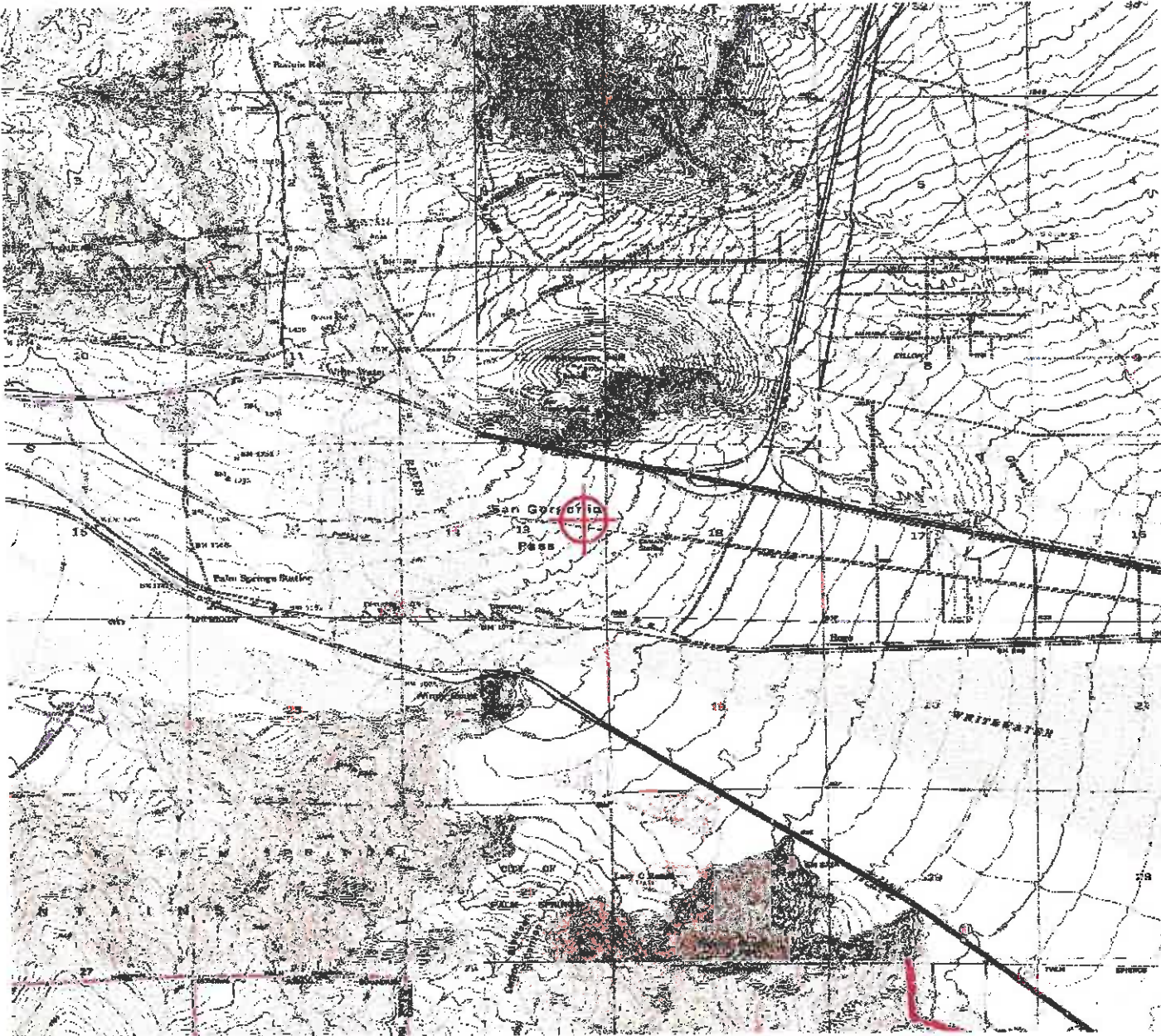
Additional information for ASN 2020-WTW-8082-OE

NO RADAR IMPACT

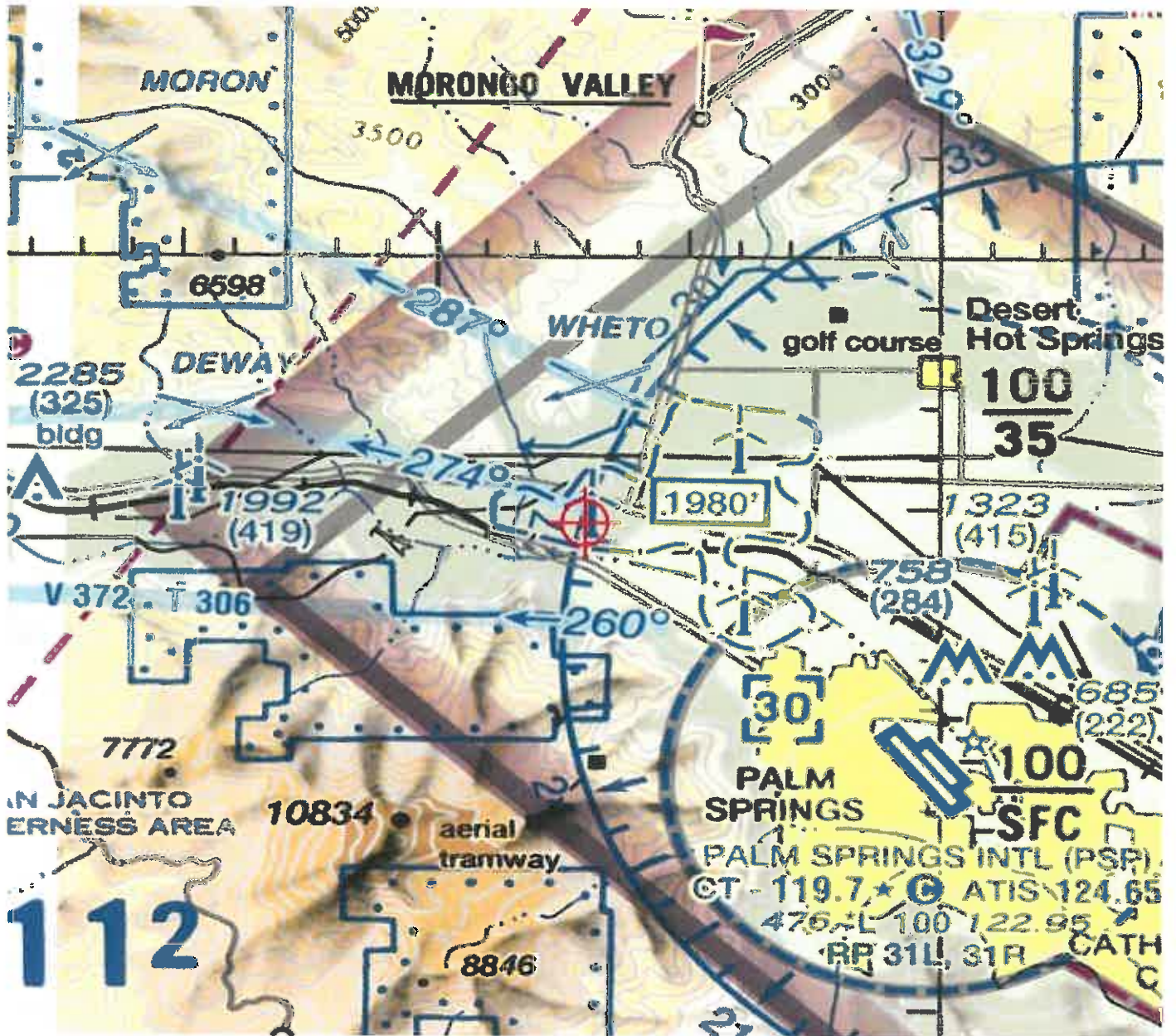
Analysis indicates that all 10 turbines studied in this project would be in the line of sight for the Palm Springs (PSP) Airport Surveillance Radar-9 (ASR-9); however, Air Traffic has determined this would not create a substantial adverse impact on their operations at this time.

OBSTRUCTION MARKING AND LIGHTING NOTE: A recommendation for white paint/synchronized red lights will be made for all turbines until such time as the proponent confirms that the layout is final (no changes, no additions, no removals) and all turbines can and will be built at their determined location and height. At that time, the proponent may contact this office and request a re-evaluation of the marking and lighting recommendations for the turbines within this project and a portion of the turbines may qualify for the removal of the lighting recommendation.

TOPO Map for ASN 2020-WTW-8082-OE



Sectional Map for ASN 2020-WTW-8082-OE





Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2020-WTW-2217-OE

Issued Date: 07/16/2020

Mike Hughes
 Mountain View Power Partners, LLC
 AES Southland
 690 N. Studebaker Rd.
 Long Beach, CA 90803

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Wind Turbine WTG11
 Location: Palm Springs, CA
 Latitude: 33-54-28.33N NAD 83
 Longitude: 116-37-01.38W
 Heights: 1090 feet site elevation (SE)
 492 feet above ground level (AGL)
 1582 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

As a condition to this Determination, the structure is to be marked/lighted in accordance with FAA Advisory circular 70/7460-1 L Change 2, Obstruction Marking and Lighting, white paint/synchronized red lights - Chapters 4,12&13(Turbines).

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

See attachment for additional condition(s) or information.

This determination expires on 01/16/2022 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.

(b) extended, revised, or terminated by the issuing office.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates and heights. This determination is valid for coordinates within one (1) second latitude/longitude and up to the approved AMSL height listed above (provided the AGL height does not exceed 499 feet). If a certified 1A or 2C accuracy survey was required to mitigate an adverse effect, any change in coordinates or increase in height will require a new certified accuracy survey and may require a new aeronautical study.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

Additional wind turbines or met towers proposed in the future may cause a cumulative effect on the national airspace system. All information from submission of Supplemental Notice (7460-2 Part 2) will be considered the final data (including heights) for this structure. Any future construction or alteration, including but not limited to changes in heights, requires separate notice to the FAA.

Obstruction marking and lighting recommendations for wind turbine farms are based on the scheme for the entire project. ANY change to the height, location or number of turbines within this project will require a reanalysis of the marking and lighting recommendation for the entire project. In particular, the removal of previously planned or built turbines/turbine locations from the project will often result in a change in the marking/lighting recommendation for other turbines within the project. It is the proponent's responsibility to contact the FAA to discuss the process for developing a revised obstruction marking and lighting plan should this occur.

In order to ensure proper conspicuity of turbines at night during construction, all turbines should be lit with temporary lighting once they reach a height of 200 feet or greater until such time the permanent lighting configuration is turned on. As the height of the structure continues to increase, the temporary lighting should be relocated to the uppermost part of the structure. The temporary lighting may be turned off for periods when they would interfere with construction personnel. If practical, permanent obstruction lights should be installed and operated at each level as construction progresses. An FAA Type L-810 steady red light fixture shall be used to light the structure during the construction phase. If power is not available, turbines shall be lit with self-contained, solar powered LED steady red light fixture that meets the photometric requirements of an FAA Type L-810 lighting system. The lights should be positioned to ensure that a pilot has an unobstructed view of at least one light at each level. The use of a NOTAM (D) to not light turbines within a project until the entire project has been completed is prohibited.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (206) 231-2990, or paul.holmquist@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2020-WTW-2217-OE.

Signature Control No: 435204286-445645779

(DNE -WT)

Paul Holmquist
Specialist

Attachment(s)
Additional Information
Map(s)

Additional information for ASN 2020-WTW-2217-OE

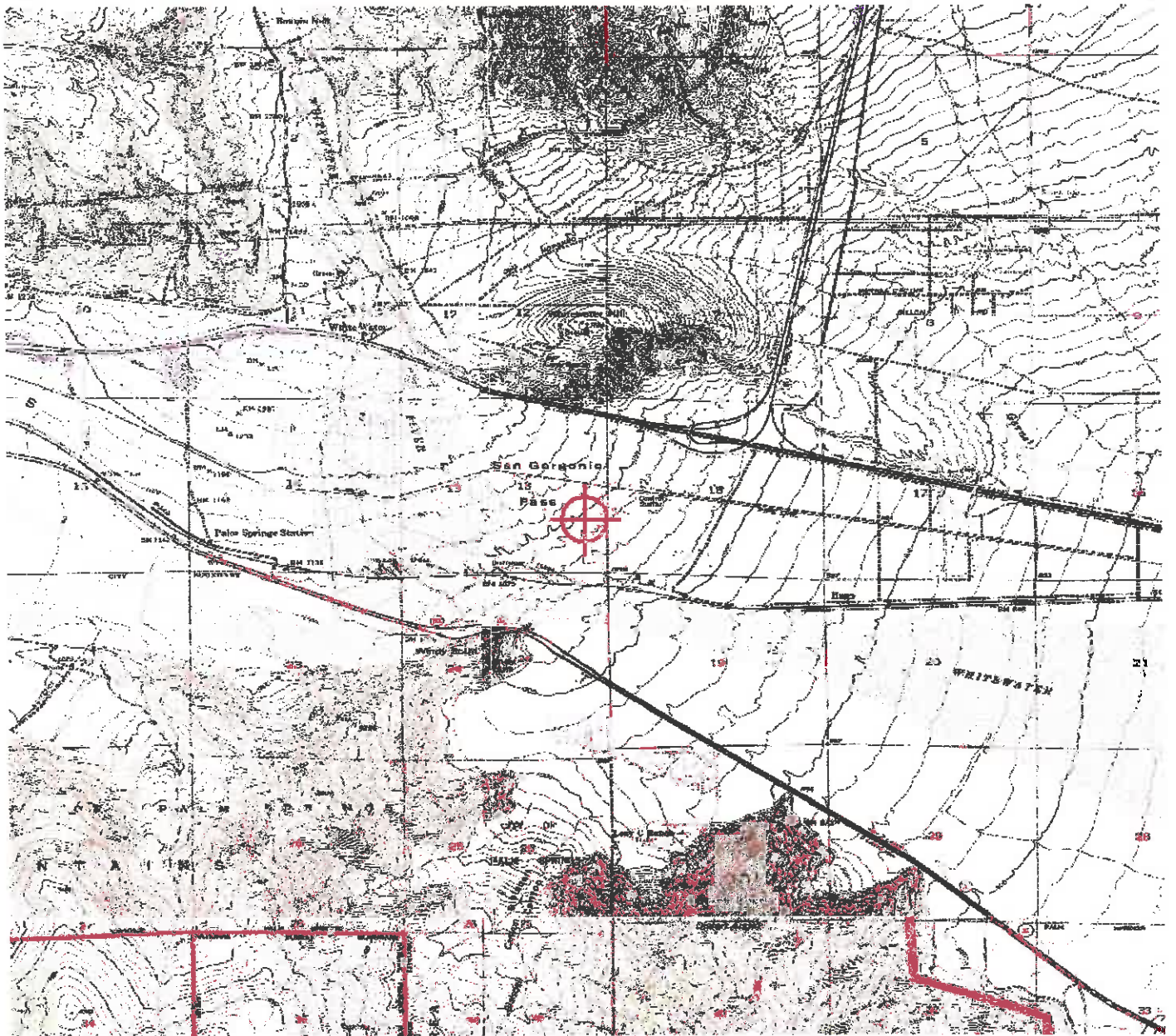
NO RADAR IMPACT

Analysis indicates that all 23 turbines studied in this project would be in the line of sight for the Palm Springs (PSP) Airport Surveillance Radar-9 (ASR-9); however, Air Traffic has determined this would not create a substantial adverse impact on their operations at this time.

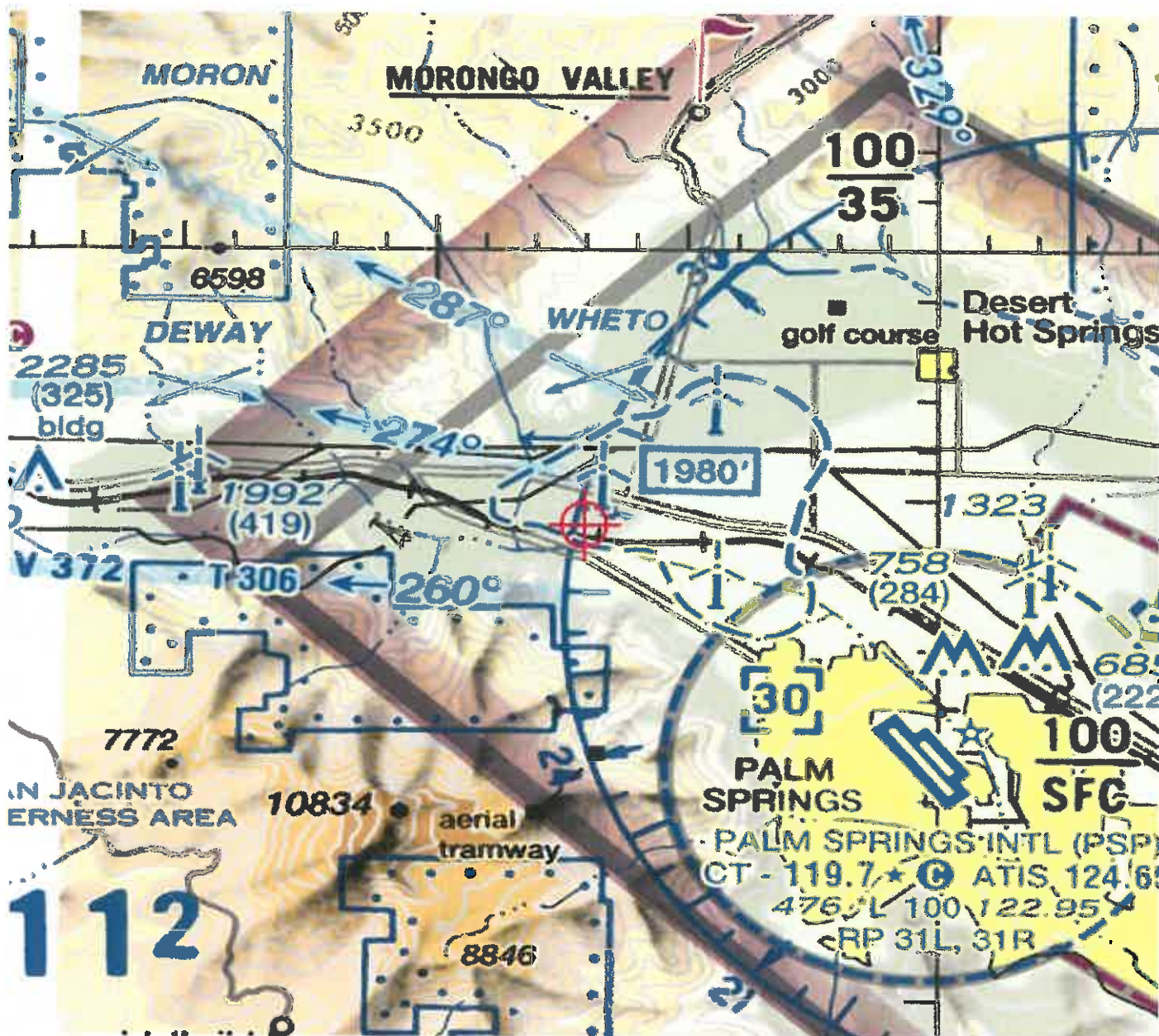
OBSTRUCTION MARKING AND LIGHTING NOTE:

A recommendation for white paint/synchronized red lights will be made for all turbines until such time as the proponent confirms that the layout is final (no changes, no additions, no removals) and all turbines can and will be built at their determined location and height. At that time, the proponent may contact this office and request a re-evaluation of the marking and lighting recommendations for the turbines within this project and a portion of the turbines may qualify for the removal of the lighting recommendation.

TOPO Map for ASN 2020-WTW-2217-OE



Sectional Map for ASN 2020-WTW-2217-OE





Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2020-WTW-2218-OE

Issued Date: 07/16/2020

Mike Hughes
 Mountain View Power Partners, LLC
 AES Southland
 690 N. Studebaker Rd.
 Long Beach, CA 90803

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Wind Turbine WTG12
 Location: Palm Springs, CA
 Latitude: 33-54-39.24N NAD 83
 Longitude: 116-36-13.71W
 Heights: 1010 feet site elevation (SE)
 492 feet above ground level (AGL)
 1502 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

As a condition to this Determination, the structure is to be marked/lighted in accordance with FAA Advisory circular 70/7460-1 L Change 2, Obstruction Marking and Lighting, white paint/synchronized red lights - Chapters 4,12&13(Turbines).

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

See attachment for additional condition(s) or information.

This determination expires on 01/16/2022 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.

(b) extended, revised, or terminated by the issuing office.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates and heights. This determination is valid for coordinates within one (1) second latitude/longitude and up to the approved AMSL height listed above (provided the AGL height does not exceed 499 feet). If a certified 1A or 2C accuracy survey was required to mitigate an adverse effect, any change in coordinates or increase in height will require a new certified accuracy survey and may require a new aeronautical study.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

Additional wind turbines or met towers proposed in the future may cause a cumulative effect on the national airspace system. All information from submission of Supplemental Notice (7460-2 Part 2) will be considered the final data (including heights) for this structure. Any future construction or alteration, including but not limited to changes in heights, requires separate notice to the FAA.

Obstruction marking and lighting recommendations for wind turbine farms are based on the scheme for the entire project. ANY change to the height, location or number of turbines within this project will require a reanalysis of the marking and lighting recommendation for the entire project. In particular, the removal of previously planned or built turbines/turbine locations from the project will often result in a change in the marking/lighting recommendation for other turbines within the project. It is the proponent's responsibility to contact the FAA to discuss the process for developing a revised obstruction marking and lighting plan should this occur.

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This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (206) 231-2990, or paul.holmquist@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2020-WTW-2218-OE.

Signature Control No: 435204287-445645798

(DNE -WT)

Paul Holmquist
Specialist

Attachment(s)
Additional Information
Map(s)

Additional information for ASN 2020-WTW-2218-OE

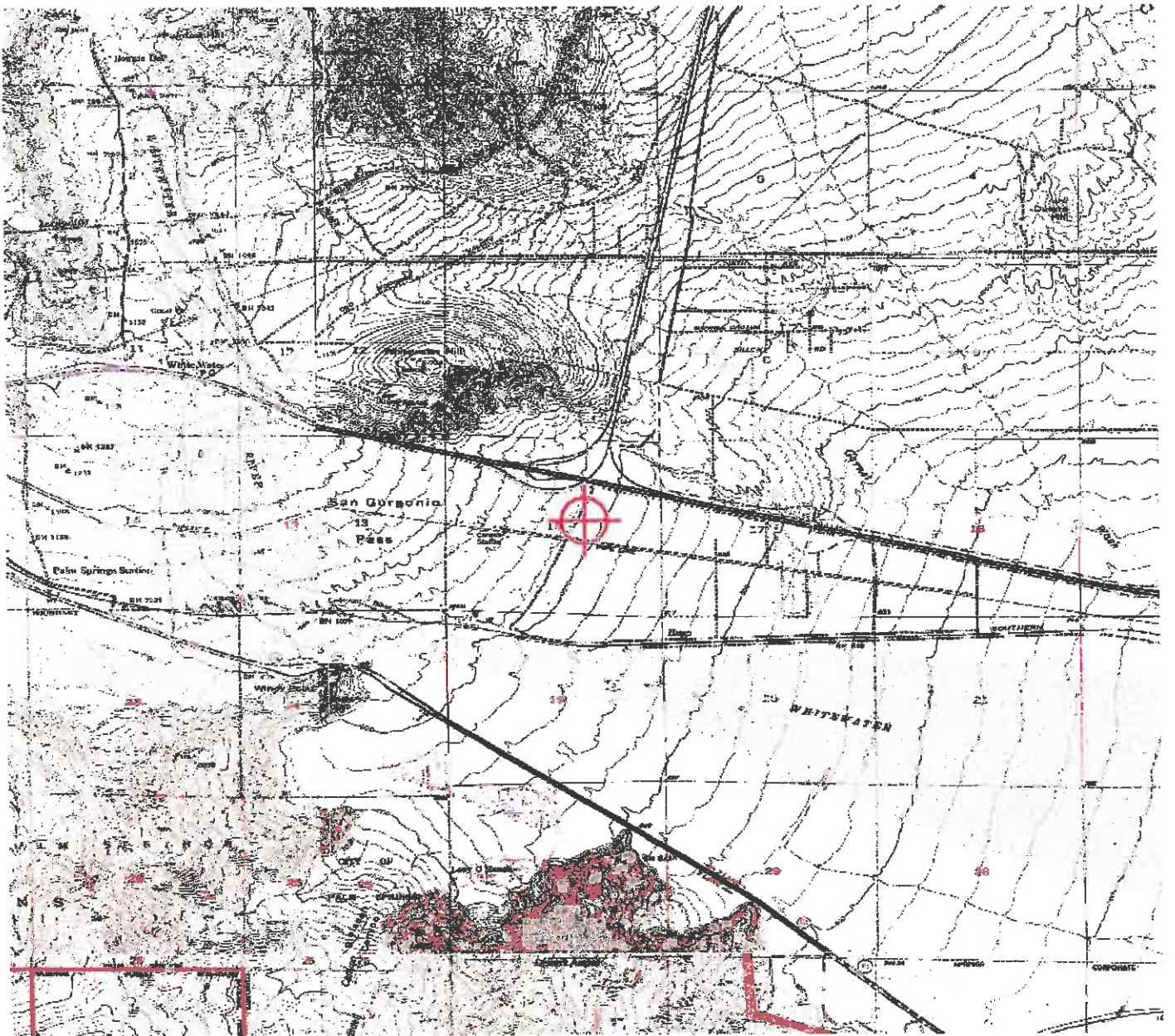
NO RADAR IMPACT

Analysis indicates that all 23 turbines studied in this project would be in the line of sight for the Palm Springs (PSP) Airport Surveillance Radar-9 (ASR-9); however, Air Traffic has determined this would not create a substantial adverse impact on their operations at this time.

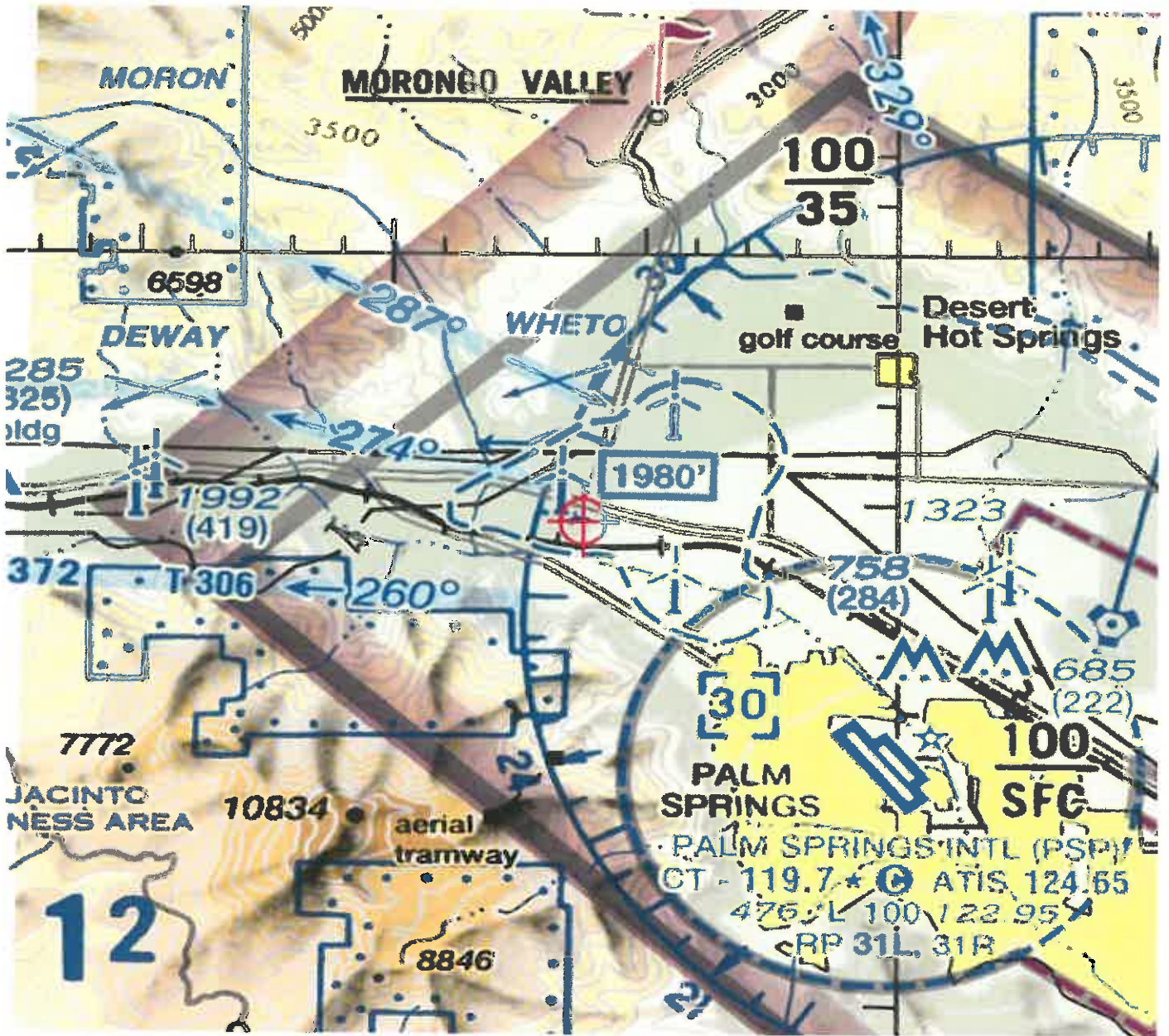
OBSTRUCTION MARKING AND LIGHTING NOTE:

A recommendation for white paint/synchronized red lights will be made for all turbines until such time as the proponent confirms that the layout is final (no changes, no additions, no removals) and all turbines can and will be built at their determined location and height. At that time, the proponent may contact this office and request a re-evaluation of the marking and lighting recommendations for the turbines within this project and a portion of the turbines may qualify for the removal of the lighting recommendation.

TOPO Map for ASN 2020-WTW-2218-OE



Sectional Map for ASN 2020-WTW-2218-OE





Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2020-WTW-2219-OE

Issued Date: 07/16/2020

Mike Hughes
 Mountain View Power Partners, LLC
 AES Southland
 690 N. Studebaker Rd.
 Long Beach, CA 90803

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Wind Turbine WTG13
 Location: Palm Springs, CA
 Latitude: 33-54-31.53N NAD 83
 Longitude: 116-36-13.60W
 Heights: 1000 feet site elevation (SE)
 492 feet above ground level (AGL)
 1492 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

As a condition to this Determination, the structure is to be marked/lighted in accordance with FAA Advisory circular 70/7460-1 L Change 2, Obstruction Marking and Lighting, white paint/synchronized red lights - Chapters 4,12&13(Turbines).

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

See attachment for additional condition(s) or information.

This determination expires on 01/16/2022 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.

(b) extended, revised, or terminated by the issuing office.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates and heights. This determination is valid for coordinates within one (1) second latitude/longitude and up to the approved AMSL height listed above (provided the AGL height does not exceed 499 feet). If a certified 1A or 2C accuracy survey was required to mitigate an adverse effect, any change in coordinates or increase in height will require a new certified accuracy survey and may require a new aeronautical study.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

Additional wind turbines or met towers proposed in the future may cause a cumulative effect on the national airspace system. All information from submission of Supplemental Notice (7460-2 Part 2) will be considered the final data (including heights) for this structure. Any future construction or alteration, including but not limited to changes in heights, requires separate notice to the FAA.

Obstruction marking and lighting recommendations for wind turbine farms are based on the scheme for the entire project. ANY change to the height, location or number of turbines within this project will require a reanalysis of the marking and lighting recommendation for the entire project. In particular, the removal of previously planned or built turbines/turbine locations from the project will often result in a change in the marking/lighting recommendation for other turbines within the project. It is the proponent's responsibility to contact the FAA to discuss the process for developing a revised obstruction marking and lighting plan should this occur.

In order to ensure proper conspicuity of turbines at night during construction, all turbines should be lit with temporary lighting once they reach a height of 200 feet or greater until such time the permanent lighting configuration is turned on. As the height of the structure continues to increase, the temporary lighting should be relocated to the uppermost part of the structure. The temporary lighting may be turned off for periods when they would interfere with construction personnel. If practical, permanent obstruction lights should be installed and operated at each level as construction progresses. An FAA Type L-810 steady red light fixture shall be used to light the structure during the construction phase. If power is not available, turbines shall be lit with self-contained, solar powered LED steady red light fixture that meets the photometric requirements of an FAA Type L-810 lighting system. The lights should be positioned to ensure that a pilot has an unobstructed view of at least one light at each level. The use of a NOTAM (D) to not light turbines within a project until the entire project has been completed is prohibited.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (206) 231-2990, or paul.holmquist@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2020-WTW-2219-OE.

Signature Control No: 435204289-445645789

(DNE -WT)

Paul Holmquist
Specialist

Attachment(s)
Additional Information
Map(s)

Additional information for ASN 2020-WTW-2219-OE

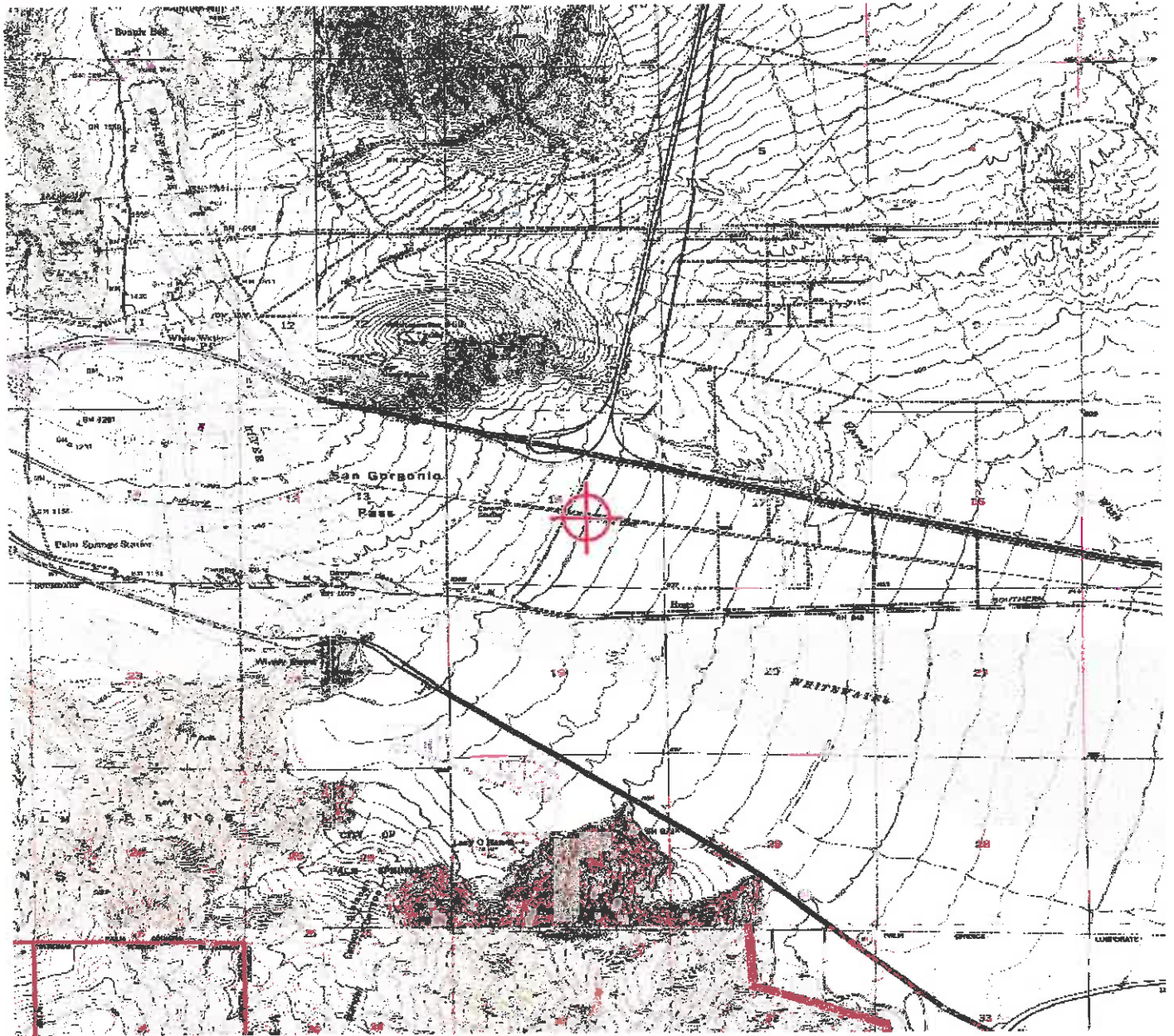
NO RADAR IMPACT

Analysis indicates that all 23 turbines studied in this project would be in the line of sight for the Palm Springs (PSP) Airport Surveillance Radar-9 (ASR-9); however, Air Traffic has determined this would not create a substantial adverse impact on their operations at this time.

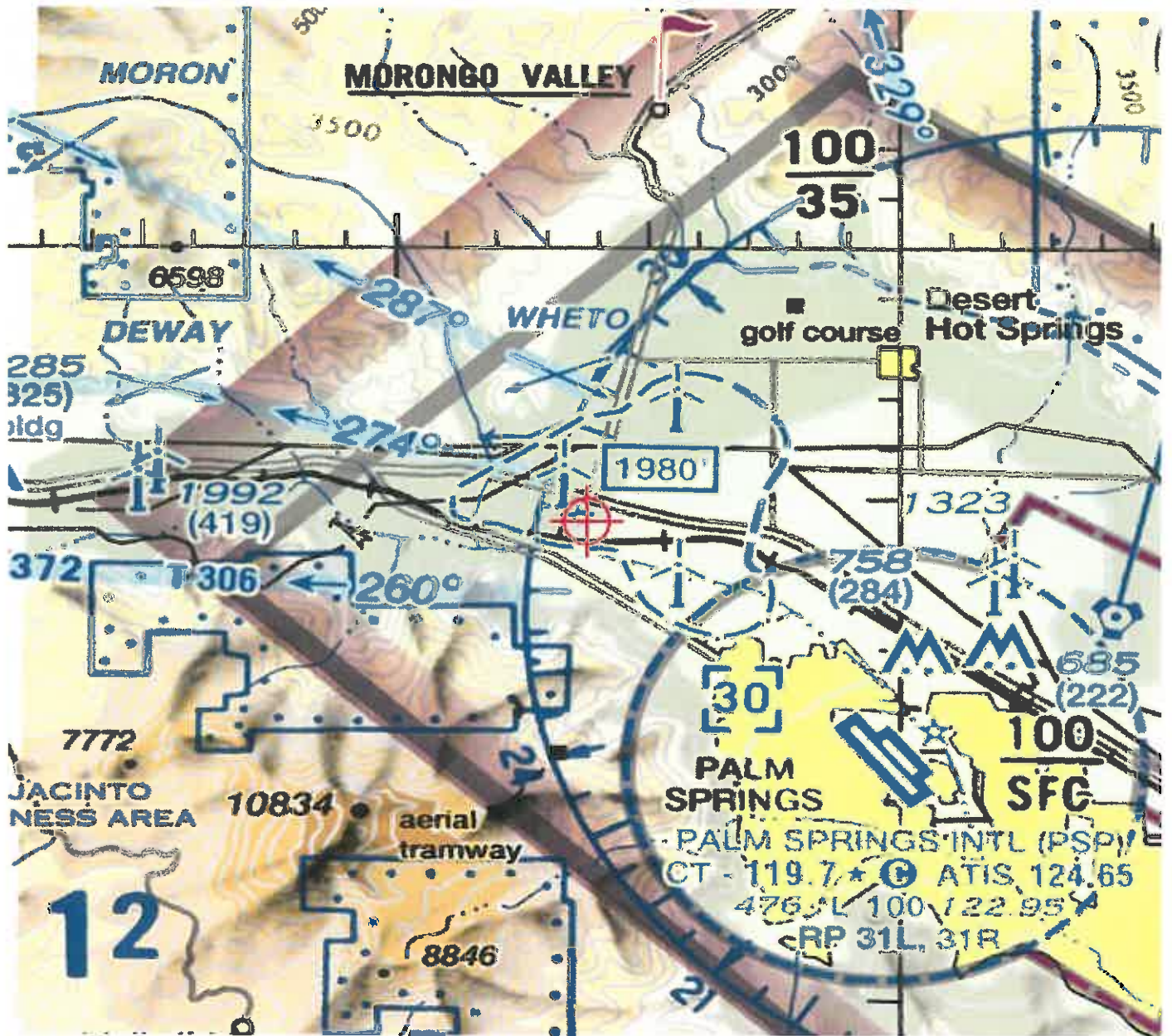
OBSTRUCTION MARKING AND LIGHTING NOTE:

A recommendation for white paint/synchronized red lights will be made for all turbines until such time as the proponent confirms that the layout is final (no changes, no additions, no removals) and all turbines can and will be built at their determined location and height. At that time, the proponent may contact this office and request a re-evaluation of the marking and lighting recommendations for the turbines within this project and a portion of the turbines may qualify for the removal of the lighting recommendation.

TOPO Map for ASN 2020-WTW-2219-OE



Sectional Map for ASN 2020-WTW-2219-OE





Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2020-WTW-2220-OE

Issued Date: 07/16/2020

Mike Hughes
 Mountain View Power Partners, LLC
 AES Southland
 690 N. Studebaker Rd.
 Long Beach, CA 90803

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Wind Turbine WTG14
 Location: Palm Springs, CA
 Latitude: 33-54-23.83N NAD 83
 Longitude: 116-36-13.68W
 Heights: 995 feet site elevation (SE)
 492 feet above ground level (AGL)
 1487 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

As a condition to this Determination, the structure is to be marked/lighted in accordance with FAA Advisory circular 70/7460-1 L Change 2, Obstruction Marking and Lighting, white paint/synchronized red lights - Chapters 4,12&13(Turbines).

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

See attachment for additional condition(s) or information.

This determination expires on 01/16/2022 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.

(b) extended, revised, or terminated by the issuing office.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates and heights. This determination is valid for coordinates within one (1) second latitude/longitude and up to the approved AMSL height listed above (provided the AGL height does not exceed 499 feet). If a certified 1A or 2C accuracy survey was required to mitigate an adverse effect, any change in coordinates or increase in height will require a new certified accuracy survey and may require a new aeronautical study.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

Additional wind turbines or met towers proposed in the future may cause a cumulative effect on the national airspace system. All information from submission of Supplemental Notice (7460-2 Part 2) will be considered the final data (including heights) for this structure. Any future construction or alteration, including but not limited to changes in heights, requires separate notice to the FAA.

Obstruction marking and lighting recommendations for wind turbine farms are based on the scheme for the entire project. ANY change to the height, location or number of turbines within this project will require a reanalysis of the marking and lighting recommendation for the entire project. In particular, the removal of previously planned or built turbines/turbine locations from the project will often result in a change in the marking/lighting recommendation for other turbines within the project. It is the proponent's responsibility to contact the FAA to discuss the process for developing a revised obstruction marking and lighting plan should this occur.

In order to ensure proper conspicuity of turbines at night during construction, all turbines should be lit with temporary lighting once they reach a height of 200 feet or greater until such time the permanent lighting configuration is turned on. As the height of the structure continues to increase, the temporary lighting should be relocated to the uppermost part of the structure. The temporary lighting may be turned off for periods when they would interfere with construction personnel. If practical, permanent obstruction lights should be installed and operated at each level as construction progresses. An FAA Type L-810 steady red light fixture shall be used to light the structure during the construction phase. If power is not available, turbines shall be lit with self-contained, solar powered LED steady red light fixture that meets the photometric requirements of an FAA Type L-810 lighting system. The lights should be positioned to ensure that a pilot has an unobstructed view of at least one light at each level. The use of a NOTAM (D) to not light turbines within a project until the entire project has been completed is prohibited.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (206) 231-2990, or paul.holmquist@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2020-WTW-2220-OE.

Signature Control No: 435204291-445645796
Paul Holmquist
Specialist

(DNE -WT)

Attachment(s)
Additional Information
Map(s)

Additional information for ASN 2020-WTW-2220-OE

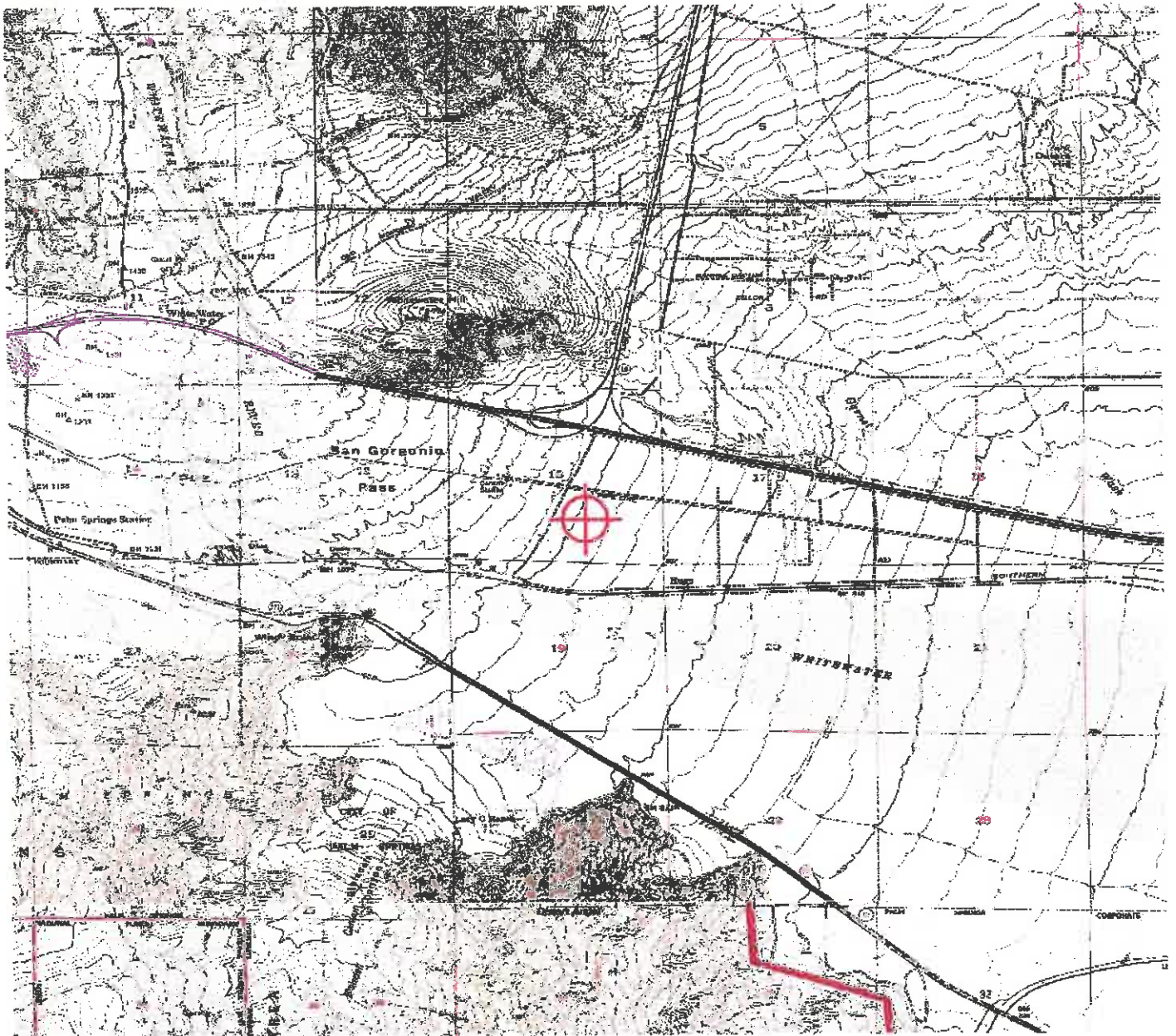
NO RADAR IMPACT

Analysis indicates that all 23 turbines studied in this project would be in the line of sight for the Palm Springs (PSP) Airport Surveillance Radar-9 (ASR-9); however, Air Traffic has determined this would not create a substantial adverse impact on their operations at this time.

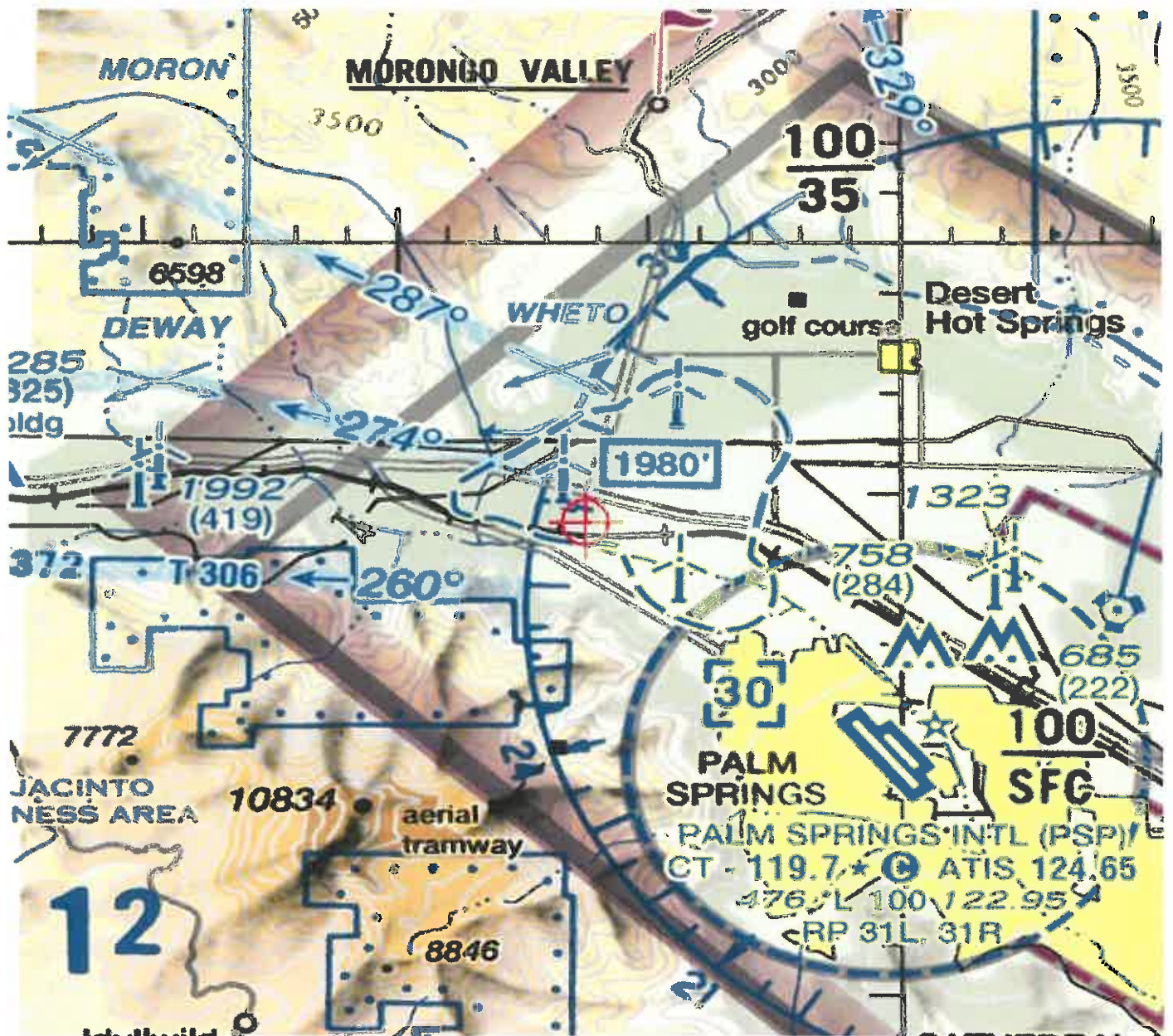
OBSTRUCTION MARKING AND LIGHTING NOTE:

A recommendation for white paint/synchronized red lights will be made for all turbines until such time as the proponent confirms that the layout is final (no changes, no additions, no removals) and all turbines can and will be built at their determined location and height. At that time, the proponent may contact this office and request a re-evaluation of the marking and lighting recommendations for the turbines within this project and a portion of the turbines may qualify for the removal of the lighting recommendation.

TOPO Map for ASN 2020-WTW-2220-OE



Sectional Map for ASN 2020-WTW-2220-OE





Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2020-WTW-2221-OE

Issued Date: 07/16/2020

Mike Hughes
 Mountain View Power Partners, LLC
 AES Southland
 690 N. Studebaker Rd.
 Long Beach, CA 90803

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Wind Turbine WTG15
 Location: Palm Springs, CA
 Latitude: 33-54-16.30N NAD 83
 Longitude: 116-36-13.71W
 Heights: 990 feet site elevation (SE)
 492 feet above ground level (AGL)
 1482 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

As a condition to this Determination, the structure is to be marked/lighted in accordance with FAA Advisory circular 70/7460-1 L Change 2, Obstruction Marking and Lighting, white paint/synchronized red lights - Chapters 4,12&13(Turbines).

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

See attachment for additional condition(s) or information.

This determination expires on 01/16/2022 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.

(b) extended, revised, or terminated by the issuing office.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates and heights. This determination is valid for coordinates within one (1) second latitude/longitude and up to the approved AMSL height listed above (provided the AGL height does not exceed 499 feet). If a certified 1A or 2C accuracy survey was required to mitigate an adverse effect, any change in coordinates or increase in height will require a new certified accuracy survey and may require a new aeronautical study.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

Additional wind turbines or met towers proposed in the future may cause a cumulative effect on the national airspace system. All information from submission of Supplemental Notice (7460-2 Part 2) will be considered the final data (including heights) for this structure. Any future construction or alteration, including but not limited to changes in heights, requires separate notice to the FAA.

Obstruction marking and lighting recommendations for wind turbine farms are based on the scheme for the entire project. ANY change to the height, location or number of turbines within this project will require a reanalysis of the marking and lighting recommendation for the entire project. In particular, the removal of previously planned or built turbines/turbine locations from the project will often result in a change in the marking/lighting recommendation for other turbines within the project. It is the proponent's responsibility to contact the FAA to discuss the process for developing a revised obstruction marking and lighting plan should this occur.

In order to ensure proper conspicuity of turbines at night during construction, all turbines should be lit with temporary lighting once they reach a height of 200 feet or greater until such time the permanent lighting configuration is turned on. As the height of the structure continues to increase, the temporary lighting should be relocated to the uppermost part of the structure. The temporary lighting may be turned off for periods when they would interfere with construction personnel. If practical, permanent obstruction lights should be installed and operated at each level as construction progresses. An FAA Type L-810 steady red light fixture shall be used to light the structure during the construction phase. If power is not available, turbines shall be lit with self-contained, solar powered LED steady red light fixture that meets the photometric requirements of an FAA Type L-810 lighting system. The lights should be positioned to ensure that a pilot has an unobstructed view of at least one light at each level. The use of a NOTAM (D) to not light turbines within a project until the entire project has been completed is prohibited.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (206) 231-2990, or paul.holmquist@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2020-WTW-2221-OE.

Signature Control No: 435204294-445645791
Paul Holmquist
Specialist

(DNE -WT)

Attachment(s)
Additional Information
Map(s)

Additional information for ASN 2020-WTW-2221-OE

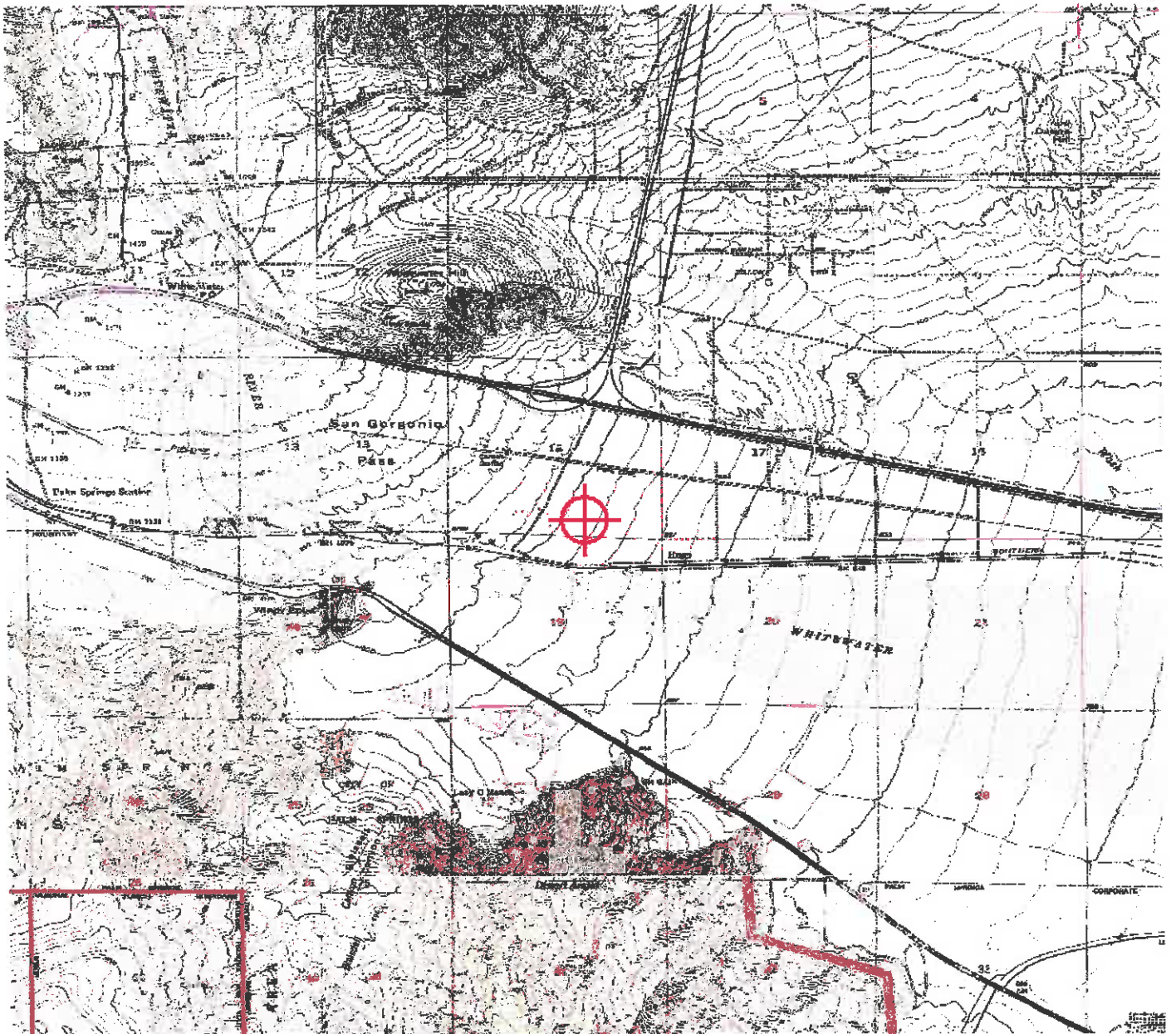
NO RADAR IMPACT

Analysis indicates that all 23 turbines studied in this project would be in the line of sight for the Palm Springs (PSP) Airport Surveillance Radar-9 (ASR-9); however, Air Traffic has determined this would not create a substantial adverse impact on their operations at this time.

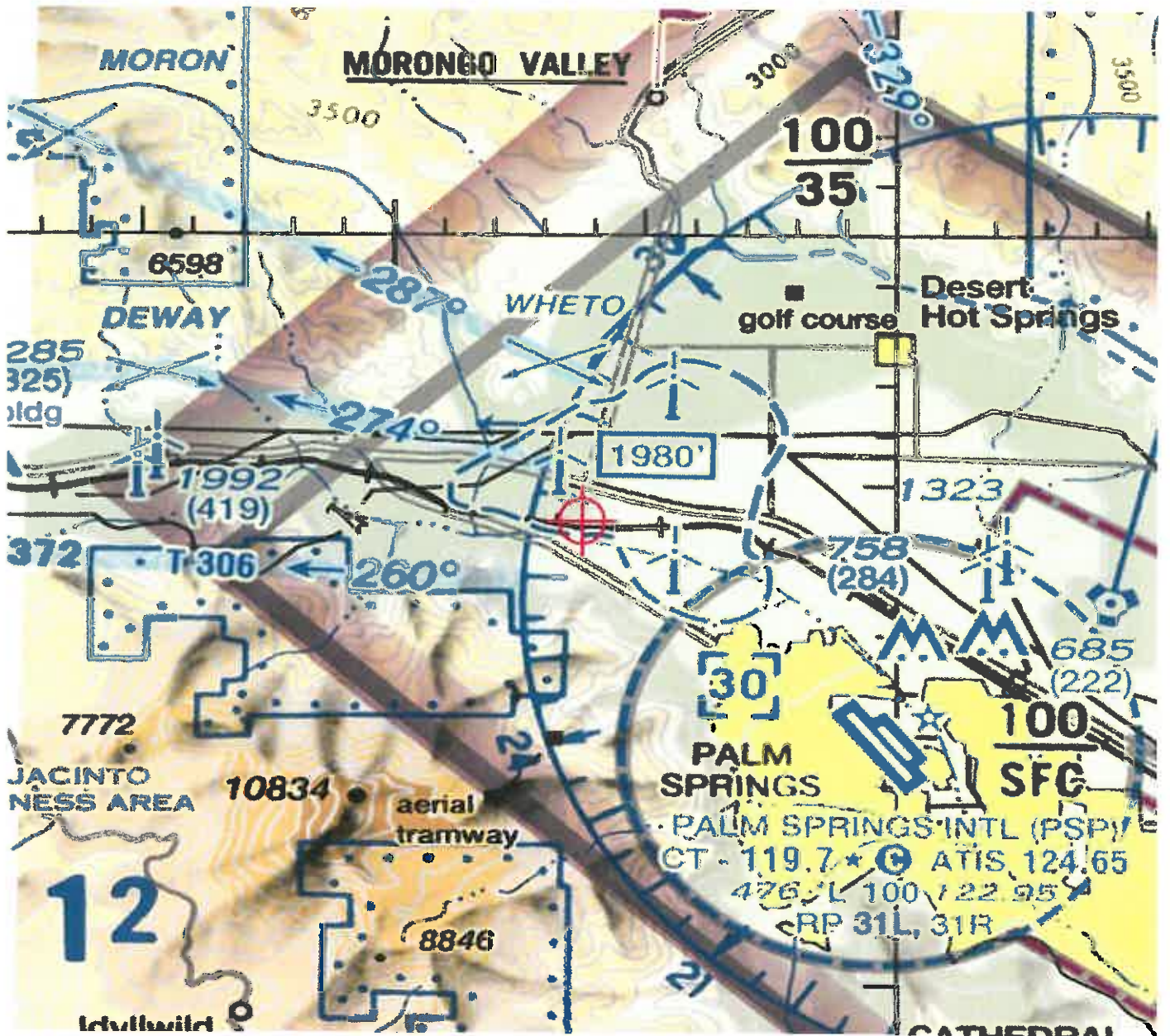
OBSTRUCTION MARKING AND LIGHTING NOTE:

A recommendation for white paint/synchronized red lights will be made for all turbines until such time as the proponent confirms that the layout is final (no changes, no additions, no removals) and all turbines can and will be built at their determined location and height. At that time, the proponent may contact this office and request a re-evaluation of the marking and lighting recommendations for the turbines within this project and a portion of the turbines may qualify for the removal of the lighting recommendation.

TOPO Map for ASN 2020-WTW-2221-OE



Sectional Map for ASN 2020-WTW-2221-OE





Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2020-WTW-8073-OE
 Prior Study No.
 2020-WTW-2222-OE

Issued Date: 10/28/2020

Mike Hughes
 Mountain View Power Partners, LLC
 AES Southland
 690 N. Studebaker Rd.
 Long Beach, CA 90803

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

| | |
|------------|---------------------------------------|
| Structure: | Wind Turbine WTG16 |
| Location: | Palm Springs, CA |
| Latitude: | 33-54-09.27N NAD 83 |
| Longitude: | 116-36-16.19W |
| Heights: | 982 feet site elevation (SE) |
| | 492 feet above ground level (AGL) |
| | 1474 feet above mean sea level (AMSL) |

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

As a condition to this Determination, the structure is to be marked/lighted in accordance with FAA Advisory circular 70/7460-1 L Change 2, Obstruction Marking and Lighting, white paint/synchronized red lights - Chapters 4,12&13(Turbines).

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

See attachment for additional condition(s) or information.

This determination expires on 04/28/2022 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.

(b) extended, revised, or terminated by the issuing office.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates and heights. This determination is valid for coordinates within one (1) second latitude/longitude and up to the approved AMSL height listed above (provided the AGL height does not exceed 499 feet). If a certified 1A or 2C accuracy survey was required to mitigate an adverse effect, any change in coordinates or increase in height will require a new certified accuracy survey and may require a new aeronautical study.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

Additional wind turbines or met towers proposed in the future may cause a cumulative effect on the national airspace system. All information from submission of Supplemental Notice (7460-2 Part 2) will be considered the final data (including heights) for this structure. Any future construction or alteration, including but not limited to changes in heights, requires separate notice to the FAA.

Obstruction marking and lighting recommendations for wind turbine farms are based on the scheme for the entire project. ANY change to the height, location or number of turbines within this project will require a reanalysis of the marking and lighting recommendation for the entire project. In particular, the removal of previously planned or built turbines/turbine locations from the project will often result in a change in the marking/lighting recommendation for other turbines within the project. It is the proponent's responsibility to contact the FAA to discuss the process for developing a revised obstruction marking and lighting plan should this occur.

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This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (206) 231-2990, or paul.holmquist@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2020-WTW-8073-OE.

Signature Control No: 450643829-455207565

(DNE -WT)

Paul Holmquist
Specialist

Attachment(s)
Additional Information
Map(s)

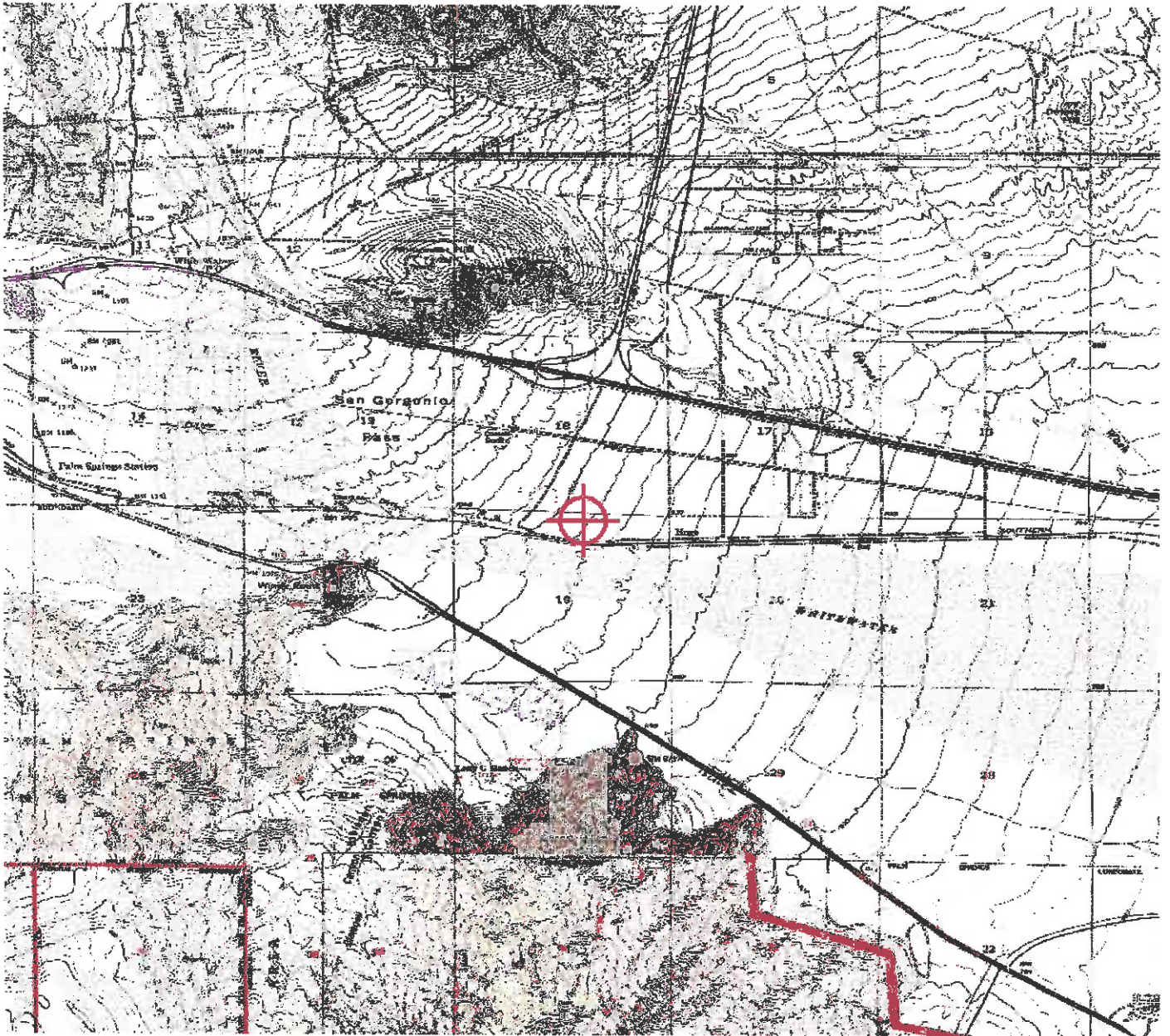
Additional information for ASN 2020-WTW-8073-OE

NO RADAR IMPACT

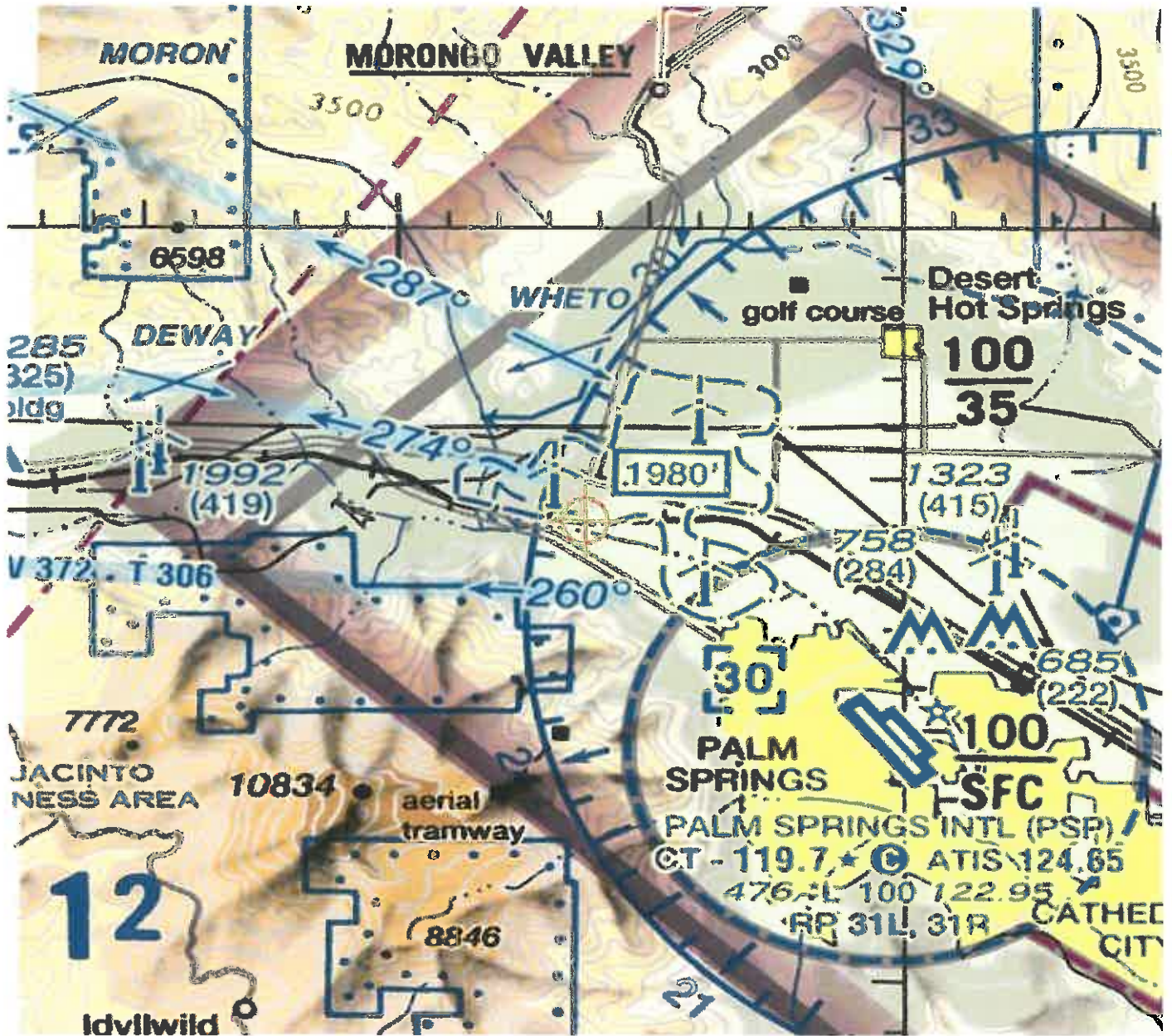
Analysis indicates that all 10 turbines studied in this project would be in the line of sight for the Palm Springs (PSP) Airport Surveillance Radar-9 (ASR-9); however, Air Traffic has determined this would not create a substantial adverse impact on their operations at this time.

OBSTRUCTION MARKING AND LIGHTING NOTE: A recommendation for white paint/synchronized red lights will be made for all turbines until such time as the proponent confirms that the layout is final (no changes, no additions, no removals) and all turbines can and will be built at their determined location and height. At that time, the proponent may contact this office and request a re-evaluation of the marking and lighting recommendations for the turbines within this project and a portion of the turbines may qualify for the removal of the lighting recommendation.

TOPO Map for ASN 2020-WTW-8073-OE



Sectional Map for ASN 2020-WTW-8073-OE



SUPPLEMENTAL INFORMATION

AIRPORT LAND USE COMMISSION APPLICATION FOR MAJOR LAND USE ACTION REVIEW

Project Assessor's Parcel Number(s):

The project site encompasses 42 parcels and a portion of two additional parcels, as listed below and shown on the Existing Conditions and Proposed Site Plan exhibits.

| | | |
|-------------|-------------|-------------|
| 668-310-017 | 668-300-005 | 668-310-028 |
| 668-290-003 | 668-300-011 | 668-310-033 |
| 668-300-002 | 668-310-019 | 668-310-045 |
| 668-310-030 | 668-310-024 | 668-310-047 |
| 668-300-015 | 668-310-025 | 668-310-044 |
| 668-310-038 | 668-310-027 | 669-020-007 |
| 669-020-005 | 668-310-029 | 669-040-017 |
| 522-070-027 | 668-310-034 | 669-040-018 |
| 669-020-008 | 668-310-036 | 668-412-001 |
| 668-300-008 | 668-310-037 | 669-040-006 |
| 668-310-014 | 668-310-039 | 668-290-008 |
| 668-310-015 | 668-310-040 | 668-310-023 |
| 668-300-009 | 668-310-043 | 668-310-046 |
| 668-300-003 | 668-300-012 | |
| 668-300-010 | 668-300-013 | |
| 668-310-032 | 668-300-014 | |
| 668-300-001 | 668-310-026 | |

Existing and Proposed Land Uses

The existing Riverside County General Plan land use designations on the project site include Rural Desert (RD) and Conservation Habitat (OS-CH). Disturbance within the OS-CH designation would be limited to new fiber-optic cable replacement beneath existing access road to the seven WTGs that would remain as part of the project. No ground disturbance is proposed within undisturbed land designated OS-CH.

The existing Riverside County zoning designations within the project site include Wind Energy Resource Zone (W-E) and Rural Residential (R-R). Although the proposed land use is consistent with the existing land use, the project applicant has submitted a Change of Zone application to the County to change the R-R zoning designation in the southwest portion of the project site to allow for development of the proposed WTGs and met tower. Upon approval of the Change of Zone, the proposed area of development within the R-R zone would be changed to W-E.

In addition, an approximately 254-acre parcel in the western portion of the project site would remain undisturbed and be conveyed to the Coachella Valley Conservation Commission to ultimately become additional conserved land within the Whitewater River Conservation Area.

Project Description:

Mountain View Power Partners LLC (applicant) proposes to repower a portion of its existing Mountain View Power Partners (MVPP) I & II wind energy projects. The proposed project would involve the

SUPPLEMENTAL INFORMATION

AIRPORT LAND USE COMMISSION APPLICATION FOR MAJOR LAND USE ACTION REVIEW

removal of 93 existing Mitsubishi 600-kilowatt (kW) WTGs and the subsequent installation of 16 Vestas 3.6 and 4.3 MW WTGs; 7 existing Mitsubishi 600 kW WTGs would remain as part of the proposed project. Six of these WTGs (WTG74-09 through WTG74-14) are located on BLM parcel number 668-310-038 (ROW Grant CACA-42139), and one WTG is located on privately owned parcel number 669-020-008 (WTG74-15). The proposed project would be capable of producing approximately 260.2 gigawatts of power per year for operational years 1 through 10.

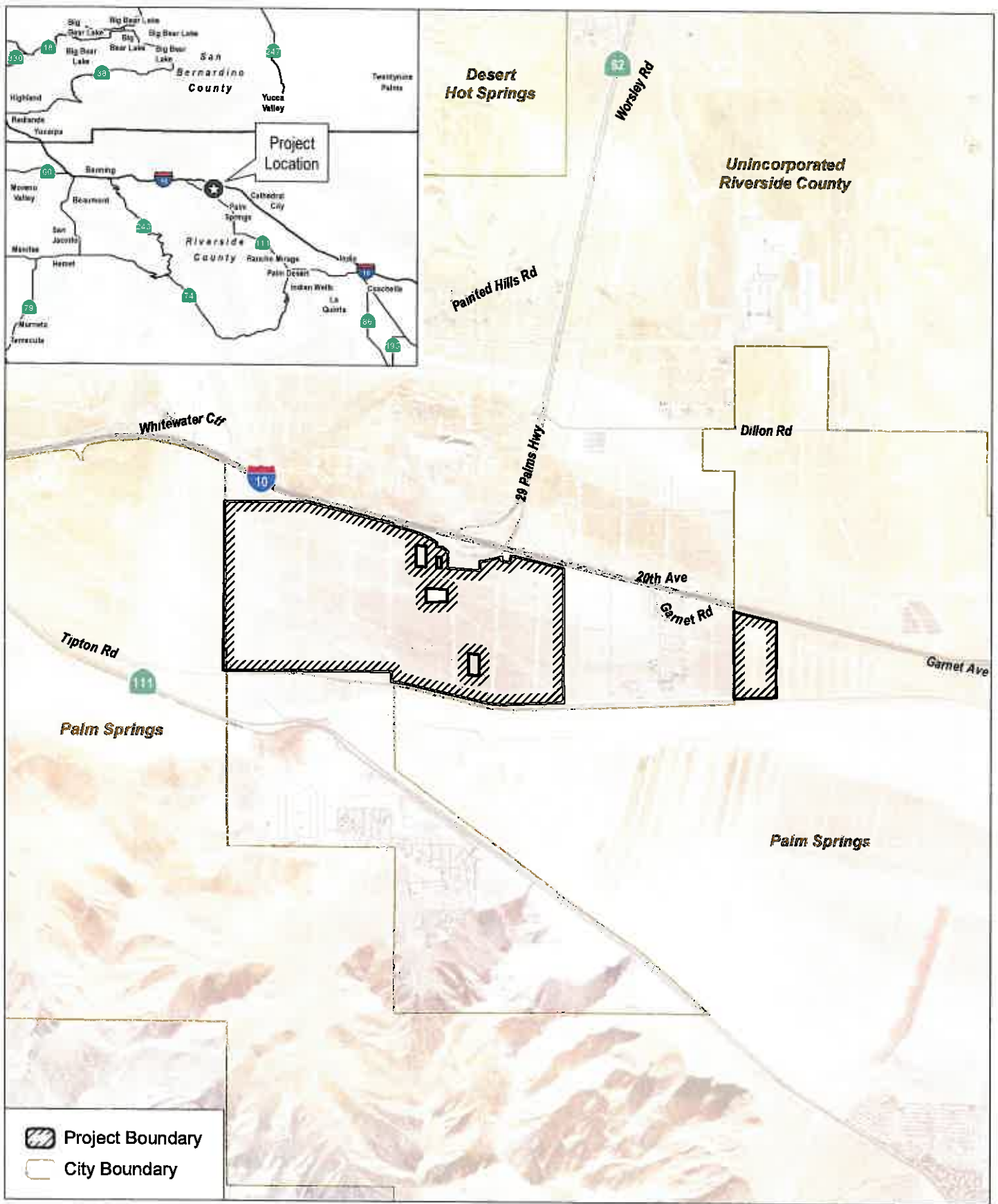
Project components include the following: WTGs (including turbine pad, safety features, and transformer contained within WTG unit), the electrical collection system, access roads, one free-standing met tower, and temporary laydown and parking.

The maximum characteristics of WTGs for the proposed project are described as follows:

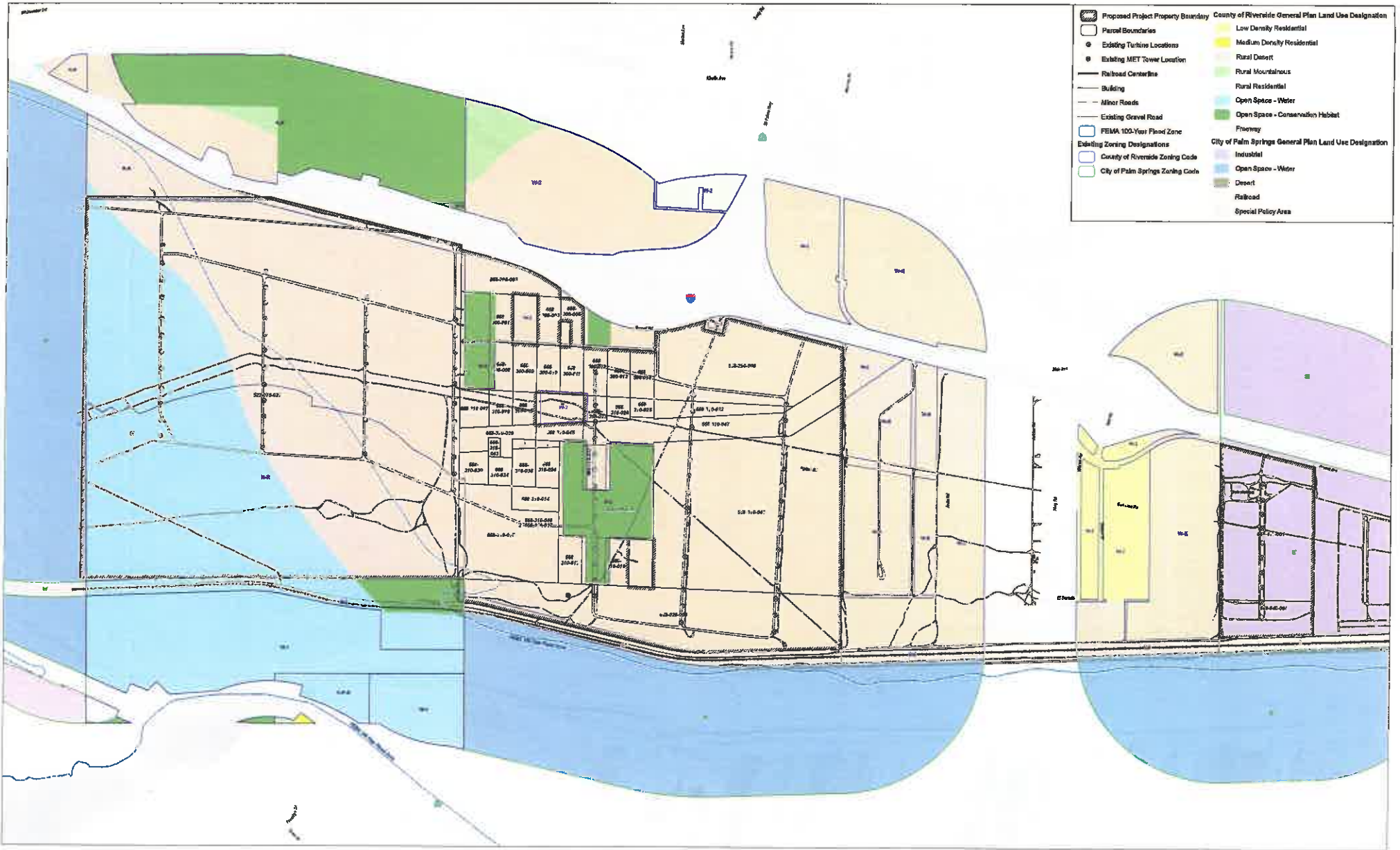
- Tubular steel towers
- Rotor diameter – 117 meters (384 feet)
 - Blade length – 57.15 meters (188 feet)
 - Three blades per WTG
- Hub height – 91.5 meters (300 feet)
- Total height of WTG (highest point) – 150 meters (approximately 492 feet)

One new free-standing lattice meteorological (met) tower would be erected within the southwest portion of the project site. The proposed tower would be up to 100 meters (approximately 328 feet) tall and would be equipped with applicable FAA-compliant marking or lighting for aviation safety. Preferred lighting color has not yet been finalized but is anticipated to be in warm tones (e.g., reds or oranges) as opposed to LED or bright lighting in order to lower increased predation risk for small mammals. A total of 0.5 acres of new ground disturbance would be required for construction of the proposed met tower and associated components. The three existing met towers within the project site, one of which is located within the WFCA, would be decommissioned prior to project construction.

Project construction is anticipated to begin in September 2021. Construction of the proposed project is anticipated to be completed in 8 months.



SOURCE: Aerials by Riverside County 2016, OpenStreetMap 2019

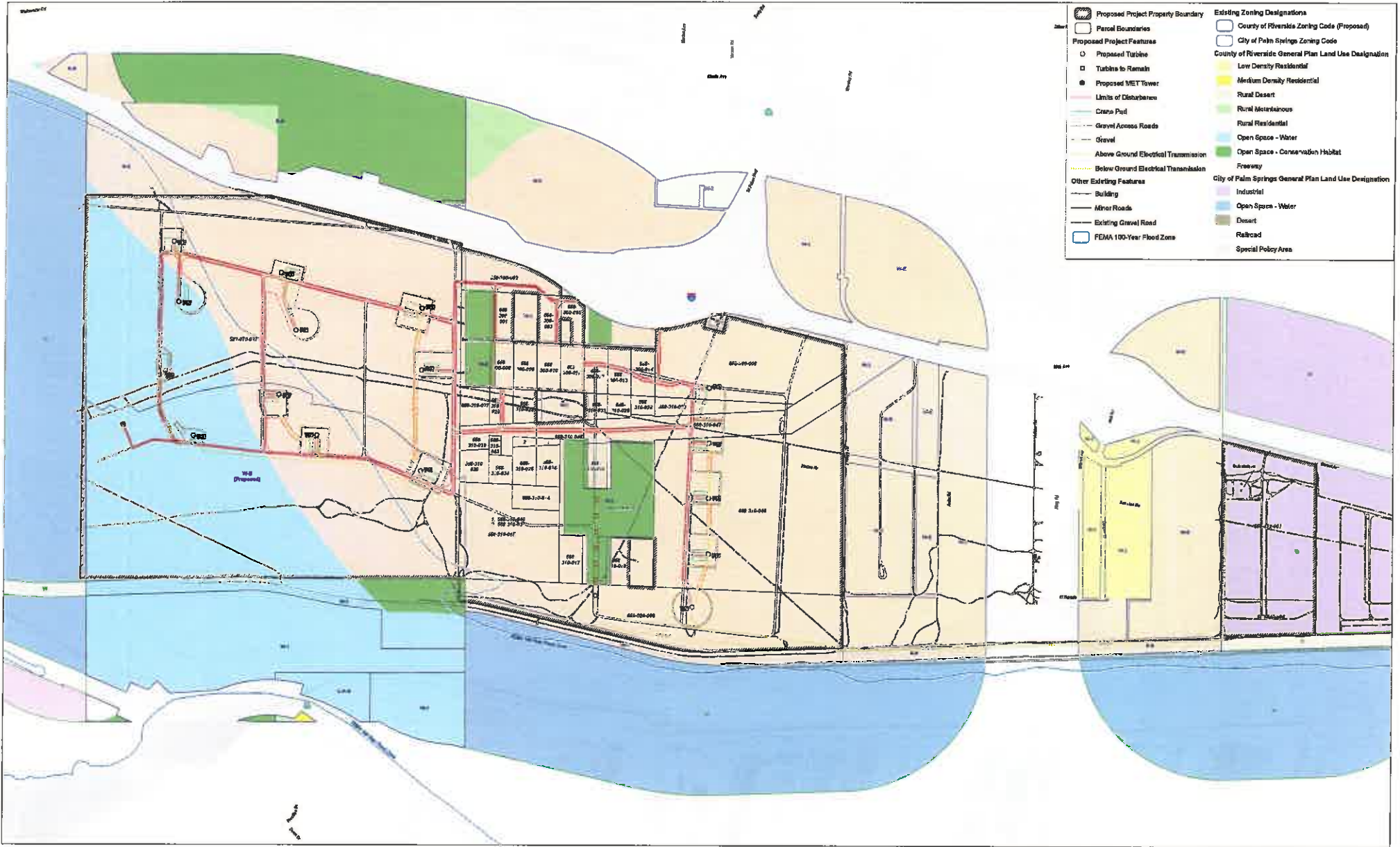


SOURCE: Parcel by AEC 2002, Aerials by Microsoft Earth, 2016 TERN, USGS 2016



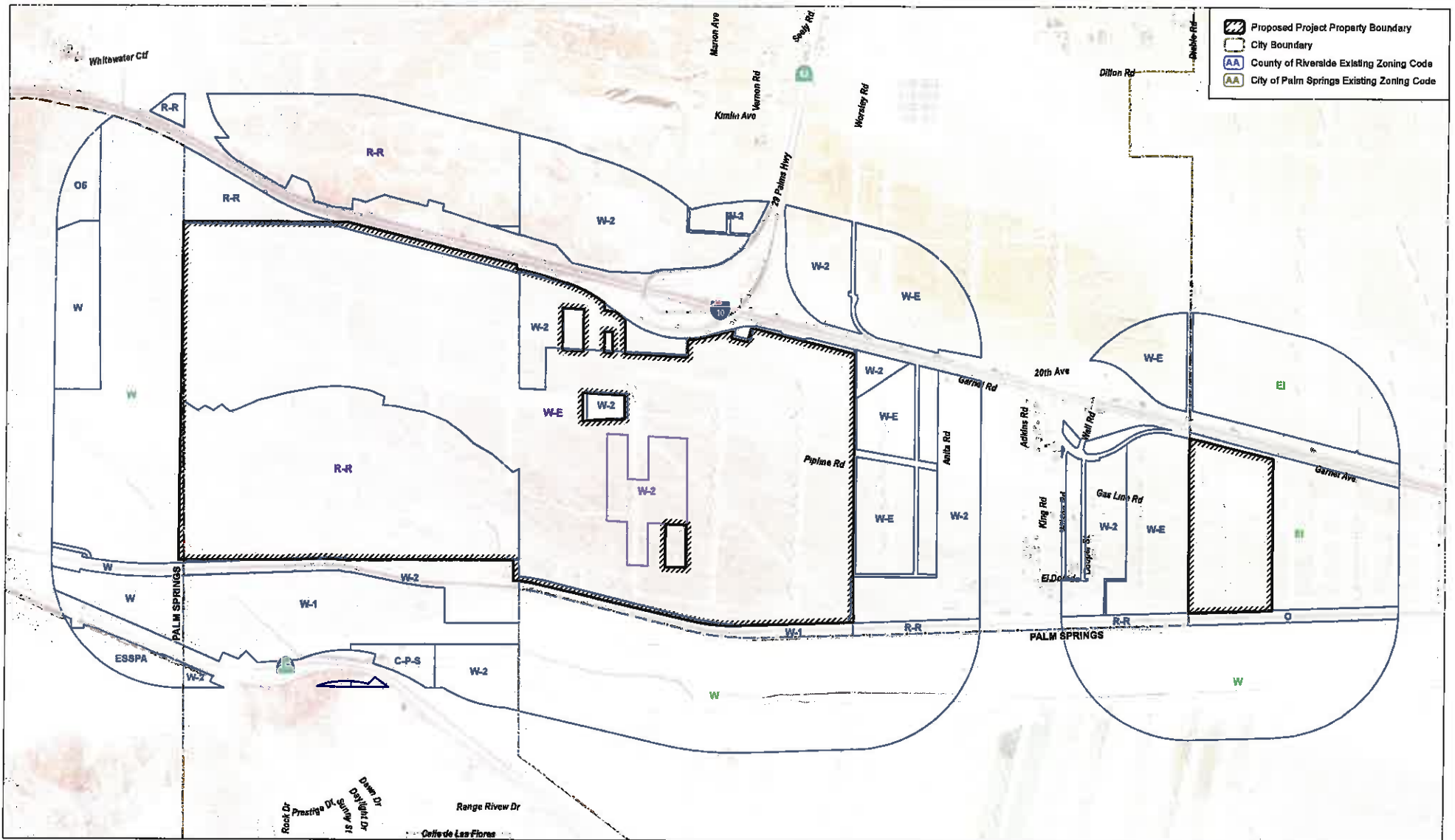
Existing Conditions

Mountain View Power Partners (MVP) Proposed Wind Energy Repower



SOURCE: Topo by AES 2020, Aerials by Riverside County 2018, FEMA, SCAG 2018





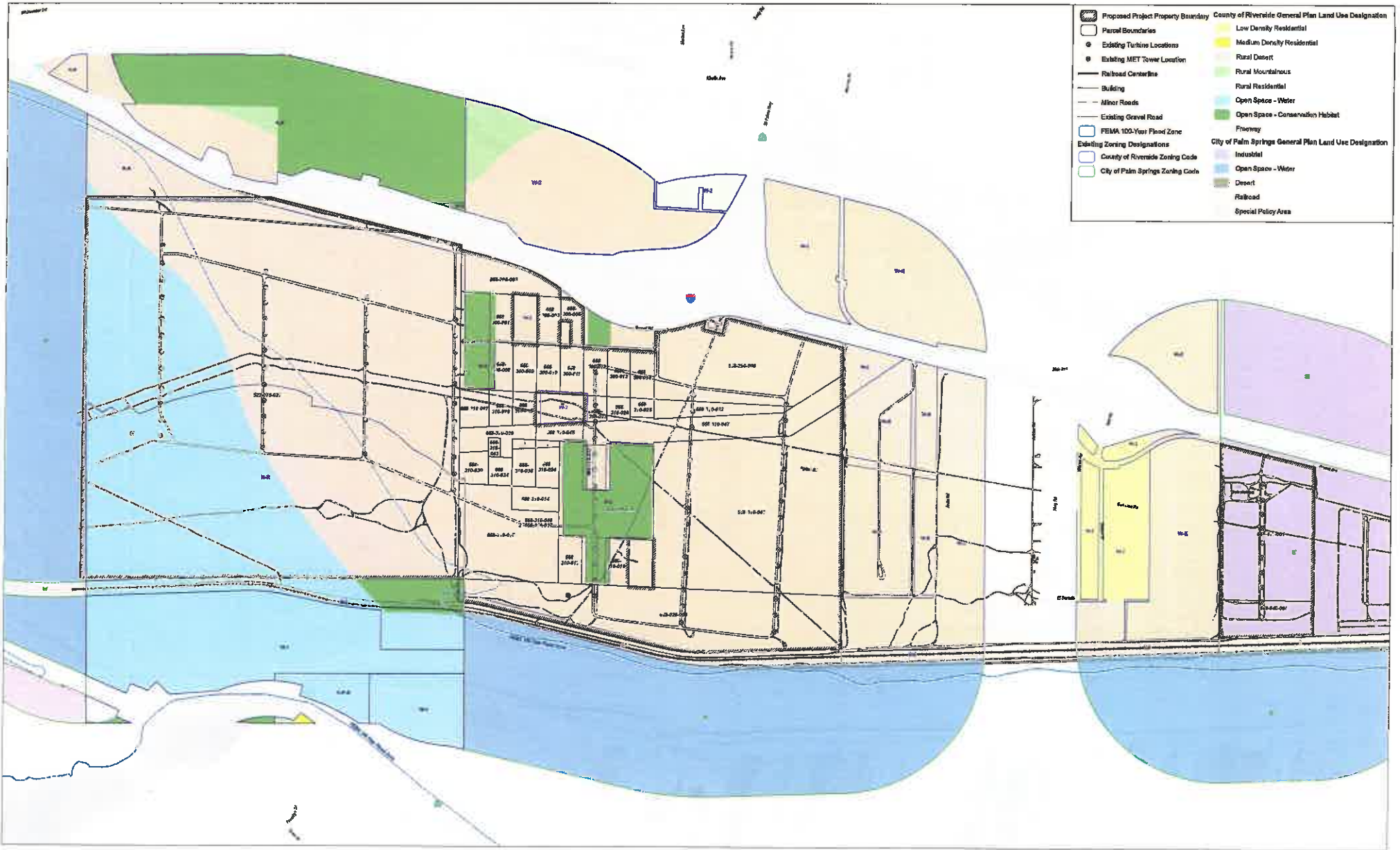
SOURCE: SCAG, County of Riverside



FIGURE 2-5

Existing Zoning Designations

Mountain View Power Partners Wind Repower Project

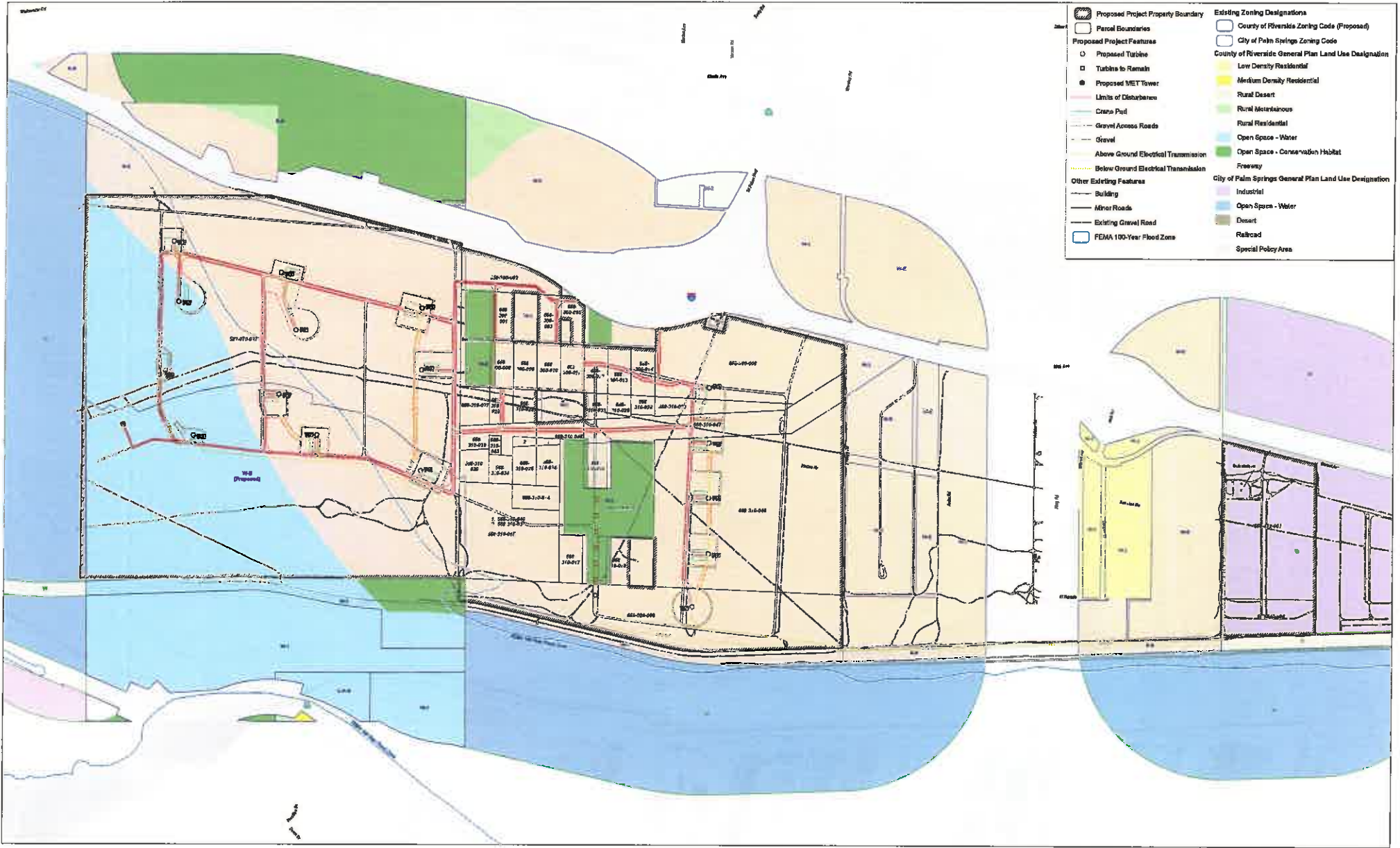


SOURCE: Parcel by AEC 2002, Aerials by Microsoft Earth, 2016 TERN, USGS 2016



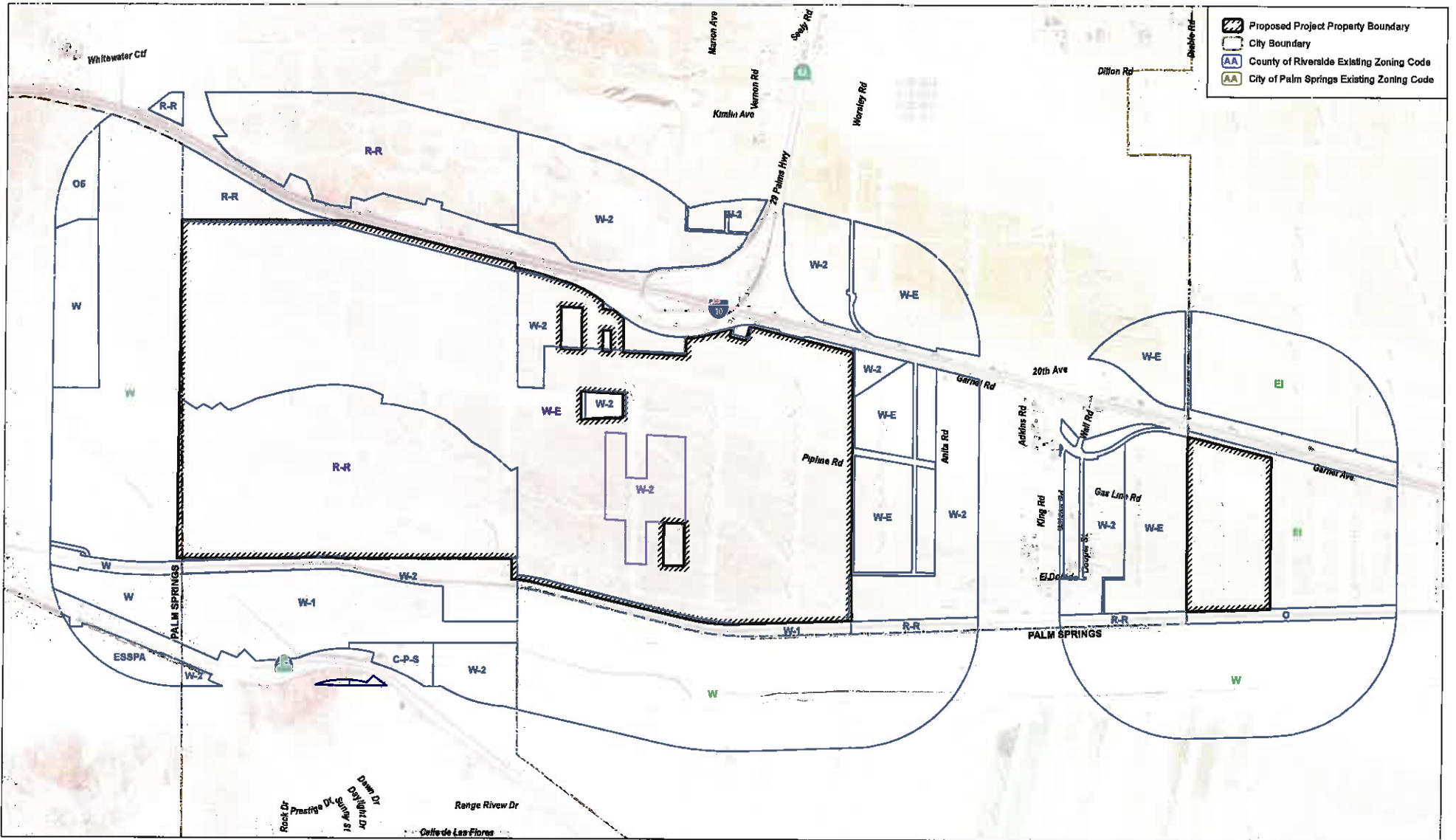
Existing Conditions

Mountain View Power Partners (MVP) Proposed Wind Energy Repower



SOURCE: Topo by AES 2020, Aerials by Riverside County 2018, FEMA, SCAG 2018

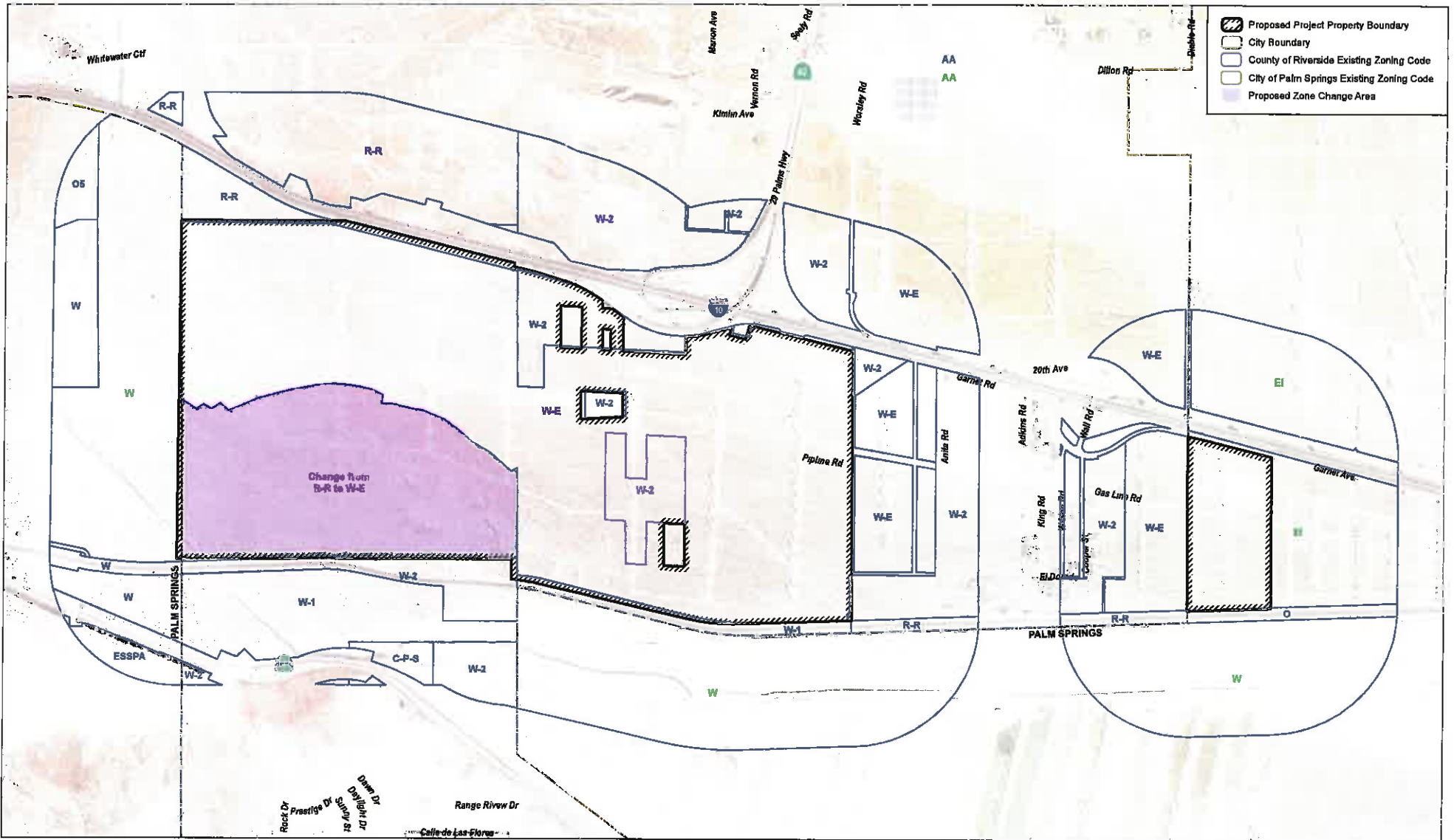




SOURCE: SCAG, County of Riverside



FIGURE 2-5
Existing Zoning Designations
Mountain View Power Partners Wind Repower Project



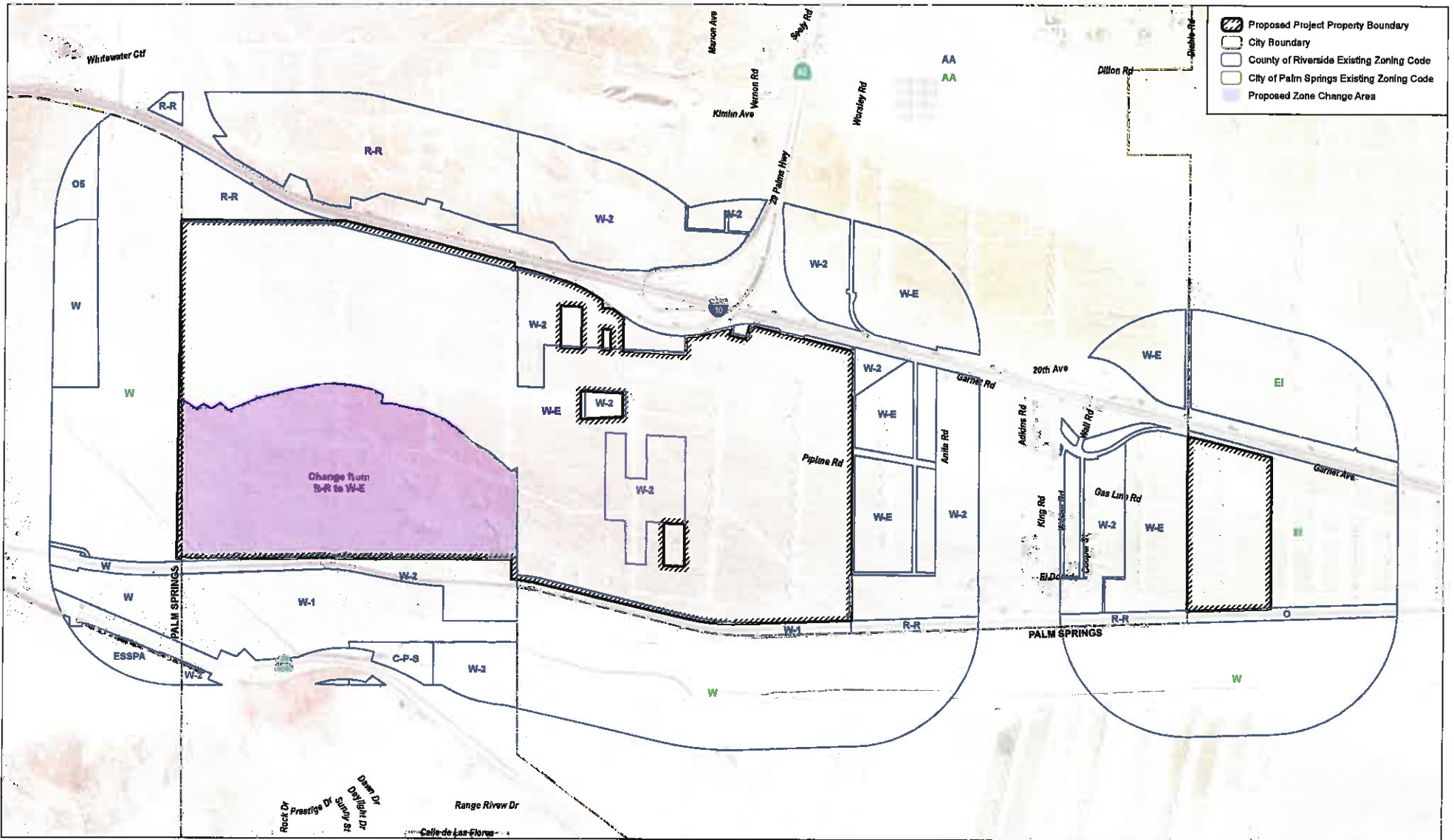
SOURCE: SCAG, County of Riverside



FIGURE 2-6

Proposed Zoning Designations

Mountain View Power Partners Wind Repower Project



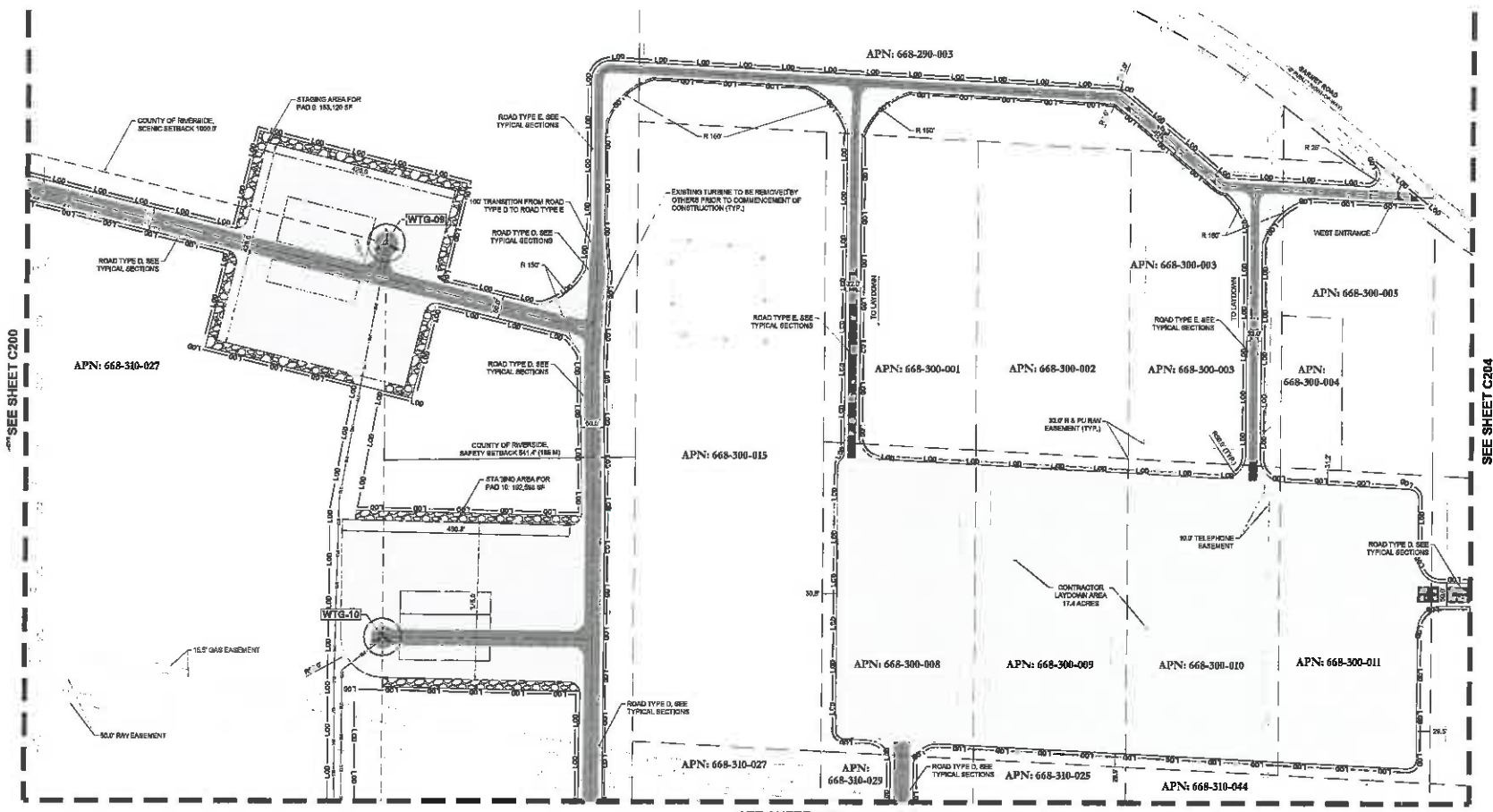
SOURCE: SCAG, County of Riverside



FIGURE 2-6

Proposed Zoning Designations

Mountain View Power Partners Wind Repower Project



| LEGEND | |
|--------|---|
| | PARCEL BOUNDARY |
| | ACCESS ROAD CENTERLINE |
| | LIMITS OF EASEMENT |
| | ELECTRIC COLLECTION LINE (OWNED BY OTHER) |
| | EXISTING OVERHEAD ELECTRIC LINE |
| | EXISTING UNDERGROUND ELECTRIC LINE |
| | EXISTING UNDERGROUND WATER LINE |
| | EXISTING UNDERGROUND SEWER LINE |
| | EXISTING UNDERGROUND GAS LINE |
| | PROPOSED 8 FOOT GAS MAIN RIGHT-OF-WAY |
| | EXISTING WATER MAIN |
| | EXISTING SEWER MAIN |
| | PROPOSED 10 FOOT TURBINE LOCATION |
| | EXISTING WETLAND AREA |
| | GROUND SURFACE, HORIZONTAL WETLAND BOUNDARY |
| | PERMANENT ACCESS ROAD (SEE TYPICAL SECTIONS) |
| | TEMPORARY USE OF ACCESS ROAD (SEE TYPICAL SECTIONS) |
| | UNDERPINNING/PLACEMAKING DISTURBANCE AREA |
| | UNEXCAVATED PLACEMAKING DISTURBANCE AREA |
| | PROPOSED TURBINE LOCATION (AREA TO BE EXCAVATED BY CONTRACTOR OR INSTALLER) |
| | AUTHORIZED DISTURBANCE AREA |
| | PROPOSED BOULDER STORAGE AREA |

- ### NOTES
- REFER TO SHEET C-200 FOR ROAD SPECIFICATIONS.
 - REFER TO SHEET C-204 FOR TEMPORARY ROAD SPECIFICATIONS AND CONSTRUCTION.
 - REFER TO SHEET C-201 FOR WATER ACCESS ROAD TYPICAL SECTIONS.
 - THE TEMPORARY ACCESS ROAD SHALL BE REMOVED AND THE INTERSECTION RECONSTRUCTED AT THE PROJECT COMPLETION.
 - IF FIELD CONDITIONS REQUIRE TO BE ADDED FOR A CATEGORY NOT SHOWN ON THE PLAN, PLEASE NOTIFY THE BUREAU FOR REVIEW AND APPROVAL.
 - CONTRACTOR TO OBTAIN LAND DEVELOPMENT/USE OF ANY FENCE OR WALL FROM NEIGHBORING PROPERTY TO CONSTRUCTION.
 - ELECTRIC ROAD BOUNDARY FOR RESPONSE ONLY. REFER TO THE ACCESS ROAD TYPICAL SECTIONS FOR CONSTRUCTION.



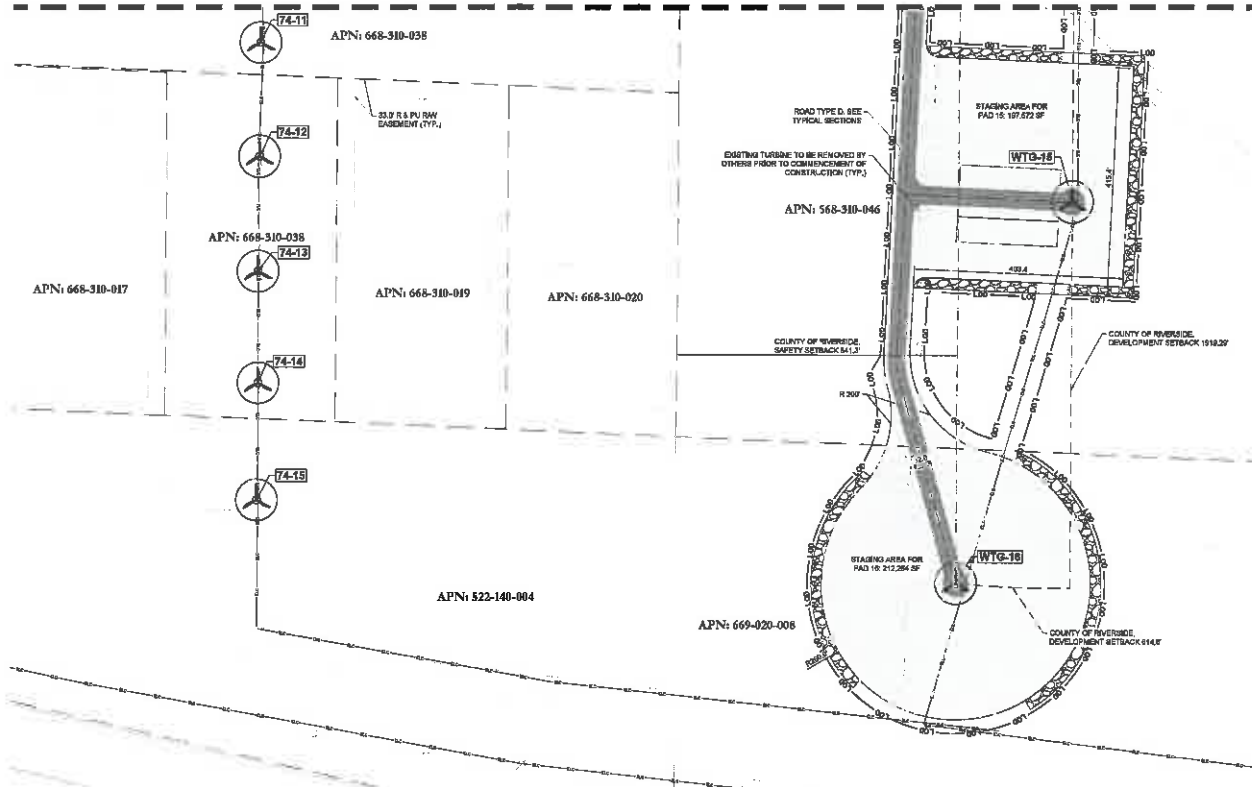
SOURCE: Kimley-Horn 2020

DUDEK

SHEET C202
Grading Plans

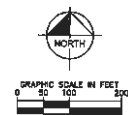
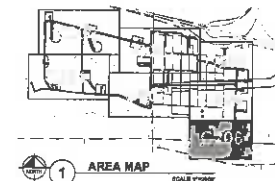
North Arrow, Graphic Scale, ANFA MAP, and other small details.

SEE SHEET C205



| LEGEND | |
|--------|---|
| | PRIVATE EASEMENT |
| | ACCESS ROAD CONSTRUCTION |
| | LIMITS OF SURVEYANCE |
| | ELECTRIC COLLECTION LINE (CONCRETE OR BRICK) |
| | EXISTING UNDERGROUND ELECTRIC LINE |
| | EXISTING UNDERGROUND WATER LINE |
| | EXISTING UNDERGROUND COMMUNICATION LINE |
| | EXISTING UNDERGROUND GAS LINE |
| | PROPOSED 3 FOOT CONCRETE CURB AROUND EXISTING FENCE |
| | EXISTING OVERHEAD UTILITY LINE |
| | PROPOSED PROJECT TURBINE LOCATION |
| | EXISTING WETLAND AREA |
| | WETLAND EASEMENT, HARVEST WITH AN ENGINEER |
| | PERMANENT ACCESS ROAD, SEE TYPICAL SECTIONS |
| | TEMPORARY MEMBER OF ACCESS ROAD, SEE TYPICAL SECTIONS |
| | WET WEATHER FLOODPLAIN, LIMITED TURBINE AREA |
| | WET WEATHER FLOODPLAIN, CONSERVATION AREA |
| | PROPOSED TURBINE LOCATION & ACTIONS PROVIDED BY OTHERS ON 1/26/2016 |
| | AUTHORIZED DISTURBANCE AREA |
| | PROPOSED EROSION STORAGE AREA |

| NOTES |
|--|
| 1. REFER TO VERTICALLY ALIGNED INFORMATION. |
| 2. REFER TO SHEET C204 FOR TEMPORARY FUGAL ENGINEERING INFORMATION. |
| 3. REFER TO SHEET C206 FOR THE SITE ACCESS ROAD TYPICAL SECTIONS. |
| 4. THE TEMPORARY ACCESS ROAD SHALL BE DEMOLISHED AND THE SITE RESTORED TO ORIGINAL CONDITION AT THE END OF CONSTRUCTION. |
| 5. IF FIELD CONDITIONS REQUIRE, THE USER FOR A CLARITY NOT SPECIFIED ON THE PLANS, PLEASE NOTIFY THE ENGINEER FOR NECESSARY REVISIONS. |
| 6. CONTRACTOR TO OBTAIN LAND OWNER APPROVAL OF ANY SERVICE REFUGES BEFORE PROCEEDING WITH CONSTRUCTION. |
| 7. ELECTRICAL, PLUMBING AND MECHANICAL SHALL REFER TO COLLECTION CHAMBER FOR DRAIN INFORMATION. |

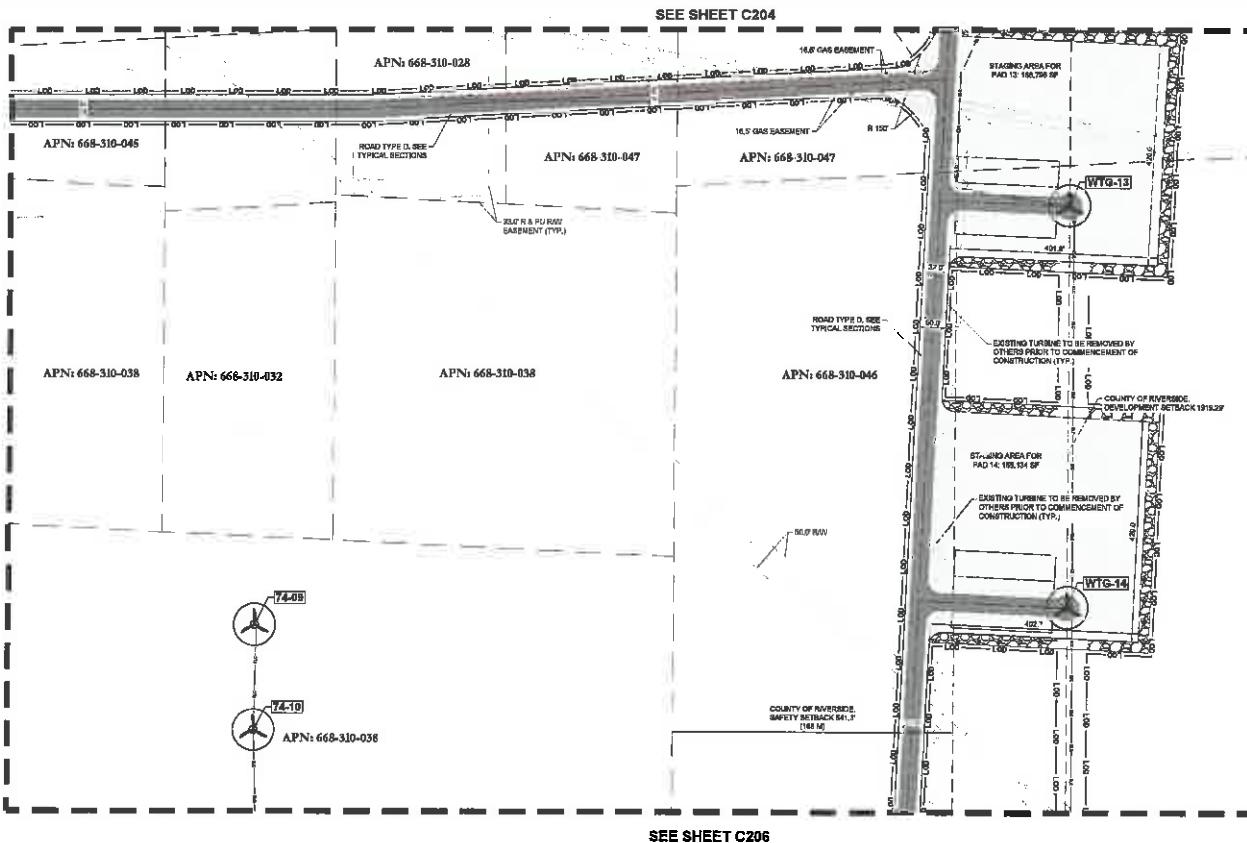


SOURCE: Kinley Horn 2020

DUDEK

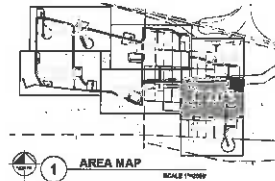
SHEET C206
Grading Plans

North Arrow/Scale/Sheet Number and Date



| LEGEND | |
|--------|--|
| | PANEL BOUNDARY |
| | ACCESS ROAD CENTERLINE |
| | LIMITS OF DISTURBANCE |
| | ELECTRIC COLLECTION LINE (SEEN BY OTHERS) |
| | EXISTING OVERHEAD ELECTRIC LINE |
| | EXISTING UNDERGROUND ELECTRIC LINE |
| | EXISTING UNDERGROUND WATER LINE |
| | EXISTING UNDERGROUND GAS LINE |
| | PROPOSED 12-FOOT CONTOUR AND EXISTING EXISTING FENCE |
| | EXISTING OVERHEAD UTILITY LINE |
| | PROPOSED PROJECT TURBINE LOCATION |
| | EXISTING WETLAND AREA |
| | GROUNDWATER HAZARD OF 100-YEAR FLOOD |
| | PERMANENT ACCESS ROAD, SEE TYPICAL SECTIONS |
| | TEMPORARY ACCESS ROAD, SEE TYPICAL SECTIONS |
| | WETWATER FLOODPLAIN COMPLIANCE AREA |
| | WETWATER TURBINE LOCATION, AS SHOWN PROVIDED BY OTHERS ON MAPS (E) |
| | AUTHORIZED ENCROACHMENT AREA |
| | PROPOSED SAFETY SETBACK AREA |

- | NOTES |
|---|
| 1. REFER TO VERTICAL CURVE NOTES SHEET 001. |
| 2. REFER TO SHEET C-04 FOR TEMPORARY FACILITY GEOMETRIC INFORMATION. |
| 3. REFER TO SHEET C-04 FOR TRUCKEE ACCESS ROAD TYPICAL SECTIONS. |
| 4. THE TEMPORARY ACCESS ROADS WILL BE REMOVED AND THE INTERSECTIONS RESTORED BY THE PROJECT COMPLETION. |
| 5. IF FIELD CONDITIONS REQUIRE THE NEED FOR A QUALITY SURVEY, THE CONTRACTOR SHALL VERIFY THE DIMENSIONS FOR THESE ENCROACHMENTS. |
| 6. CONTRACTOR TO OBTAIN LAND OWNER APPROVAL OF ANY FENCE REDUCTION NECESSARY FOR ROADWAYS FROM TO CONSTRUCTION. |
| 7. ELECTRICAL RAS SHOWN FOR INFORMATION ONLY. REFER TO COLLECTION SYSTEMS FOR DESIGN INFORMATION. |

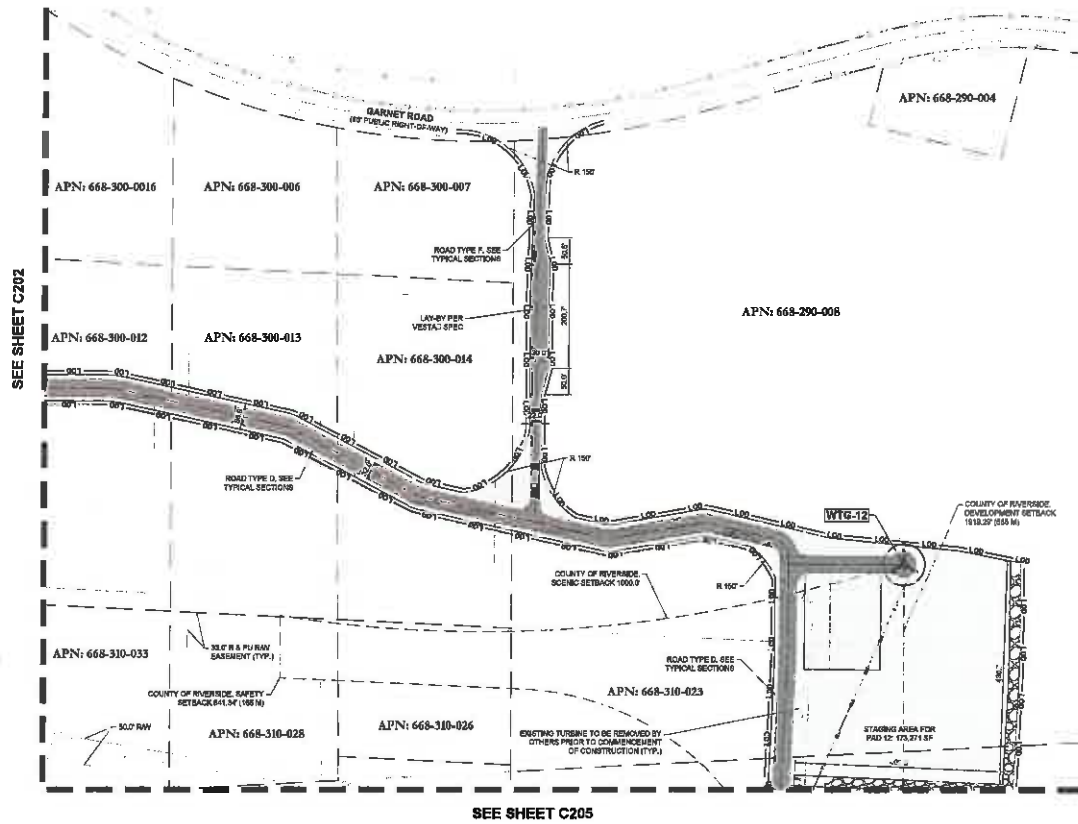


SOURCE: Kinley Horn 2020

DUDEK

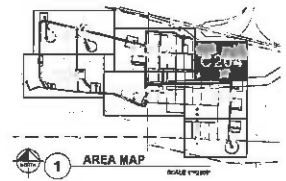
SHEET C205
Grading Plans

Small text at the bottom right corner, likely a project or drawing number.

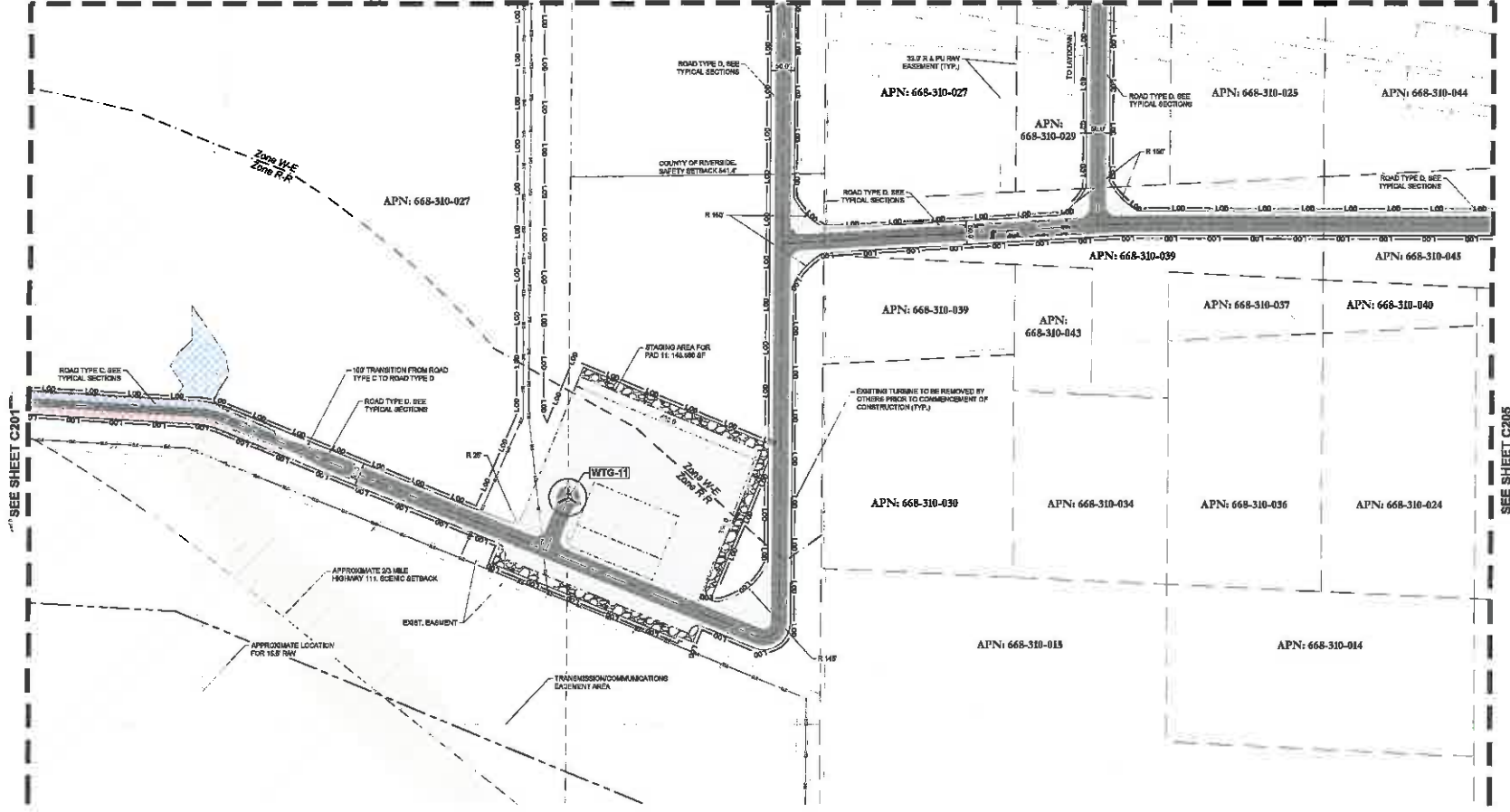


| LEGEND | |
|--------|---|
| | PARCEL BOUNDARY |
| | ACCESS ROAD CENTERLINE |
| | LIMITS OF DISTURBANCE |
| | ELECTRIC COLLECTION LINE (SEE BY OTHERS) |
| | EXISTING OVERHEAD ELECTRIC LINE |
| | EXISTING UNDERGROUND ELECTRIC LINE |
| | EXISTING UNDERGROUND COMMUNICATION LINE |
| | EXISTING UNIMPROVED RIGHT-OF-WAY |
| | PROPOSED RIGHT-OF-WAY CONSTRUCTION (100' BUFFERING) |
| | EXISTING OVERHEAD UTILITY LINE |
| | PROPOSED PROJECT TURBINE LOCATION |
| | EXISTING WETLAND AREA |
| | SHADED BUFFERING (NOTED IN CHAPTER) |
| | PERMANENT ACCESS ROAD, SEE TYPICAL SECTIONS |
| | TEMPORARY ACCESS ROAD, SEE TYPICAL SECTIONS |
| | WETLAND FLOORPLAN DISTURBANCE AREA |
| | WETLAND CONSTRUCTION AREA |
| | PROPOSED TURBINE LOCATION (AREA TO BE PROVIDED BY OWNER OR SUBCONTRACTOR) |
| | AUTHORIZED DISTURBANCE AREA |
| | PROPOSED STORAGE AREA |

- NOTES**
1. REFER TO VESTAS CIVIL WORKS SPECIFICATIONS.
 2. REFER TO SHEET C-104 FOR TEMPORARY ACCESS ROAD GEOMETRIC INFORMATION.
 3. REFER TO SHEET C-105 FOR THE BREAKDOWN ROAD TYPICAL SECTION.
 4. THE TEMPORARY ACCESS ROAD SHALL BE REMOVED AND THE INTERSECTION RESTORED AT THE PROJECT COMPLETION.
 5. IF FIELD CONDITIONS REQUIRE THE DESIGN FOR A TOWER NOT SPECIFIED ON THE PLANS, PLEASE NOTIFY THE DESIGNER FOR DESIGN REVISIONS.
 6. CONTACT THE DEVELOPING OWNER APPROVAL OF ANY FENCE AND LOCAL TOWNSHIP FOR NEW ROAD PRIOR TO CONSTRUCTION.
 7. ELECTRICAL BLUE BOOK PER REQUIREMENTS ONLY REFER TO COLLECTION DESIGN FOR DESIGN APPROVAL.



SEE SHEET C202

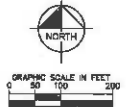
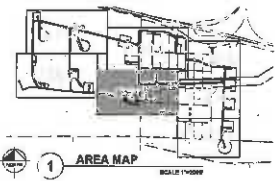


SEE SHEET C201

SEE SHEET C205

| LEGEND | |
|--------|---|
| | TYPICAL BOUNDARIES |
| | ACCESS ROAD EASEMENT |
| | EASEMENT OF 25' MINIMUM |
| | ELECTRIC COLLECTION LINE (DESIGN BY OTHERS) |
| | EXISTING OVERHEAD BUILDING LINE |
| | EXISTING UNDERGROUND ELECTRIC LINE |
| | EXISTING UNDERGROUND WATER LINE |
| | EXISTING UNDERGROUND COMMUNICATION LINE |
| | PROPOSED STREET CONTROL (NOT SHOWN) |
| | EXISTING FENCE |
| | EXISTING OVERHEAD UTILITY LINE |
| | PROPOSED PROJECT TURNING LOCATION |
| | EXISTING WETLAND AREA |
| | ROAD AND AIRPORT HAZARD WARNING SYMBOL |
| | PERMANENT 4:1 SIDE HAUL, 10:1 TYPICAL, 160' MIN. |
| | NON-PERMANENT WIDENING OF ACCESS ROAD, SEE TYPICAL SECTION |
| | 10% TEMPORARY FLOOD PLAIN DISTURBANCE AREA |
| | 10% TEMPORARY FLOOD PLAIN CONSERVATION AREA |
| | PROPOSED TURNING LOCATION (AS SHOWN PROVIDED BY OTHERS OR SURVEY) |
| | AUTHORIZED DISTURBANCE AREA |
| | PROPOSED BULLDOZER STORAGE AREA |

| NOTES | |
|-------|---|
| 1. | REFER TO VERTICAL CURVE SPECIFICATIONS. |
| 2. | REFER TO SHEET C-10 FOR TEMPORARY FLOOD DISTURBANCE OF ZONES. |
| 3. | REFER TO SHEET C-10 FOR THE 10% ACCESS ROAD TYPICAL SECTION. |
| 4. | THE 10% TEMPORARY FLOOD PLAIN DISTURBANCE SHALL BE REINSTATED AND THE DISTURBANCE RESTORED BY THE PROJECT OWNER. |
| 5. | IF FIELD CONDITIONS UNDESIRABLE TO THE USER FOR A CALIBRATED ACT OBTAINED ON THE PLANS, PLEASE NOTIFY THE ENGINEER FOR REVISIONS BEFORE PROCEEDING WITH CONSTRUCTION. |
| 6. | CONTRACTOR TO OBTAIN LAND OWNER APPROVAL OF ANY PERMITS AND CONDITIONS NEEDED FOR ANY ROAD PRIOR TO CONSTRUCTION. |
| 7. | ELECTRIC, GAS, WATER AND TELEPHONE LINES, REFER TO COLLECTION DRAWINGS FOR DEPTH INFORMATION. |



SOURCE: Kinley Horn 2020

DUDEK

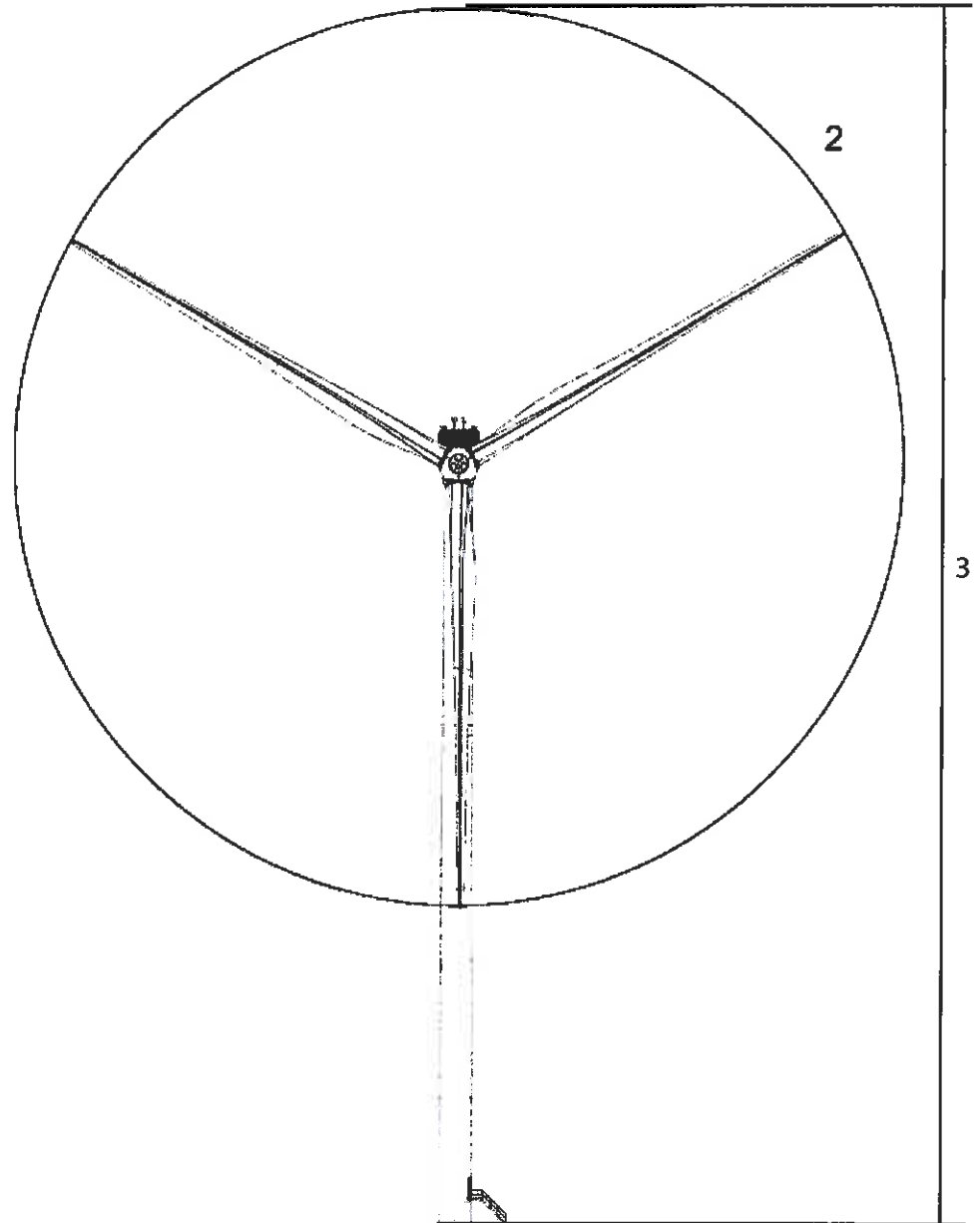
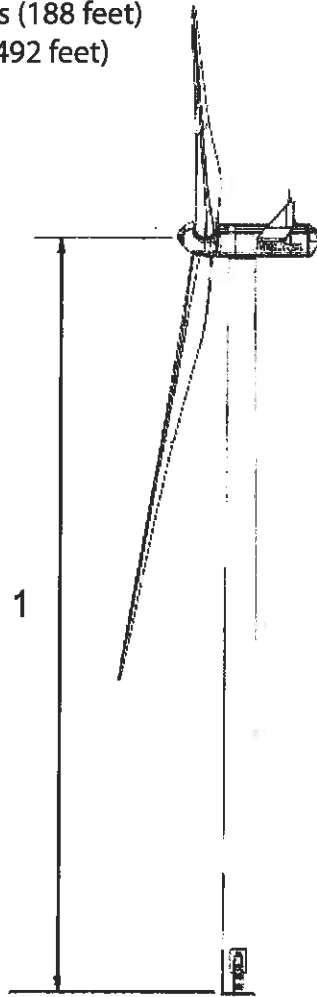
SHEET C203
Grading Plans

Source: Kinley Horn 2020

Vestas[®]

V117-4.3 MW & V117-3.6 MW WTGs

- 1. Hub Height: 91.5 meters (300 feet)
- 2. Rotor Diameter: 117 meters (188 feet)
- 3. Total Height - 150 meters (492 feet)

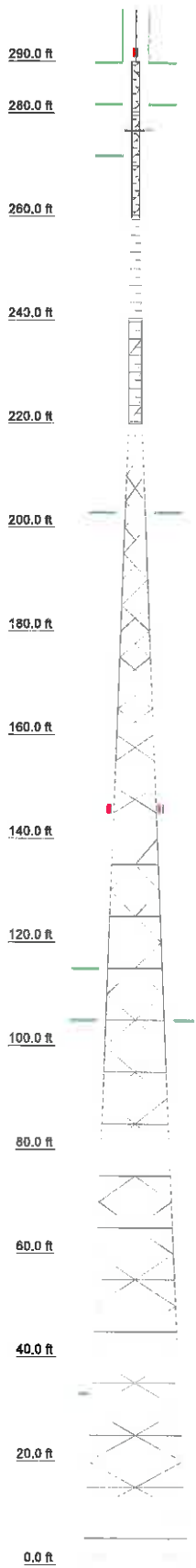


Source: Vestas 2020

DUDEK 

Proposed Wind Turbine Height

Mountain View Power Partners Wind Repower Project



DESIGNED APPURTENANCE LOADING

| TYPE | ELEVATION | TYPE | ELEVATION |
|-------------------------|-----------|---------------------------------|-----------|
| Beacon Lighting | 290 | Junction Box (20.6" X 19" X 6") | 277 |
| Lightning Rod | 290 | Side Boom w/ Wind Equip | 272 |
| Goal Post Left | 290 | (2) Side Boom w/ Wind Equip | 203 |
| Goal Post Right | 290 | (2) Side Boom w/ Wind Equip | 203 |
| Side Boom w/ Wind Equip | 290 | Beacon Lighting | 145 |
| Side Boom w/ Wind Equip | 290 | Beacon Lighting | 145 |
| Side Boom w/ Wind Equip | 282 | (2) Side Boom w/ Wind Equip | 115 |
| Side Boom w/ Wind Equip | 282 | (2) Side Boom w/ Wind Equip | 105 |
| Sensor | 277 | (2) Side Boom w/ Wind Equip | 105 |
| (2) Sensor | 277 | Sensor | 33 |

SYMBOL LIST

| MARK | SIZE | MARK | SIZE |
|------|----------|------|------|
| A | SR 1 1/2 | | |

Note: The manufacturer and model of the proposed met tower have not been determined at this time. The proposed met tower is anticipated to be approximately 93.6 meters (307.1 feet). The met tower will not exceed 100 meters (328.1 feet). If the final design of the met tower is taller than 100 meters, the applicant will submit a new request for FAA Construction Evaluation and ALUC review for the taller tower.

NOTICE OF PUBLIC HEARING
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION
www.rcaluc.org

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. For more information please contact **ALUC Planner Paul Rull at (951) 955-6893**. The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan.

The County of Riverside Planning Department should be contacted on non-ALUC issues. For more information please contact County of Riverside Planner Mr. Ken Baez at (951) 955-2009.

The proposed project application may be viewed by a prescheduled appointment and on the ALUC website www.rcaluc.org. Written comments may be submitted at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Friday from 8:00 a.m. to 3:30 p.m., or by e-mail to prull@rivco.org. Individuals with disabilities requiring reasonable modifications or accommodations, please telephone Barbara Santos at (951) 955-5132.

PLACE OF HEARING: Riverside County Administration Center
4080 Lemon Street, 1st Floor Board Chambers
Riverside California

DATE OF HEARING: January 14, 2021

TIME OF HEARING: 9:30 A.M.

Pursuant to Executive Order N-25-20, this meeting will be conducted by teleconference and at the Place of Hearing, as listed above. Public access to the meeting location will be allowed, but limited to comply with the Executive Order. Information on how to participate in the hearing will be available on the ALUC website at www.rcaluc.org

CASE DESCRIPTION:

ZAP1091PS20 – Mountain View Power Partners, LLC (Representative: Dudek) – County of Riverside Case Nos. CZ2000032 (Change of Zone), WCS200003 (WECS Permit). A proposal to decommission and remove 93 existing commercial wind turbines (wind energy conversion systems, abbreviated as “WECS”) and install 16 new commercial wind turbines with a maximum height of 492 feet above ground level on 1,255 acres, as well decommission 3 existing meteorological towers and install 1 new 328 foot tall meteorological tower, turbine pad, safety features, transformers, electrical collection system, access roads, temporary laydown and parking, located southerly of Interstate 10, easterly of Whitewater Cutoff/Tipton Road, westerly of Indian Canyon Drive, and northerly of State Highway 111. The applicant also proposes to change the zoning of 281 acres located in the southwest portion of the project from Rural Residential Zone (R-R) to Wind Energy Resource Zone (W-E) (Not located within an Airport Compatibility Zone).



RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC CASE NUMBER: ZAP1091PS20 DATE SUBMITTED: December 2, 2020

OUTSIDE
P.S
AIA

APPLICANT / REPRESENTATIVE / PROPERTY OWNER CONTACT INFORMATION

| | | | |
|-----------------|---|--------------|-------------------------------|
| Applicant | <u>Mountain View Power Partners, LLC</u> | Phone Number | <u>(562) 493-7307</u> |
| Mailing Address | <u>AES Southland</u> | Email | <u>michael.hughes@aes.com</u> |
| | <u>690 N. Studebaker Rd.</u> | | |
| | <u>Long Beach, CA 90803</u> | | |
| Representative | <u>Michael Hughes</u> | Phone Number | <u>(562) 493-7307</u> |
| Mailing Address | <u>AES Southland</u> | Email | <u>michael.hughes@aes.com</u> |
| | <u>690 N. Studebaker Rd.</u> | | |
| | <u>Long Beach, CA 90803</u> | | |
| Property Owner | <u>Gabrych Family Limited Partnership</u> | Phone Number | <u>(714) 343-4155</u> |
| Mailing Address | <u>2425 N. Riverside Dr.</u> | Email | <u>j.wianecki@yahoo.com</u> |
| | <u>Santa Ana, CA 92706</u> | | |

LOCAL JURISDICTION AGENCY

| | | | |
|-------------------------|---|-------------------------------------|--|
| Local Agency Name | <u>County of Riverside</u> | Phone Number | <u>(951) 955-2009</u> |
| Staff Contact | <u>Ken Baez</u> | Email | <u>kbaez@rivco.org</u> |
| Mailing Address | <u>Planning Department, County of Riverside</u> | Case Type | |
| | <u>4080 Lemon Street, 12th Floor</u> | <input type="checkbox"/> | General Plan / Specific Plan Amendment |
| | <u>Riverside, California 92501</u> | <input type="checkbox"/> | Zoning Ordinance Amendment |
| Local Agency Project No | <u>WCS 200003</u> | <input checked="" type="checkbox"/> | Use Permit |
| | | <input type="checkbox"/> | Site Plan Review/Plot Plan |
| | | <input type="checkbox"/> | Other |

PROJECT LOCATION

Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways

| | | | |
|-----------------------|---|---|--------------------------------|
| Street Address | <u>South of Garnet Rd., Community of Whitewater, Riverside County, CA 92282</u> | | |
| Assessor's Parcel No. | <u>Multiple (refer to attached Supplemental Information page)</u> | Gross Parcel Size | <u>~1,256 acres</u> |
| Subdivision Name | <u>Not applicable</u> | Nearest Airport and distance from Airport | <u>Palm Springs, ~7 mi. SE</u> |
| Lot Number | <u>Not applicable</u> | | |

PROJECT DESCRIPTION

If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed

Existing Land Use (describe) Please refer to the detailed project description on attached supplemental information page.

| | | | |
|--------------------------------------|--|--|-----|
| Proposed Land Use (describe) | Please refer to discussion of existing and proposed land uses on attached supplemental information page. | | |
| For Residential Uses | Number of Parcels or Units on Site (exclude secondary units) | Not applicable | |
| For Other Land Uses (See Appendix C) | Hours of Operation | The wind energy facility would operate 24 hours per day | |
| | Number of People on Site | Maximum Number | 10 |
| | Method of Calculation | During project operations, there will only be people on site during maintenance activities. | |
| | | Anticipated max would be 10 at any given time (same as existing conditions); typically 2 or 3 at any given time. | |
| Height Data | Site Elevation (above mean sea level) | 975 to 1,260 | ft. |
| | Height of buildings or structures (from the ground) | 492 | ft. |
| Flight Hazards | Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight? <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No | | |
| | If yes, describe | Due to their height (492 feet), the wind turbines could cause hazards to air navigation. The applicant has submitted the turbine locations to the FAA and has received Determinations of No Hazard to Air Navigation for all 16 wind turbine locations. The applicant expects to receive the FAA determination for the proposed Met Tower within the next few weeks. | |

- A. **NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.
- B. **REVIEW TIME:** Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.
- C. **SUBMISSION PACKAGE:**
1. Completed ALUC Application Form
 1. ALUC fee payment
 1. Plans Package (24x36 folded) (site plans, floor plans, building elevations, grading plans, subdivision maps)
 1. Plans Package (8.5x11) (site plans, floor plans, building elevations, grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
 1. CD with digital files of the plans (pdf)
 1. Vicinity Map (8.5x11)
 1. Detailed project description
 1. Local jurisdiction project transmittal
 3. Gummed address labels for applicant/representative/property owner/local jurisdiction planner
 3. Gummed address labels of all surrounding property owners within a 300 foot radius of the project site. **(Only required if the project is scheduled for a public hearing Commission meeting)**

**COUNTY OF RIVERSIDE
AIRPORT LAND USE COMMISSION**

STAFF REPORT

AGENDA ITEM: 3.3

HEARING DATE: January 14, 2021

CASE NUMBER: ZAP1035FL20 – Old Plantation Investors, L.P.,
(Representative: Cirus Development Co. Inc.)

APPROVING JURISDICTION: City of Jurupa Valley

JURISDICTION CASE NO: MA19216 [CZ20013, CUP19005] (Change of Zone,
Conditional Use Permit)

LAND USE PLAN: 2004 Flabob Airport Land Use Compatibility Plan

Airport Influence Area: Flabob Airport

Land Use Policy: Zones B1, C

Noise Levels: Portion within 55-60 CNEL contour from aircraft, and portion
outside the 55 CNEL contour

MAJOR ISSUES: The project proposes adding 9 mobile home spaces to an existing 223 space mobile home park located in Compatibility Zones B1 and C. The existing density of the mobile home park, 8.04 dwelling units per acre, already exceeds the density criteria for these compatibility zones. The proposal to add 9 mobile home spaces to the existing 223 space mobile home spaces (for a total of 232 spaces), results in a density of 8.37 dwelling units per acre, which is also inconsistent with these zones residential density criteria of a maximum 0.05 dwelling units per acre (one dwelling unit per 20 acres) for Zone B1, and a maximum 0.2 dwelling units per acre (one dwelling unit per 5 acres) for Zone C.

The mobile home park is an existing use not in conformance with the Compatibility Plan. Expansion of nonconforming uses may be permitted only if “the expansion does not result in more dwelling units than currently exist on the parcel.” The Compatibility Plan’s infill provisions are not applicable in Zone B1, and would only allow densities up to 0.4 dwelling units per acre (one dwelling unit per 2½ acres) in Zone C.

The project is also inconsistent with the ALUC open area requirements (none provided). The project does not provide the required ALUC open area used for emergency landings, largely because there is no room as the site is already fully developed with the existing 223 space mobile home park.

RECOMMENDATION: Staff recommends that the Commission find the proposed Change of Zone **INCONSISTENT** with the 2004 Flabob Airport Land Use Compatibility Plan, and find the proposed Conditional Use Permit also **INCONSISTENT**, based on the fact that the proposed residential density and ALUC open area are inconsistent with the Compatibility Zones B1 and C criteria.

PROJECT DESCRIPTION: The applicant proposes to add nine (9) new mobile home (spaces) to the existing 223-space (total 232 spaces) “Old Plantation” mobile home park on a combined total acreage of 27.72 acres over two parcels, located at 3825 Crestmore Road. The applicant also proposes changing the zoning of the site from Planned Residential (R-4) and General Commercial (C-1/C-P) to Mobile Home Subdivisions and Mobile Home Parks Zone (R-T).

A similar proposal (ZAP1010FL08) to add nine (9) new mobile home spaces to the existing 223-space mobile home park at this site was found inconsistent by the Commission on September 11, 2008 (when the project was under the jurisdiction of Riverside County).

PROJECT LOCATION: The project site is located at 3825 Crestmore Road, southerly of Mission Boulevard, both westerly and easterly of Crestmore Road, and northerly of Capary Road, approximately 1,290 feet northeasterly of the northeasterly terminus of Ruway 6-24 at Flabob Airport.

BACKGROUND:

Original Project ZAP1010FL08: On September 11, 2008, the Commission found a proposal to add 9 new mobile home spaces to an existing 223-space mobile home park, inconsistent, based on the fact that the resulting density of 8.37 dwelling units per acre was inconsistent with the Compatibility Zones B1 and C residential density criteria. It was also identified that the existing 223-space mobile home park density of 8.04 dwelling units per acre was also inconsistent (a copy of the original staff report is included in this staff report package).

Residential Density: Pursuant to the Flabob Airport Land Use Compatibility Plan, the project site is located within Compatibility Zone B1 (22.08 acres) and Zone C (5.64 acres), which Zone B1 restricts residential density to a maximum 0.05 dwelling units per acre (one dwelling unit per 20 acres), and Zone C restricts residential density to a maximum 0.2 dwelling units per acre (one dwelling unit per 5 acres).

The applicant proposes adding 9 new mobile home spaces to the existing 223 mobile home spaces (total 232) on a combined total acreage of 27.72 acres over two parcels, resulting in a density of 8.37 dwelling units per acre.

When we calculate the density based on the airport zone, the project proposes adding 7 new mobile home spaces to the existing 189 spaces in Zone B1 (total 196 spaces), resulting in a density of 8.87

dwelling units per acre, which is inconsistent with the Zone B1 residential density criteria of a maximum of 0.05 dwelling units per acre (one dwelling unit per 20 acres). The project also proposes adding 2 new mobile home spaces to the existing 34 spaces in Zone C (total 36 spaces), resulting in a density of 6.38 dwelling units per acre, which again is inconsistent with the Zone C residential density criteria of a maximum 0.02 dwelling units per acre (one dwelling unit per 5 acres).

Countywide Policy 3.3.1 (Infill) allows infill development of similar land uses in Compatibility Zone C where development not in conformance with Compatibility Plan criteria already exists. The infill criteria does not apply to Compatibility Zones A and B1. However, pursuant to Section 3.3.1 (b), the average development density shall not exceed “double the density permitted in accordance with the criteria for that location as indicated in the Compatibility Criteria matrix, Table 2A”. Therefore, the maximum allowable density would be 0.4 dwelling units per acre (one dwelling unit per 2 ½ acre) for Compatibility Zone C, and would not support the project’s inconsistent density.

Pursuant to Section 3.3.2 of the Countywide Policies, “Existing uses...not in conformance with this Compatibility Plan may only be expanded...provided that the expansion does not result in more dwelling units than currently exist on the parcel....” The existing 223-space mobile home park has an existing density of 8.04 dwelling units per acres, and is proposing an expansion by adding 9 mobile homes spaces, resulting in an increase in density to 8.37 dwelling units per acre. Therefore, the proposed expansion does result in more dwelling units than currently exist on the parcel, and therefore cannot be considered an existing non-conforming use.

Prohibited and Discouraged Uses: The applicant does not propose any uses specifically prohibited or discouraged in Compatibility Zones B1 or C (children’s schools, day care centers, libraries, hospitals, nursing homes, places of worship, buildings with more than two aboveground habitable floors, critical community infrastructure facilities, and aboveground bulk storage of 6,000 gallons or more of flammable or hazardous materials, highly noise-sensitive outdoor nonresidential uses), other than the proposed prohibited residential density range.

Noise: The Flabob Airport Land Use Compatibility Plan depicts a portion of the project being located within the 55-60 CNEL contour range from aircraft noise, and a portion outside the 55 CNEL contour. Mobile homes are marginally acceptable within this contour range. The mobile homes would be affected by aircraft generated noise. However, standard construction is normally considered to provide for a 15 dB reduction from exterior noise levels. A condition is recommended to incorporate noise attenuation measures into the design of the mobile homes to such extent as may be required to ensure that interior noise levels from aircraft operations are at or below 45 CNEL.

Part 77: The elevation of Runway 6-24 at its easterly terminus is approximately 766.8 feet above mean sea level (766.8 feet AMSL). As the runway length does not exceed 3,200 feet, the relevant slope for purposes of determining Federal Aviation Administration notice requirements is 50:1. At a distance of approximately 1,290 feet from the closest point of the runway, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding 791 feet AMSL. The project’s site highest elevation is 772 feet AMSL, with a proposed maximum

building height of 18 feet, resulting in the project's highest top point elevation of 790 feet AMSL. Therefore, review of buildings by the FAA Obstruction Evaluation Service (FAAOES) for height/elevation reasons was not required

Hazards to Flight: Land use practices that attract or sustain hazardous wildlife populations on or near airports significantly increase the potential of Bird Aircraft Strike Hazards (BASH). The FAA strongly recommends that storm water management systems located within 5,000 or 10,000 feet of the Airport Operations Area, depending on the type of aircraft, be designed and operated so as not to create above-ground standing water. To facilitate the control of hazardous wildlife, the FAA recommends the use of steep-sided, rip-rap lined, narrow, linearly shaped water detention basins. All vegetation in and around detention basins that provide food or cover for hazardous wildlife should be eliminated. (FAA Advisory Circular 5200-33C). The project is located 1,290 feet from the runway, and therefore would be subject to the above requirement.

Although the nearest portion of the proposed project is located within 10,000 feet of the runway (approximately 1,290 feet), the project utilizes an existing underground detention systems which will not contain surface water or attract wildlife and, therefore, would not constitute a hazard to flight.

Open Area: Compatibility Zone B1 requires a minimum of 30% and Zone C requires 20% of open area for projects 10 acres or larger be set aside as open area that could potentially serve as emergency landing areas. Approximately 22.08 acres is located within Zone B1, which requires the project to provide a minimum 6.62 acres of open area consistent with ALUC open area criteria (300 feet by 75 feet minimum shape, and prohibit obstructions greater than 4 feet in height that are at least 4 inches in diameter). The project's 5.64 acres located within Zone C would not require ALUC open area as it is below 10 acres in size.

The applicant has not provided any ALUC open area, as the site is already 100% developed as a 223 space mobile home park. The project site is physically adjacent to the Santa Ana River, which could serve as the ALUC open area if a Special Policy 3.3.6 was to be considered.

Change of Zone: The applicant also proposes changing the zoning of the site from Planned Residential (R-4) and General Commercial (C-1/C-P) to Mobile Home Subdivisions and Mobile Home Parks Zone (R-T). The proposed change of zone would be consistent with the site's existing underlying General Plan land use designation of High Density Residential (8 to 14 dwelling units per acre), however, both of these would be inconsistent with the Compatibility Zones B1 and C residential density criteria as described above. Therefore, the proposed change of zone is inconsistent with the 2004 Flabob Airport Land Use Compatibility Plan.

CONDITIONS (in the event of an overrule):

1. Any outdoor lighting installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.

2. The following uses shall be prohibited:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, outdoor production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Children's schools, day care centers, libraries, hospitals, nursing homes, places of worship, buildings with more than two aboveground habitable floors, critical community infrastructure facilities, and aboveground bulk storage of 6,000 gallons or more of flammable or hazardous materials.
 - (f) Highly noise-sensitive outdoor nonresidential uses.
 - (g) Other hazards to flight.
3. Prior to issuance of building permits, the property owner shall convey an avigation easement to Flabob Airport. Copies of the recorded avigation easement shall be forwarded to the Airport Land Use Commission and to the City of Jurupa Valley.
4. Any proposed stormwater basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the stormwater basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at RCALUC.ORG which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin.

5. Buildings shall be limited to a maximum height of 18 feet and a maximum top point elevation of 790 feet above mean sea level unless a "Determination of No Hazard to Air Navigation" letter authorizing a higher top point elevation has been issued by the Federal Aviation Administration Obstruction Evaluation Service.
6. Noise attenuation measures shall be incorporated into the design of the mobile homes structures, to the extent such measures are necessary to ensure that interior noise levels from aircraft operations are at or below 45 CNEL.
7. The project does not propose rooftop solar panels at this time. However, if the project were to propose solar rooftop panels in the future, the applicant/developer shall prepare a solar glare study that analyzes glare impacts, and this study shall be reviewed by the Airport Land Use Commission.

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

NOTICE

THERE IS AN AIRPORT NEARBY.

**THIS STORM WATER BASIN IS DESIGNED TO HOLD
STORM WATER FOR ONLY 48 HOURS AND
NOT TO ATTRACT BIRDS**

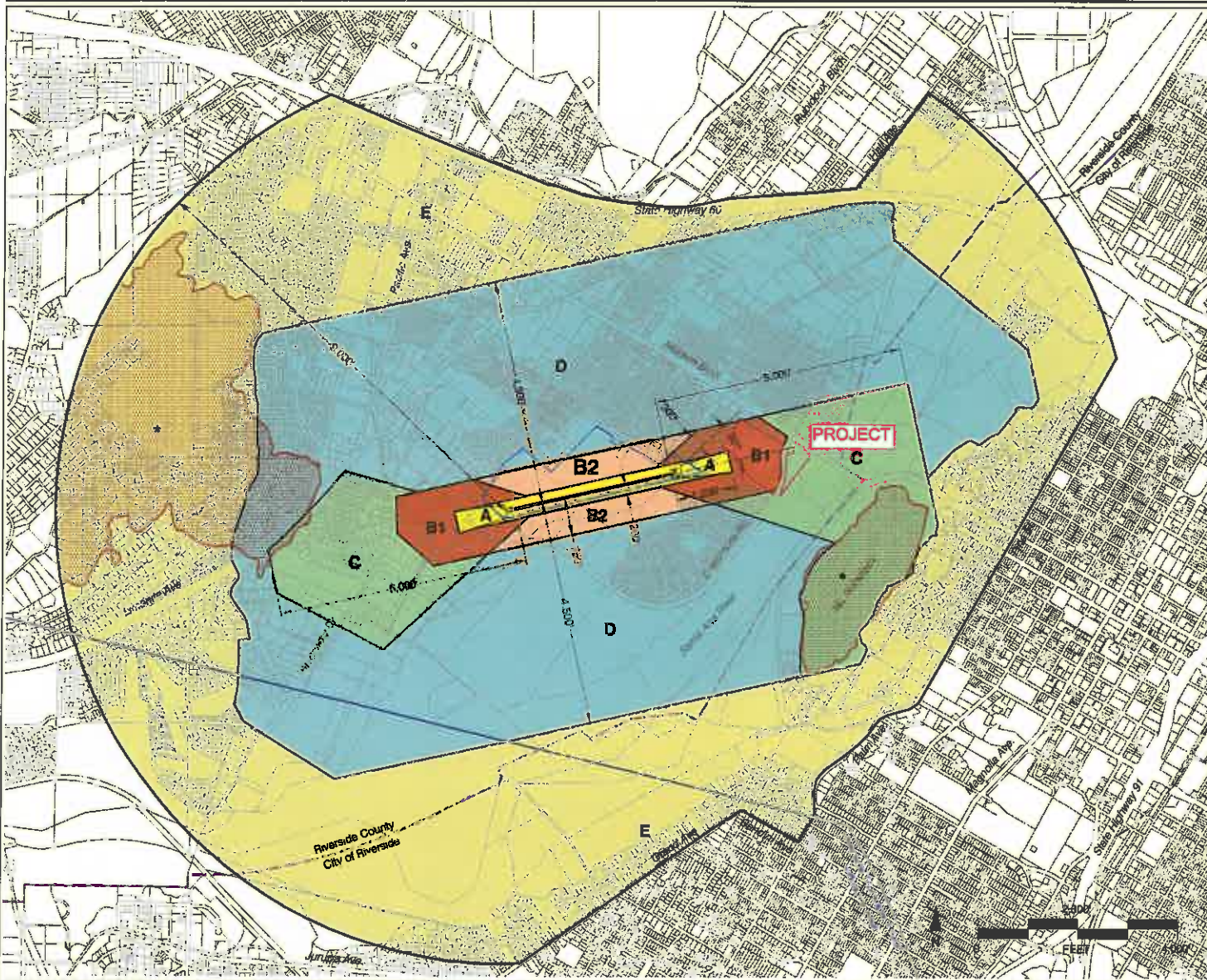
**PROPER MAINTENANCE IS NECESSARY TO AVOID
BIRD STRIKES**



IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:

Name: _____

Phone: _____



Legend

Compatibility Zones

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E
- Height Review Overlay Zone

Boundary Lines

- Airport Property Line
- City Limits

Note

Airport influence boundary measured from a point 200 feet beyond runway ends in accordance with FAA airspace protection criteria (FAR Part 77). All other dimensions measured from runway ends and centerlines.

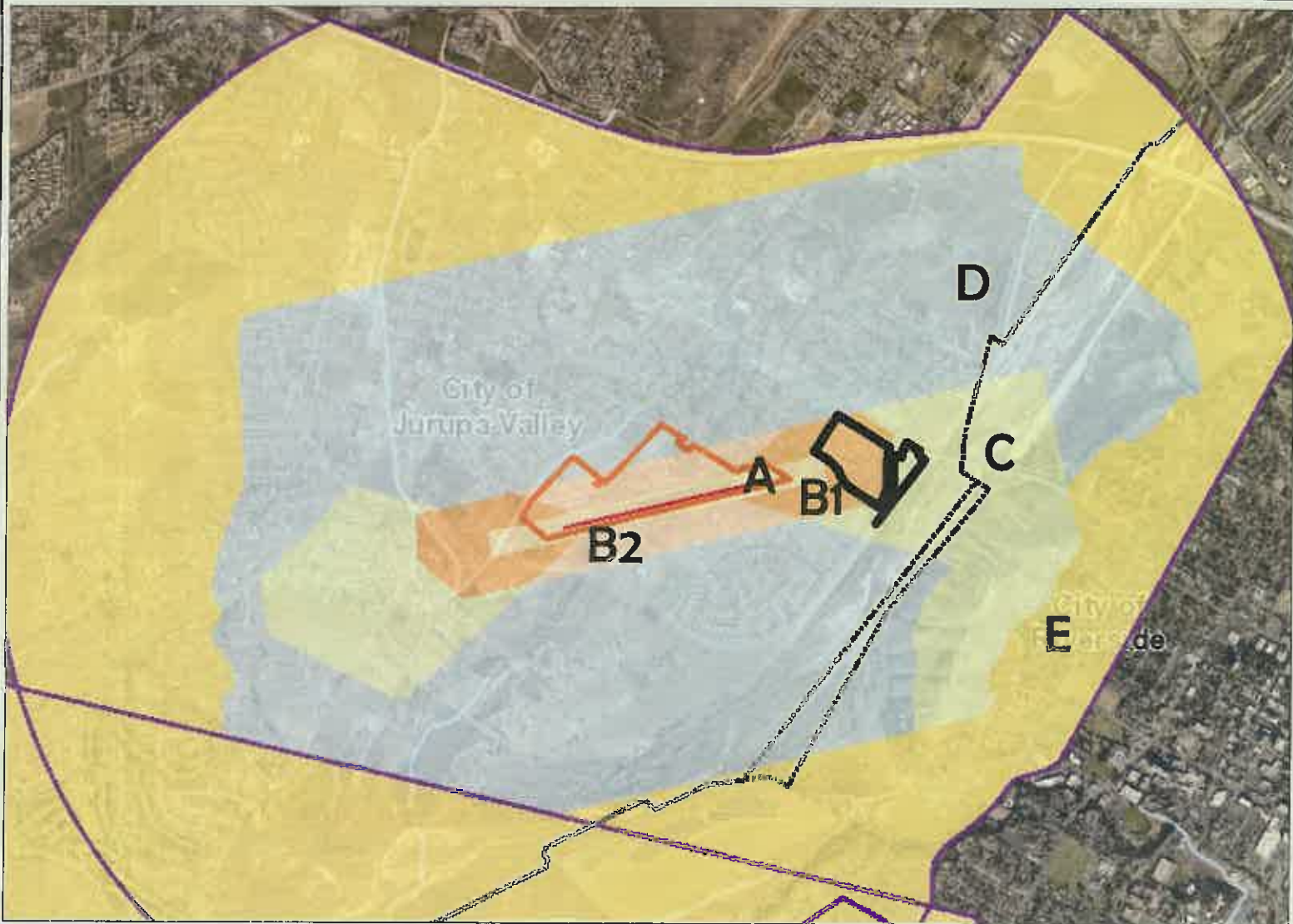
See Chapter 2, Table 2A for compatibility criteria associated with this map.

Riverside County
Airport Land Use Commission
Riverside County
Airport Land Use Compatibility Plan
Policy Document
 (Adapted December 2004)

Map FL-1

Compatibility Map
Flabob Airport

Map My County Map



Legend

- Runways
- ▣ Airports
- ▣ Airport Influence Areas
- Airport Compatibility Zones**
- ▣ OTHER COMPATIBILITY_ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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Notes

Map My County Map



Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5



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Notes

Map My County Map



- Legend**
-  Parcels
 -  Blue Line Streams
 -  City Areas
 -  World Street Map



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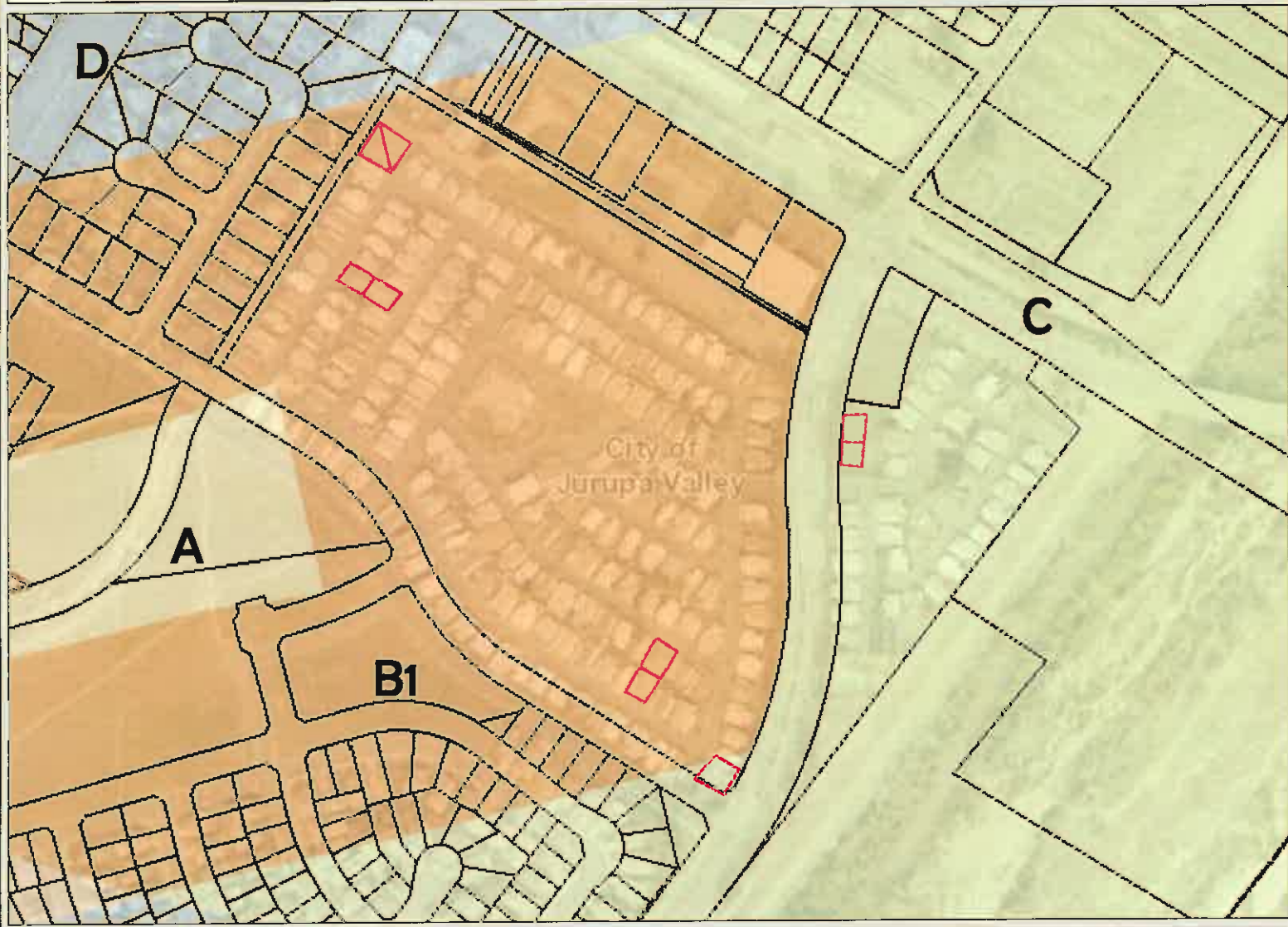
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Notes

Map My County Map



Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5



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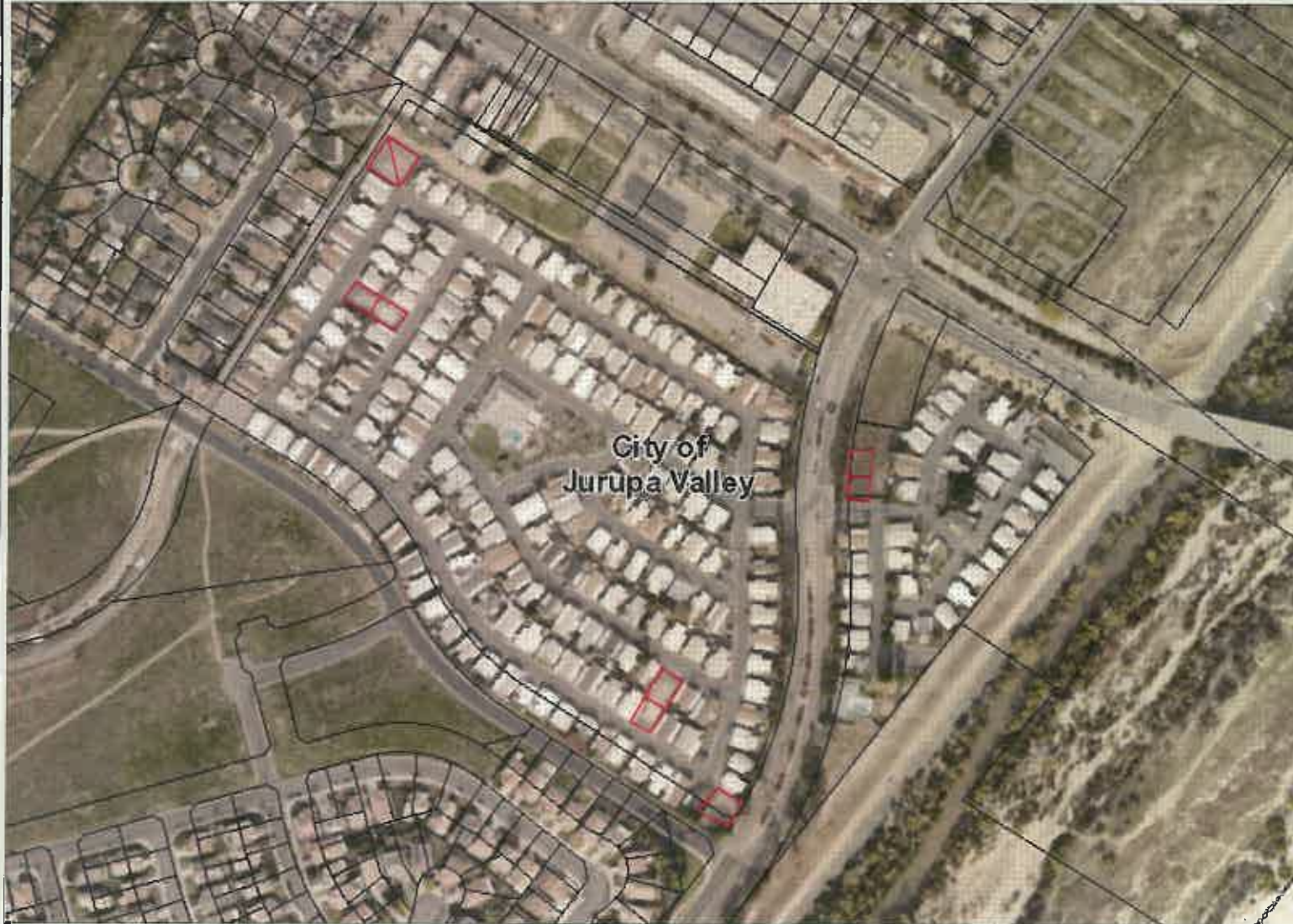


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Notes

Map My County Map



Legend

- Parcels
- Blueline Streams
- City Areas
- World Street Map



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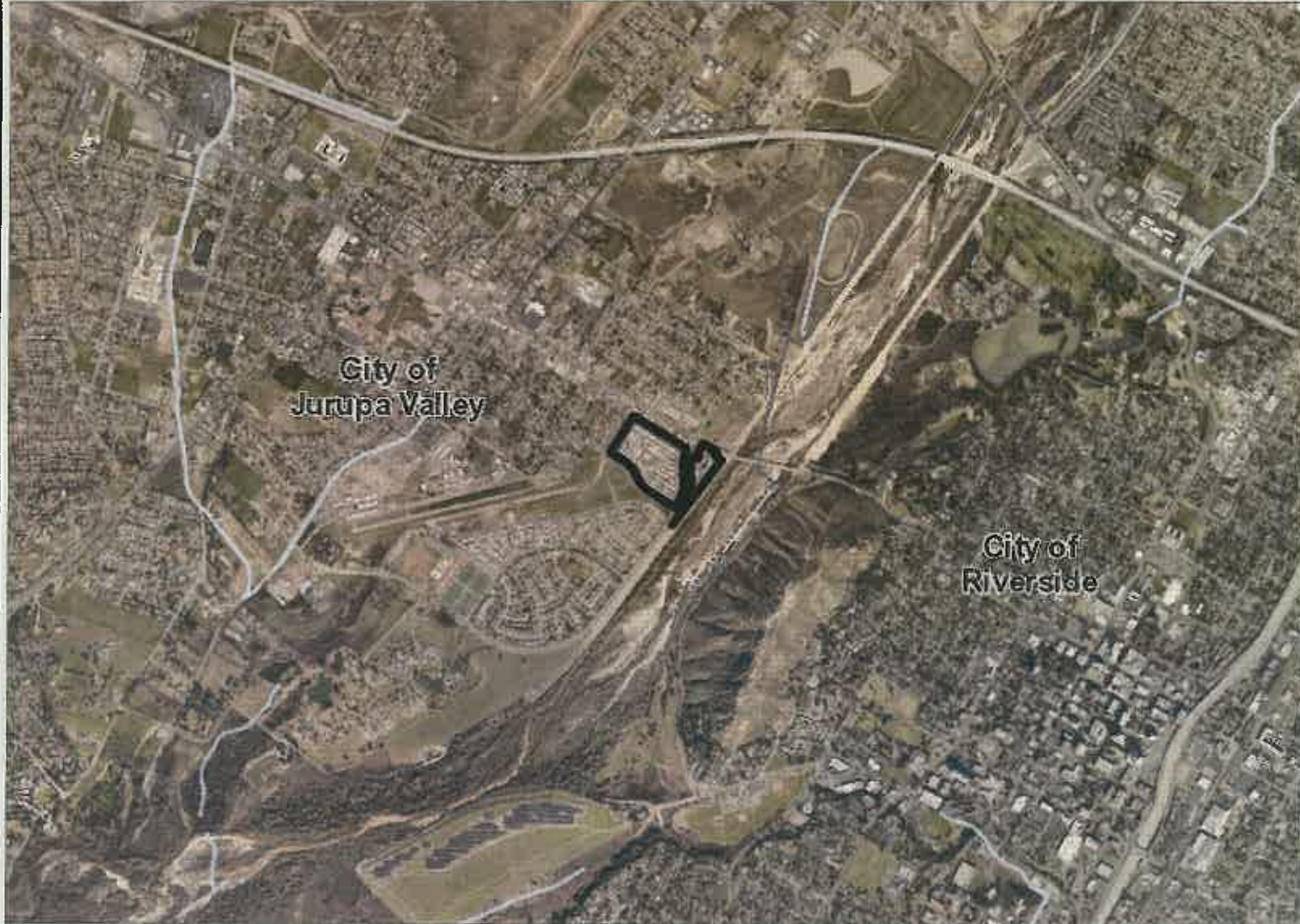
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Map My County Map



Legend

- Blueline Streams
- City Areas
- World Street Map



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0 3,000 6,019 Feet

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Notes

Map My County Map



Legend

- Blueline Streams
- ▣ City Areas
- World Street Map

Notes



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Map My County Map



- Legend**
-  Parcels
 -  Blueline Streams
 -  City Areas
 -  World Street Map



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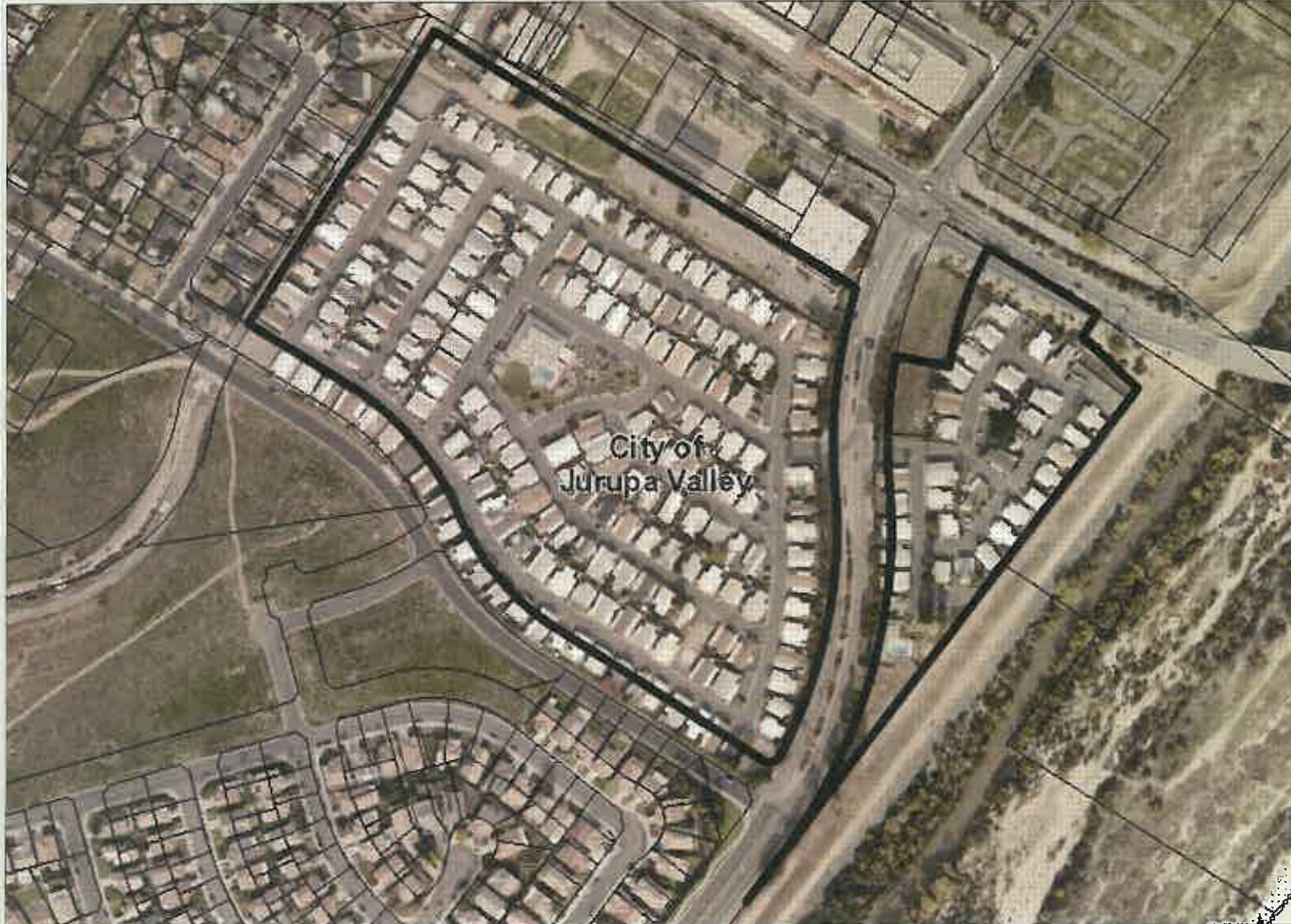


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Notes

Map My County Map



Legend

-  Parcels
-  Blueline Streams
-  City Areas
-  World Street Map



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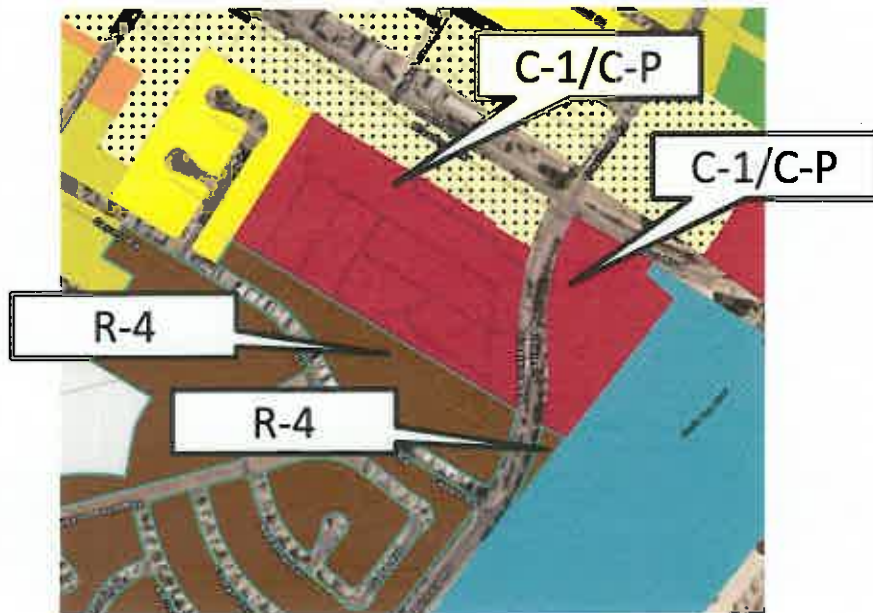
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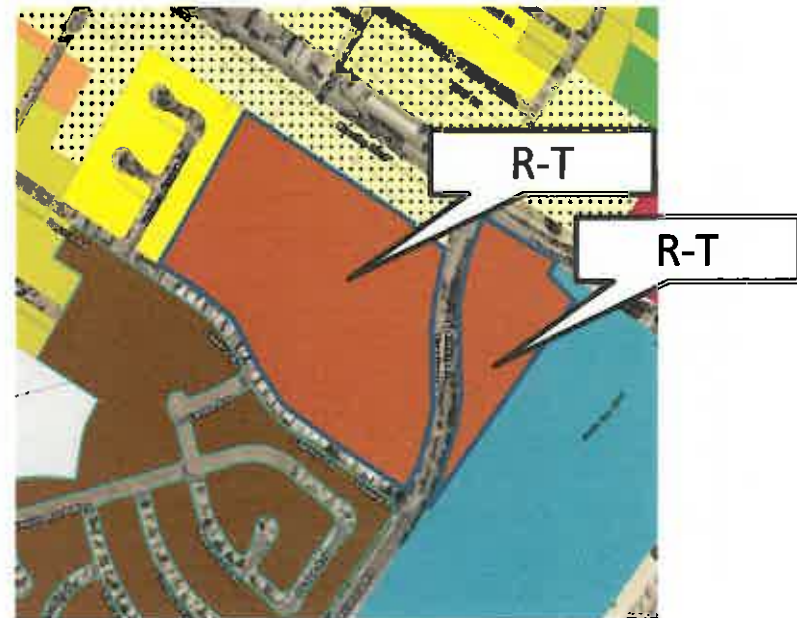
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Notes

Existing Zoning



Proposed Zoning



ZONING INFORMATION

CONVENTIONAL USE PERMIT
ZONED R4 & C1/P
HIGH DENSITY RESIDENTIAL & COMMERCIAL
RETRACTED PER COUNTY OF INVERSIDE (PERMANENT STRUCTURE ONLY) (LEGAL NON CONFORMING)
FRONT: 10' & 0" SIDE: 8' & 0" REAR: HOME REAR: 0'
FOR STATE OF CALIFORNIA (MOBILE HOME PARK) (TITLE 25)

(1) THE MINIMUM DISTANCE REQUIRED FOR THE SEPARATION OF A MOBILE HOME FROM A PERMANENT BUILDING SHALL BE 10.00 FEET. THE MINIMUM DISTANCE REQUIRED FOR THE SEPARATION OF A MOBILE HOME FROM ANOTHER MOBILE HOME SHALL BE 10.00 FEET FROM SIDE TO SIDE, 10.00 FEET FROM SIDE TO FRONT AND 10.00 FEET FROM FRONT TO FRONT OR FRONT TO REAR. A MOBILE HOME SHALL BE A MOBILE HOME UNLESS IT IS OCCUPIED BY A PERSON WHOSE RESIDENCE IS NOT REQUIRED FROM A LOT LINE BOUNDING A RESIDENCE.

(2) IN MOBILE HOME PARKS, OR PORTAGES, CONSTRUCTED PRIOR TO SEPTEMBER 15, 1981, NO MOBILE HOME SHALL BE LOCATED CLOSER THAN 4.00 FEET FROM ANY PERMANENT BUILDING OR ANOTHER MOBILE HOME.

BULK: N/A
HEIGHT: 40' & 35'
PARKING RESTRICTIONS:
2 SPACES PER UNIT

THIS INFORMATION WAS OBTAINED FROM THE COUNTY OF INVERSIDE, 4500 LEMAY AVE. #2501, PH. (951) 953-3200 ATTN: PUBLIC COUNCIL

FLOOD ZONE INFORMATION

ZONED: B
AREAS BETWEEN LIMITS OF THE 100 YEAR FLOOD AND 500 YEAR FLOOD, OR CERTAIN AREAS SUBJECT TO 100 YEAR FLOODING WITH AREAS OF THE 100 YEAR FLOOD, SHALL BE SUBJECT TO THE CONTINGENT FLOODING AREA OR LESS THAN ONE SQUARE MILE, OR AREA PROTECTED BY LEVES FROM THE BAY FLOOD. (METER 942080)

PANEL NO. 060348 0710A
DATED: APRIL 15, 1980

NO FIELD SURVEYING WAS PERFORMED TO DETERMINE THIS ZONE AND AN ELEVATION CERTIFICATE MAY BE NEEDED TO VERIFY THIS DETERMINATION OF APPLY FOR PERMITS FROM THE FEDERAL EMERGENCY MANAGEMENT AGENCY.

GENERAL NOTES

- 1. SURVEYOR HAS RELIED UPON INVESTIGATION SUPPLIED IN THE CURRENT TITLE REPORT BY STORVING TITLE GUARANTY COMPANY, REPORT NO. 519280738 DATED AUGUST 14, 2004, TO DISCLOSE RECORD ENCUMBRANCES THAT BURDEN OR BOUND THIS PROPERTY.
- 2. BUILDING SETBACK LINES SHOWN HEREIN INDICATE THE INFORMATION AS SUPPLIED BY THE CITY REPRESENTATIVE HEREIN.
- 3. THE PHRASE "FLOOD HAZARD" WITHIN THE DEDICATION HEREIN REFERS TO SPECIAL FLOOD HAZARD AREAS AS DEFINED BY FEMA AND THE NFP AND ONLY REFERS TO THOSE AREAS ON FLOOD INSURANCE RATE MAPS AS BEING WITHIN ZONES A OR V OF SAID MAPS.
- 4. BY OBSERVATION, NO ENCUMBRANCES ARE KNOWN OR FOUND ON SUBJECT PROPERTY.
- 5. NO MENTIONMENTION WAS FOUND OR SET AT THE PROPERTY CORNERS UNLESS NOTED OTHERWISE.
- 6. ASSASSIN'S PARCEL NUMBER FOR THIS PROPERTY ARE 181-130-008 & 181-130-103.
- 7. DISTANCES AND DISTANCES ARE RECORD AND MEASURED PER BOOK 10, PAGES 23, 24 AND 54 RECORDS OF SAID COUNTY, UNLESS NOTED OTHERWISE.
- 8. SURVEYOR TAKES NO RESPONSIBILITY OF ITEMS OBTAINED DUE TO A RESULT OF VISUAL OBSERVATIONS, SUCH AS AUTOMOBILES, EQUIPMENT IN TRUCK SPACES, ETC.
- 9. THE BUILDING SQUARE FOOTAGE INDICATED ON THIS PLAN IS BASED ON EXTERIOR DIMENSIONS AT GRADE LEVEL ONLY.
- 10. THIS PLAN AND/OR DATA FILES INCLUDING ALL CONTENTS HEREIN ARE FOR THE BULK LINES AND PARTS INDICATED HEREIN INCLUDING THEIR SUCCESSORS AND JOINTS. ANY DEVIATION OR MODIFICATION OF THIS PLAN AND/OR DATA FILES WITHOUT PRIOR WRITTEN AGREEMENT BY ANACAL, CONDUCTING IS PROHIBITED AND IS THE RESPONSIBILITY OF THE PARTIES USING SAID DRAWING AND/OR DATA FILES. UPON THE BASIS OF THIS PLAN AND/OR DATA FILES ANACAL ENGINEERS INC. ASSUMES ALL RESPONSIBILITIES OF THE ACCURACY AND GENERAL CONTENT OF SAID PLAN AND/OR DATA FILES CONTAINED HEREIN.
- 11. THERE IS NO EVIDENCE OF SURVEY MARKS WORK, EXTERIOR BUILDING CONSTRUCTION OR BUILDING ADDITIONS OBSERVED THAT HAVE BEEN DONE WITHIN RECENT MONTHS.
- 12. THERE IS NO EVIDENCE OF SOLID WASTE DUMP, SLUMP OR SANITARY LAND FILL USE.
- 13. THROUGH RESEARCH DONE AT THE CITY'S &/OR COUNTY'S PUBLIC WORKS DEPARTMENT THERE ARE NO CHANGES IN THE STREET RIGHT OF WAY LINES AS SHOWN HEREIN. FURTHERMORE THERE IS NO CONSIDERABLE EVIDENCE OF ANY RECENT STREET OR SIDEWALK CONSTRUCTION OR REPAIRS.
- 14. THIS PLAN IS NOT FOR CONSTRUCTION PURPOSES.

BASIS OF BEARINGS

THE BASIS OF BEARINGS USED IN THIS SURVEY IS THE CENTERLINE OF CAPARY STREET BEARING NORTH 77°45'17" WEST AS SHOWN ON BOOK 10 PAGES 22 THROUGH 54 RECORDS OF SAID COUNTY.

EASEMENT NOTES

THE FOLLOWING ITEMS WERE FOUND IN TITLE REPORT NO. 519280738 DATED AUGUST 14, 2004 BY STORVING TITLE GUARANTY COMPANY:

- 1. AN EASEMENT OVER THE NEIGHBORHOOD PROPERTY FOR THE CONSTRUCTION AND MAINTENANCE OF ALL NECESSARY WATER UTILITIES, POWER LINES AND APPURTEINANCES FOR THE PURPOSES OF ERECTION AND INDUSTRIAL USE IN FAVOR OF INVERSIDE LAND AND IMPROVING COMPANY, RECORDED DECEMBER 27, 1981 IN BOOK 848 PAGE 427, EASEMENT AFFECTS SUBJECT PROPERTY AND IS BLANKET IN NATURE.
- 2. AN EASEMENT FOR EITHER OF BOTH POLE LINES, CONDUITS OR UNDERGROUND FACILITIES AND APPURTEINANCES TO SOUTHERN CALIFORNIA GAS COMPANY, RECORDED JULY 6, 1939 IN BOOK 610 PAGE 457, EASEMENT AFFECTS SUBJECT PROPERTY AND IS NOT PLATTED HEREIN.
- 3. AN EASEMENT FOR EITHER OF BOTH POLE LINES, CONDUITS OR UNDERGROUND FACILITIES AND APPURTEINANCES TO SOUTHERN CALIFORNIA GAS COMPANY, RECORDED JULY 22, 1939 IN BOOK 588 PAGE 343, OCEANO RECREATION, EASEMENT AFFECTS SUBJECT PROPERTY AND IS PLATTED HEREIN.
- 4. AN EASEMENT TO W. H. INYON, RECORDED JULY 1, 1933 IN BOOK 482 PAGE 108, OCEANO RECREATION, EASEMENT AFFECTS SUBJECT PROPERTY AND IS PLATTED HEREIN.
- 5. AN EASEMENT FOR PUBLIC UTILITIES PURPOSES TO SOUTHERN CALIFORNIA GAS COMPANY, RECORDED FEBRUARY 1, 1971 AS INSTRUMENT NO. 10096, OCEANO RECREATION, EASEMENT AFFECTS SUBJECT PROPERTY AND IS BLANKET IN NATURE.

LEGAL DESCRIPTION

THE LAND REFERRED TO IN THE COMMITMENT IS SITUATED IN THE STATE OF CALIFORNIA, COUNTY OF INVERSIDE, AND IS DESCRIBED AS FOLLOWS:

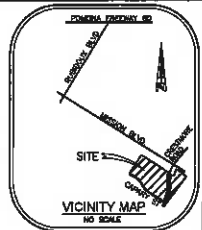
PANEL 1:
THAT PORTION OF LOT 8 OF THE PHASE 2B BANCROFT, AS SHOWN BY MAP ON FILE IN BOOK 10 PAGES 23 AND 54 OF MAPS, RECORDS OF INVERSIDE COUNTY, CALIFORNIA; IN THE PARCEL BANCROFT MORE PARTICULARLY DESCRIBED AS FOLLOWS:
BEGINNING AT A POINT SOUTH 89° 22' 30" EAST, 400.00 FEET AND SOUTH 33° 07' 40" WEST, 50.01 FEET FROM THE INTERSECTION OF THE NORTHERLY LINE OF CAPARY ROAD AND THE CENTERLINE OF THE CENTER TRACT TO SAID INTERSECTION AS SHOWN BY MAP ON FILE IN BOOK 21 PAGE 10 OF MAPS, RECORDS OF INVERSIDE COUNTY, CALIFORNIA;
THENCE SOUTH 89° 20' 30" EAST, 130.10 FEET TO THE BEGINNING OF A CURVE CONCAVE SOUTHWESTERLY HAVING A RADIUS OF 363.00 FEET, AN ARC LENGTH OF 238.63 FEET, TO THE BEGINNING OF A CURVE CONCAVE SOUTHWESTERLY HAVING A RADIUS OF 363.00 FEET, THENCE SOUTHWESTERLY ALONG SAID CURVE THROUGH A CENTRAL ANGLE OF 23° 17' 14", AN ARC LENGTH OF 238.63 FEET, TO THE BEGINNING OF A CURVE CONCAVE SOUTHWESTERLY HAVING A RADIUS OF 363.00 FEET, THENCE SOUTHWESTERLY ALONG SAID CURVE THROUGH A CENTRAL ANGLE OF 23° 17' 14", AN ARC LENGTH OF 238.63 FEET, THENCE SOUTH 87° 48' 33" WEST, 532.86 FEET TO THE BEGINNING OF A CURVE CONCAVE WESTERLY HAVING A RADIUS OF 363.00 FEET, THENCE SOUTHWESTERLY ALONG SAID CURVE THROUGH A CENTRAL ANGLE OF 63° 07' 42", AN ARC LENGTH OF 165.82 FEET TO A POINT IN A COMPOUND CURVE CONCAVE WESTERLY HAVING A RADIUS OF 363.00 FEET;
THENCE SOUTHWESTERLY ALONG SAID CURVE THROUGH A CENTRAL ANGLE OF 2° 01' 01", AN ARC LENGTH OF 18.57 FEET, THENCE SOUTH 82° 38' 10" WEST, 110.00 FEET, THENCE NORTH 37° 21' 59" EAST, 1194.18 FEET, THENCE NORTH 82° 29' 58" WEST, 134.00 FEET, THENCE NORTH 32° 27' 45" EAST, 363.01 FEET, THENCE NORTH 82° 27' 45" WEST, 1254.30 FEET TO A POINT IN A CURVE CONCAVE SOUTHWESTERLY HAVING A RADIUS OF 363.00 FEET, A BEARING BEARING TO SAID POINT BEARS NORTH 82° 35' 48" WEST, THENCE SOUTHWESTERLY ALONG SAID CURVE THROUGH A CENTRAL ANGLE OF 14° 41' 01", AN ARC LENGTH OF 218.82 FEET, THENCE LEAVING SAID CURVE, NORTH 77° 38' 48" WEST, 103.00 FEET, ON BEARING LINE TO A POINT IN A CURVE CONCAVE SOUTHWESTERLY HAVING A RADIUS OF 363.00 FEET, THENCE SOUTHWESTERLY ALONG SAID CURVE THROUGH A CENTRAL ANGLE OF 19° 08' 37", AN ARC LENGTH OF 250.04 FEET, THENCE NORTH 87° 27' 31" WEST, 88.51 FEET, THENCE NORTH 87° 43' 54" WEST, 43.12 FEET TO A POINT IN A CURVE CONCAVE SOUTHWESTERLY HAVING A RADIUS OF 108.00 FEET, A BEARING LINE TO SAID POINT BEARS NORTH 84° 07' 18" WEST, THENCE SOUTHWESTERLY ALONG SAID CURVE THROUGH A CENTRAL ANGLE OF 12° 44' 43", AN ARC LENGTH OF 234.88 FEET, THENCE NORTH 87° 48' 53" WEST, 182.88 FEET, THENCE NORTH 37° 01' 34" EAST, 13.00 FEET, THENCE NORTH 37° 40' 33" WEST, 85.92 FEET, THENCE SOUTH 37° 07' 40" EAST, 78.17 FEET TO THE POINT OF BEGINNING;

EXCEPT THEREFROM THAT PORTION THEREOF COVERED TO THE COUNTY OF INVERSIDE BY DEED RECORDED MAY 18, 1971 AS INSTRUMENT NO. 25414 OF OFFICIAL RECORDS OF INVERSIDE COUNTY, CALIFORNIA.

PANEL 2:
THAT PORTION OF LOT 8 OF PHASE 2B BANCROFT, AS PER MAP THEREOF, RECORDED IN BOOK 10, PAGES 23 THROUGH 54, RECORDS OF INVERSIDE COUNTY, CALIFORNIA, MORE PARTICULARLY DESCRIBED AS FOLLOWS:

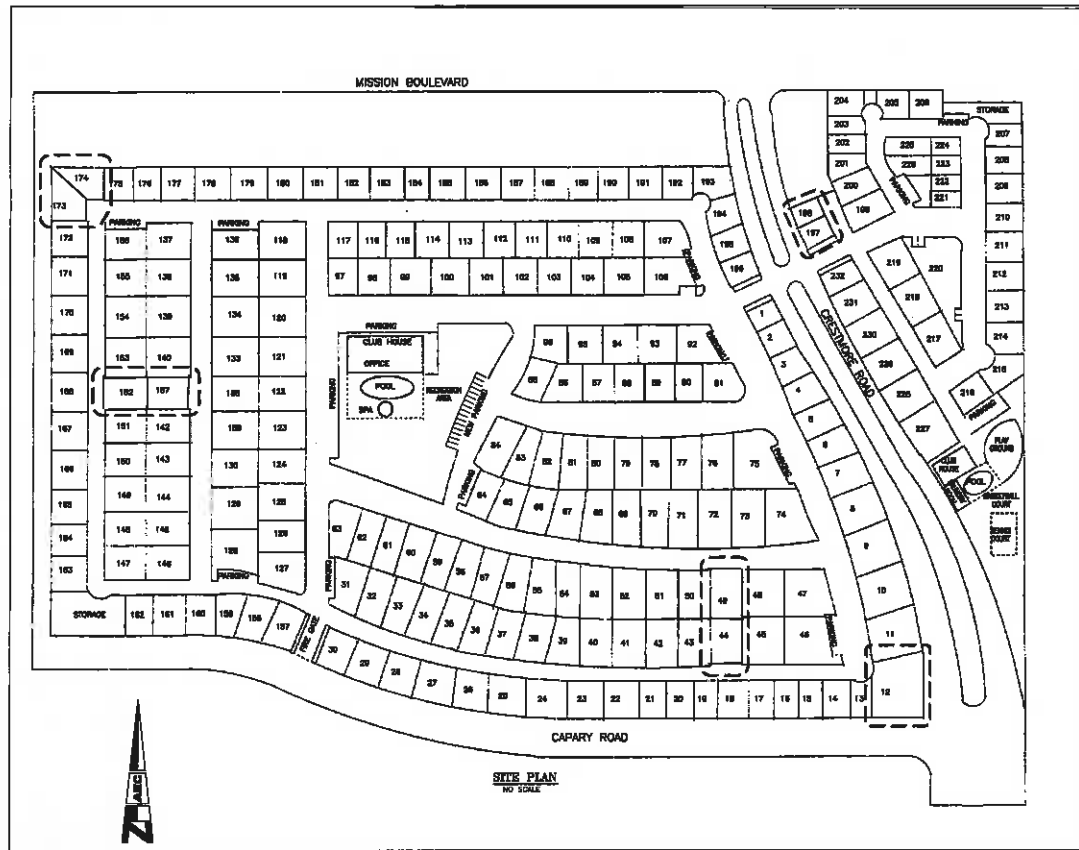
BEGINNING AT A POINT IN THE SOUTHERLY LINE OF MISSION BOLLWATER AT THE INTERSECTION OF THE CENTERLINE OF CRESTMORE ROAD BEARING SOUTHERLY SAID POINT ALSO BEING A 12" DEEP CONCRETE DIMENSION, 12" DEEP PER RECORD OF SURVEY RECORDED IN BOOK 11, PAGES 58 AND 57, RECORDS OF SAID INVERSIDE COUNTY; THENCE SOUTH 87° 27' 31" EAST, 68.51 FEET ALONG SAID SOUTHERLY LINE TO THE TRUE POINT OF BEGINNING; THENCE SOUTHWESTERLY ALONG SAID CURVE CONCAVE WESTERLY HAVING A RADIUS OF 363.00 FEET, A RADIAL LINE THROUGH SAID POINT BEARS NORTH 82° 27' 12" WEST, THENCE SOUTHWESTERLY ALONG SAID CURVE THROUGH A CENTRAL ANGLE OF 15° 08' 37", AN ARC DISTANCE OF 330.04 FEET, THENCE SOUTH 77° 38' 48" WEST, 103.00 FEET TO A POINT ON A CURVE CONCAVE WESTERLY HAVING A RADIUS OF 363.00 FEET, A RADIAL LINE THROUGH SAID POINT BEARS NORTH 77° 38' 48" WEST, THENCE NORTHWESTERLY ALONG SAID CURVE THROUGH A CENTRAL ANGLE OF 14° 41' 01", AN ARC DISTANCE OF 218.82 FEET TO A POINT IN SAID SOUTHERLY LINE OF MISSION BOLLWATER, THENCE NORTH 87° 27' 31" WEST, 184.57 FEET TO THE TRUE POINT OF BEGINNING.

EXCEPT THEREFROM ANY AND ALL PORTIONS OF SAID LAND WITHIN THE RIGHT OF WAY TO CRESTMORE ROAD AS ESTABLISHED BY THE DEED RECORDED MAY 18, 1971 AS INSTRUMENT NO. 25414.



LAND AREA
1,217.99 SQUARE FEET
27.996 ACRES

THOMAS BROTHER MAP
283 CENTER
PAGE 685; GRID C-1



NOTES
EXISTING LIMITS - 233
PROPOSED LIMITS - 8
TOTAL LIMITS - 235
UNIT PARKING - 48 @ 2 PER UNIT
ADDITIONAL SPACES
REGULAR STALLS - 13
PARKING STALLS - 9
TOTAL - 59
- - - - - INDICATES PROPOSED UNIT LOCATION

PROPOSED UNITS
LOT 12 = 5,431 S.F.
LOT 44 = 3,207 S.F.
LOT 46 = 2,708 S.F.
LOT 184 = 2,794 S.F.
LOT 187 = 2,491 S.F.
LOT 173 = 8,316 S.F.
LOT 174 = 5,508 S.F.
LOT 187 = 5,067 S.F.
LOT 188 = 4,858 S.F.

OWNER / APPLICANT
ORION DEVELOPMENT COMPANY
6740 NEWPORT PARKING
17300 RED HILL AVE. SUITE 2200
DOWNS CA 92521
PHONE: (949) 852-3372
CITY ACTVA

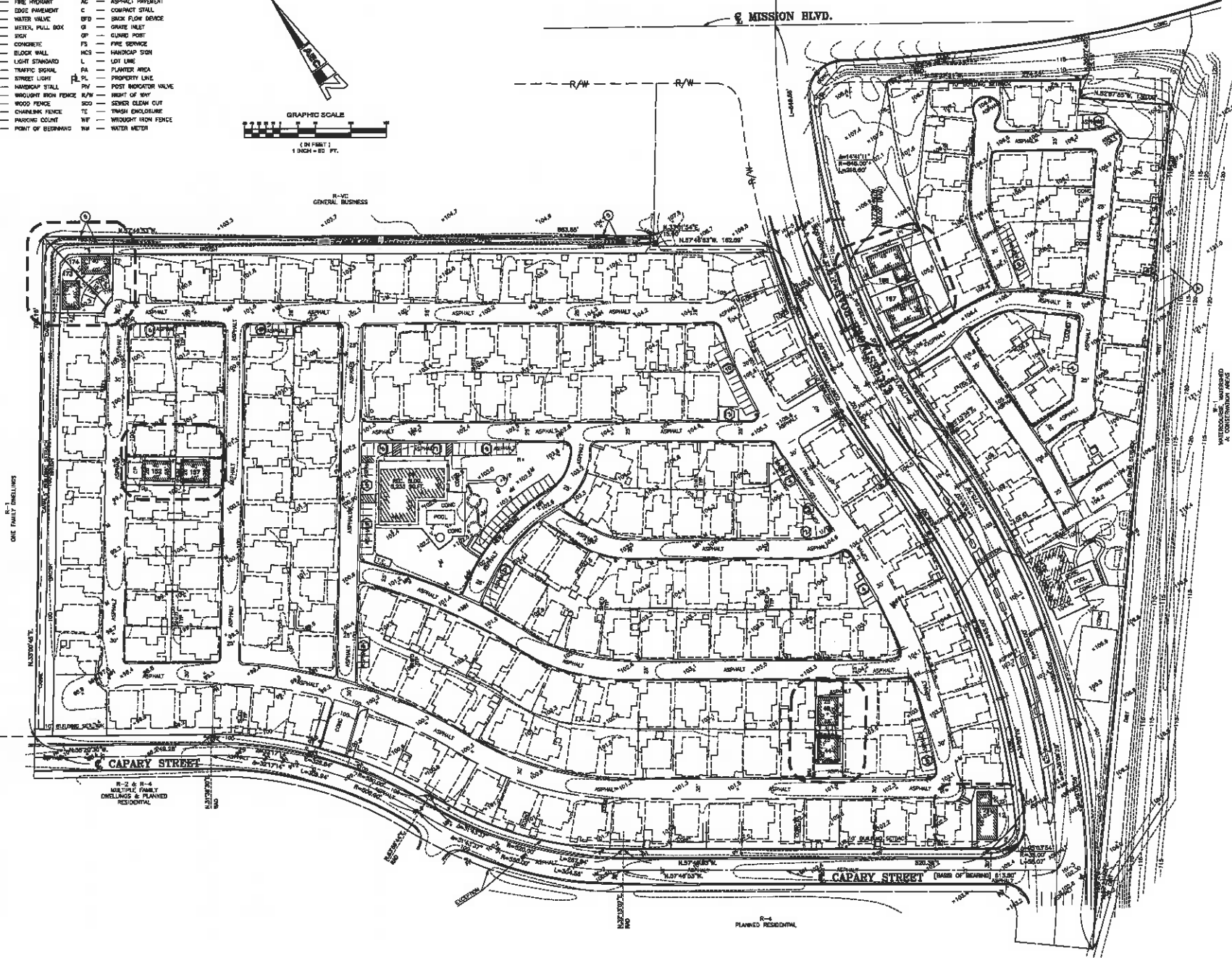
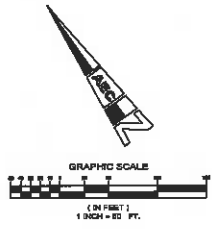
ANACAL ENGINEERING COMPANY
CIVIL ENGINEERING & LAND SURVEYING
1211 WEST 8TH STREET, SUITE 200
PHOENIX, ARIZONA 85001
PHONE: (714) 774-1763 FAX: (714) 774-1807
E-MAIL: ANACAL@ANACALENGINEERING.COM
WEB SITE: ANACALENGINEERING.COM

SITE PLAN CUP AMENDMENT
OLD PLANTATION MOBILE HOME PARK
3825 CRESTMORE DRIVE

DATE: 9/6/20
SCALE: AS SHOWN
DRAWN: JWB/MSK
CHECKED: JWB/MSK
PROJECT NO.: 18-08-01CUP

1, 3

- LEGEND**
- | | | | |
|----|--------------------|-----|----------------------|
| d | FIRE HYDRANT | AC | ASPHALT PAVEMENT |
| AP | SIDE PAVEMENT | C | CONCRETE WALL |
| H | WATER VALVE | BFD | BRICK FLOW DEVICE |
| W | METER, PULL BOX | G | GRADE INLET |
| + | CONCRETE | GP | GUARD POST |
| + | BLOCK WALL | FS | FIRE SERVICE |
| + | LIGHT STANDARD | HCS | HANDICAP SIGN |
| + | TRAFFIC SIGNAL | L | LIFT LANE |
| + | STREET LIGHT | PA | PLANTED AREA |
| + | HANDICAP STALL | PL | PROPERTY LINE |
| + | WROUGHT IRON FENCE | PV | POST INDICATOR VALVE |
| + | WOOD FENCE | R/W | RIGHT OF WAY |
| + | CHARLEMINE FENCE | SD | SEWER CLEAN OUT |
| + | PARKING COUNT | TE | TRASH ENCLOSURE |
| + | POINT OF BEGINNING | WF | WROUGHT IRON FENCE |
| | | WM | WATER METER |

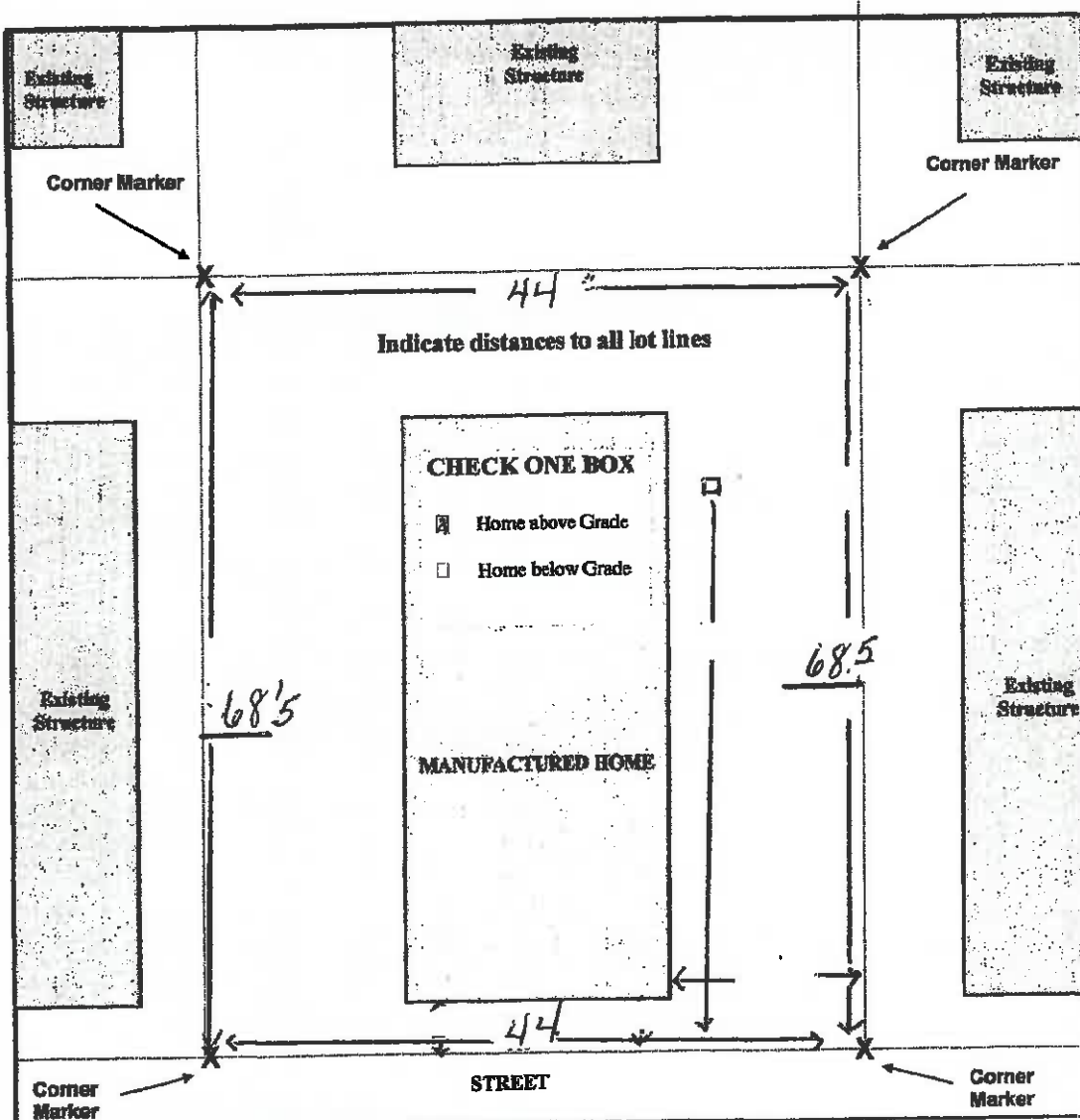


SITE PLAN CUP AMENDMENT
OLD PLANTATION MOBILE HOME PARK
3825 CRESTMORE DRIVE

ANACAL ENGINEERING COMPANY
 CIVIL ENGINEERING & LAND SURVEYING
 1211 NORTH DUBIN AVENUE - ANAHEIM, CALIFORNIA 92807
 PHONE: (714) 774-1763 FAX: (714) 774-4880
 E-MAIL: ANACAL@ANACALENGINEERING.COM
 WEB SITE: ANACALENGINEERING.COM

DATE: 1/8/20
 SCALE: AS SHOWN
 DRAWN BY: WJL/JAG
 CHECKED BY: GJG
 D. C. G.

SHEET NO. 2
 JOB NO. 15-081CIP
 3



Indicate distances to all lot lines

CHECK ONE BOX
 Home above Grade
 Home below Grade

MANUFACTURED HOME

Width and length of lot: 44 x 68.5 Width and length of home 28 x 40.

LOT PLOT PLAN AND PARK INFORMATION

- A) Park Name Old Plantation MHP
 Homeowner Name _____
 Homeowner Address 3825 Crestmore Sp# 344
 City Jurupa Valley Zip 92509
- B) Design Information:
 Home Amperage: 50 Pedestal Amperage: _____
 Home Voltage: _____ Pedestal Voltage: _____
 Home Roof Load: _____ PSF
 Roof Load for locality: _____ PSF
- C) Is the park located in a snow area requiring 30 lb or greater roof loading?
 YES NO
- D) The lot line corners at the front and rear are clearly and permanently marked pursuant to Title 25 of the California Code of Regulations, Sections 1104 or 2104 in the following manner:
Pins & Rebar

NOTE: Each lot line corner shall be clearly and permanently marked prior to installation and inspection.

**STATEMENT OF RESPONSIBILITY
 (ORIGINAL SIGNATURE REQUIRED)**

As the park owner or operator, or as his or her authorized representative, I hereby certify that the information provided on this plot plan relative to the location of the manufactured home, all related accessory structure locations and separations and the park and homeowner information is true, accurate and complete. Lot corners have been identified as in item D above.

Signature of Park Owner, Operator, or Manager

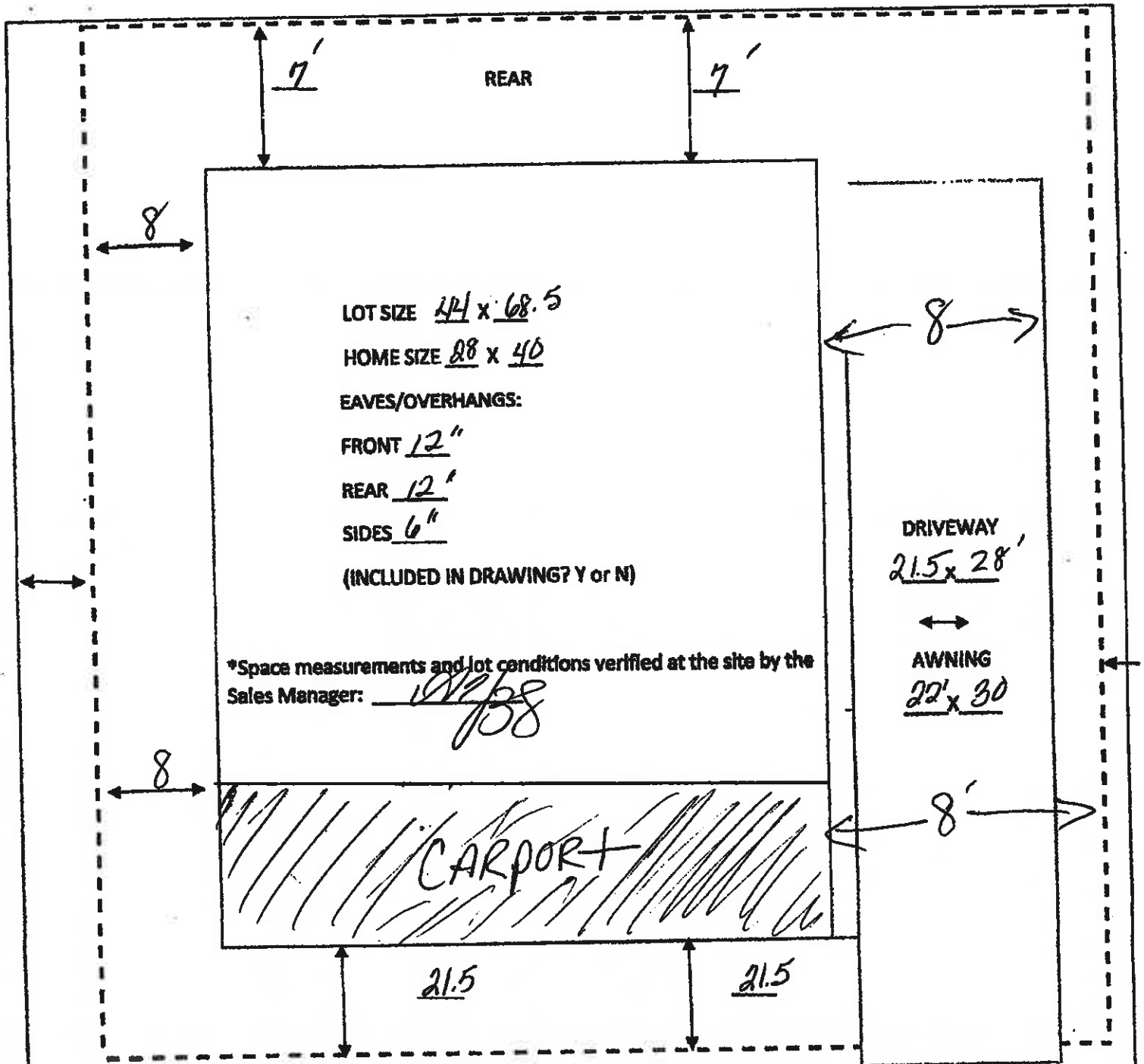
State of California
 Department of Housing and Community Development
 Division of Codes and Standards



Northern Area Office
 9342 Tech Center Drive, Suite 550
 Sacramento, CA 95826

Southern Area Office
 3737 Main St. Ste 400
 Riverside, CA 92501

1. Draw any proposed structure(s) and existing structures on the diagram above at the approximate location and identify the type of structures (e.g. deck, awning, etc). Indicate the distance from the lot line to the proposed structure. Also indicate the length and width of the structure.
2. Indicate the exact distances from structures on adjacent lots if located within 10 (ten) feet of your lot line.
3. Enter length & width of the manufactured home (including caves) and length & width of lot.
4. No vegetation is allowed under the manufactured home or habitable accessory structure. Lot must be properly graded to ensure that water cannot accumulate beneath the manufactured home.



THIS SITE PLAN IS REQUIRED FOR ALL HOME INSTALLATIONS. Must include the dimensions of all requested items, INCLUDING DOORS; draw each item on site plan above; and note the distances between each item, the home, to the front and rear of home, lot lines & structures on adjoining spaces.

1. Back door & steps: Landing Size 4 x 8 Material TRXX Up & In Up & Over X

2. Front door steps: Landing Size 4 x 8 Material TRXX / Up & In X Up & Over

3. Back Door Awning: N x A 4. Front door awning: 4 x 8 5. Porch: N x A Material

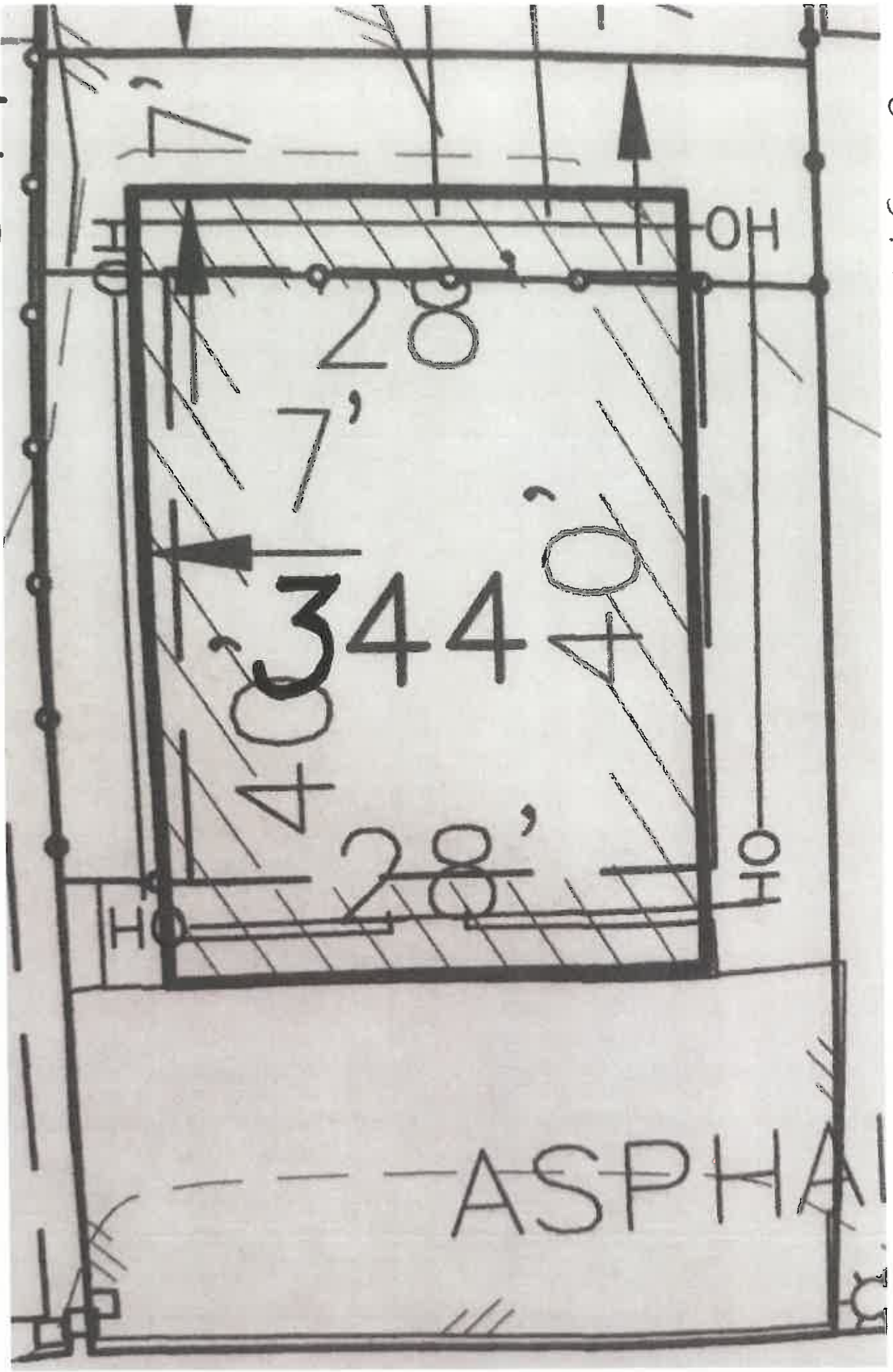
6. Shed: N x A Material: N/A 7. Location of Utilities & distance to front & rear of home

*Also include location and size of the concrete pads, walkways, masonry planter, etc., to be installed.

PARK Old Plan SPACE 344 REG MGR APPROVAL & DATE

HHS#

Floorplan
CM-3403B

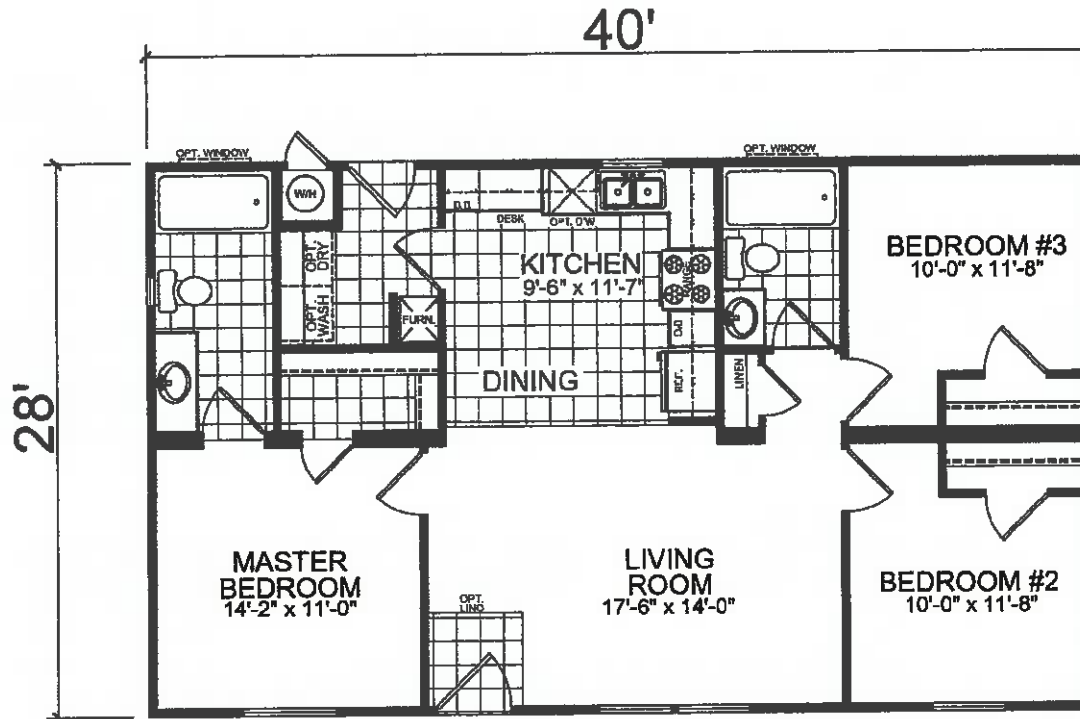


ASPHAL

4467#

3

Space
344



ALL DIMENSIONS ARE APPROXIMATE, AND MAY BE SUBJECT TO CHANGES BY CHAMPION HOME BUILDERS CO. LINDSAY, CA

REDMAN HOMES.
CHAMPION
840 W. Palm Ave. Lindsay, CA 93247

DAPIA SEAL

MODIFICATIONS

MODEL: 09-CM-3403B

SHEET:

TITLE: LITERATURE PLAN

L-101

DRAWN BY: M. SANCHEZ

DATE: 2-12-2015

SCALE: 1/8" = 1'-0"

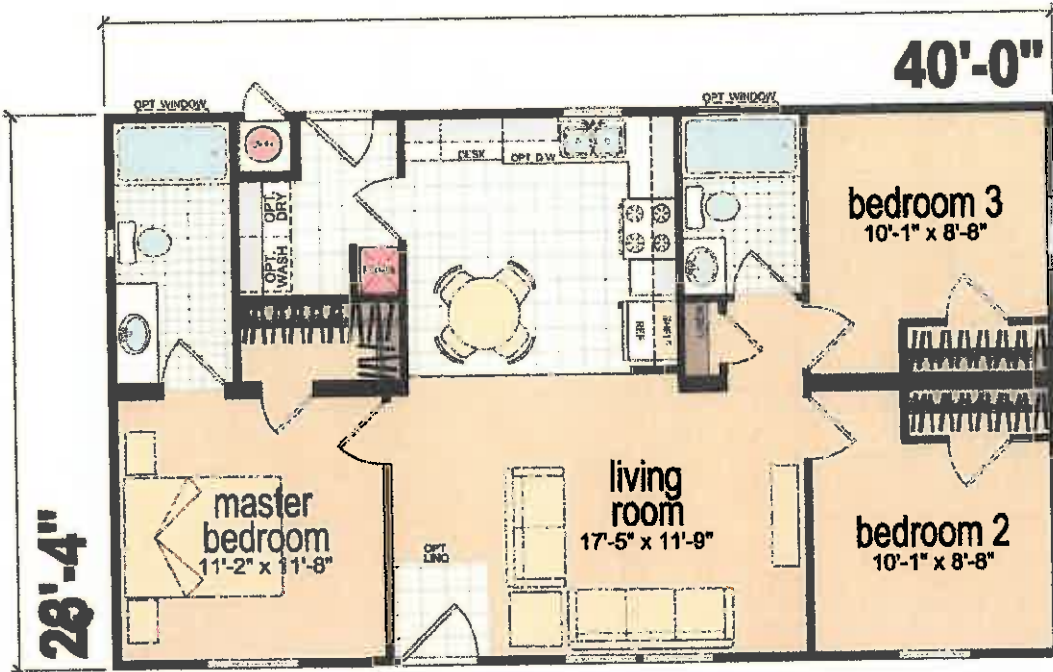
PROPRIETARY AND CONFIDENTIAL
THESE DRAWINGS AND SPECIFICATIONS ARE ORIGINAL.
PROPRIETARY AND CONFIDENTIAL MATERIALS OF CHAMPION.
COPYRIGHT © 1976-2004 BY CHAMPION

Creekside Manor 3403B

933 Square Feet, 3 Bedrooms, 2 Bathrooms, Multi-Section

This manufactured home is built to the federal (HUD) building code for manufactured housing.

Floor Plan

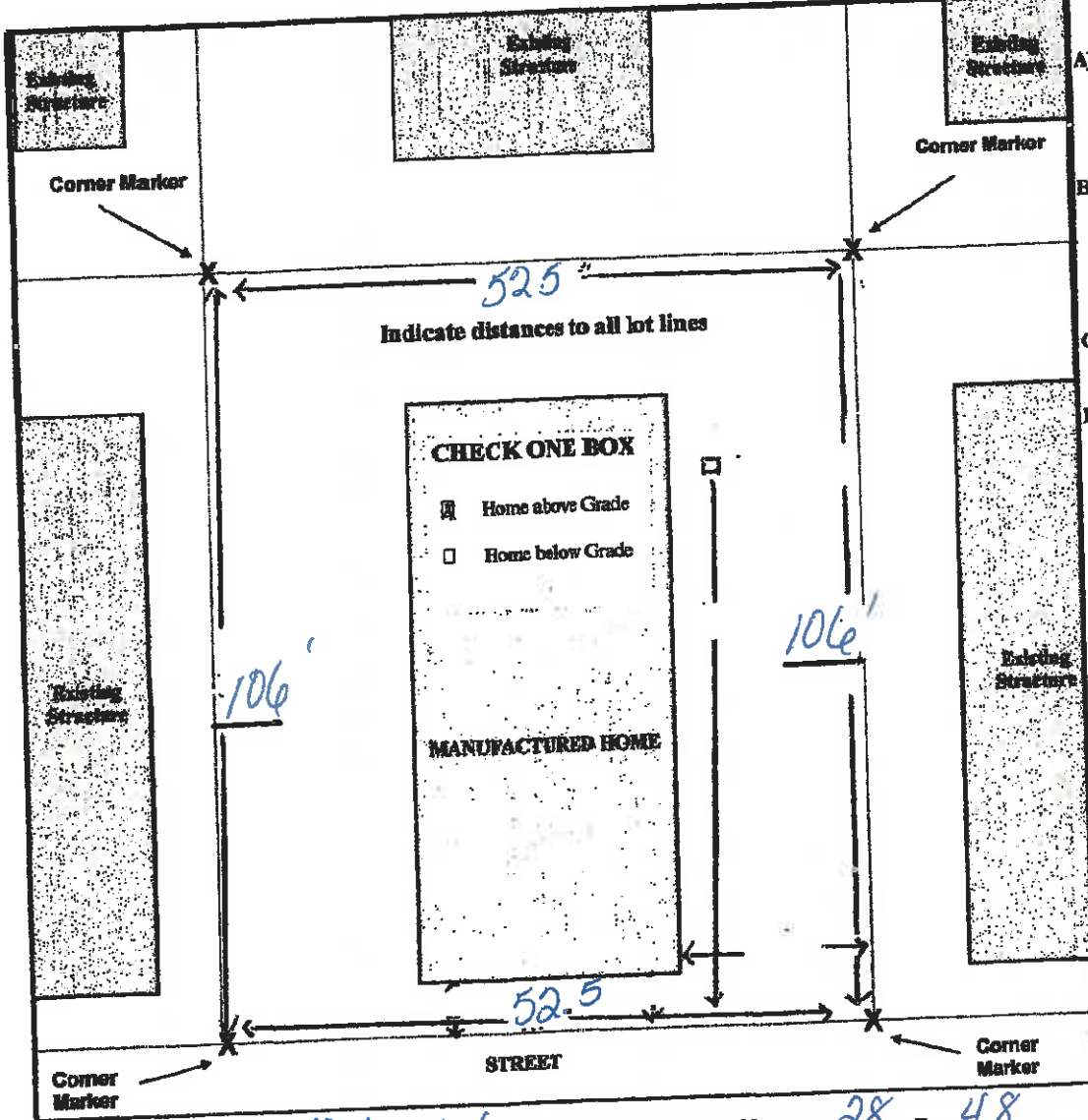


Elevation



Dimensions, floor plans, elevations, features and other information are subject to change without notice. Square footage and other dimensions are approximations. Elevations, photography and videos are often shown with optional features and/or third party additions, such as garages and porches that may not be available in all regions or situations.

LOT PLOT PLAN AND PARK INFORMATION



A) Park Name Old Plantation
 Homeowner Name _____
 Homeowner Address 3825 Crestmore Sp# 498
 City Jurupa Valley Zip 92509

B) Design Information:
 Home Amperage: 50 Pedestal Amperage: _____
 Home Voltage: _____ Pedestal Voltage: _____
 Home Roof Load: _____ PSF
 Roof Load for locality: _____ PSF

C) Is the park located in a snow area requiring 30 lb or greater roof loading?
 YES NO

D) The lot line corners at the front and rear are clearly and permanently marked pursuant to Title 25 of the California Code of Regulations, Sections 1104 or 2104 in the following manner:

NOTE: Each lot line corner shall be clearly and permanently marked prior to installation and inspection.

**STATEMENT OF RESPONSIBILITY
 (ORIGINAL SIGNATURE REQUIRED)**

As the park owner or operator, or as his or her authorized representative, I hereby certify that the information provided on this plot plan relative to the location of the manufactured home, all related accessory structure locations and separations and the park and homeowner information is true, accurate and complete. Lot corners have been identified as in item D above.

Signature of Park Owner, Operator, or Manager

State of California
 Department of Housing and Community Development
 Division of Codes and Standards

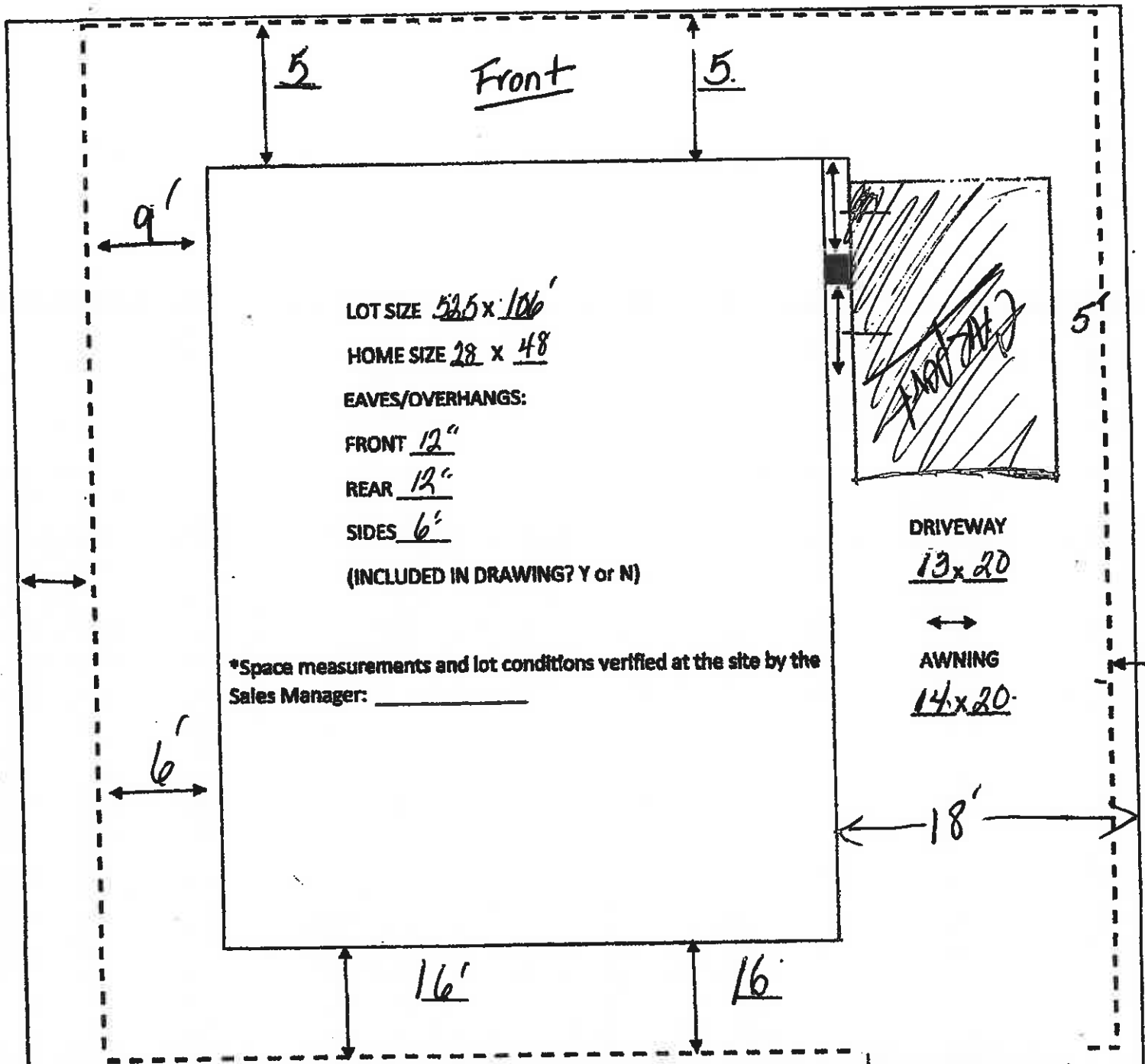
Width and length of lot: 52.5 x 106 Width and length of home 28 x 48

1. Draw any proposed structure(s) and existing structures on the diagram above at the approximate location and identify the type of structures (e.g. deck, awning, etc). Indicate the distance from the lot line to the proposed structure. Also indicate the length and width of the structure.
2. Indicate the exact distances from structures on adjacent lots if located within 10 (ten) feet of your lot line.
3. Enter length & width of the manufactured home (including caves) and length & width of lot.
4. No vegetation is allowed under the manufactured home or habitable accessory structure. Lot must be properly graded to ensure that water cannot accumulate beneath the manufactured home.



Northern Area Office
 9342 Tech Center Drive, Suite 550
 Sacramento, CA 95826

Southern Area Office
 3737 Main St. Ste 408
 Riverside, CA 92501



THIS SITE PLAN IS REQUIRED FOR ALL HOME INSTALLATIONS. Must include the dimensions of all requested items, INCLUDING DOORS; draw each item on site plan above; and note the distances between each item, the home, to the front and rear of home, lot lines & structures on adjoining spaces.

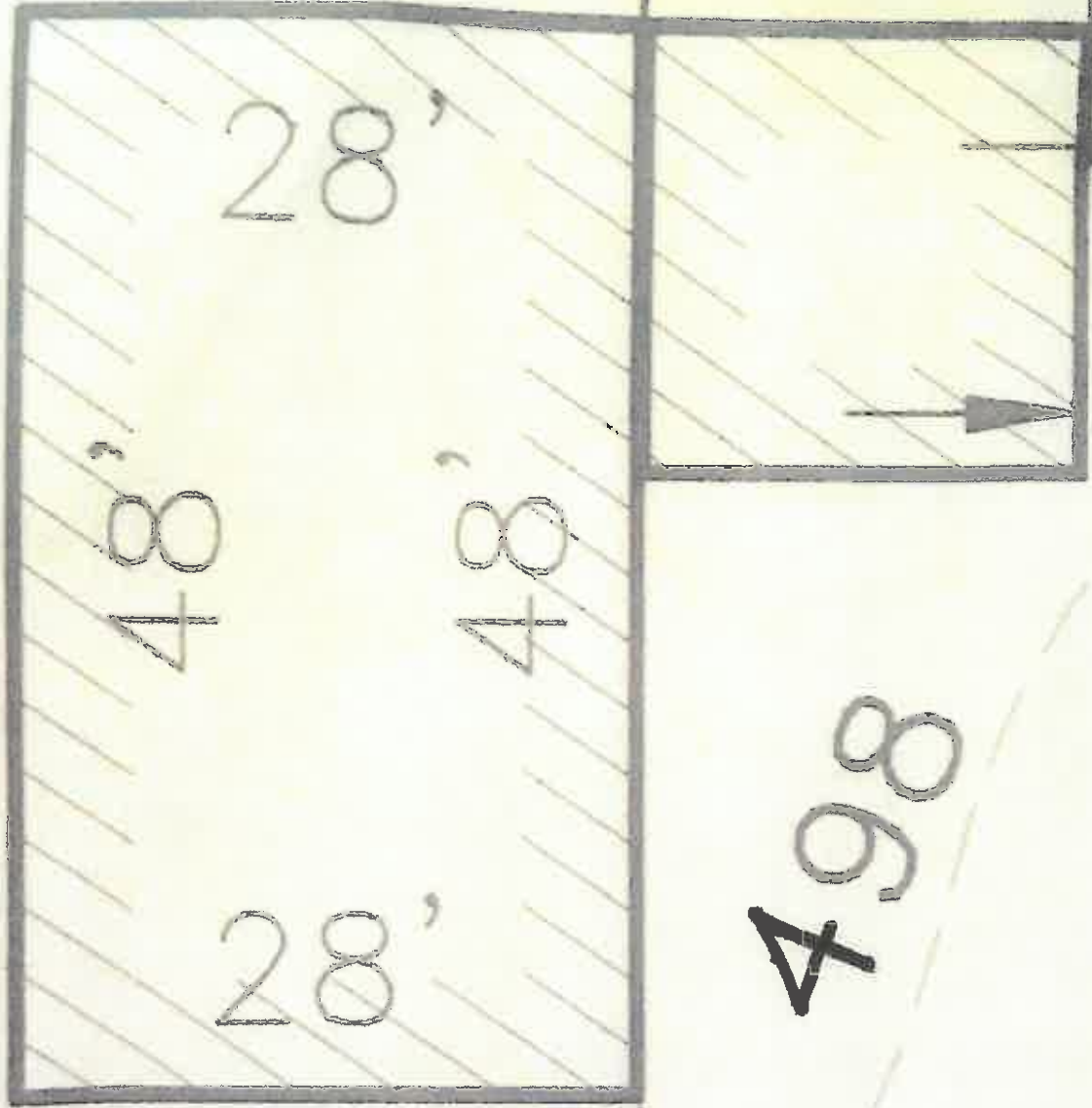
1. Back door & steps: Landing Size ___ x ___ Material _____ / Up & In ___ Up & Over ___
 2. Front door steps: Landing Size ___ x ___ Material _____ / Up & In ___ Up & Over ___
 3. Back Door Awning: ___ x ___ 4. Front door awning: ___ x ___ 5. Porch: ___ x ___ Material _____
 6. Shed: ___ x ___ Material: _____ 7. Location of Utilities & distance to front & rear of home _____

*Also include location and size of the concrete pads, walkways, masonry planter, etc., to be installed.

PARK Old Plan SPACE 498 REG MGR APPROVAL & DATE _____

498

Floorplan CM-4483B

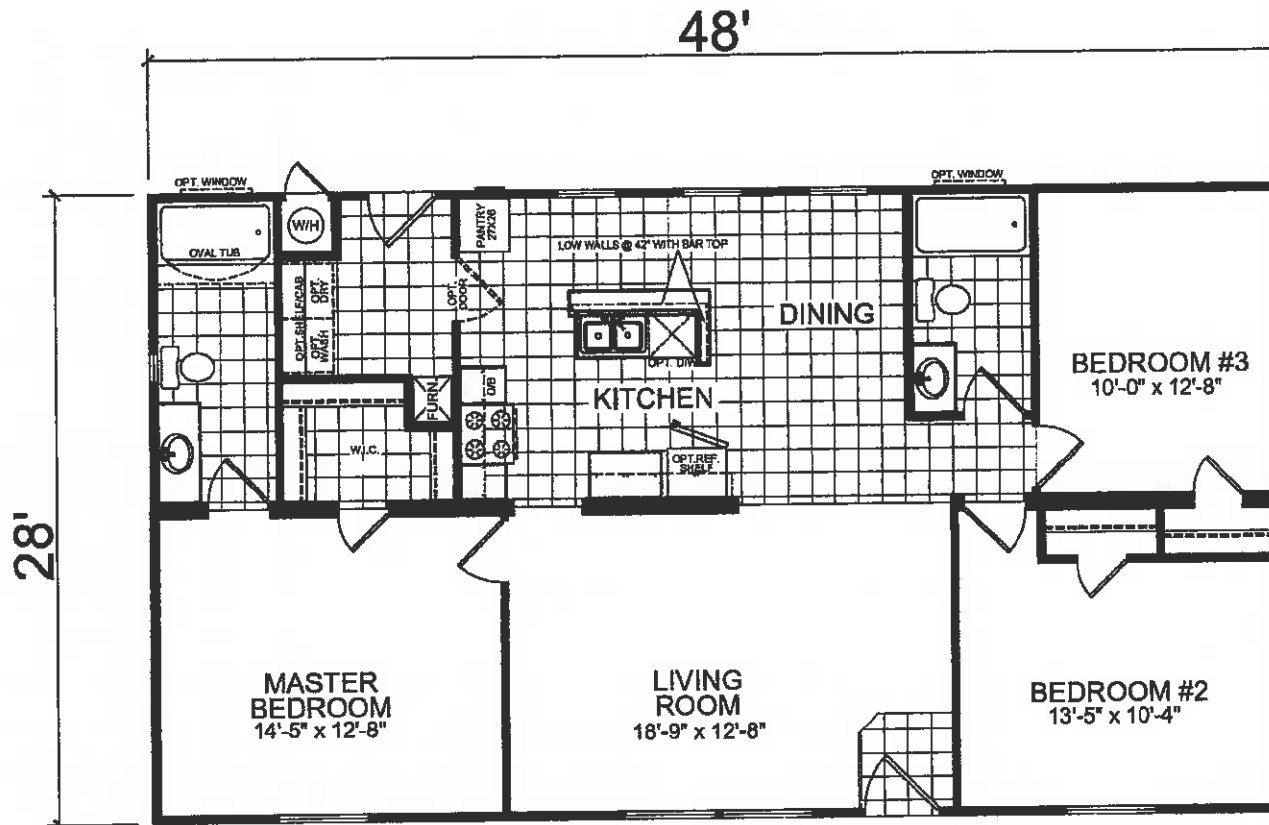


498

7D8

4

Space # 498



ALL DIMENSIONS ARE APPROXIMATE, AND MAY BE SUBJECT TO CHANGES BY CHAMPION HOME BUILDERS CO. LINDSAY, CA

REDMAN HOMES.
CHAMPION
 840 W. Palm Ave. Lindsay, CA 93247

| DAPIA SEAL |
|------------|
| |

| MODIFICATIONS | |
|---------------|--|
| | |
| | |
| | |
| | |

PROPRIETARY AND CONFIDENTIAL
 THESE DRAWINGS AND SPECIFICATIONS ARE ORIGINAL.
 PROPRIETARY AND CONFIDENTIAL MATERIALS OF CHAMPION.
 COPYRIGHT © 1976-2006 BY CHAMPION

| |
|-------------------------------|
| MODEL: 09-CM-4483B |
| TITLE: LITERATURE PLAN |
| DRAWN BY: M. SANCHEZ |
| DATE: 3-6-2015 |
| SCALE: 1/8" = 1'-0" |
| APPLICABLE MODEL # 09-000 |

| |
|--|
| SHEET: L-101 |
| 3 BEDROOM, 2 BATH NOMINAL SIZE: 28' x 48' ACTUAL SIZE: 26'-8" x 46' TOTAL AREA: 1,260 SQ. FT. |

Elevation



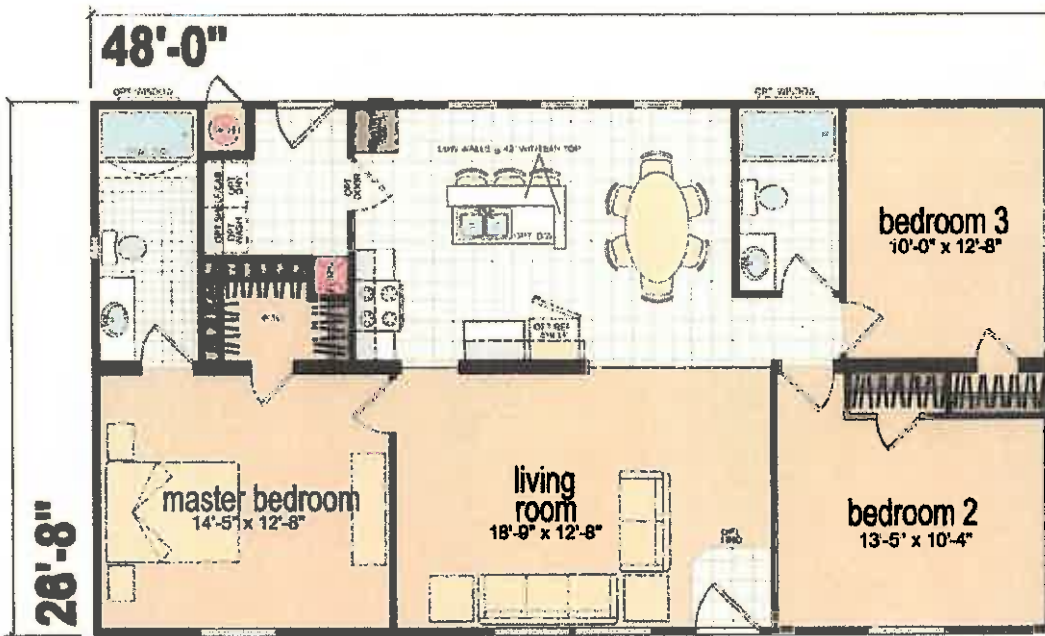
Dimensions, floor plans, elevations, features and other information are subject to change without notice. Square footage and other dimensions are approximations. Elevations, photography and videos are often shown with optional features and/or third party additions, such as garages and porches that may not be available in all regions or situations.

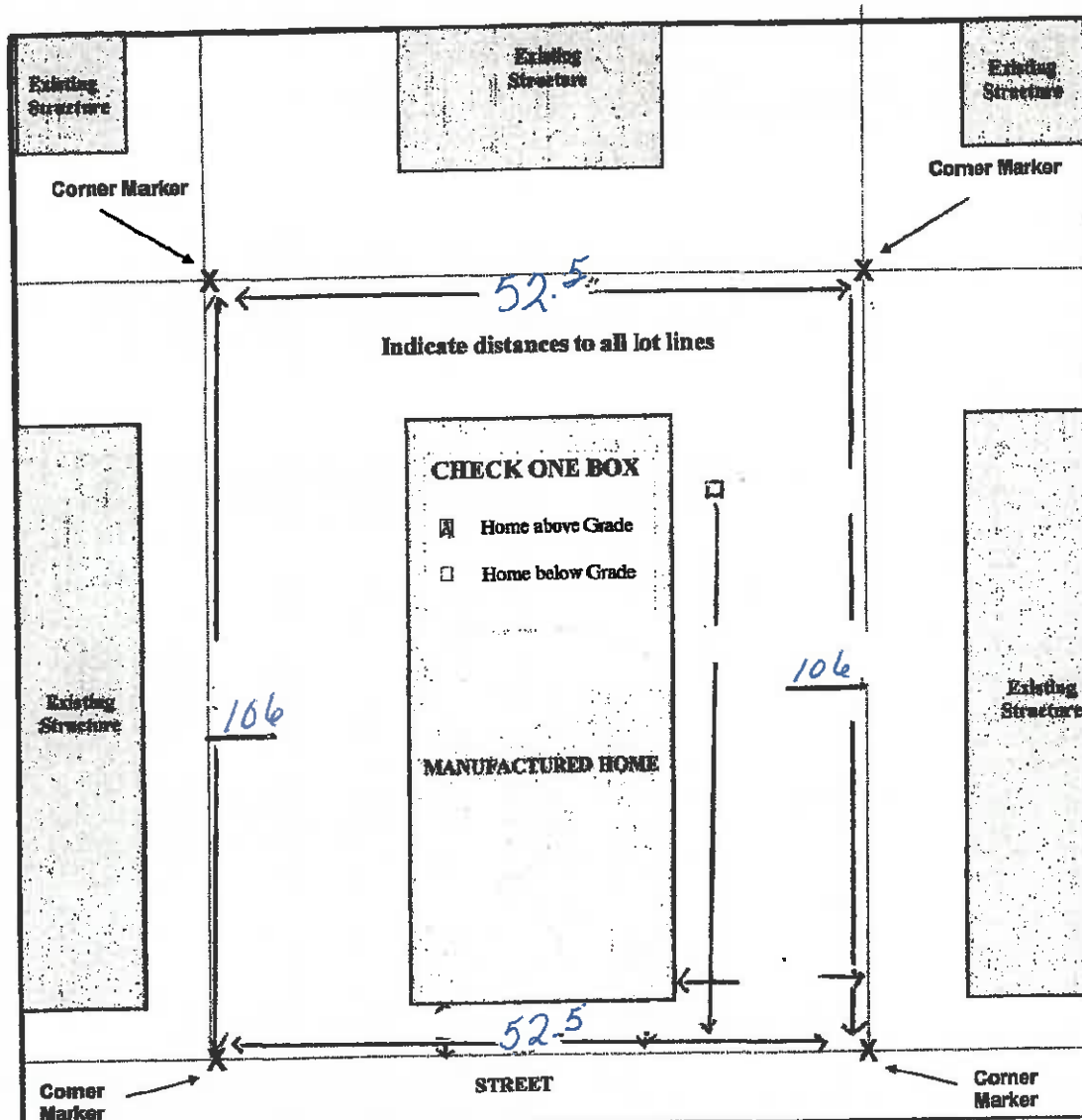
Creekside Manor 4483B

1280 Square Feet, 3 Bedrooms, 2 Bathrooms, Multi-Section

This manufactured home is built to the federal (HUD) building code for manufactured housing.

Floor Plan





LOT PLOT PLAN AND PARK INFORMATION

A) Park Name Old Plantation
 Homeowner Name 3825 Crestmore
 Homeowner Address Sp# 497
 City Juniper Valley Zip 92509

B) Design Information:
 Home Amperage: 50 Pedestal Amperage: _____
 Home Voltage: _____ Pedestal Voltage: _____
 Home Roof Load: _____ PSF
 Roof Load for locality: _____ PSF

C) Is the park located in a snow area requiring 30 lb or greater roof loading? YES NO

D) The lot line corners at the front and rear are clearly and permanently marked pursuant to Title 25 of the California Code of Regulations, Sections 1104 or 2104 in the following manner:
Pins & Rebar

NOTE: Each lot line corner shall be clearly and permanently marked prior to installation and inspection.

**STATEMENT OF RESPONSIBILITY
 (ORIGINAL SIGNATURE REQUIRED)**

As the park owner or operator, or as his or her authorized representative, I hereby certify that the information provided on this plot plan relative to the location of the manufactured home, all related accessory structure locations and separations and the park and homeowner information is true, accurate and complete. Lot corners have been identified as in item D above.

Signature of Park Owner, Operator, or Manager

State of California
 Department of Housing and Community Development
 Division of Codes and Standards

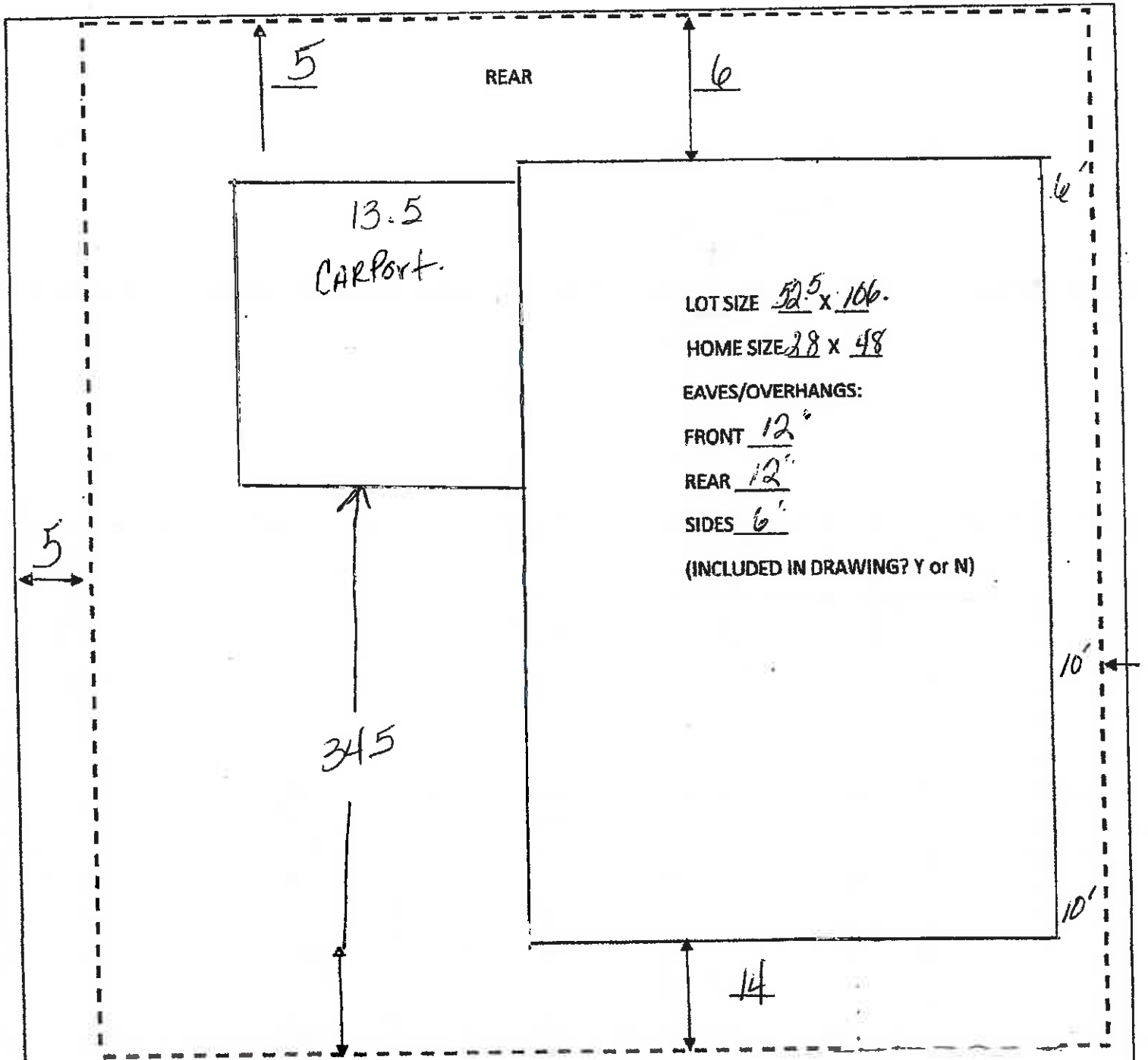
Width and length of lot: 52.5 x 106 Width and length of home 28 x 48

1. Draw any proposed structure(s) and existing structures on the diagram above at the approximate location and identify the type of structures (e.g. deck, awning, etc). Indicate the distance from the lot line to the proposed structure. Also indicate the length and width of the structure.
2. Indicate the exact distances from structures on adjacent lots if located within 10 (ten) feet of your lot line.
3. Enter length & width of the manufactured home (including eaves) and length & width of lot.
4. No vegetation is allowed under the manufactured home or habitable accessory structure. Lot must be properly graded to ensure that water cannot accumulate beneath the manufactured home.



Northern Area Office
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 Sacramento, CA 95826

Southern Area Office
 3737 Main St. Ste 400
 Riverside, CA 92501



THIS SITE PLAN IS REQUIRED FOR ALL HOME INSTALLATIONS. Must include the dimensions of all requested items, INCLUDING DOORS; draw each item on site plan above; and note the distances between each item, the home, to the front and rear of home, lot lines & structures on adjoining spaces.

1. Back door & steps: Landing Size 4 x 8 Material Trex / Up & In x Up & Over

2. Front door steps: Landing Size 4 x 8 Material Trex / Up & In Up & Over x

3. Back Door Awning: N/A 4. Front door awning: 4 x 8 5. Porch: — x — Material N/A

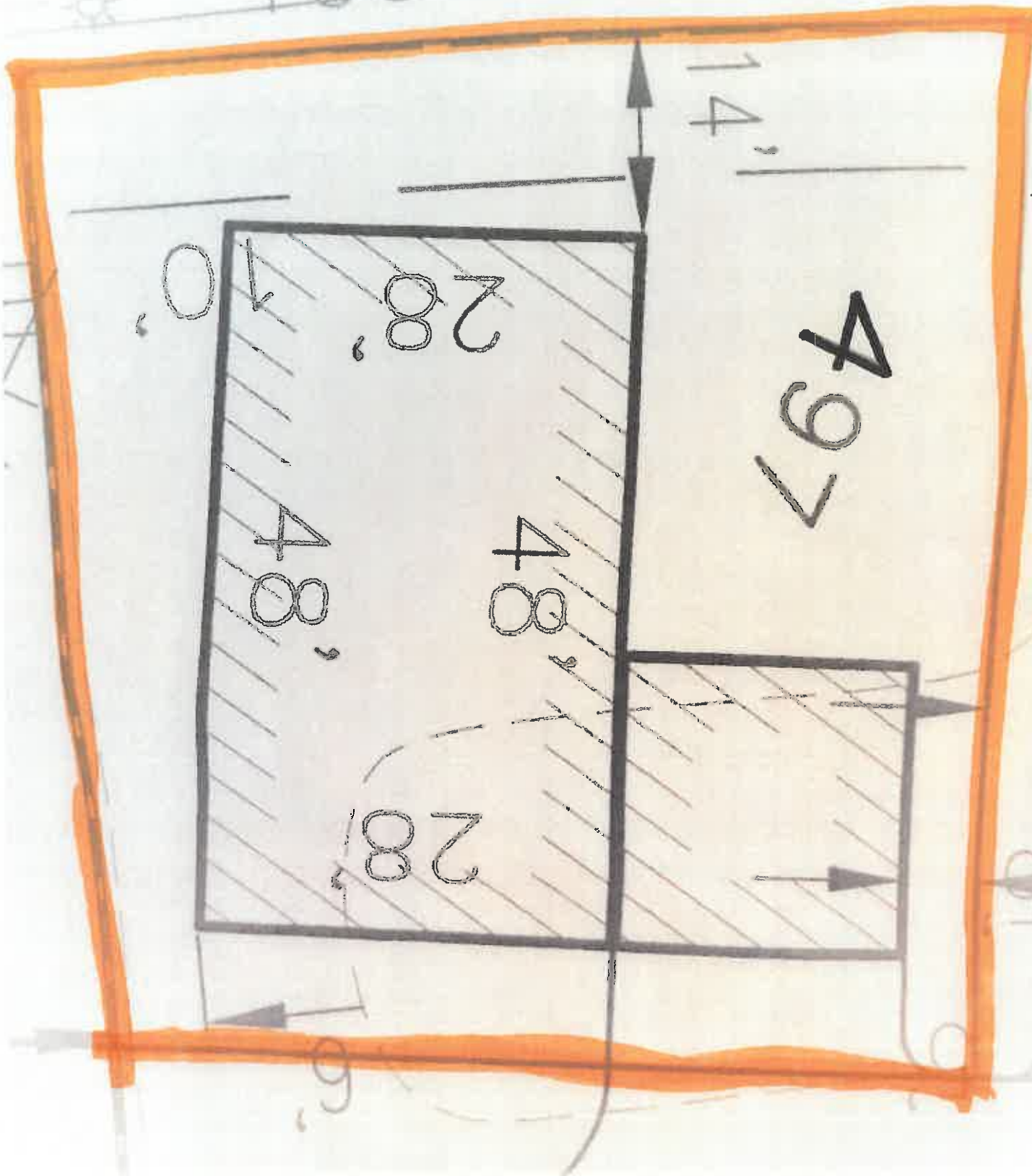
6. Shed: N/A Material: N/A 7. Location of Utilities & distance to front & rear of home =

*Also include location and size of the concrete pads, walkways, masonry planter, etc., to be installed.

PARK Old Plan SPACE 497 REG MGR APPROVAL & DATE

#497

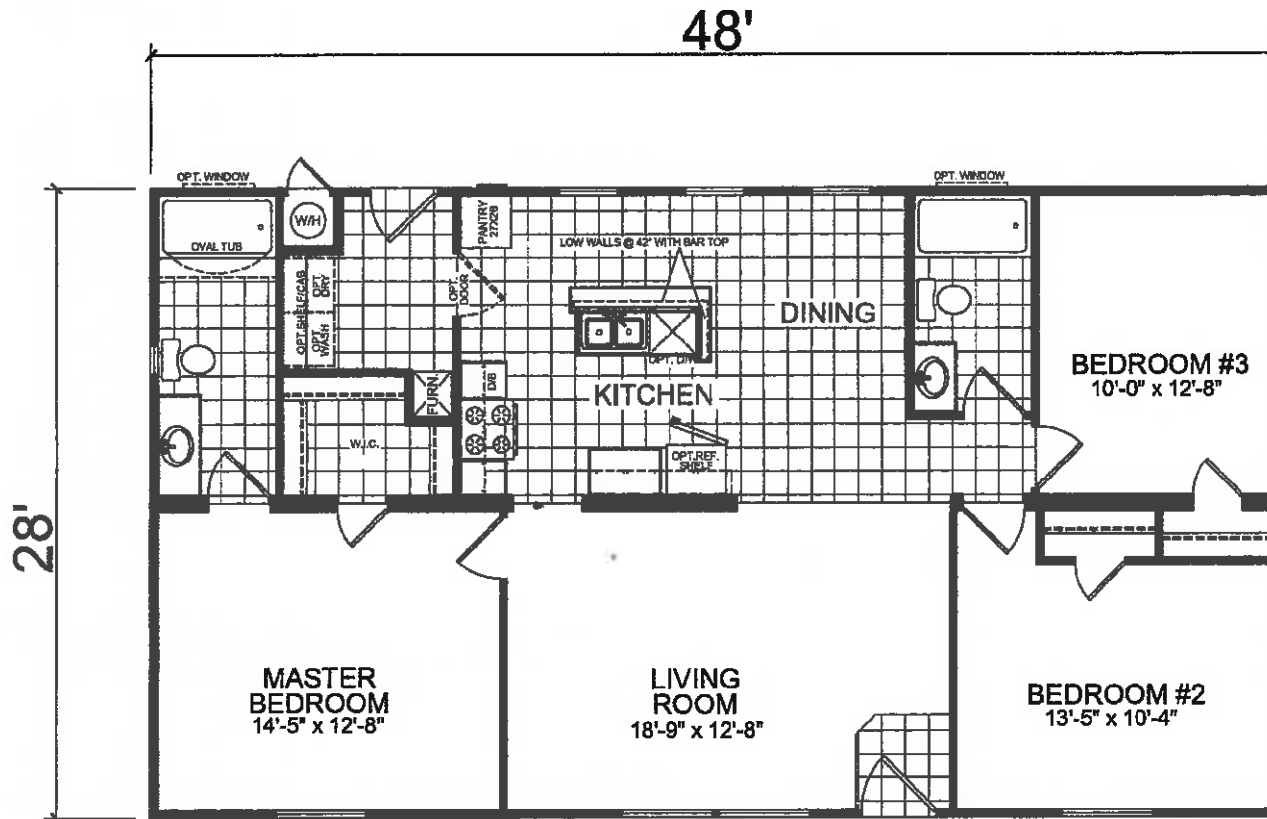
Floor plan CM-4483B



11/11/11

4

Space #497



ALL DIMENSIONS ARE APPROXIMATE, AND MAY BE SUBJECT TO CHANGES BY CHAMPION HOME BUILDERS CO. LINDSAY, CA

REDMAN HOMES.

CHAMPION
 840 W. Palm Ave. Lindsay, CA 93247

DAPIA SEAL

MODIFICATIONS

MODEL: 09-CM-4483B

SHEET:

TITLE: LITERATURE PLAN

L-101

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DRAWN BY: M. SANCHEZ

DATE: 3-6-2015

SCALE: 1/8" = 1'-0"

APPLICABLE MODEL #
09-000

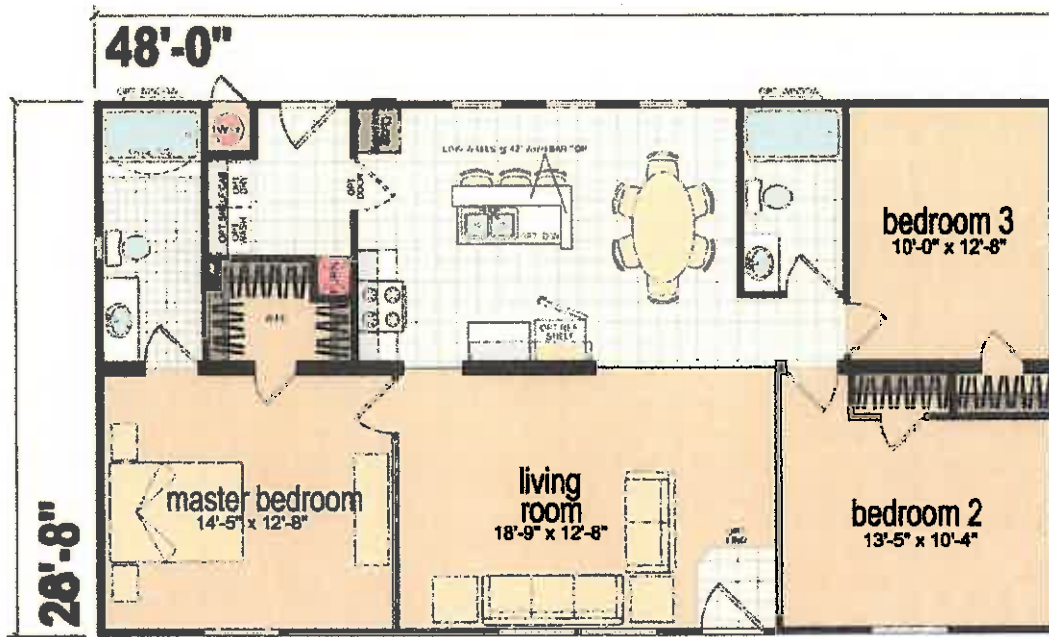
3 BEDROOM, 2 BATH
 NOMINAL SIZE: 28' x 48'
 ACTUAL SIZE: 26'-6" x 48'
 TOTAL AREA: 1,280 SQ. FT.

Creekside Manor 4483B

1280 Square Feet, 3 Bedrooms, 2 Bathrooms, Multi-Section

This manufactured home is built to the federal (HUD) building code for manufactured housing.

Floor Plan

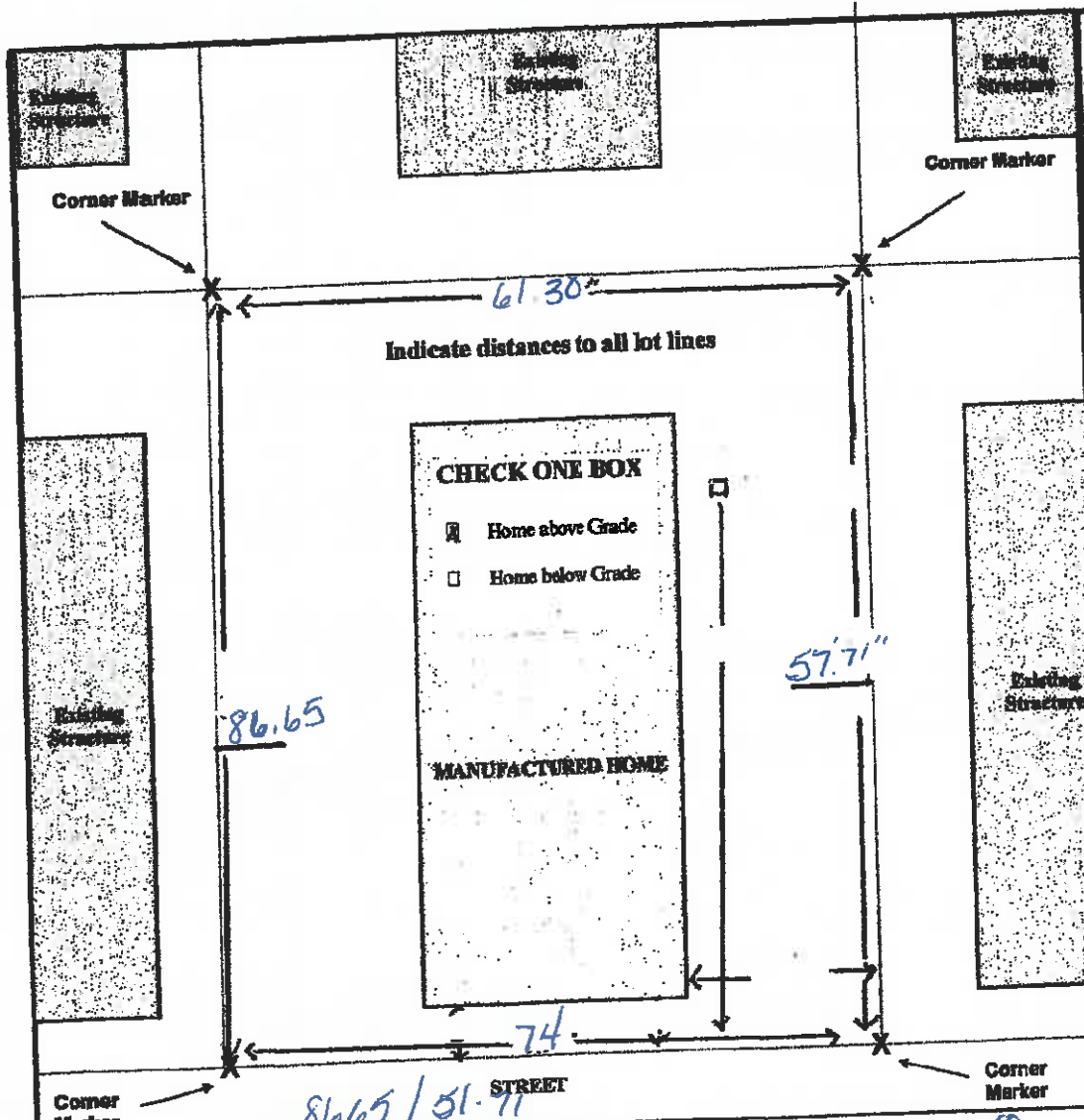


Elevation



Dimensions, floor plans, elevations, features and other information are subject to change without notice. Square footage and other dimensions are approximations. Elevations, photography and videos are often shown with optional features and/or third party additions, such as garages and porches that may not be available in all regions or situations.

LOT PLOT PLAN AND PARK INFORMATION



A) Park Name Old Plantation
 Homeowner Name 3825 Crestmore
 Homeowner Address Sp# 474
 City Surpa Valley Zip 92509

B) Design Information:
 Home Amperage: 50 Pedestal Amperage: _____
 Home Voltage: _____ Pedestal Voltage: _____
 Home Roof Load: _____ PSF
 Roof Load for locality: _____ PSF

C) Is the park located in a snow area requiring 30 lb or greater roof loading?
 YES NO

D) The lot line corners at the front and rear are clearly and permanently marked pursuant to Title 25 of the California Code of Regulations, Sections 1104 or 2104 in the following manner:
Rebar & Pins

NOTE: Each lot line corner shall be clearly and permanently marked prior to installation and inspection.

**STATEMENT OF RESPONSIBILITY
 (ORIGINAL SIGNATURE REQUIRED)**

As the park owner or operator, or as his or her authorized representative, I hereby certify that the information provided on this plot plan relative to the location of the manufactured home, all related accessory structure locations and separations and the park and homeowner information is true, accurate and complete. Lot corners have been identified as in item D above.

Signature of Park Owner, Operator, or Manager

State of California
 Department of Housing and Community Development
 Division of Codes and Standards

Width and length of lot: 61.30 x 86.65 / 51.11 STREET
 Width and length of home 74 x 40

1. Draw any proposed structure(s) and existing structures on the diagram above at the approximate location and identify the type of structures (e.g. deck, awning, etc). Indicate the distance from the lot line to the proposed structure. Also indicate the length and width of the structure.
2. Indicate the exact distances from structures on adjacent lots if located within 10 (ten) feet of your lot line.
3. Enter length & width of the manufactured home (including eaves) and length & width of lot.
4. No vegetation is allowed under the manufactured home or habitable accessory structure. Lot must be properly graded to ensure that water cannot accumulate beneath the manufactured home.



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Southern Area Office
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#474

86.65

41.30

LOT SIZE $\frac{9.3}{86.65} \times \frac{41.30}{51.71}$

HOME SIZE 24 x 40

EAVES/OVERHANGS:

FRONT 12"

REAR 12"

SIDES 0"

DRIVEWAY

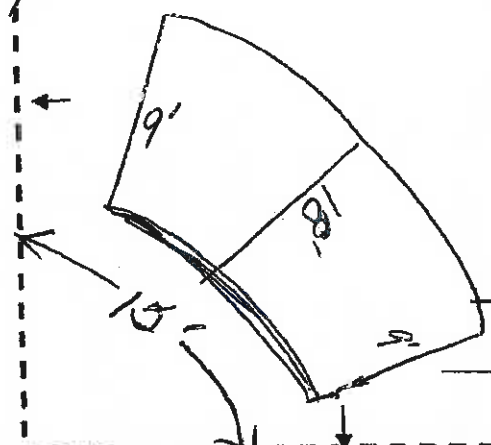
9' x 18'



AWNING

10' x 20'

51.71



THIS SITE PLAN IS REQUIRED FOR ALL HOME INSTALLATIONS. Must include the dimensions of all requested items, **INCLUDING DOORS**; draw each item on site plan above; and note the distances between each item, the home, to the front and rear of home, lot lines & structures on adjoining spaces.

1. Back door & steps: Landing Size ___ x ___ Material _____ / Up & In ___ Up & Over ___

2. Front door steps: Landing Size ___ x ___ Material _____ / Up & In ___ Up & Over ___

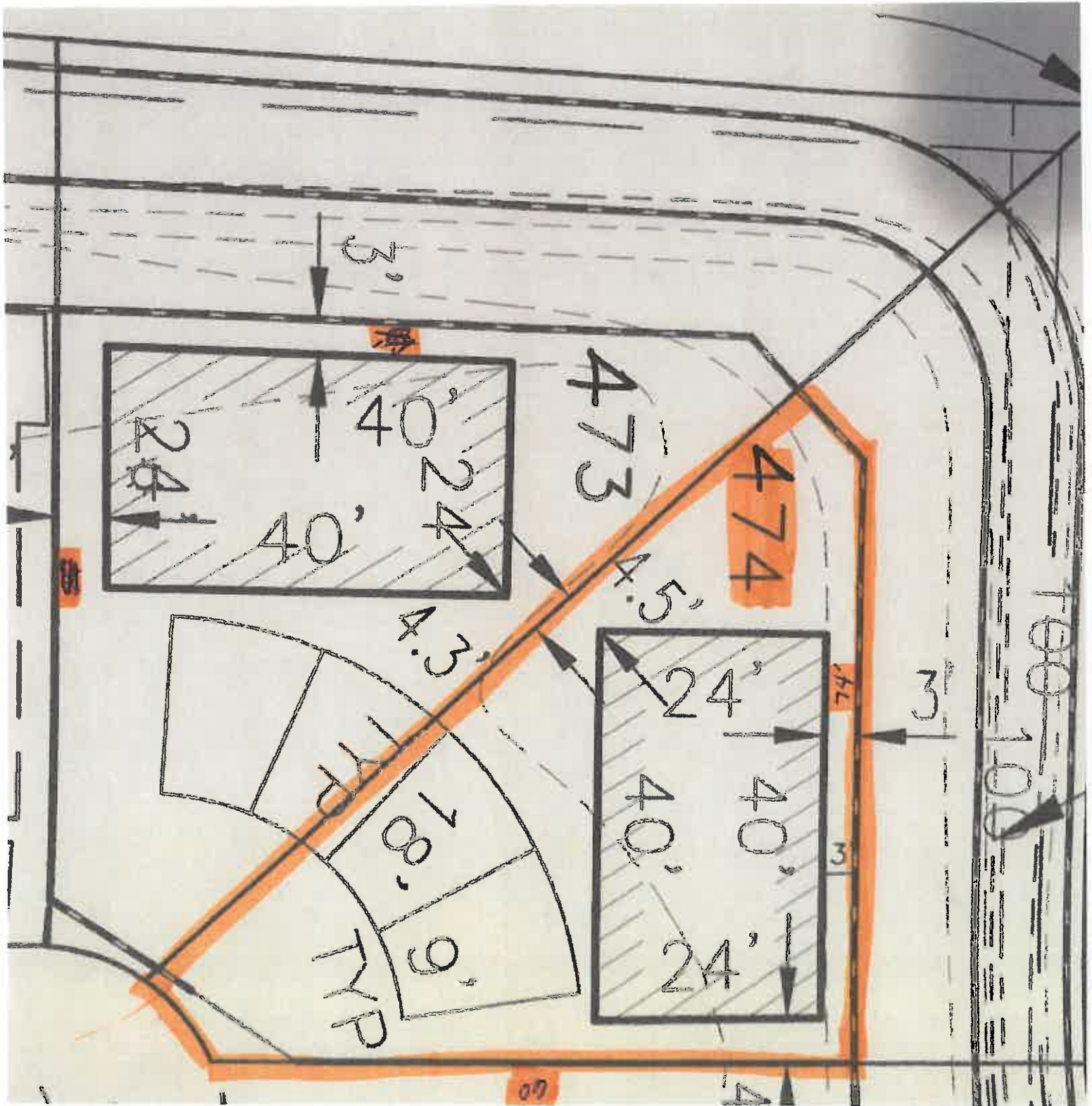
3. Back Door Awning: ___ x ___ 4. Front door awning: ___ x ___ 5. Porch: ___ x ___ Material _____

6. Shed: ___ x ___ Material: _____ 7. Location of Utilities & distance to front & rear of home =

*Also include location and size of the concrete pads, walkways, masonry planter, etc., to be installed.

PARK Old Plan. SPACE 474 REG MGR APPROVAL & DATE _____

474



✓



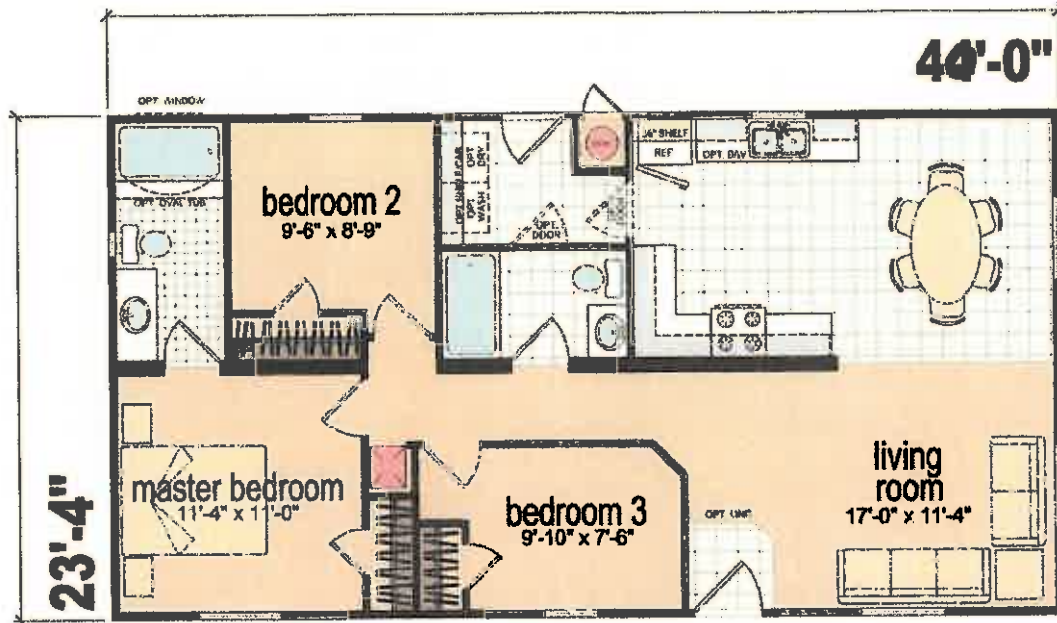
4. 1. 1. 1.

Creekside Manor 3443L

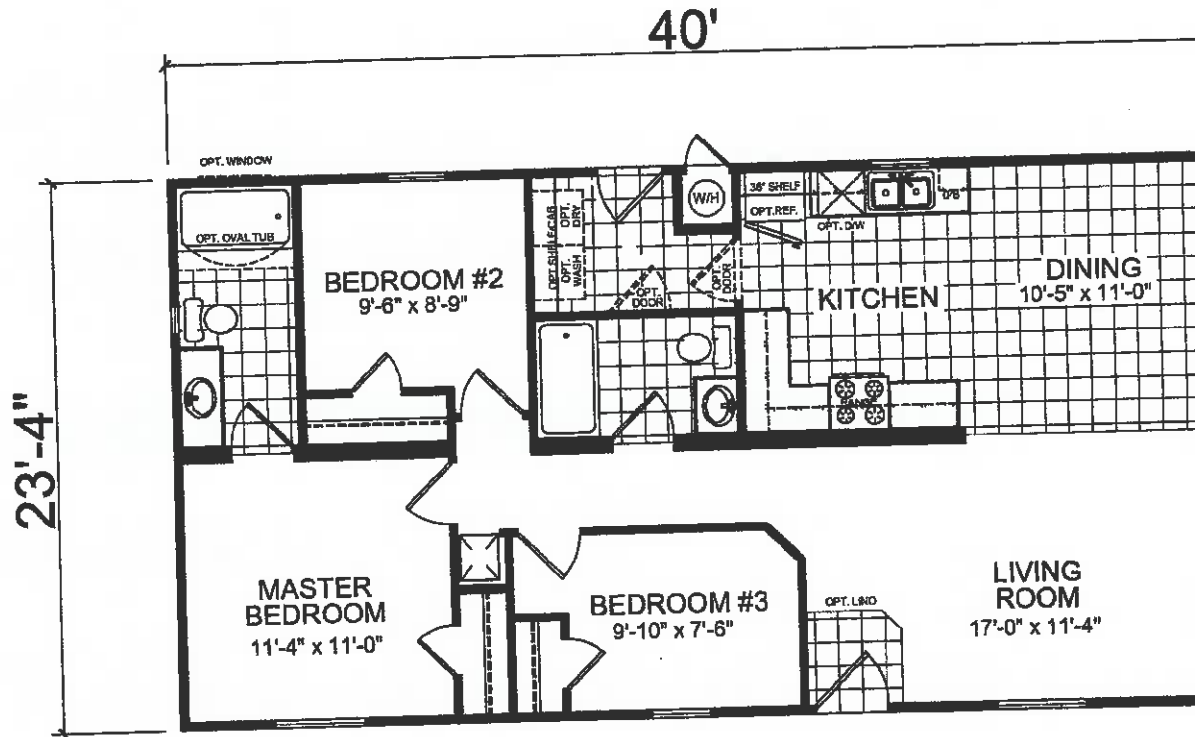
1027 Square Feet, 3 Bedrooms, 2 Bathrooms, Multi-Section

This manufactured home is built to the federal (HUD) building code for manufactured housing.

Floor Plan



Space # 474



ALL DIMENSIONS ARE APPROXIMATE, AND MAY BE SUBJECT TO CHANGES BY CHAMPION HOME BUILDERS CO. LINDSAY, CA

REDMAN HOMES.
CHAMPION
 840 W. Palm Ave. Lindsay, CA 93247

| DAPIA SEAL | MODIFICATIONS |
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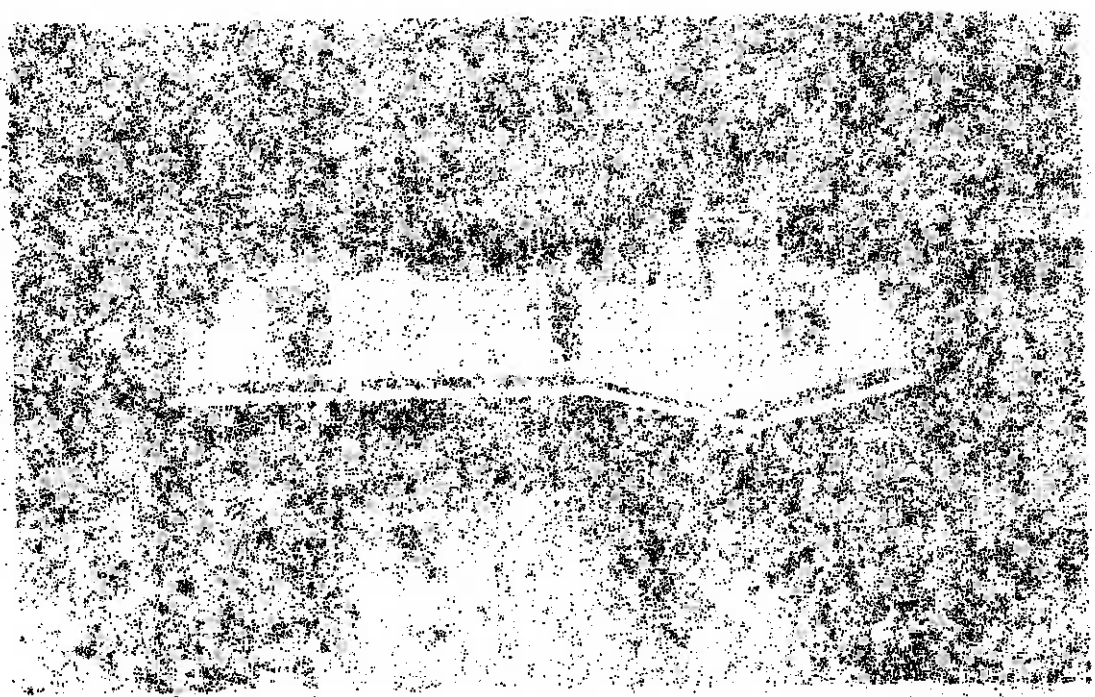
| | |
|-------------------------------|--|
| MODEL: 09-CM-3443L | SHEET: |
| TITLE: LITERATURE PLAN | L-101 |
| DRAWN BY: M. SANCHEZ | DATE: 2-16-2015 |
| SCALE: 1/8" = 1'-0" | 3 BEDROOM, 2 BATH NOMINAL SIZE: 24' x 44' ACTUAL SIZE: 23'-4" x 40' TOTAL AREA: 1,207 SQ. FT. |

Elevation

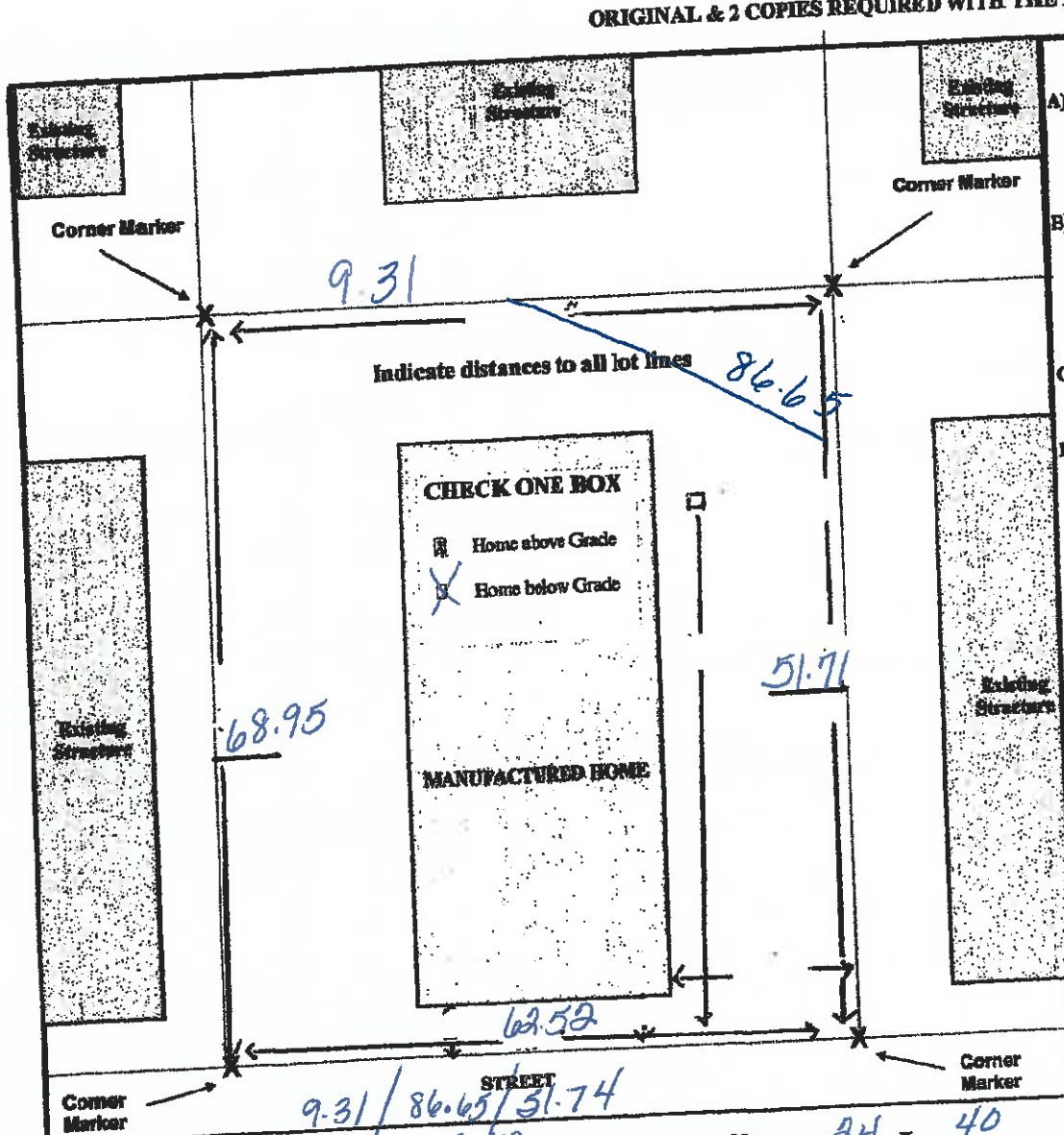


Dimensions, floor plans, elevations, features and other information are subject to change without notice. Square footage and other dimensions are approximations. Elevations, photography and videos are often shown with optional features and/or third party additions, such as garages and porches that may not be available in all regions or situations.

The following information is provided for your information. The information is for informational purposes only and is not intended to be used for any other purpose. The information is provided as a service to our customers and is subject to change without notice.



Page 1 of 1



LOT PLOT PLAN AND PARK INFORMATION

A) Park Name Old Plantation
 Homeowner Name _____
 Homeowner Address 3825 Crestmont Sp# 473
 City Surupa Valley Zip 92509

B) Design Information:
 Home Amperage: 50 Pedestal Amperage: _____
 Home Voltage: _____ Pedestal Voltage: _____
 Home Roof Load: _____ PSF
 Roof Load for locality: _____ PSF

C) Is the park located in a snow area requiring 30 lb or greater roof loading?
 YES NO

D) The lot line corners at the front and rear are clearly and permanently marked pursuant to Title 25 of the California Code of Regulations, Sections 1104 or 2104 in the following manner:
Pins / Markers

NOTE: Each lot line corner shall be clearly and permanently marked prior to installation and inspection.

**STATEMENT OF RESPONSIBILITY
 (ORIGINAL SIGNATURE REQUIRED)**

As the park owner or operator, or as his or her authorized representative, I hereby certify that the information provided on this plot plan relative to the location of the manufactured home, all related accessory structure locations and separations and the park and homeowner information is true, accurate and complete. Lot corners have been identified as in item D above.

Signature of Park Owner, Operator, or Manager

State of California
 Department of Housing and Community Development
 Division of Codes and Standards

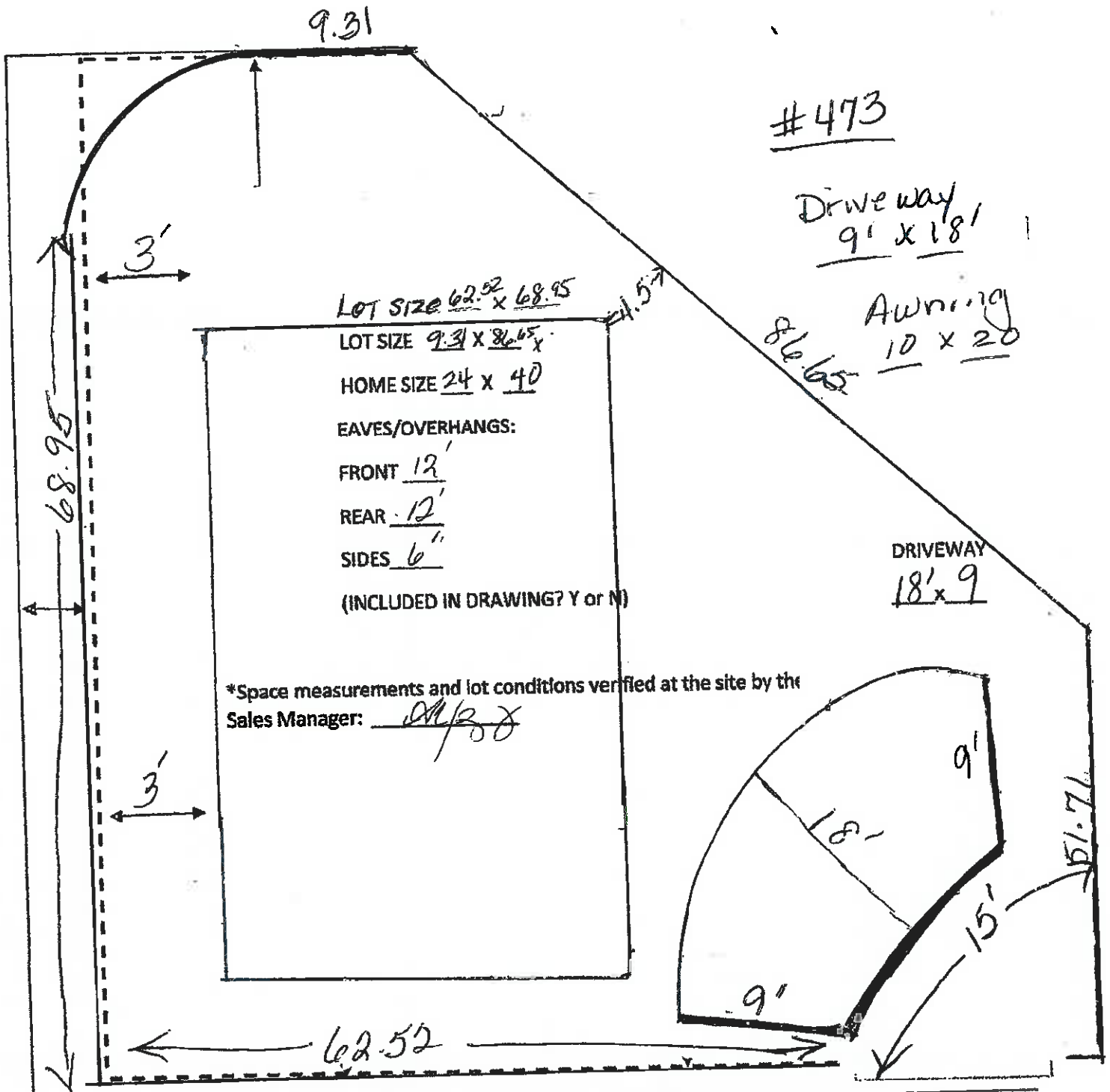
Width and length of lot: 68.95 x 62.52 Width and length of home 24 x 40

1. Draw any proposed structure(s) and existing structures on the diagram above at the approximate location and identify the type of structures (e.g. deck, awning, etc). Indicate the distance from the lot line to the proposed structure. Also indicate the length and width of the structure.
2. Indicate the exact distances from structures on adjacent lots if located within 10 (ten) feet of your lot line.
3. Enter length & width of the manufactured home (including eaves) and length & width of lot.
4. No vegetation is allowed under the manufactured home or habitable accessory structure. Lot must be properly graded to ensure that water cannot accumulate beneath the manufactured home.



Northern Area Office
 9342 Tech Center Drive, Suite 550
 Sacramento, CA 95826

Southern Area Office
 3737 Main St. Ste 408
 Riverside, CA 92501



THIS SITE PLAN IS REQUIRED FOR ALL HOME INSTALLATIONS. Must include the dimensions of all requested items, INCLUDING DOORS; draw each item on site plan above; and note the distances between each item, the home, to the front and rear of home, lot lines & structures on adjoining spaces.

1. Back door & steps: Landing Size 8 x 4 Material Trex / Up & In Up & Over

2. Front door steps: Landing Size 8 x 4 Material Trex / Up & In Up & Over

3. Back Door Awning: N/A 4. Front door awning: 8 x 4 5. Porch: W x A Material ---

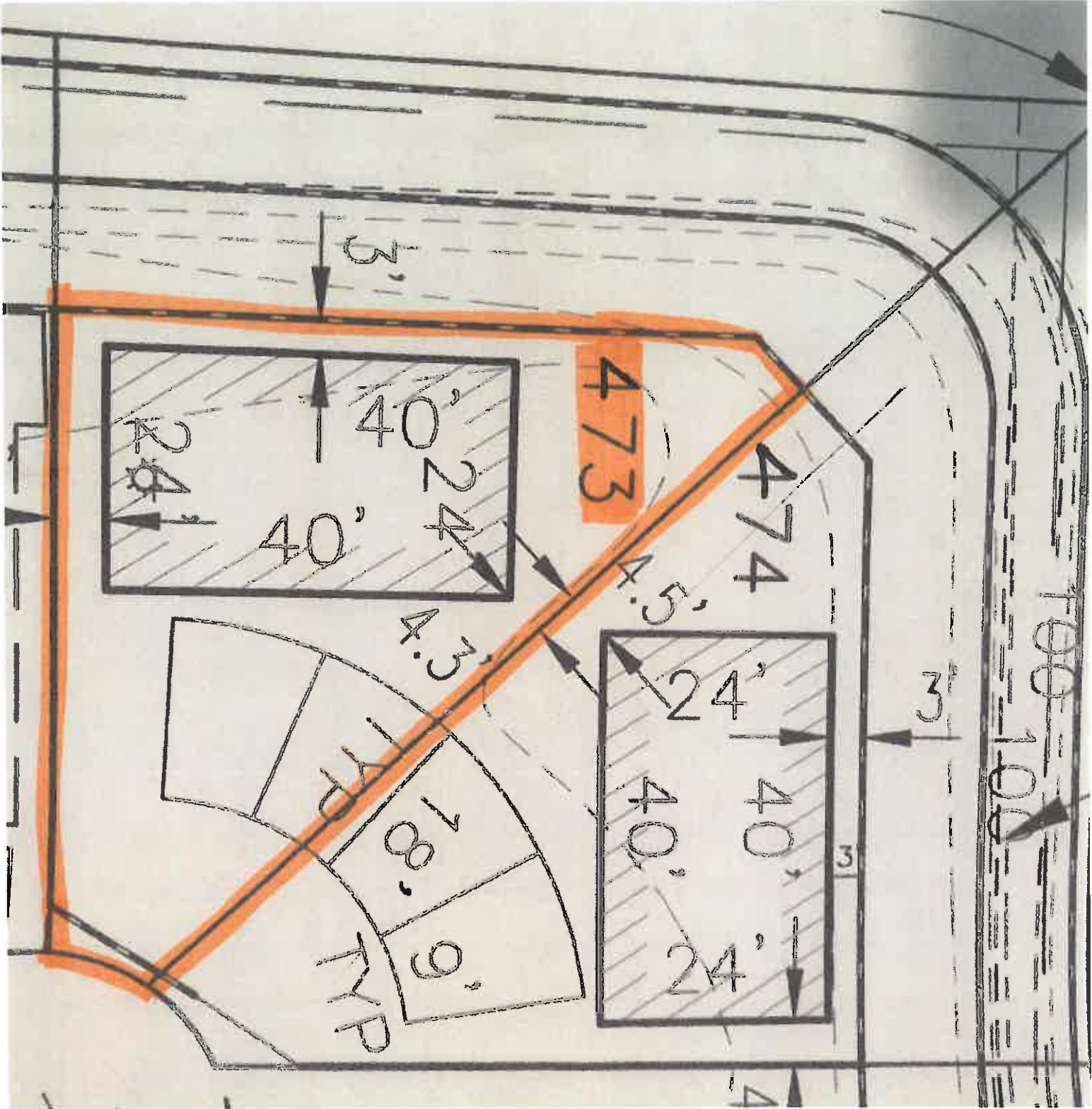
6. Shed: N x A Material: N/A 7. Location of Utilities & distance to front & rear of home =

*Also include location and size of the concrete pads, walkways, masonry planter, etc., to be installed.

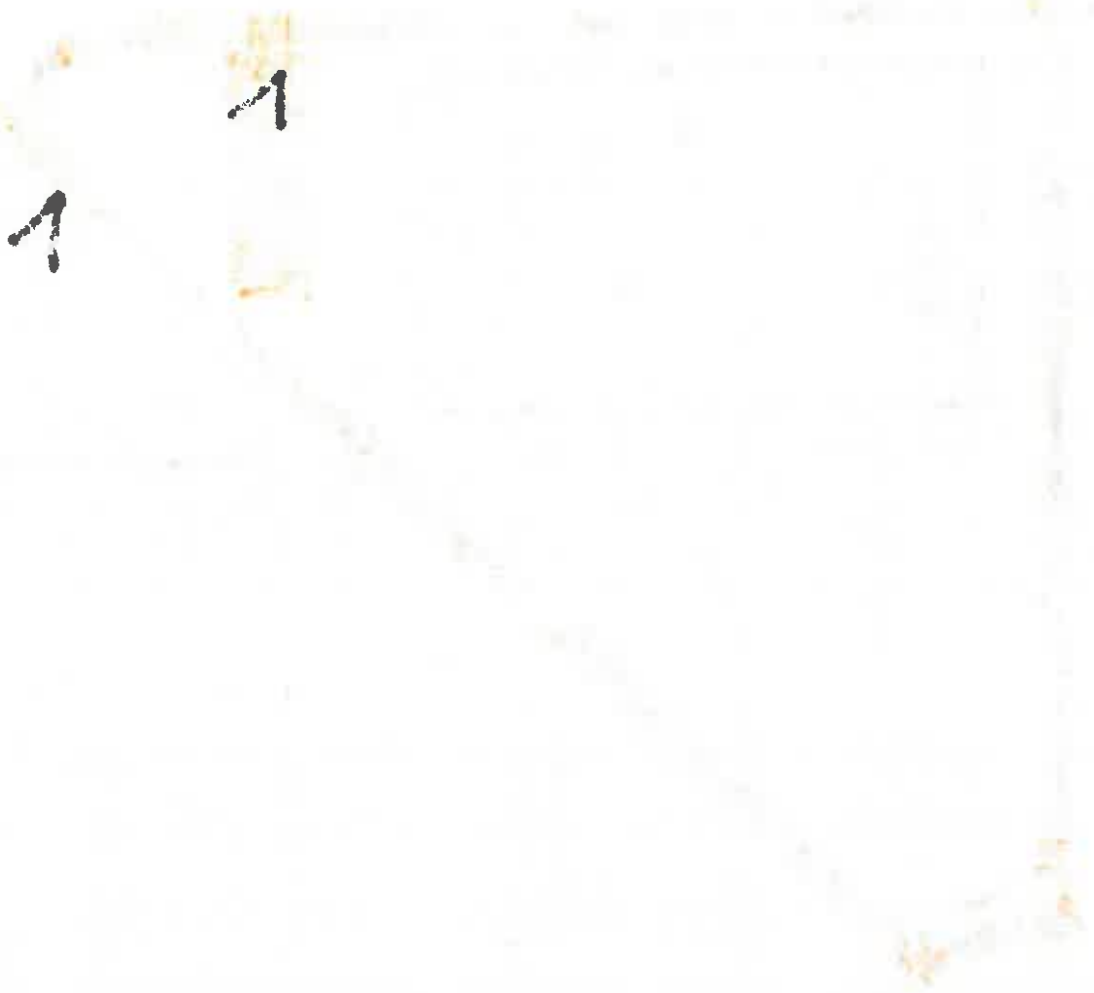
PARK Old Plantations SPACE 473 REG MGR APPROVAL & DATE _____

#473

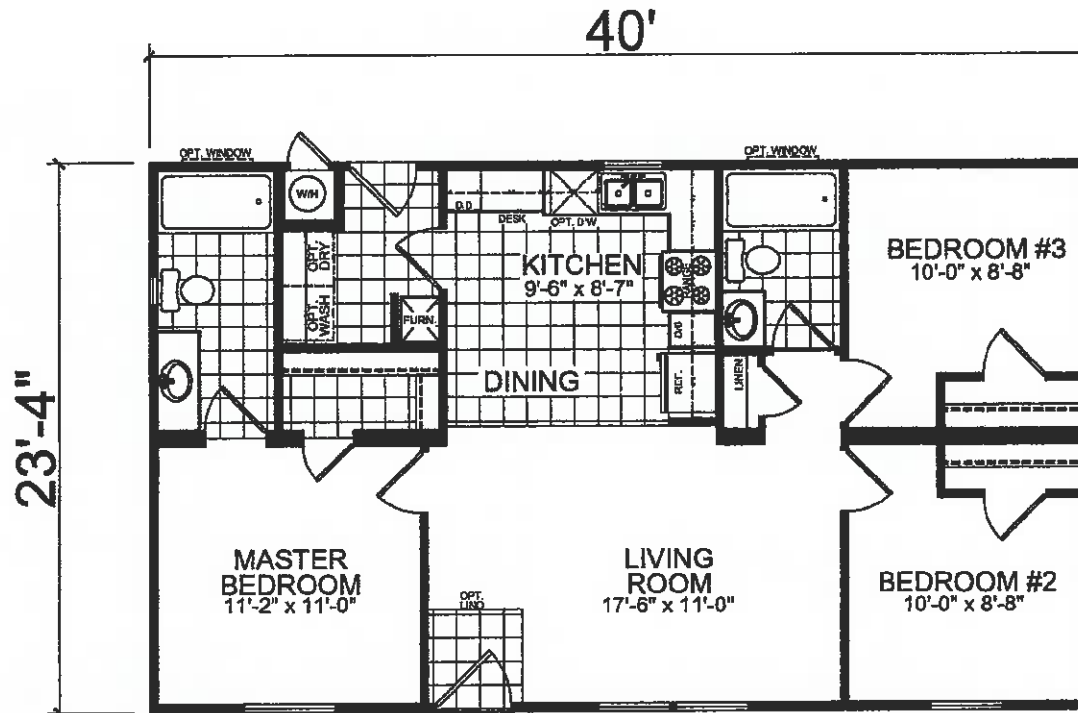
Floorplan
84 x 40
CM-3443L
Space# 473



#112



Space #474



ALL DIMENSIONS ARE APPROXIMATE, AND MAY BE SUBJECT TO CHANGES BY CHAMPION HOME BUILDERS CO. LINDSAY, CA

REDMAN HOMES.

CHAMPION
 840 W. Palm Ave. Lindsay, CA 93247

DAPIA SEAL

MODIFICATIONS

MODEL: 09-CM-3403B

SHEET:

TITLE: LITERATURE PLAN

L-101

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DRAWN BY: M. SANCHEZ

DATE: 2-12-2015

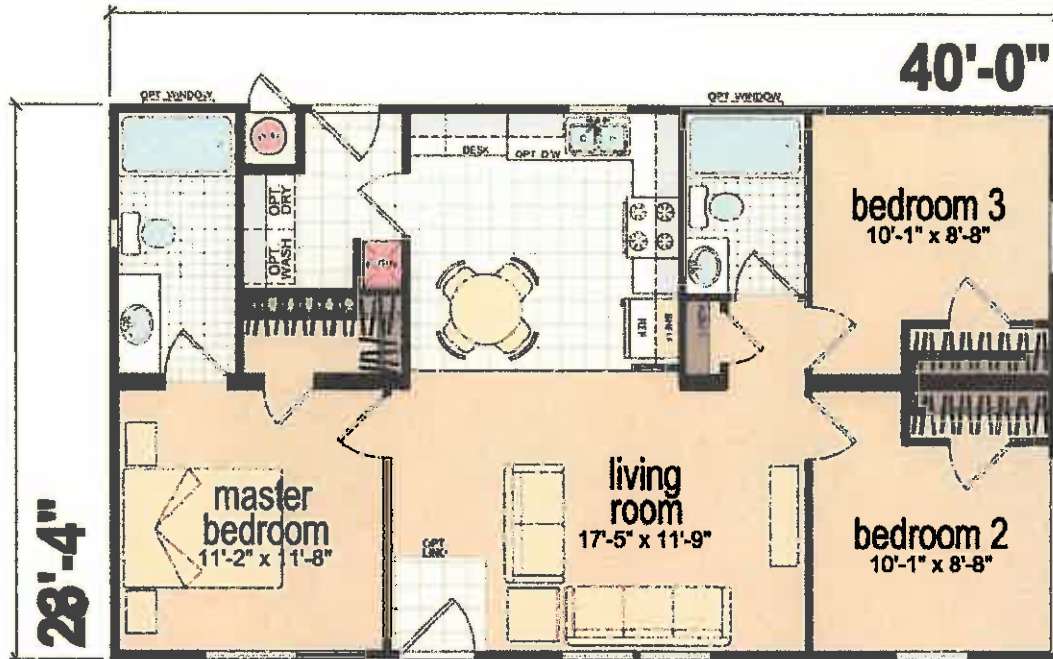
SCALE: 1/8" = 1'-0"

Creekside Manor 3403B

933 Square Feet, 3 Bedrooms, 2 Bathrooms, Multi-Section

This manufactured home is built to the federal (HUD) building code for manufactured housing.

Floor Plan

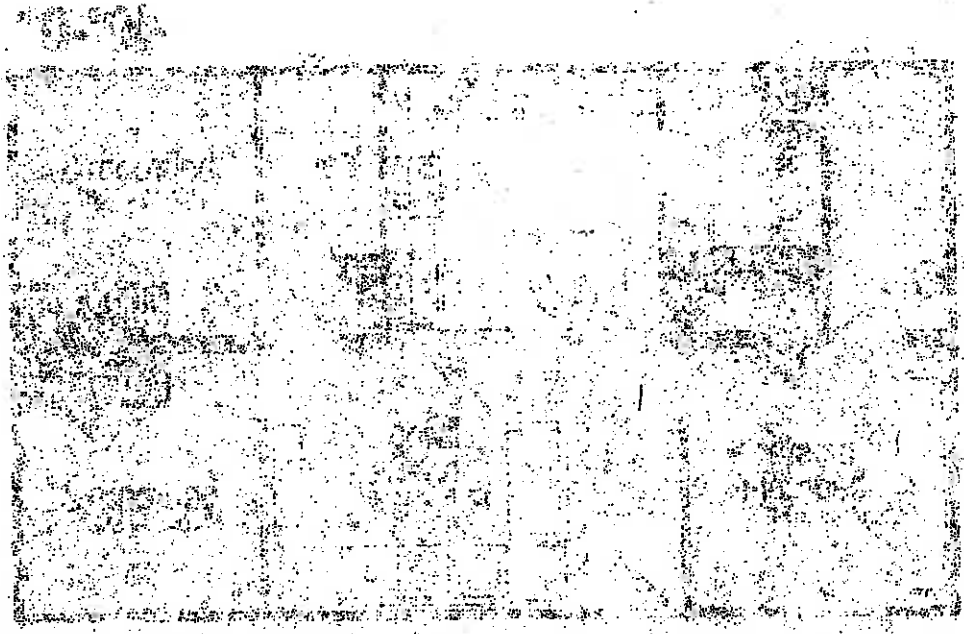


Crekide Manor 3403B

999-800-1234 (Tel: 999-800-1234) 2010-10-10

10/10/2010 10:10:10 AM

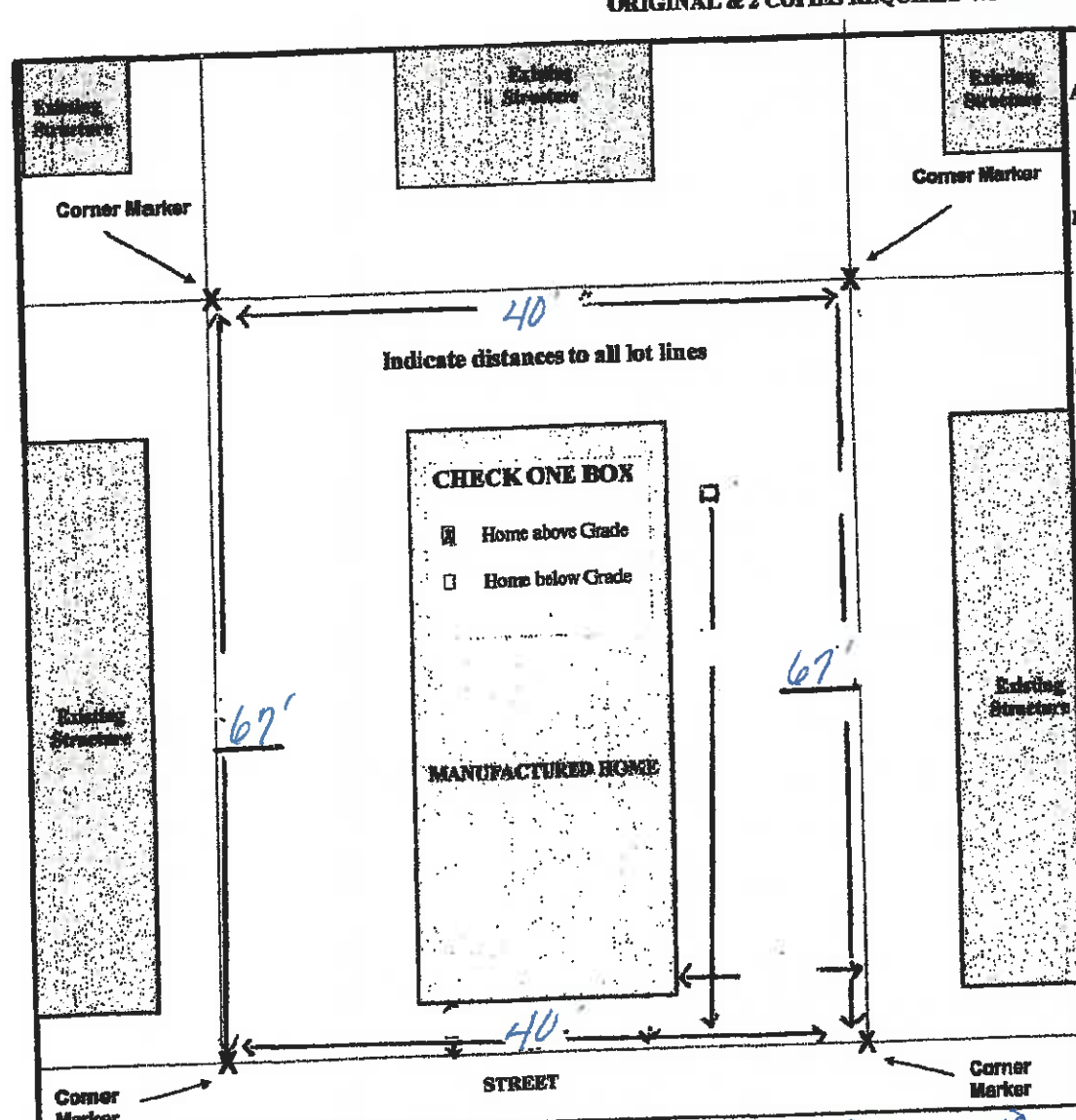
10/10/2010



Elevation



Dimensions, floor plans, elevations, features and other information are subject to change without notice. Square footage and other dimensions are approximations. Elevations, photography and videos are often shown with optional features and/or third party additions, such as garages and porches that may not be available in all regions or situations.



LOT PLOT PLAN AND PARK INFORMATION

A) Park Name Old Plantation
 Homeowner Name _____
 Homeowner Address 3820 Crestmeir Sp# 452
 City Yuba Valley Zip 92509

B) Design Information:
 Home Amperage: 50 Pedestal Amperage: _____
 Home Voltage: _____ Pedestal Voltage: _____
 Home Roof Load: _____ PSF
 Roof Load for locality: _____ PSF

C) Is the park located in a snow area requiring 30 lb or greater roof loading?
 YES NO

D) The lot line corners at the front and rear are clearly and permanently marked pursuant to Title 25 of the California Code of Regulations, Sections 1104 or 2104 in the following manner:
Pins & Markers.

NOTE: Each lot line corner shall be clearly and permanently marked prior to installation and inspection.

STATEMENT OF RESPONSIBILITY
 (ORIGINAL SIGNATURE REQUIRED)

As the park owner or operator, or as his or her authorized representative, I hereby certify that the information provided on this plot plan relative to the location of the manufactured home, all related accessory structure locations and separations and the park and homeowner information is true, accurate and complete. Lot corners have been identified as in item D above.

Signature of Park Owner, Operator, or Manager

State of California
 Department of Housing and Community Development
 Division of Codes and Standards

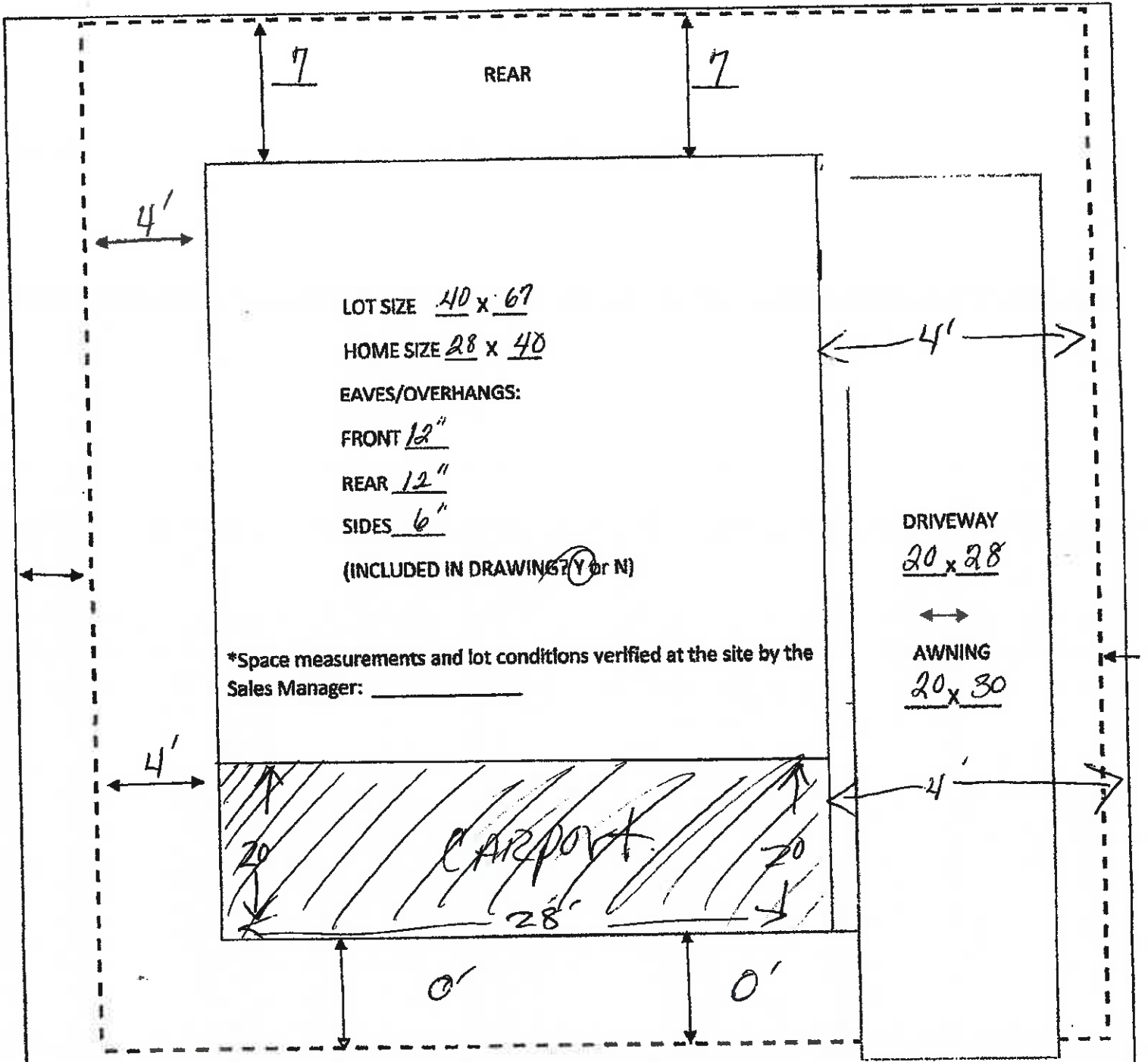
Width and length of lot: 40 x 67 Width and length of home 28 x 40

1. Draw any proposed structure(s) and existing structures on the diagram above at the approximate location and identify the type of structures (e.g. deck, awning, etc). Indicate the distance from the lot line to the proposed structure. Also indicate the length and width of the structure.
2. Indicate the exact distances from structures on adjacent lots if located within 10 (ten) feet of your lot line.
3. Enter length & width of the manufactured home (including eaves) and length & width of lot.
4. No vegetation is allowed under the manufactured home or habitable accessory structure. Lot must be properly graded to ensure that water cannot accumulate beneath the manufactured home.



Northern Area Office
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 Sacramento, CA 95826

Southern Area Office
 3737 Main St. Ste 400
 Riverside, CA 92501



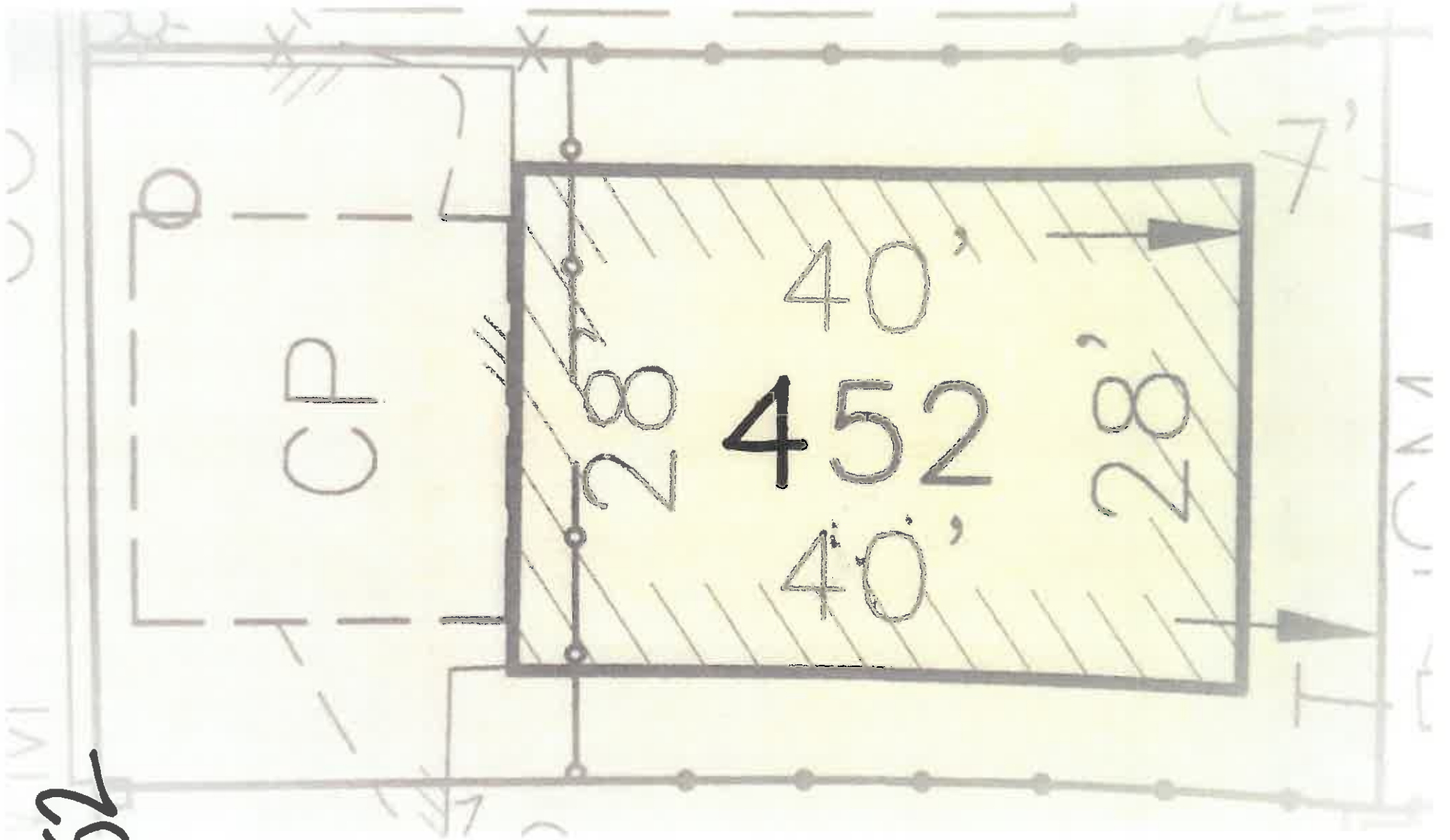
THIS SITE PLAN IS REQUIRED FOR ALL HOME INSTALLATIONS. Must include the dimensions of all requested items, INCLUDING DOORS; draw each item on site plan above; and note the distances between each item, the home, to the front and rear of home, lot lines & structures on adjoining spaces.

1. Back door & steps: Landing Size 4 x 8 Material Trex / Up & In X Up & Over _____
2. Front door steps: Landing Size 4 x 8 Material Trex / Up & In _____ Up & Over X
3. Back Door Awning: N x A 4. Front door awning: 8 x 4 5. Porch: N x A Material _____
6. Shed: N x A Material: _____ 7. Location of Utilities & distance to front & rear of home = ■

*Also include location and size of the concrete pads, walkways, masonry planter, etc., to be installed.

PARK Old Plan SPACE 452 REG MGR APPROVAL & DATE _____

Floorplan
CM-3403B

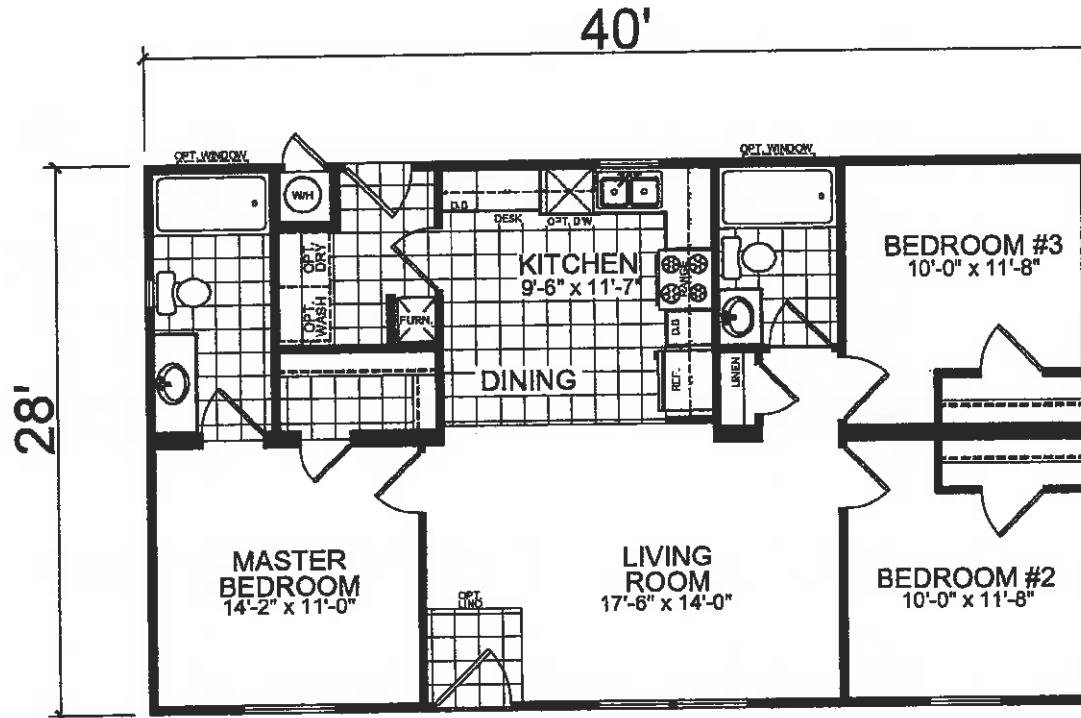


#452

10/17/07
#

4

Space #452



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REDMAN HOMES.

CHAMPION
 840 W. Palm Ave. Lindsay, CA 93247

| DAPIA SEAL | MODIFICATIONS |
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| |
|---|
| MODEL: 09-CM-3403B |
| TITLE: LITERATURE PLAN |
| DRAWN BY: M. SANCHEZ DATE: 2-12-2015 |
| SCALE: 1/8" = 1'-0" |

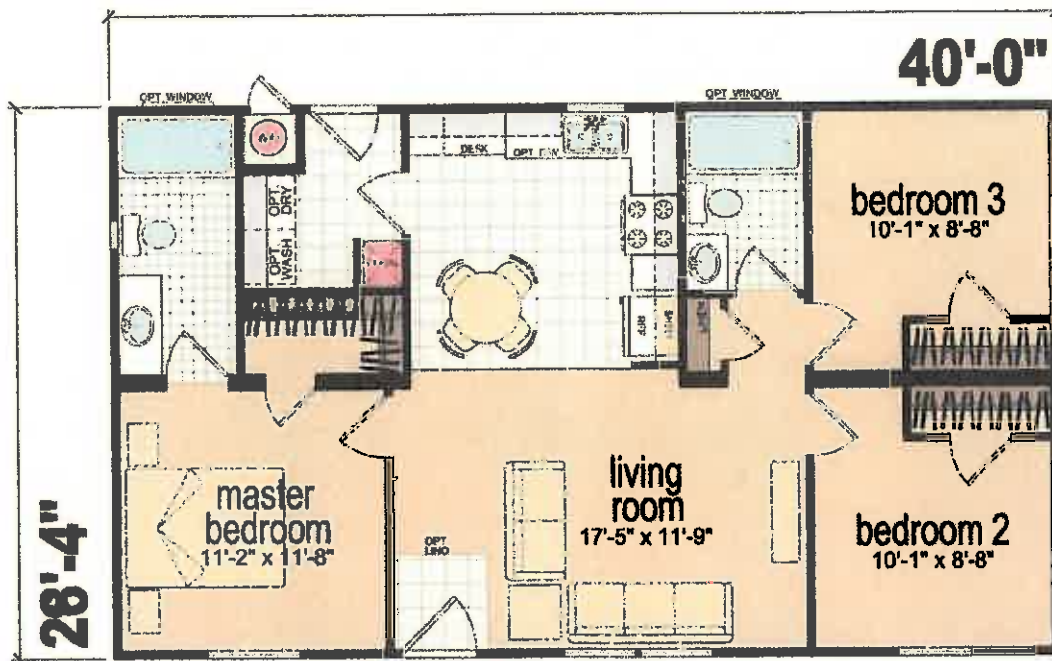
| |
|--------------|
| SHEET: L-101 |
|--------------|

Creekside Manor 3403B

933 Square Feet, 3 Bedrooms, 2 Bathrooms, Multi-Section

This manufactured home is built to the federal (HUD) building code for manufactured housing.

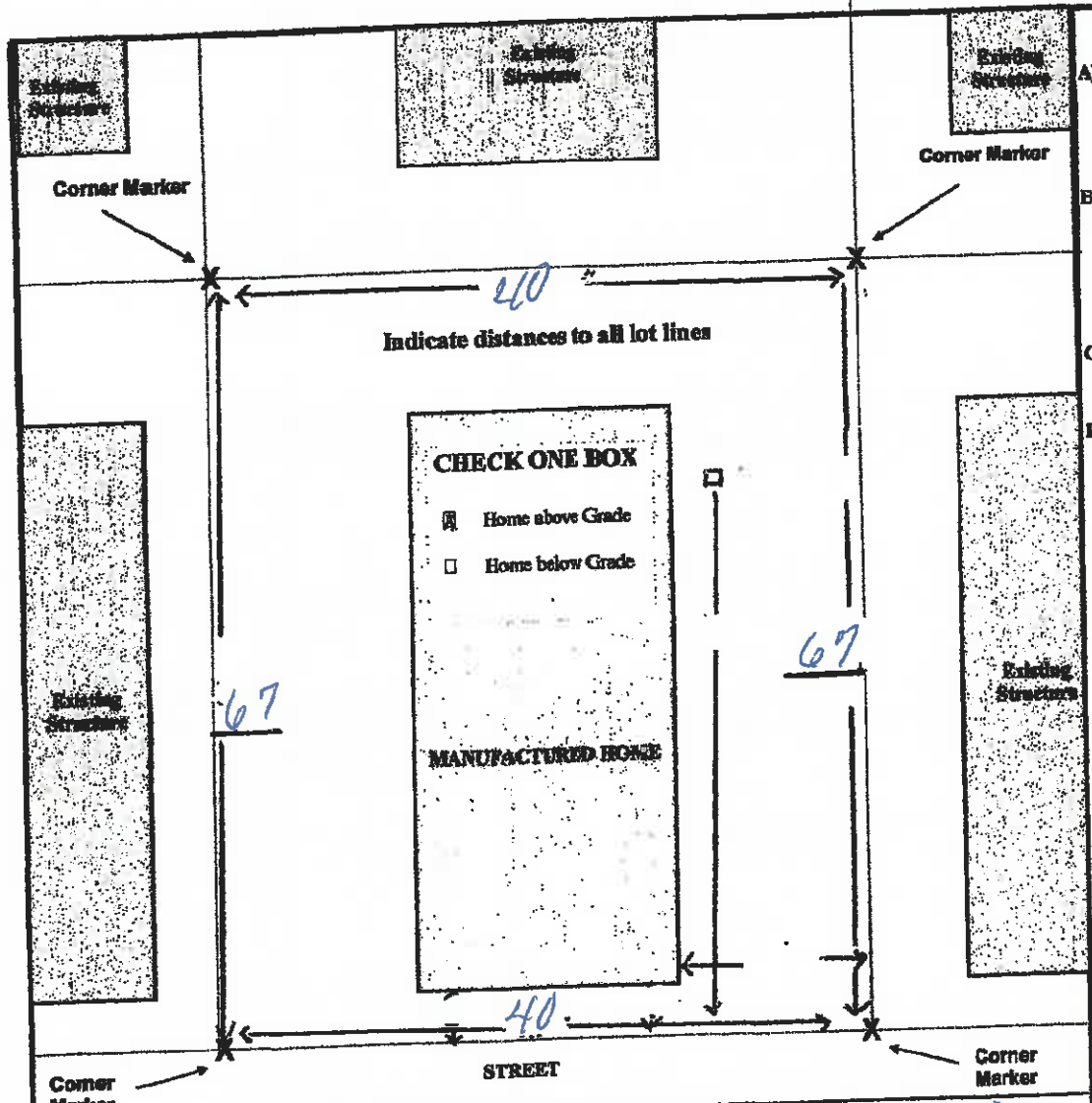
Floor Plan



Elevation



Dimensions, floor plans, elevations, features and other information are subject to change without notice. Square footage and other dimensions are approximations. Elevations, photography and videos are often shown with optional features and/or third party additions, such as garages and porches that may not be available in all regions or situations.



Indicate distances to all lot lines

CHECK ONE BOX

- Home above Grade
- Home below Grade

MANUFACTURED HOME

Width and length of lot: 40 x 67 Width and length of home 28 x 40

LOT PLOT PLAN AND PARK INFORMATION

A) Park Name Old Plantation
 Homeowner Name _____
 Homeowner Address 3825 Crestmore Sp# 441
 City Surupa Valley Zip 92509

B) Design Information:
 Home Amperage: 50 Pedestal Amperage: _____
 Home Voltage: _____ Pedestal Voltage: _____
 Home Roof Load: _____ PSF
 Roof Load for locality: _____ PSF

C) Is the park located in a snow area requiring 30 lb or greater roof loading?
 YES NO

D) The lot line corners at the front and rear are clearly and permanently marked pursuant to Title 25 of the California Code of Regulations, Sections 1104 or 2104 in the following manner:

Pin / MARKERS / Rf bar

NOTE: Each lot line corner shall be clearly and permanently marked prior to installation and inspection.

**STATEMENT OF RESPONSIBILITY
 (ORIGINAL SIGNATURE REQUIRED)**

As the park owner or operator, or as his or her authorized representative, I hereby certify that the information provided on this plot plan relative to the location of the manufactured home, all related accessory structure locations and separations and the park and homeowner information is true, accurate and complete. Lot corners have been identified as in item D above.

Signature of Park Owner, Operator, or Manager

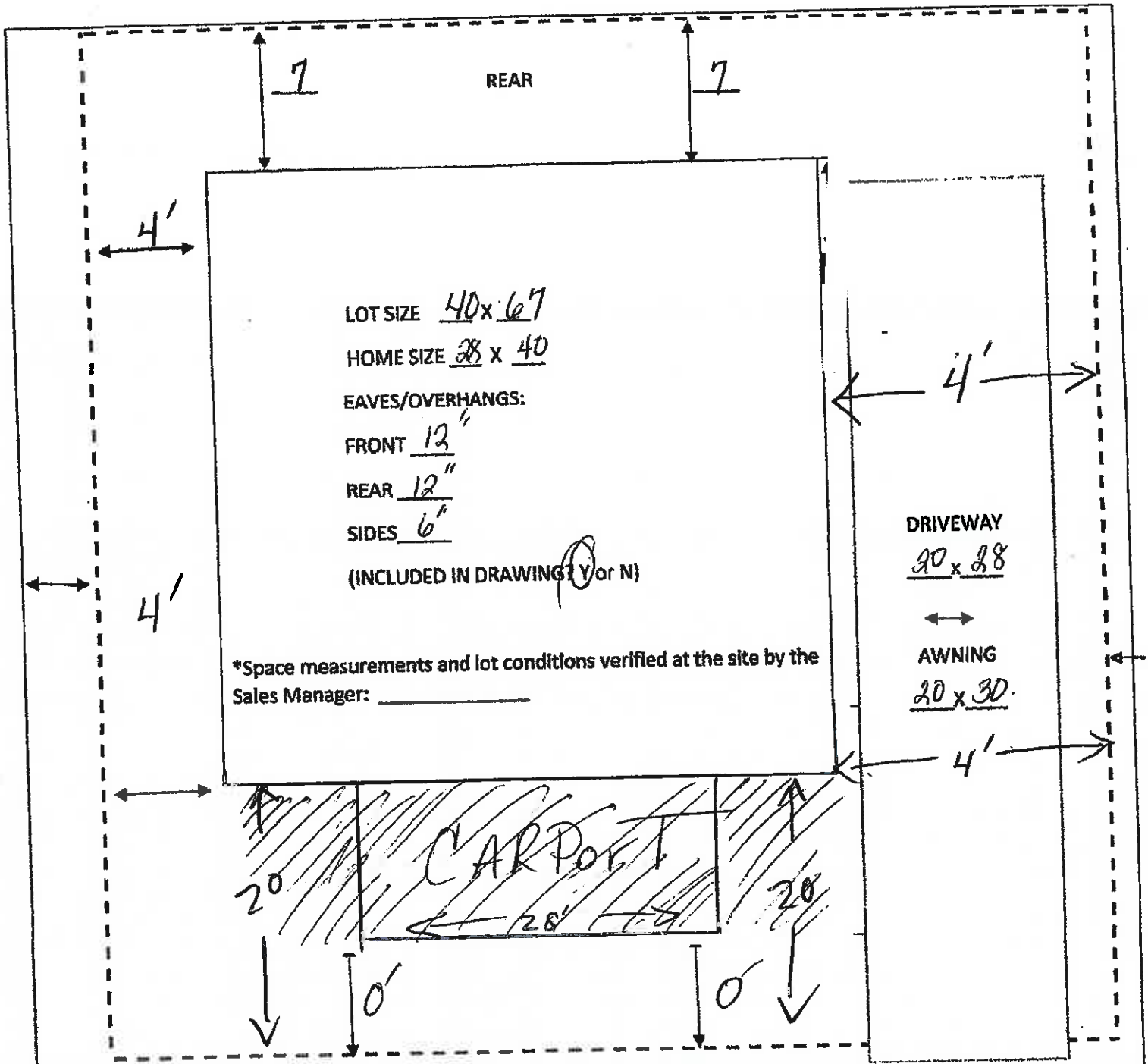
State of California
 Department of Housing and Community Development
 Division of Codes and Standards

1. Draw any proposed structure(s) and existing structures on the diagram above at the approximate location and identify the type of structures (e.g. deck, awning, etc). Indicate the distance from the lot line to the proposed structure. Also indicate the length and width of the structure.
2. Indicate the exact distances from structures on adjacent lots if located within 10 (ten) feet of your lot line.
3. Enter length & width of the manufactured home (including eaves) and length & width of lot.
4. No vegetation is allowed under the manufactured home or habitable accessory structure. Lot must be properly graded to ensure that water cannot accumulate beneath the manufactured home.



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 Sacramento, CA 95826

Southern Area Office
 3737 Main St. Ste 406
 Riverside, CA 92501

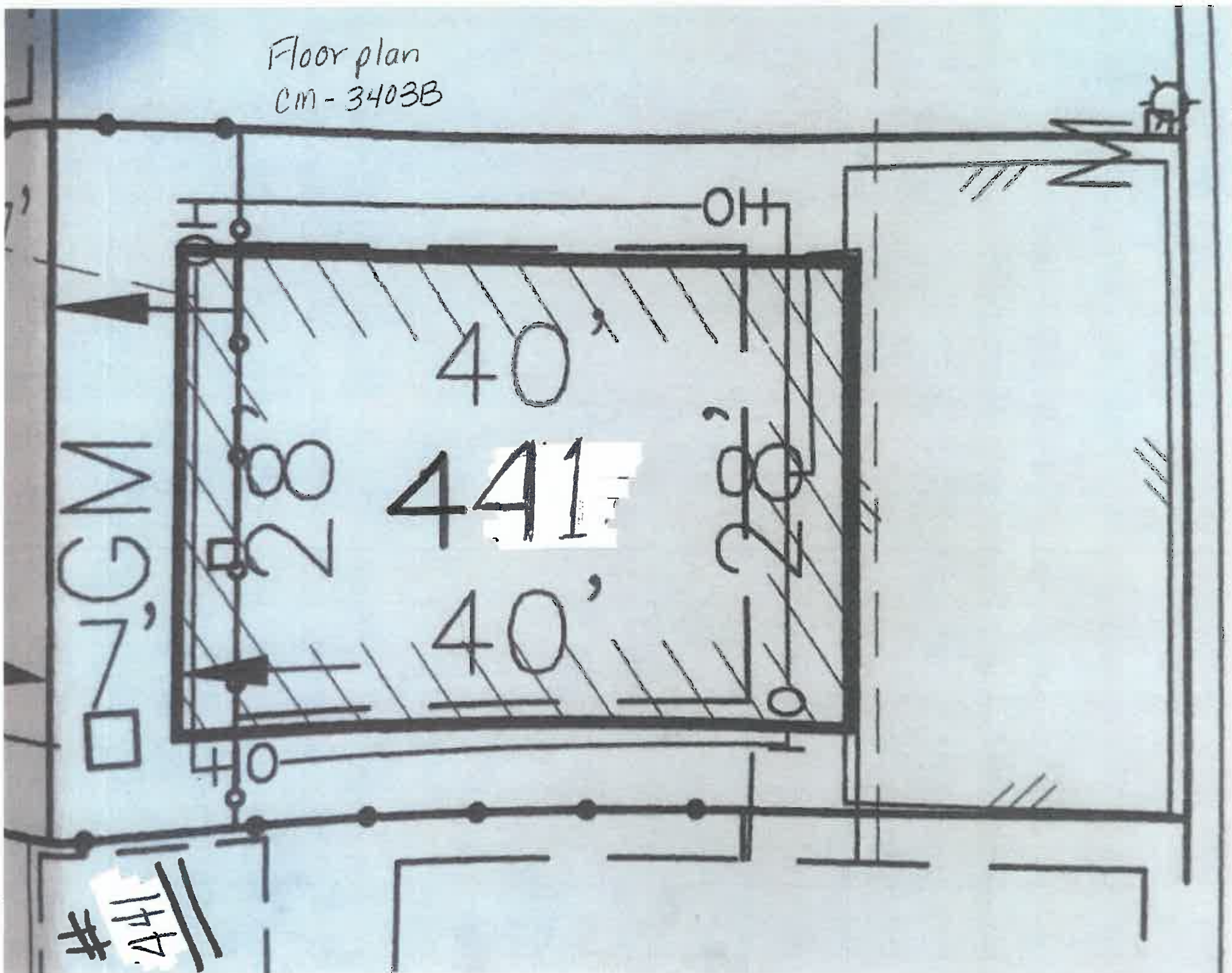


THIS SITE PLAN IS REQUIRED FOR ALL HOME INSTALLATIONS. Must include the dimensions of all requested items, INCLUDING DOORS; draw each item on site plan above; and note the distances between each item, the home, to the front and rear of home, lot lines & structures on adjoining spaces.

1. Back door & steps: Landing Size 4 x 8 Material Trax / Up & In X Up & Over _____
 2. Front door steps: Landing Size 4 x 8 Material Trax / Up & In _____ Up & Over X
 3. Back Door Awning: N x A 4. Front door awning: 8 x 4 5. Porch: N x A Material N/A
 6. Shed: N x A Material: _____ 7. Location of Utilities & distance to front & rear of home =
- *Also include location and size of the concrete pads, walkways, masonry planter, etc., to be installed.

PARK Old Plan SPACE 441 REG MGR APPROVAL & DATE _____

Floor plan
CM-3403B

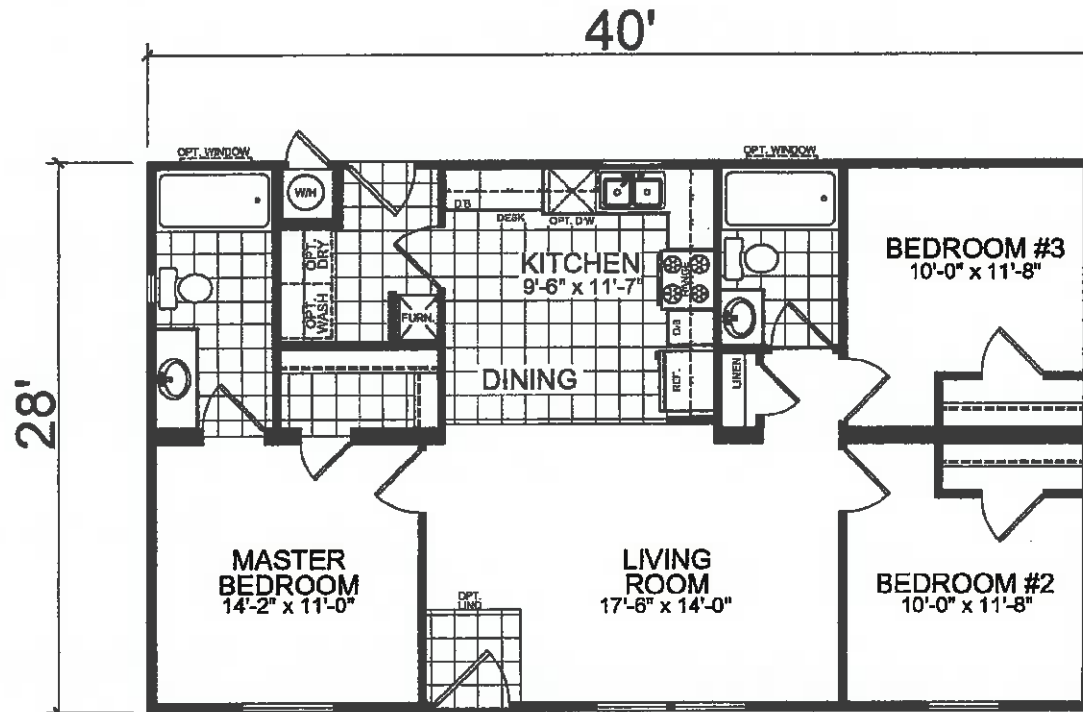


441

12/21

2

Space # 441



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REDMAN HOMES.
CHAMPION
 840 W. Palm Ave. Lindsay, CA 93247

DAPIA SEAL

MODIFICATIONS

MODEL: 09-CM-3403B

SHEET:

TITLE: LITERATURE PLAN

L-101

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DRAWN BY: M. SANCHEZ DATE: 2-12-2015

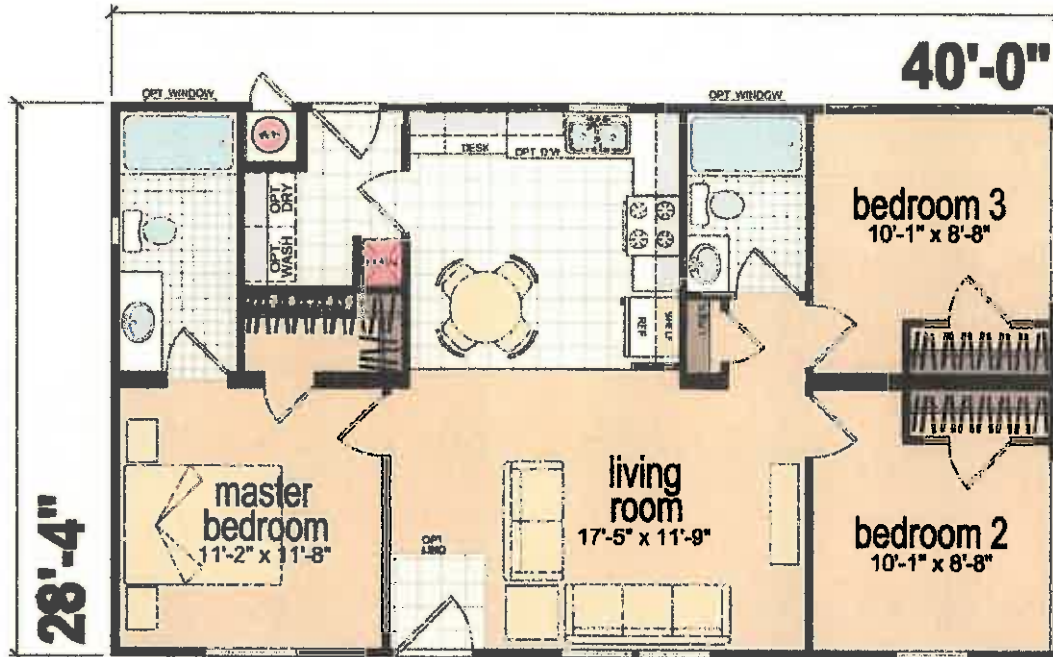
SCALE: 1/8" = 1'-0"

Creekside Manor 3403B

933 Square Feet, 3 Bedrooms, 2 Bathrooms, Multi-Section

This manufactured home is built to the federal (HUD) building code for manufactured housing.

Floor Plan



Elevation

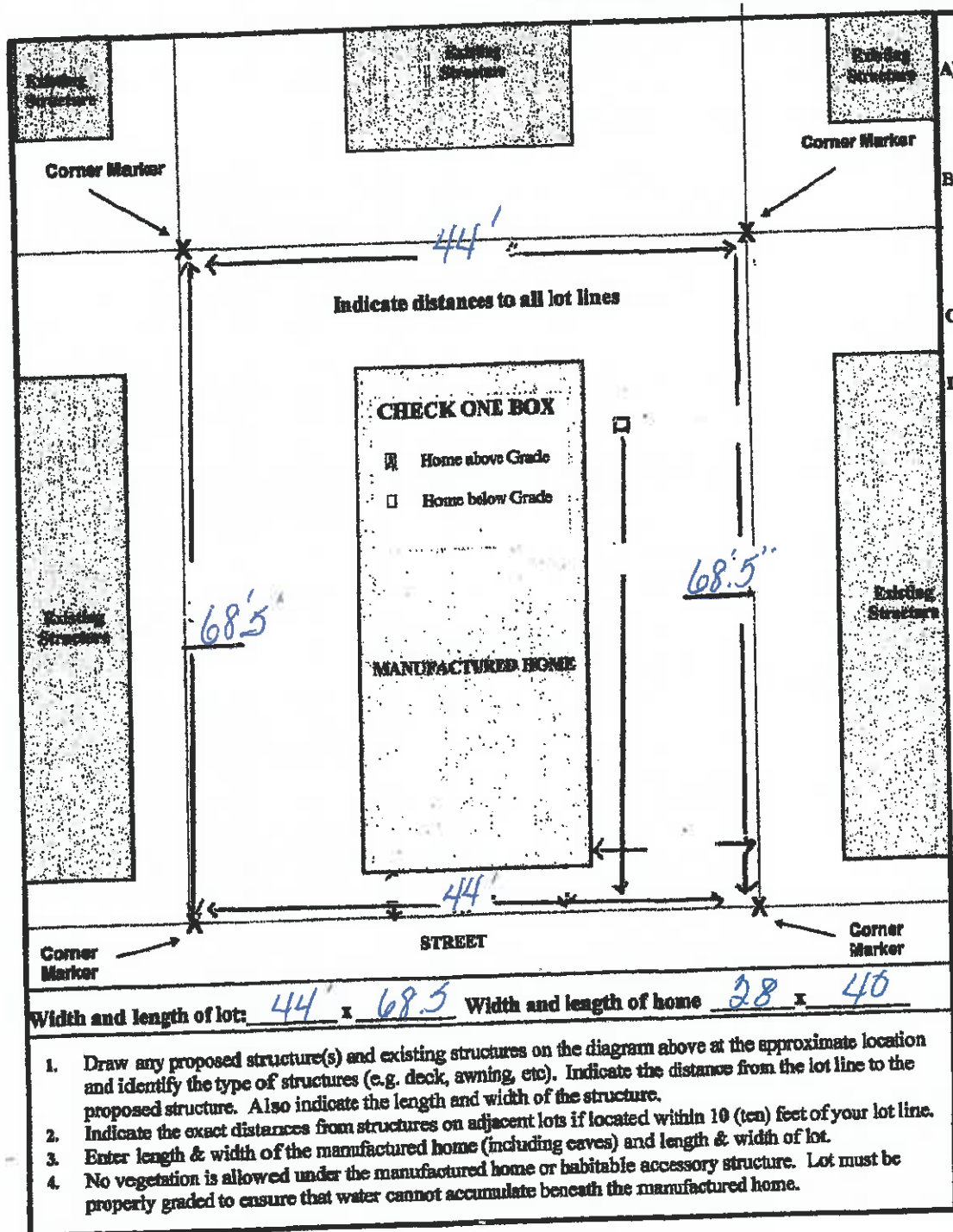


Dimensions, floor plans, elevations, features and other information are subject to change without notice. Square footage and other dimensions are approximations. Elevations, photography and videos are often shown with optional features and/or third party additions, such as garages and porches that may not be available in all regions or situations.

...



... ..



LOT PLOT PLAN AND PARK INFORMATION

A) Park Name Old Plantation
 Homeowner Name _____
 Homeowner Address 3825 Crestline Sp# 349
 City Sacramento Valley Zip 92509

B) Design Information:
 Home Amperage: 50 Pedestal Amperage: _____
 Home Voltage: _____ Pedestal Voltage: _____
 Home Roof Load: _____ PSF
 Roof Load for locality: _____ PSF

C) Is the park located in a snow area requiring 30 lb or greater roof loading? YES NO

D) The lot line corners at the front and rear are clearly and permanently marked pursuant to Title 25 of the California Code of Regulations, Sections 1104 or 2104 in the following manner:
Pins: markers

NOTE: Each lot line corner shall be clearly and permanently marked prior to installation and inspection.

**STATEMENT OF RESPONSIBILITY
 (ORIGINAL SIGNATURE REQUIRED)**

As the park owner or operator, or as his or her authorized representative, I hereby certify that the information provided on this plot plan relative to the location of the manufactured home, all related accessory structure locations and separations and the park and homeowner information is true, accurate and complete. Lot corners have been identified as in item D above.

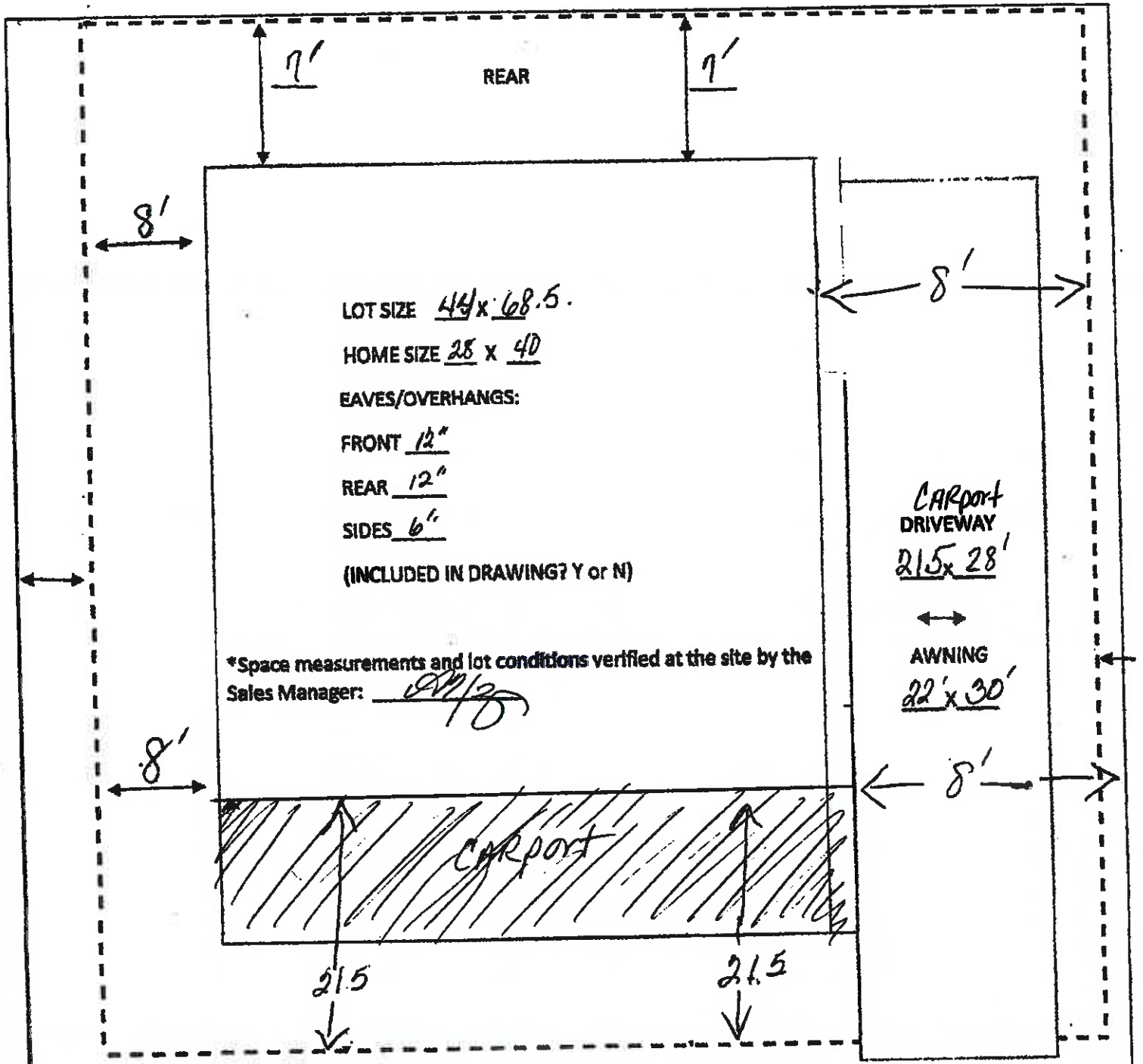
Signature of Park Owner, Operator, or Manager

State of California
 Department of Housing and Community Development
 Division of Codes and Standards



Northern Area Office
 9342 Tech Center Drive, Suite 350
 Sacramento, CA 95826

Southern Area Office
 3737 Main St. Ste 400
 Riverside, CA 92501



THIS SITE PLAN IS REQUIRED FOR ALL HOME INSTALLATIONS. Must include the dimensions of all requested items, INCLUDING DOORS; draw each item on site plan above; and note the distances between each item, the home, to the front and rear of home, lot lines & structures on adjoining spaces.

1. Back door & steps: Landing Size 4x8 Material TERRAZO / Up & In X Up & Over _____

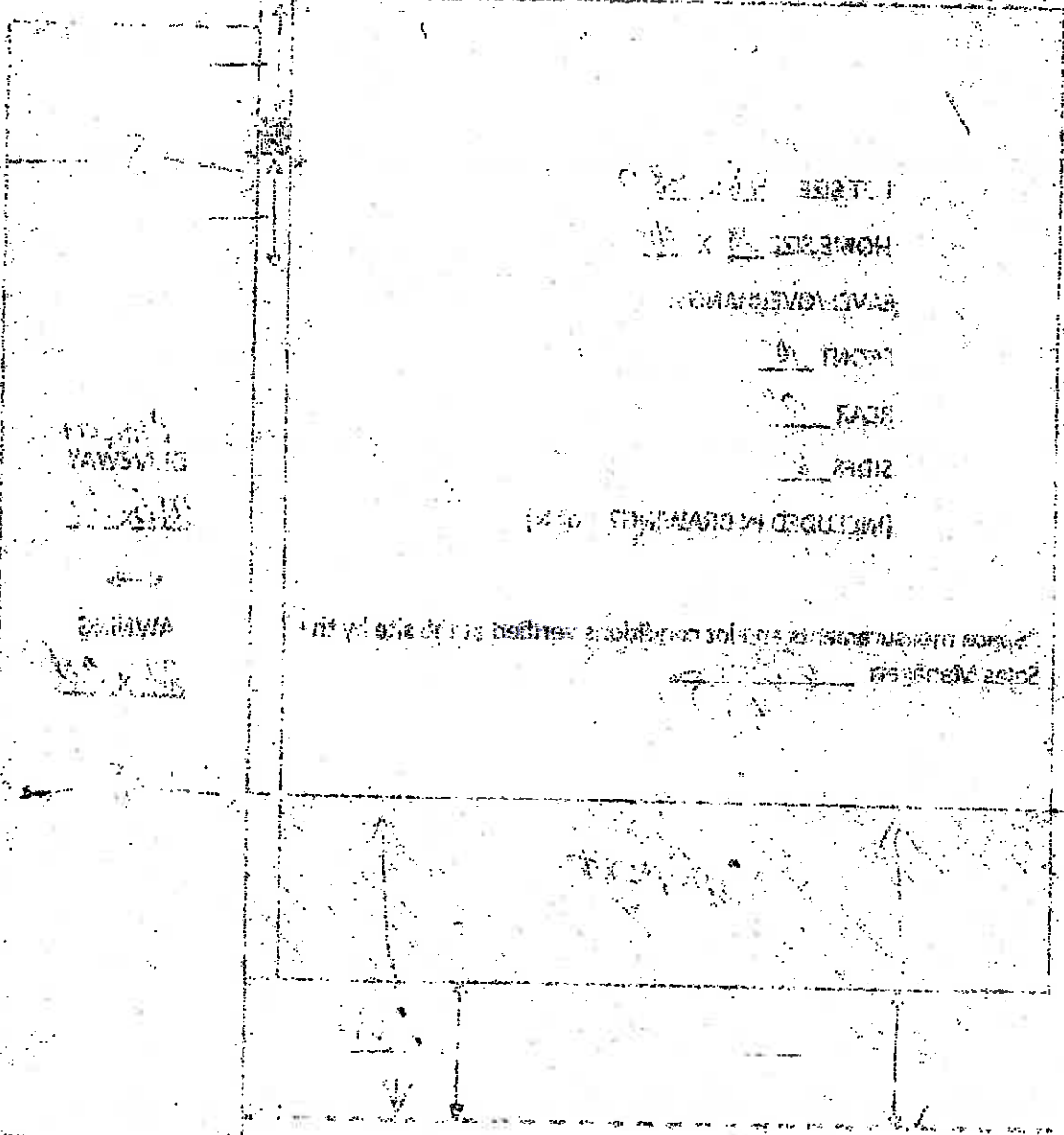
2. Front door steps: Landing Size 4x8 Material TERRAZO / Up & In X Up & Over _____

3. Back Door Awning: 4x10 4. Front door awning: N/A 5. Porch: N/A Material N/A

6. Shed: N/A Material: N/A 7. Location of Utilities & distance to front & rear of home: T&P?

*Also include location and size of the concrete pads, walkways, masonry planter, etc., to be installed.

PARK Old Plan SPACE 349 REG MGR APPROVAL & DATE _____



STAIRWAY

STAIRWAY

STAIRWAY

STAIRWAY

STAIRWAY

STAIRWAY

STAIRWAY

STAIRWAY

STAIRWAY

STAIRWAY

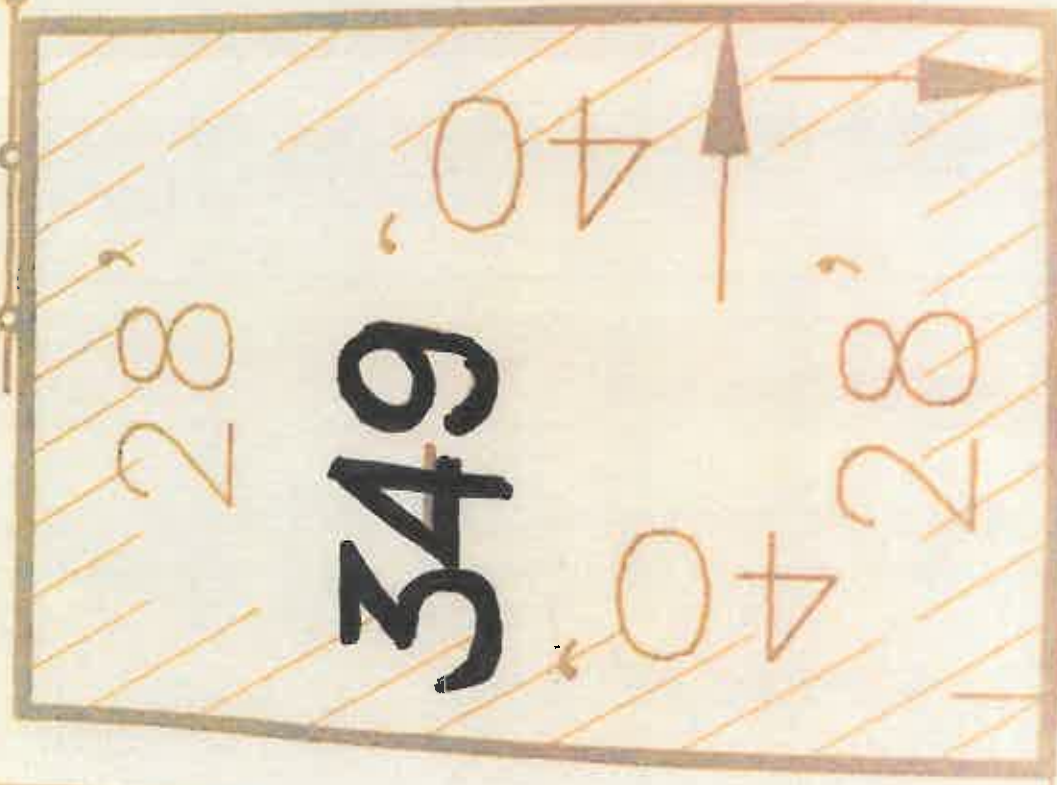
THIS PLAN IS SUBJECT TO THE APPROVAL OF THE LOCAL AUTHORITY AND THE NATIONAL FIRE BRIGADE AUTHORITY. THE LOCAL AUTHORITY IS NOT RESPONSIBLE FOR THE DESIGN OR CONSTRUCTION OF THE BUILDING. THE NATIONAL FIRE BRIGADE AUTHORITY IS NOT RESPONSIBLE FOR THE DESIGN OR CONSTRUCTION OF THE BUILDING. THE DESIGNER IS RESPONSIBLE FOR THE DESIGN AND CONSTRUCTION OF THE BUILDING. THE CONTRACTOR IS RESPONSIBLE FOR THE CONSTRUCTION OF THE BUILDING. THE LOCAL AUTHORITY IS NOT RESPONSIBLE FOR THE DESIGN OR CONSTRUCTION OF THE BUILDING. THE NATIONAL FIRE BRIGADE AUTHORITY IS NOT RESPONSIBLE FOR THE DESIGN OR CONSTRUCTION OF THE BUILDING. THE DESIGNER IS RESPONSIBLE FOR THE DESIGN AND CONSTRUCTION OF THE BUILDING. THE CONTRACTOR IS RESPONSIBLE FOR THE CONSTRUCTION OF THE BUILDING.

STAIRWAY

Floor plan
Cm 3403B

#349

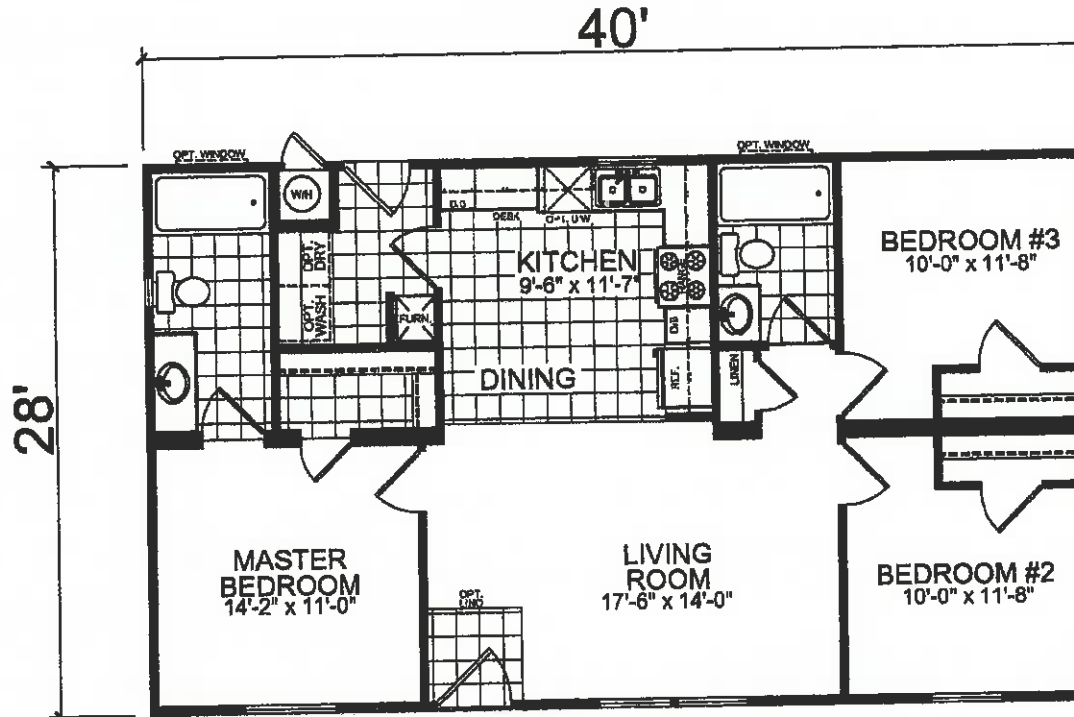
ASPH



27a

#37d

Space #349



ALL DIMENSIONS ARE APPROXIMATE, AND MAY BE SUBJECT TO CHANGES BY CHAMPION HOME BUILDERS CO. LINDSAY, CA

REDMAN HOMES.
CHAMPION
 840 W. Palm Ave. Lindsay, CA 93247

DAPIA SEAL

MODIFICATIONS

MODEL: 09-CM-3403B

SHEET:

TITLE: LITERATURE PLAN

L-101

PROPRIETARY AND CONFIDENTIAL
 THESE DRAWINGS AND SPECIFICATIONS ARE ORIGINAL
 PROPRIETARY AND CONFIDENTIAL MATERIALS OF CHAMPION.
 COPYRIGHT © 1978-2004 BY CHAMPION

DRAWN BY: M. SANCHEZ

DATE: 2-12-2015

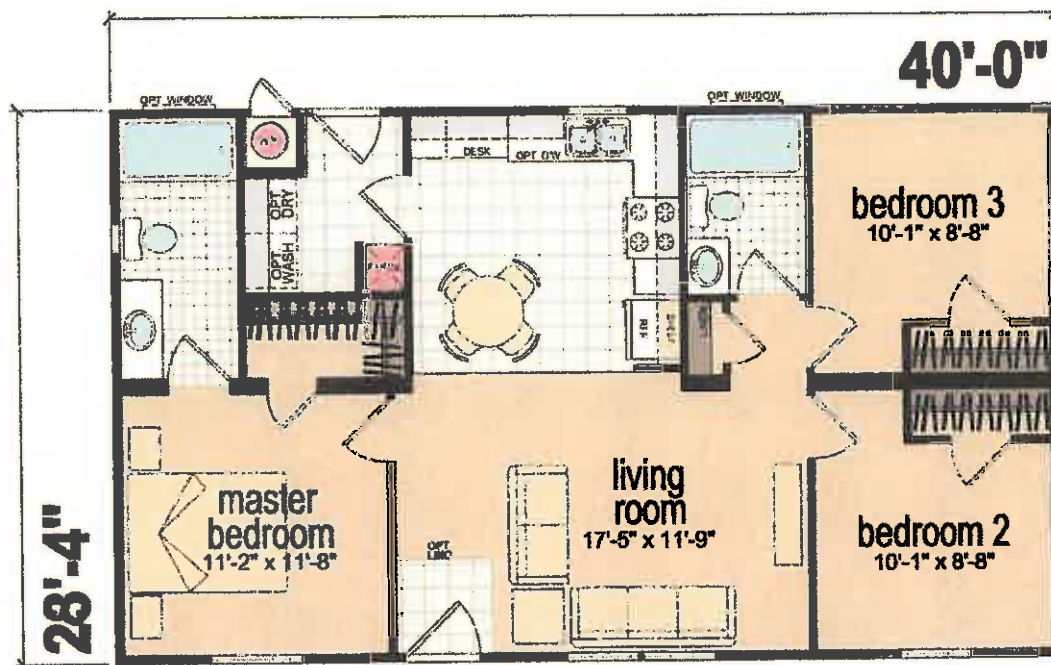
SCALE: 1/8" = 1'-0"

Creekside Manor 3403B

933 Square Feet, 3 Bedrooms, 2 Bathrooms, Multi-Section

This manufactured home is built to the federal (HUD) building code for manufactured housing.

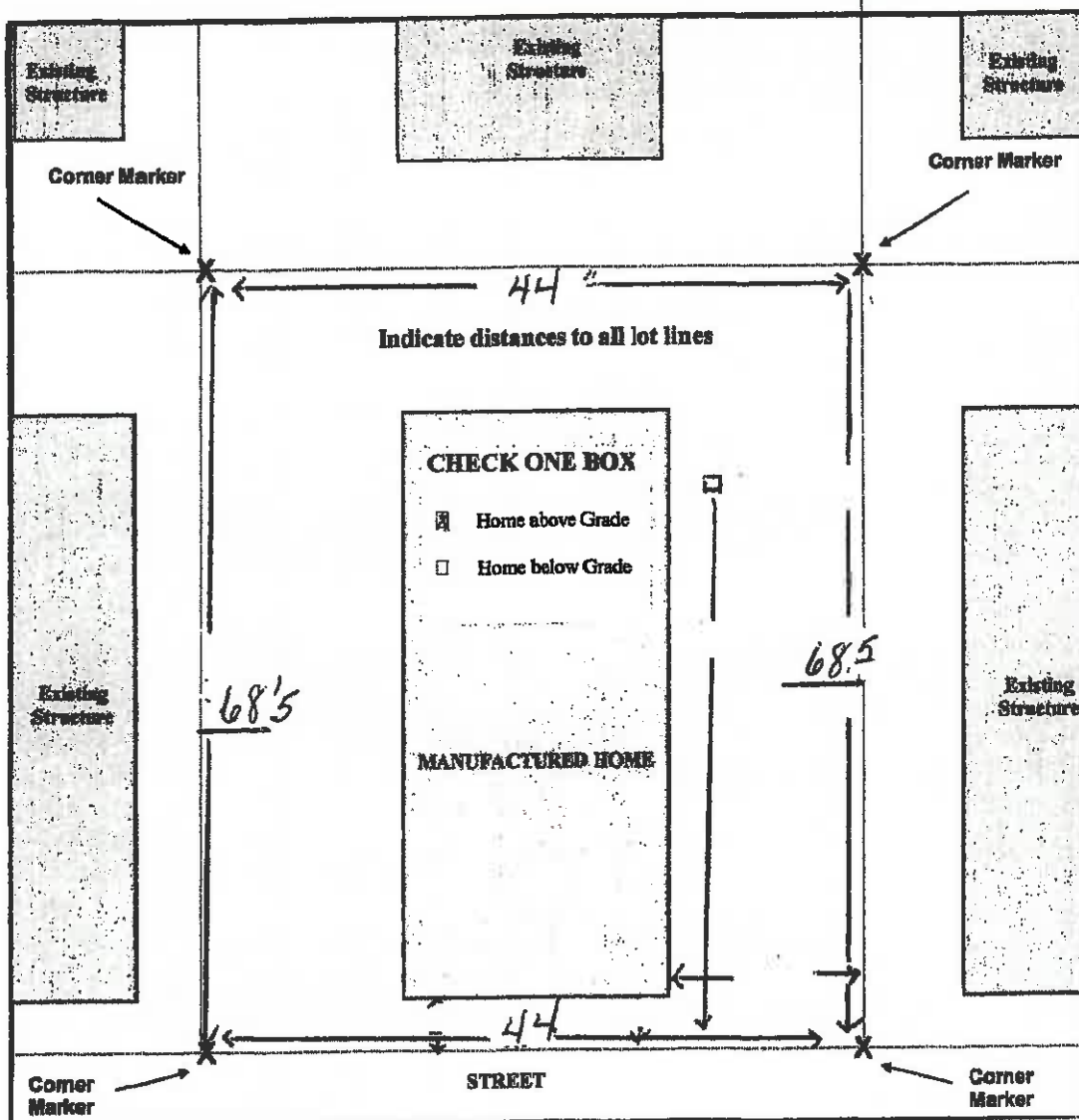
Floor Plan



Elevation



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Width and length of lot: 44 x 68.5 Width and length of home 28 x 40.

1. Draw any proposed structure(s) and existing structures on the diagram above at the approximate location and identify the type of structures (e.g. deck, awning, etc). Indicate the distance from the lot line to the proposed structure. Also indicate the length and width of the structure.
2. Indicate the exact distances from structures on adjacent lots if located within 10 (ten) feet of your lot line.
3. Enter length & width of the manufactured home (including caves) and length & width of lot.
4. No vegetation is allowed under the manufactured home or habitable accessory structure. Lot must be properly graded to ensure that water cannot accumulate beneath the manufactured home.

LOT PLOT PLAN AND PARK INFORMATION

A) Park Name Old Plantation MHP.
 Homeowner Name _____
 Homeowner Address 3825 Crestmore Sp# 344
 City Jurupa Valley Zip 92509

B) Design Information:
 Home Amperage: 50 Pedestal Amperage: _____
 Home Voltage: _____ Pedestal Voltage: _____
 Home Roof Load: _____ PSF
 Roof Load for locality: _____ PSF

C) Is the park located in a snow area requiring 30 lb or greater roof loading?
 YES NO

D) The lot line corners at the front and rear are clearly and permanently marked pursuant to Title 25 of the California Code of Regulations, Sections 1104 or 2104 in the following manner:
Pins & Rebar

NOTE: Each lot line corner shall be clearly and permanently marked prior to installation and inspection.

**STATEMENT OF RESPONSIBILITY
 (ORIGINAL SIGNATURE REQUIRED)**

As the park owner or operator, or as his or her authorized representative, I hereby certify that the information provided on this plot plan relative to the location of the manufactured home, all related accessory structure locations and separations and the park and homeowner information is true, accurate and complete. Lot corners have been identified as in item D above.

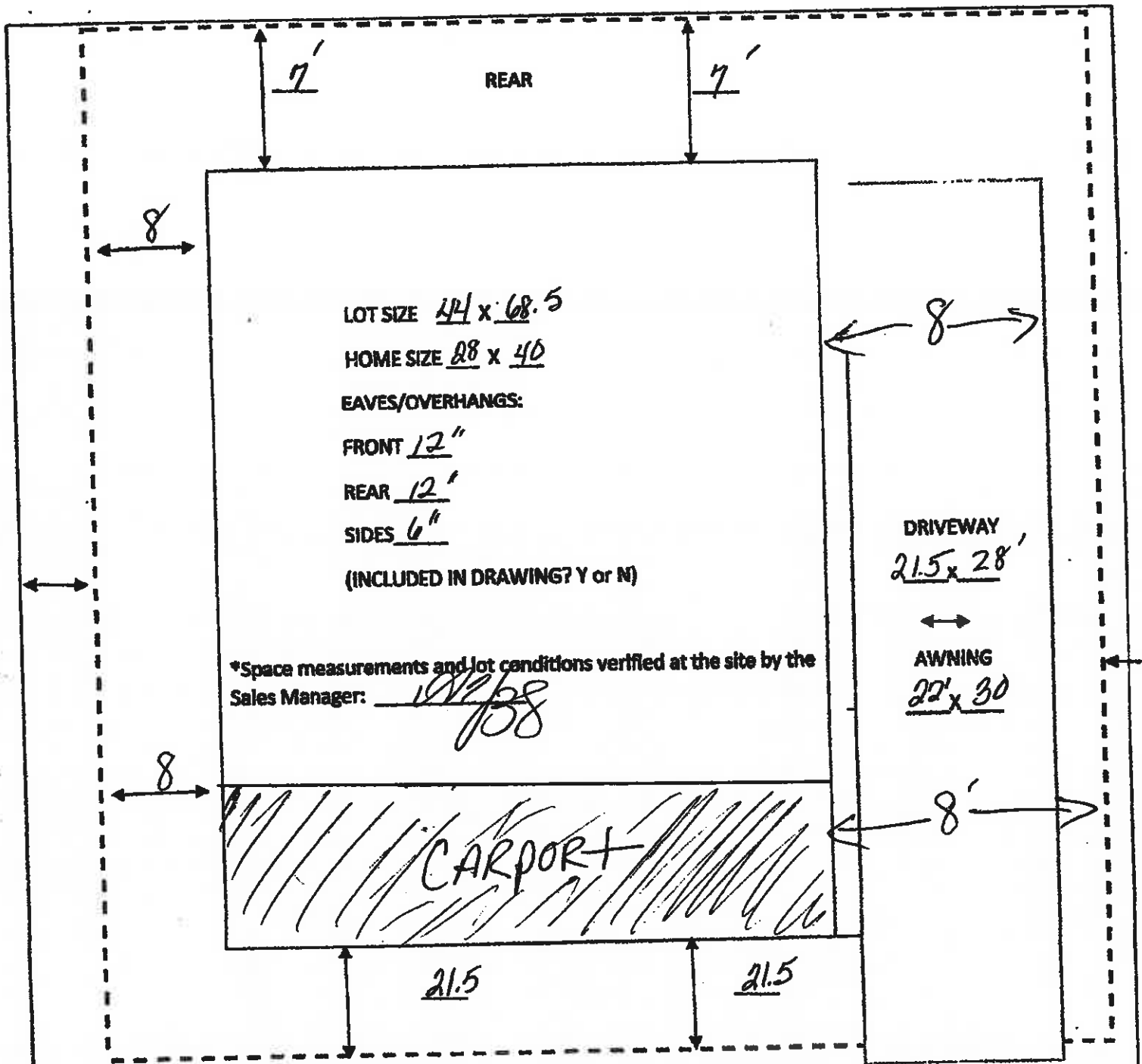
Signature of Park Owner, Operator, or Manager

State of California
 Department of Housing and Community Development
 Division of Codes and Standards



Northern Area Office
 9342 Tech Center Drive, Suite 550
 Sacramento, CA 95826

Southern Area Office
 3737 Main St. Ste 400
 Riverside, CA 92501



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1. Back door & steps: Landing Size 4 x 8 Material Trex / Up & In — Up & Over X

2. Front door steps: Landing Size 4 x 8 Material Trex / Up & In X Up & Over —

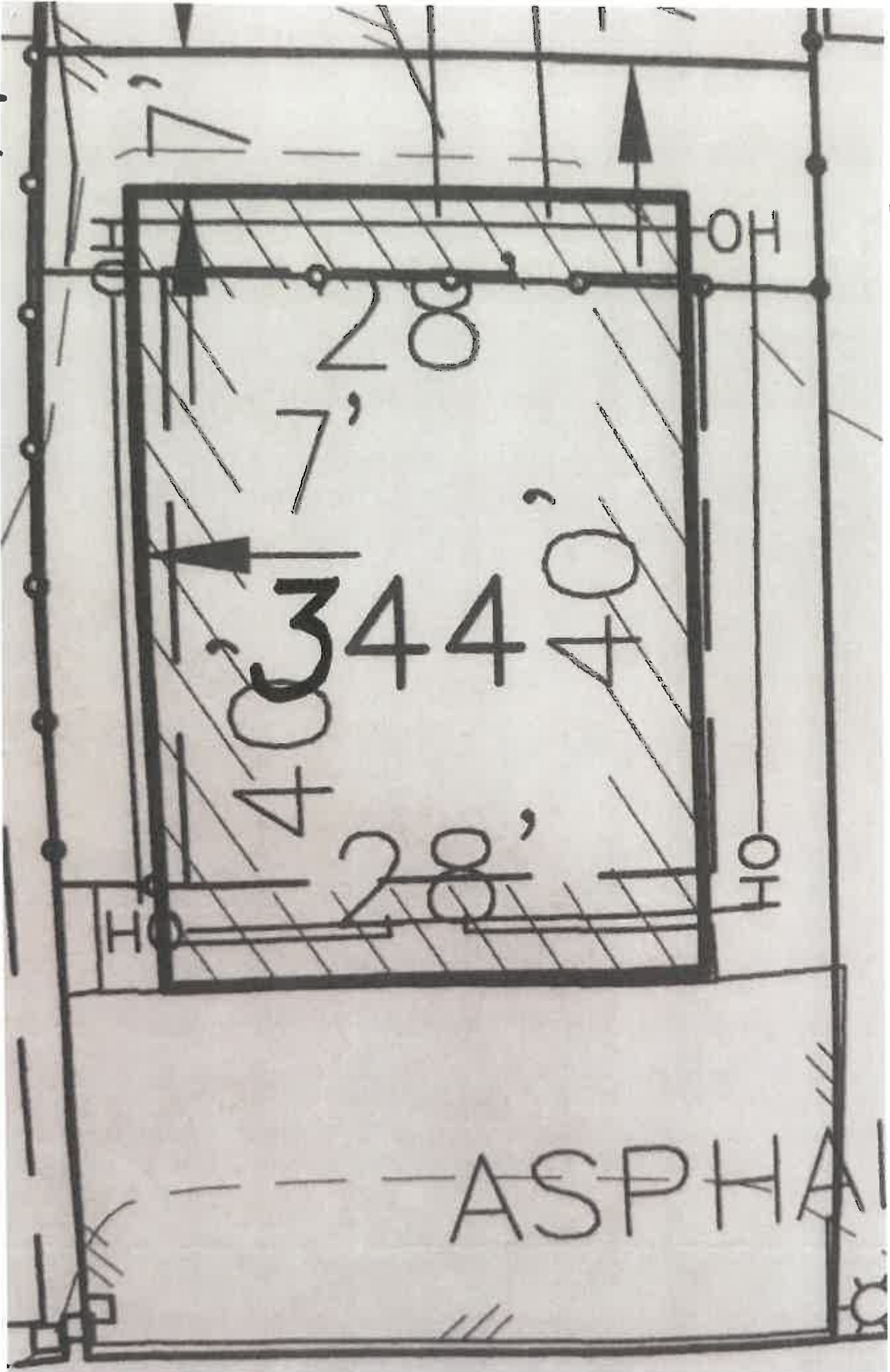
3. Back Door Awning: N x A 4. Front door awning: 4 x 8 5. Porch: N x A Material —

6. Shed: N x A Material: N/A 7. Location of Utilities & distance to front & rear of home —

*Also include location and size of the concrete pads, walkways, masonry planter, etc., to be installed.

PARK Old Plan SPACE 344 REG MGR APPROVAL & DATE —

#344

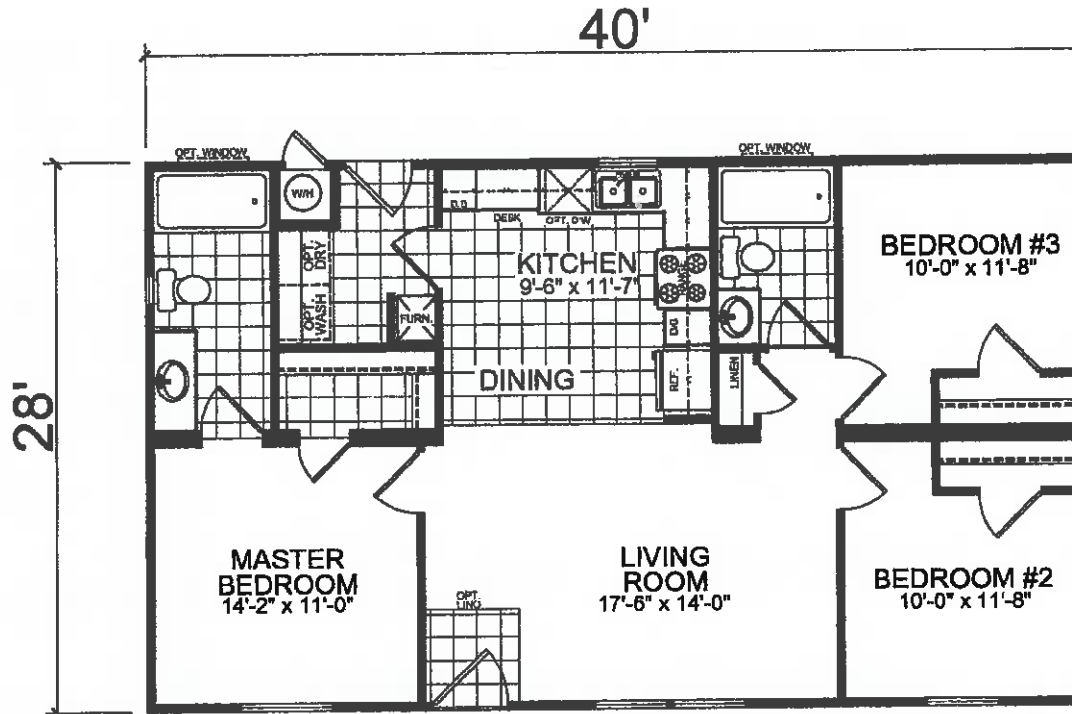


Floorplan
CM - 3403B

333

3

Space
344



ALL DIMENSIONS ARE APPROXIMATE, AND MAY BE SUBJECT TO CHANGES BY CHAMPION HOME BUILDERS CO. LINDSAY, CA

REDMAN  HOMES.
CHAMPION
840 W. Palm Ave. Lindsay, CA 93247

DAPIA SEAL

MODIFICATIONS

MODEL: 09-CM-3403B

SHEET:

TITLE: LITERATURE PLAN

L-101

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PROPRIETARY AND CONFIDENTIAL MATERIALS OF CHAMPION.
COPYRIGHT © 1976-2004 BY CHAMPION

DRAWN BY: M. SANCHEZ DATE: 2-12-2015

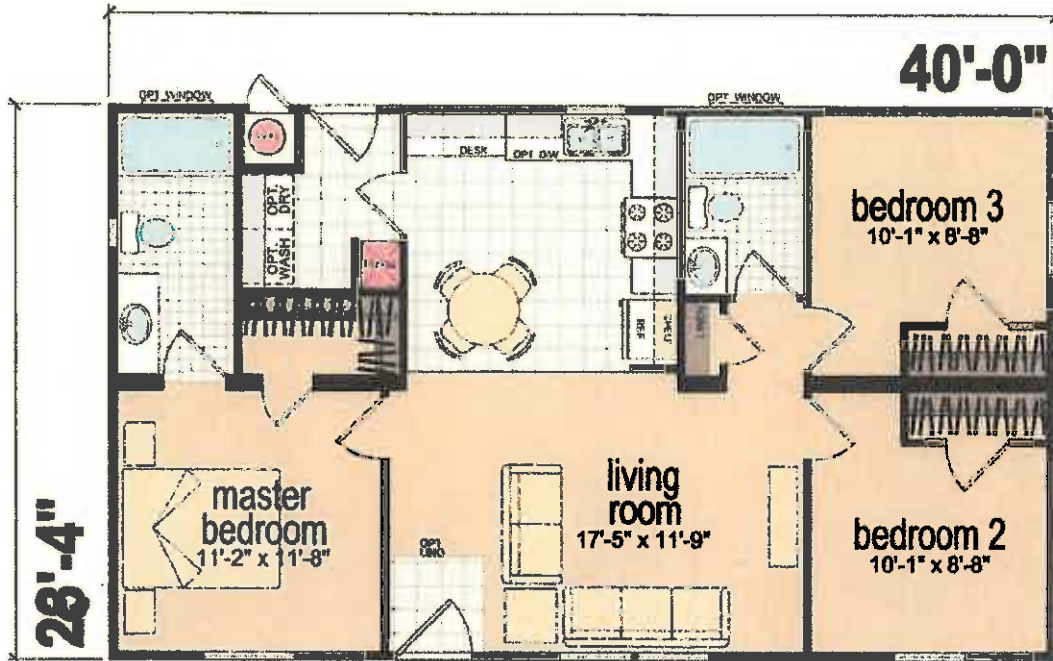
SCALE: 1/8" = 1'-0"

Creekside Manor 3403B

933 Square Feet, 3 Bedrooms, 2 Bathrooms, Multi-Section

This manufactured home is built to the federal (HUD) building code for manufactured housing.

Floor Plan



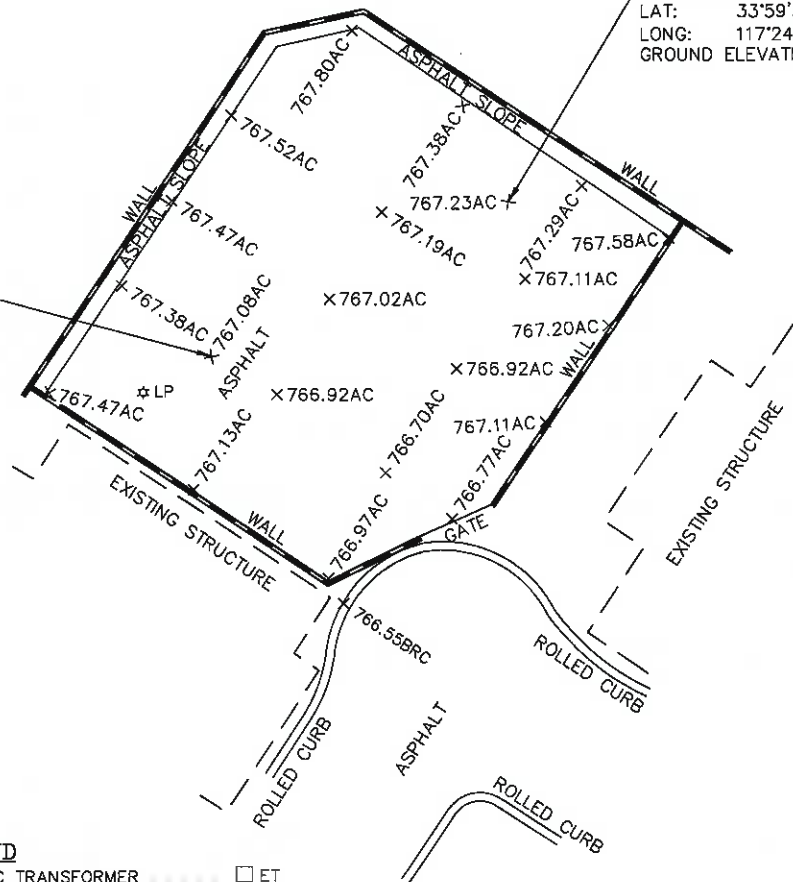
Elevation



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LOT 173
 LAT: 33°59'32.7784"
 LONG: 117°24'01.4790"
 GROUND ELEVATION=767.08

LOT 174
 LAT: 33°59'33.0356"
 LONG: 117°24'00.9023"
 GROUND ELEVATION=767.23



BASIS OF COORDINATES

THE BASIS OF COORDINATES FOR THIS SURVEY IS THE CALIFORNIA SPATIAL REFERENCE NETWORK, NAD83, ZONE 6, BASED ON STATION MLFP.

BASIS OF ELEVATIONS

THE BASIS OF ELEVATION FOR THIS SURVEY IS NGS SURVEY CONTROL STATION UF 624, NAVD88 ELEVATION TAKEN AS 828.08 FEET.

SURVEYOR'S STATEMENT:

THIS PLAT OF SURVEY WAS PREPARED BY ME OR UNDER MY DIRECTION AND IS BASED UPON A FIELD SURVEY.

CASEY R. LYNCH LS 8380

DATE: 11/25/2020



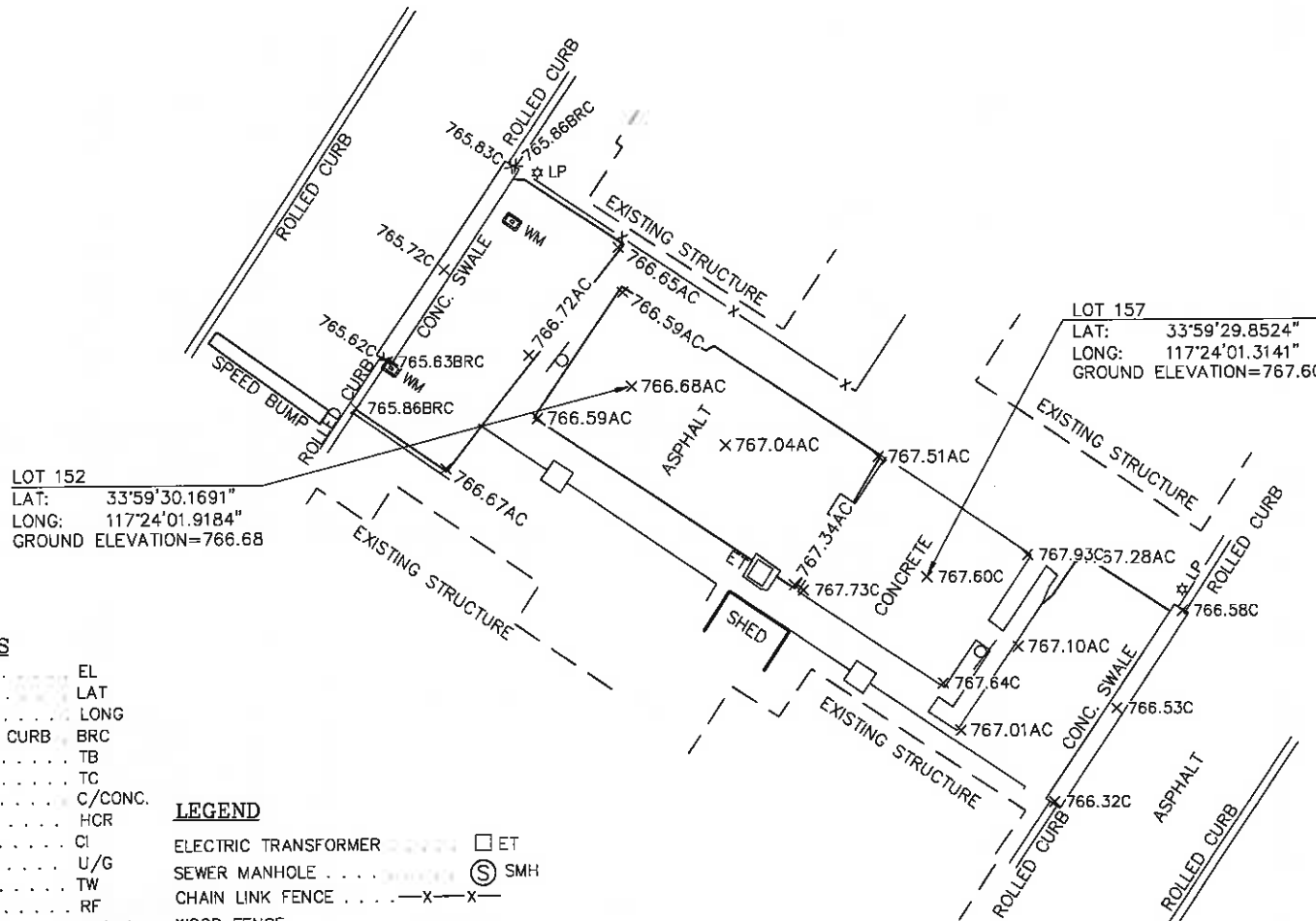
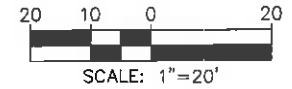
ABBREVIATIONS

| | |
|--------------------------------|---------|
| ELEVATION | EL |
| LATITUDE | LAT |
| LONGITUDE | LONG |
| BACK OF ROLLING CURB | BRC |
| TOP OF BERM | TB |
| TOP OF CURB | TC |
| CONCRETE | C/CONC. |
| HANDICAP RAMP | HCR |
| CURB INLET | CI |
| UNDERGROUND | U/G |
| TOP OF WALL | TW |
| ROOF | RF |
| RIGHT OF WAY | R.O.W. |
| LIGHT POLE | LP |
| UNKNOWN CLEANOUT | UCO |
| UNKNOWN RISER | UR |
| BLOW OFF VALVE | BOV |
| DRIVEWAY | DWY |
| WALKWAY | WKWY |
| PARKING METER | PM |
| MONUMENT | MON. |

LEGEND

| | |
|--------------------------------|---------|
| ELECTRIC TRANSFORMER | □ ET |
| SEWER MANHOLE | ⊙ SMH |
| CHAIN LINK FENCE | -x-x- |
| WOOD FENCE | -o-o- |
| WALL | — |
| CURB & GUTTER | — |
| BUILDING FOOTPRINT | - - - - |
| SIGN | ⊕ |
| STREET LIGHT | ☆ SL |
| DECIDUOUS TREE | ⊗ |

3825 CRESTMORE DRIVE
 TOPIC:
LOTS 173/174



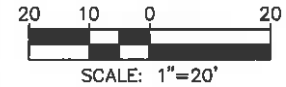
ABBREVIATIONS

| | |
|--------------------------------|---------|
| ELEVATION | EL |
| LATITUDE | LAT |
| LONGITUDE | LONG |
| BACK OF ROLLING CURB | BRC |
| TOP OF BERM | TB |
| TOP OF CURB | TC |
| CONCRETE | C/CONC. |
| HANDICAP RAMP | HCR |
| CURB INLET | CI |
| UNDERGROUND | U/G |
| TOP OF WALL | TW |
| ROOF | RF |
| RIGHT OF WAY | R.O.W. |
| LIGHT POLE | LP |
| UNKNOWN CLEANOUT | UCO |
| UNKNOWN RISER | UR |
| BLOW OFF VALVE | BOV |
| DRIVEWAY | DWY |
| WALKWAY | WKWY |
| PARKING METER | PM |
| MONUMENT | MON. |

LEGEND

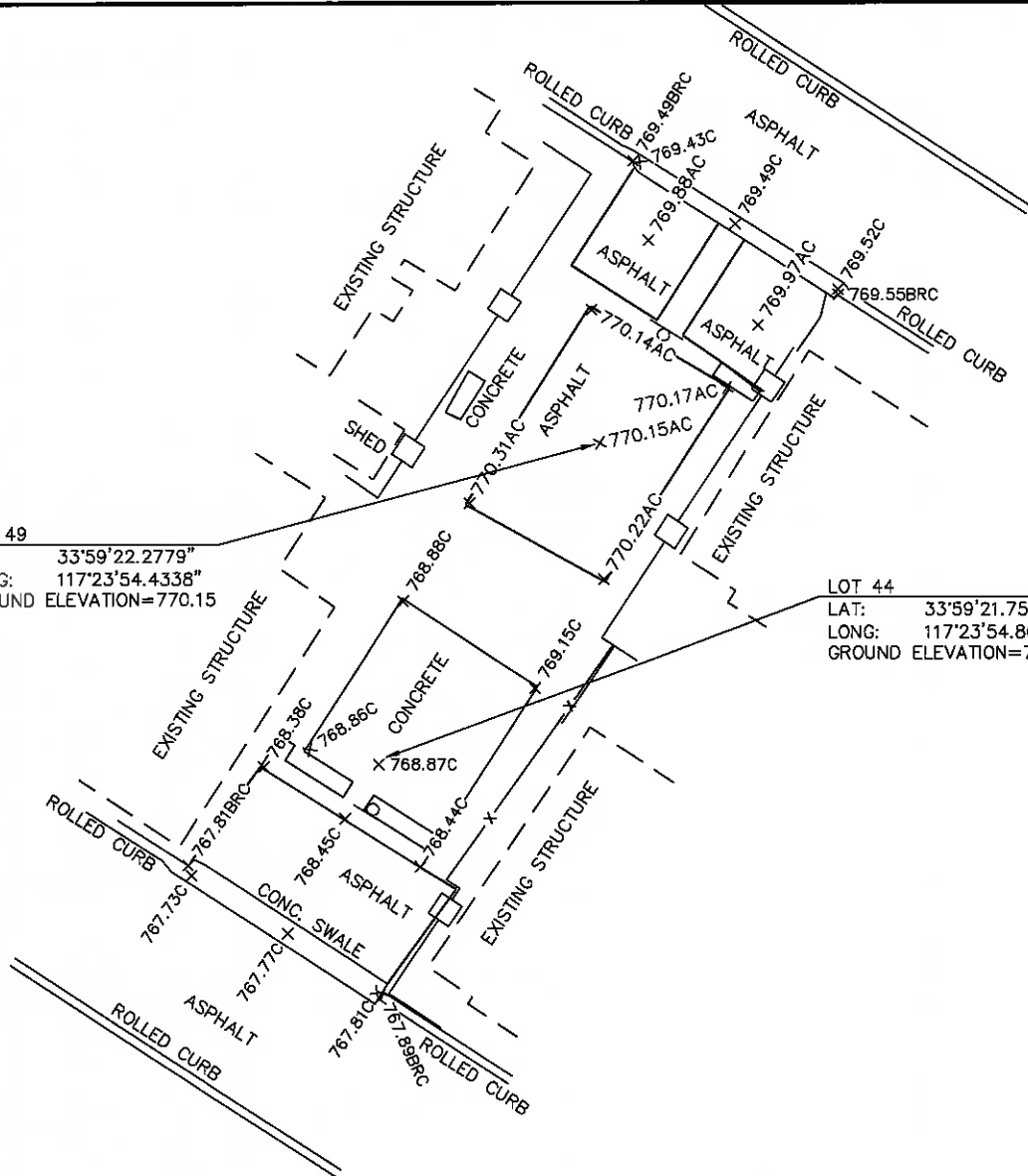
| | |
|--------------------------------|-----------|
| ELECTRIC TRANSFORMER | ET |
| SEWER MANHOLE | SMH |
| CHAIN LINK FENCE | X-X |
| WOOD FENCE | □-□ |
| WALL | ▬ |
| CURB & GUTTER | ▬▬▬ |
| BUILDING FOOTPRINT | - - - - - |
| SIGN | ⊕ |
| STREET LIGHT | ☆ SL |
| DECIDUOUS TREE | 🌳 |

3825 CRESTMORE DRIVE
 TOPIC:
LOTS 152/157

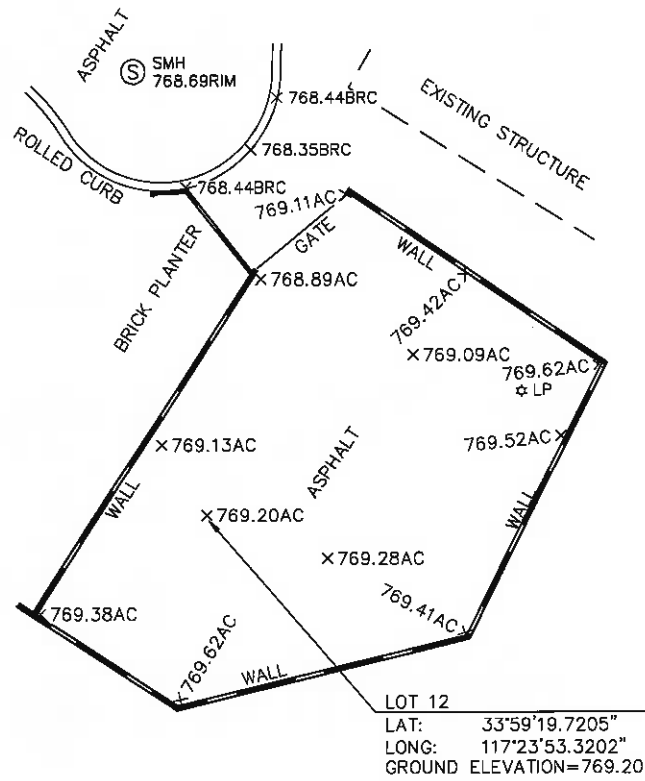


LOT 49
LAT: 33°59'22.2779"
LONG: 117°23'54.4338"
GROUND ELEVATION=770.15

LOT 44
LAT: 33°59'21.7553"
LONG: 117°23'54.8615"
GROUND ELEVATION=766.87



| |
|----------------------|
| 3825 CRESTMORE DRIVE |
| TOPIC: |
| LOTS 49/44 |



| | |
|----------------------|--------|
| 3825 CRESTMORE DRIVE | |
| TOPIC: | LOT 12 |

COUNTY OF RIVERSIDE
AIRPORT LAND USE COMMISSION
STAFF REPORT

AGENDA ITEM: 5.1

HEARING DATE: September 11, 2008

CASE NUMBER: ZAP1010FL08 – Old Plantation Investors, LP

APPROVING JURISDICTION: County of Riverside

JURISDICTION CASE NO: CUP00997R1 (Conditional Use Permit No. 997, Revised No. 1)

MAJOR ISSUES: The proposed Revised Conditional Use Permit proposes to add dwelling units to an existing mobile home park located in Airport Zones B1 and C. The existing density already exceeds density criteria for these airport zones. The mobile home park is an existing use not in conformance with the Compatibility Plan. Expansion of nonconforming uses may be permitted only if “the expansion does not result in more dwelling units than currently exist on the parcel.” The Compatibility Plan’s infill provisions are not applicable in Airport Zone B1, and would only allow densities up to one dwelling unit per 2½ acres in Airport Zone C.

RECOMMENDATION: Staff recommends a finding of INCONSISTENCY for the Revised Conditional Use Permit.

PROJECT DESCRIPTION:

CUP00997R1 is a proposal to add nine (9) new mobile home spaces to an existing 223-space mobile home park. No additional acreage would be added to the park; the spaces would be established at locations previously in use as laundry rooms, wash rooms, and storage rooms.

PROJECT LOCATION:

The project site is located southerly (southwesterly) of Mission Boulevard, both westerly and easterly of Crestmore Road, and northerly of Capary Road, approximately 1,122 feet northeasterly of the northeasterly terminus of Runway 6-24 at Flabob Airport, in the unincorporated community of Rubidoux. The site consists of two parcels, with the larger parcel located westerly of Crestmore Road.

LAND USE PLAN: 2004 Flabob Airport Land Use Compatibility Plan

Adjacent Airport:

- a. Airport Influence Area: Flabob Airport
- b. Land Use Policy: Airport Zones B1 and C
- c. Noise Levels: From below 55 CNEL to 60 CNEL.

BACKGROUND:

Land Use-Residential Density: The site is located in Airport Zones B1 and C of the Flabob Airport Influence Area. The larger parcel is 22.08 acres in area and is located predominantly in Airport Zone B1. The smaller parcel is 5.64 acres in area and is located in Airport Zone C. Pursuant to Countywide compatibility criteria, Zone C allows a density of 0.2 dwelling units per acre or one dwelling unit per 5 acres, and Zone B1 allows a density of 0.05 dwelling units per acre (one dwelling unit per 20 acres). Thus, the Compatibility Plan criteria would allow for a total of two dwelling units in this area (one per parcel). However, there are already 223 mobile home spaces in existence here, for a net density of 8.04 dwelling units per acre (excluding existing rights-of-way). The applicant is proposing to increase the number of mobile home spaces to 232, which would increase density to 8.37 dwelling units per acre. While this level of density is permissible – indeed, encouraged, in Airport Zone D, it is not permissible in Airport Zones B1 and C.

Section 3.3.1 allows infill development of similar land uses in Airport Zone C where development not in conformance with Compatibility Plan criteria already exists. However, pursuant to Section 3.3.1 (b), the average development density shall not exceed “double the density permitted in accordance with the criteria for that location as indicated in the Compatibility Criteria matrix, Table 2A” (Countywide Policies, pages 2-18 and 2-19). Therefore, the maximum allowable intensity even if all five infill criteria were met would be $(0.2 \times 2) = 0.4$ dwelling units per acre, or one dwelling unit per 2 ½ acres. Infill criteria do not apply in Airport Zones A and B1.

The site is designated High Density Residential (8 to 14 dwelling units per acre) on the Jurupa Area Plan Land Use Map of the Riverside County General Plan. This General Plan designation would be inconsistent with the airport land use compatibility criteria of the Countywide Policies of the Compatibility Plan if the land were vacant. However, as discussed above, the density of existing development on the site already exceeds 8.0 dwelling units per acre, so the land use designation is not necessarily inconsistent.

While the existing land use conforms to General Plan density criteria, it is not in conformance with the adopted Compatibility Plan. Pursuant to Section 3.3.2 of the Countywide Policies in the 2004 Riverside County Airport Land Use Compatibility Plan, “Existing uses...not in conformance with this Compatibility Plan may only be expanded...provided that the expansion does not result in more dwelling units than currently exist on the parcel....”

Noise: The site is largely within the 55 CNEL contour. A minimum noise level reduction of 20dB in residences (including mobile homes) is required in Airport Zone C, and a minimum noise level reduction of 25dB in residences (including mobile homes) is required in Airport Zone B1. Recommended conditions in the event of an overrule require incorporation of noise attenuation measures in the proposed mobile homes.

PART 77: The maximum elevation at this site is 788 feet above mean sea level (AMSL), and it is expected that the mobile homes will not exceed a height of 18 feet. Therefore, the expected highest point of any structure would not exceed 806 feet AMSL. The runway elevation at its northeasterly end is 766.8 feet AMSL. At a distance of 1,122 feet from the runway and with a 50:1 slope based on the runway length, any structure over 789.2 feet top elevation would require FAA review. Therefore, the new structures will require FAA notice and review.

In the event that the County of Riverside chooses to overrule a determination of inconsistency, the County should require the following as conditions of its approval. Implementation of these conditions does NOT render the project consistent with the Flabob Airport Land Use Compatibility Plan and may not be sufficient to mitigate potential safety hazards to below a level of significance pursuant to the California Environmental Quality Act.

CONDITIONS:

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky.
2. The following uses shall be prohibited:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area.
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.

- (e) Children's schools, day care centers, libraries, hospitals, nursing homes, and highly noise-sensitive outdoor uses.
- 3. The attached notice shall be provided to all potential purchasers and tenants.
- 4. Prior to issuance of building permits, the land owner shall provide documentation to the Riverside County Airport Land Use Commission that an avigation easement has been conveyed to Flabob Airport and has been recorded.
- 5. Noise attenuation measures shall be incorporated into mobile home design so as to provide an exterior-to-interior noise level reduction of 25dB for the units westerly of Crestmore Road and 20dB for the units easterly of Crestmore Road, in order to ensure that interior noise levels from aircraft operations do not exceed 45 dB (A) CNEL.
- 6. Prior to issuance of building permits for any structure whose elevation in feet above mean sea level at top of roof or top point exceeds 789.2, the applicant shall file a Notice of Proposed Construction or Alteration (Form 7460-1) with the Federal Aviation Administration for such structure and shall have received a determination of "No Hazard to Air Navigation."

NOTICE OF PUBLIC HEARING
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION
www.rcaluc.org

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. For more information please contact **ALUC Planner Paul Rull at (951) 955-6893**. The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan.

The City of Jurupa Valley Planning Department should be contacted on non-ALUC issues. For more information please contact City of Jurupa Valley Planner Ms. Andrea Hoff at (951) 332-6464 x210.

The proposed project application may be viewed by a prescheduled appointment and on the ALUC website www.rcaluc.org. Written comments may be submitted at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Friday from 8:00 a.m. to 3:30 p.m., or by e-mail to prull@rivco.org. Individuals with disabilities requiring reasonable modifications or accommodations, please telephone Barbara Santos at (951) 955-5132.

PLACE OF HEARING: Riverside County Administration Center
4080 Lemon Street, 1st Floor Board Chambers
Riverside California

DATE OF HEARING: January 14, 2021

TIME OF HEARING: 9:30 A.M.

Pursuant to Executive Order N-25-20, this meeting will be conducted by teleconference and at the Place of Hearing, as listed above. Public access to the meeting location will be allowed, but limited to comply with the Executive Order. Information on how to participate in the hearing will be available on the ALUC website at www.rcaluc.org

CASE DESCRIPTION:

ZAP1035FL20 – Old Plantation Investors, L.P. (Representative: Cirus Development Co. Inc.) – City of Jurupa Valley Case No. MA19216 [CZ20013, CUP19005] (Change of Zone, Conditional Use Permit). A proposal to add nine (9) new mobile home (spaces) to the existing 223-space (total 232 spaces) “Old Plantation” mobile home park on a combined total acreage of 27.72 acres over two parcels, located at 3825 Crestmore Road, southerly of Mission Boulevard, both westerly and easterly of Crestmore Road, and northerly of Capary Road. The applicant also proposes changing the zoning of the site from Planned Residential (R-4) and General Commercial (C-1/C-P) to Mobile Home Subdivisions and Mobile Home Parks Zone (R-T). (A similar proposal to add nine (9) new mobile home spaces to the existing 223-space mobile home park at this site was found inconsistent by the ALUC on September 11, 2008) (Airport Compatibility Zones B1 and C of the Flabob Airport Influence Area).



RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC CASE NUMBER: ZAP 1035 FL20 DATE SUBMITTED: 11-5-20

APPLICANT / REPRESENTATIVE / PROPERTY OWNER CONTACT INFORMATION

| | | |
|-----------------|--|---|
| Applicant | <u>CIRUS DEVELOPMENT CO. INC.</u> | Phone Number <u>949-852-5575</u> |
| Mailing Address | <u>17300 RED HILL AVENUE, SUITE 280</u> <u>IRVINE, CA 92614</u> | Email <u>MARLENE@NEWPORTPACIFIC.COM</u> |

| | | |
|-----------------|--|---|
| Representative | <u>CIRUS DEVELOPMENT CO. INC.</u> | Phone Number <u>949-852-5575</u> |
| Mailing Address | <u>17300 RED HILL AVENUE, SUITE 280</u> <u>IRVINE, CA 92614</u> | Email <u>MARLENE@NEWPORTPACIFIC.COM</u> |

| | | |
|-----------------|--|---|
| Property Owner | <u>OLD PLANTATION INVESTORS, L.P.</u> | Phone Number <u>949-501-8382</u> |
| Mailing Address | <u>3600 AMERICAN RIVER DRIVE, SUITE 215</u> <u>SACRAMENTO, CA 95864</u> | Email <u>MARLENE@NEWPORTPACIFIC.COM</u> |

Flabob
zone
G+C

LOCAL JURISDICTION AGENCY

| | | |
|-------------------------|---|---|
| Local Agency Name | <u>CITY OF JURUPA VALLEY</u> | Phone Number <u>951-332-6464 X 210</u> |
| Staff Contact | <u>ANDREA HOFF</u> | Email <u>AHOFF@JURUPAVALLEY.ORG</u> |
| Mailing Address | <u>3930 LIMONITE AVE.</u> <u>JURUPA VALLEY</u> | Case Type |
| Local Agency Project No | <u>MA19216 (CUP19005)</u> | <input type="checkbox"/> General Plan / Specific Plan Amendment <input type="checkbox"/> Zoning Ordinance Amendment <input type="checkbox"/> Subdivision Parcel Map / Tentative Tract <input checked="" type="checkbox"/> Use Permit <input checked="" type="checkbox"/> Site Plan Review/Plot Plan <input type="checkbox"/> Other |

PROJECT LOCATION

Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways

| | | |
|-----------------------|---|--|
| Street Address | <u>OLD PLANTATION MOBILE HOME PARK</u> <u>3825 CRESTMORE ROAD; JURUPA VALLEY</u> | Gross Parcel Size _____ |
| Assessor's Parcel No. | <u>181 130 008 & 181 220 002</u> | Nearest Airport and distance from Airport _____ |
| Subdivision Name | _____ | |
| Lot Number | _____ | <small>1.2 MILES TO FLABOB, 7.3 MILES TO RIVERSIDE</small> |

PROJECT DESCRIPTION

If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed

| | |
|------------------------------|---|
| Existing Land Use (describe) | <u>THE LAND IS CURRENTLY BEING USED AS A MOBILE HOME PARK</u> |
| | |
| | |

| | | | |
|---|--|-----------------|--|
| Proposed Land Use (describe) | ADDING AN ADDITIONAL (9) NINE MOBILE HOMES. | | |
| For Residential Uses | Number of Parcels or Units on Site (exclude secondary units) _____ | | |
| For Other Land Uses (See Appendix C) | Hours of Operation _____ | | |
| | Number of People on Site | Maximum Number | _____ |
| | Method of Calculation _____ | | |
| Height Data | Site Elevation (above mean sea level) | APPROX 760 FEET | ft. |
| | Height of buildings or structures (from the ground) | 12' | ft. |
| Flight Hazards | Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight? | | <input type="checkbox"/> Yes |
| | If yes, describe _____ | | <input checked="" type="checkbox"/> No |

- A. NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.
- B. REVIEW TIME:** Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.
- C. SUBMISSION PACKAGE:**
1. Completed ALUC Application Form
 1. ALUC fee payment
 1. Plans Package (24x36 folded) (site plans, floor plans, building elevations, grading plans, subdivision maps)
 1. Plans Package (8.5x11) (site plans, floor plans, building elevations, grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
 1. CD with digital files of the plans (pdf)
 1. Vicinity Map (8.5x11)
 1. Detailed project description
 1. Local jurisdiction project transmittal
 3. Gummed address labels for applicant/representative/property owner/local jurisdiction planner
 3. Gummed address labels of all surrounding property owners within a 300 foot radius of the project site. **(Only required if the project is scheduled for a public hearing Commission meeting)**

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

STAFF REPORT

ADMINISTRATIVE ITEMS

5.1 Director's Approvals.

- A. During the period of November 16 through December 15, 2020, as authorized pursuant to Section 1.5.2(d) of the 2004 Riverside County Airport Land Use Compatibility Plan, ALUC Director Simon Housman reviewed one non-legislative case within Zone D of the Palm Springs International Airport Influence Area.

ZAP1090PS20 (Palm Springs International Airport Zone D) pertains to the City of Cathedral City Case No. DR20-002 (Design Review), a proposal to construct a 58-unit apartment complex on 3.55 acres, located northerly of Corral Road, westerly of Canyon Vista Road, southerly of Ramon Road, and easterly of Cathedral Canyon Drive. Pursuant to the Palm Springs Airport Land Use Compatibility Plan, the project site is located within Compatibility Zone D of the Palm Springs Airport Influence Area, where Additional Compatibility Policy #2.3 restricts residential densities to either 0.2 dwelling units per acre or above 3.0 dwelling units per acre. The project proposes 58 dwelling units on 3.55 gross acres, resulting in a density of 16.4 dwelling units per acre, which is consistent with the Zone D residential density criteria. Pursuant to the Palm Springs International Airport Land Use Compatibility Plan, the project site is located within Compatibility Zone D, which restricts non-residential intensity to an average of 100 people per acre and a maximum of 300 people in any given single acre. The project includes amenities such as a 698 square foot swimming pool and 2,047 pool deck area, and a recreation building that includes 523 square feet of recreation area, 183 square feet of storage area, and 198 square feet of office area, which would accommodate an occupancy of 164 people, resulting in an average intensity of 46 people per acre, and a single acre intensity of 164 people, both of which are consistent with the Compatibility Zone D intensity criteria.

Land use practices that attract or sustain hazardous wildlife populations on or near airports significantly increase the potential of Bird Aircraft Strike Hazards (BASH). The FAA strongly recommends that storm water management systems located within 5,000 or 10,000 feet of the Airport Operations Area, depending on the type of aircraft, be designed and operated so as not to create above-ground standing water. To facilitate the control of hazardous wildlife, the FAA recommends the use of steep-sided, rip-rap lined, narrow, linearly shaped water detention basins. All vegetation in and around detention basins that provide food or cover for hazardous wildlife should be eliminated. (FAA Advisory Circular 5200-33B). The nearest portion of the project is located 9,600 feet from the runway, and therefore would be subject to the above requirement. The project utilizes underground detention systems which will not contain surface water or attract wildlife and, therefore, would not constitute a hazard to flight. The project has been conditioned to be consistent with these standards in order to reduce bird attractant: 1) new basins are to be designed so as to provide for a maximum 48-hour detention period following the conclusion of a storm event, and to remain totally dry between rainfalls, and 2) any landscaping proposed in the detention basin shall be in accordance with the ALUC "Landscaping Near Airports" and "Airports, Wildlife and Stormwater Management" brochures.

The elevation of Runway 13R-31L at Palm Springs International Airport at its southerly terminus is

approximately 395.5 feet above mean sea level (AMSL). At a distance of approximately 9,600 feet from the runway to the project property line, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with top point exceeding 491.5 feet AMSL. The site's elevation is 356 feet AMSL, and the height of the tallest proposed building is 28 feet, resulting in a maximum top point elevation of 384 feet AMSL. Therefore, review by the FAA OES was not required.

ALUC Director Simon Housman issued a determination of consistency for this project on November 26, 2020.

- B. Additionally, ALUC Director Simon Housman reviewed two local jurisdiction non-impact legislative cases (one in the City of Hemet, one in the March Joint Powers Authority jurisdiction) pursuant to ALUC Resolution No. 2011-02 and issued determinations of consistency.

ZAP1064HR20 (Hemet-Ryan Airport, Multiple Zones) pertains to the City of Hemet Case Nos. GPA20-001 (General Plan Amendment), ZOA20-002 (Zoning Ordinance Amendment), a proposal to amend the City's General Plan increasing the maximum floor area ratio for certain allowable uses in the Community Commercial land use designation from 0.4 to 1.0 FAR, and amend the City's Zoning Ordinance granting exceptions to maximum height and floor area ratios standards for hotel, medical facility and unique projects located in the commercial zones.

The City's proposed General Plan Amendment, and its companion application Zoning Ordinance Amendment seeks to provide a mechanism for hotel, medical facility, or unique development projects located in the Community Commercial General Plan land use designation, to request exceptions to the floor area ratio (FAR) standard of 0.4 to a maximum 1.0 FAR, as well as granting exceptions to building height standards. Note that FAR is a tool used to design building's floor area in relation to the size of the underlying lot or parcel. FAR does not identify the occupancy of the building, which is a requirement of the Airport Land Use Compatibility Plan.

The City's General Plan was found consistent with the 2017 Hemet-Ryan Airport Land Use Compatibility Plan by the ALUC on May 9, 2019. As such, the City is required to apply all (airport) zoning compatibility intensity criteria to these projects that are seeking a FAR exception as outlined in this amendment. In addition, the City is also required to review project's heights for Federal Aviation Administration Part 77 obstruction standards to ensure that buildings and structures do not result in a hazard to air navigation.

The proposed amendment does not involve changes in development standards or allowable land uses that would increase residential density or non-residential intensity. Therefore, these amendments have no possibility for having an impact on the safety of air navigation within airport influence areas located within the unincorporated areas of Riverside County.

ALUC Director Simon Housman issued a determination of consistency for this project on November 16, 2020.

ZAP1441MA20 (March Air Reserve Base/Inland Port Airport Multiple Zones) pertains to the March Joint Powers Authority Case Nos. GPA20-01 (General Plan Amendment), SP20-01 (Specific Plan Amendment, No. 8), a proposal to amend the MJPA General Plan and the March Business Center Specific Plan SP-1, Amendment No. 8 to rename Bandit Boulevard to Gless Ranch Road, and to update truck routes in the South

Campus portion of the plan, identifying Caroline Way as a truck route. The original project was found consistent by ALUC on September 24, 2020.

The proposed amendment will revise text, maps, plans, figures, and tables, within the MIPA General Plan and the March Business Center Specific Plan SP-1, Amendment No. 8, to correctly identify Gless Ranch Road (previously referred to as Bandit Boulevard), and to correctly reflect the approved truck route within the March Business Center. The proposed updated exhibits now show Caroline Way as a truck route.

The proposed amendment does not involve changes in development standards or allowable land uses that would increase residential density or non-residential intensity. Therefore, this amendment has no possibility for having an impact on the safety of air navigation within airport influence areas located within the unincorporated areas of Riverside County.

ALUC Director Simon Housman issued a determination of consistency for this project on December 10, 2020.

- C. Additionally, as authorized pursuant to ALUC Resolution No. 2015-01, as extended by Resolution Nos. 2016-02 and 2018-02, ALUC Director Simon Housman reviewed two legislative case sets with associated non-legislative cases within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area and issued determination of consistencies.

ZAP1438MA20 (March Air Reserve Base/Inland Port Airport Zone E) pertains to the City of Riverside Case Nos. P20-0372 (General Plan Amendment), P20-0373 (Rezone), P20-0374 (Conditional Use Permit), P20-0376 (Design Review), a proposal to amend the site's General Plan land use designation from Very Low Density Residential (VLDR) to Commercial (C), and change its zoning from R-1-1/2 Acre- SP – Single Family Residential and Specific Plan (Orangecrest) Overlay Zones to CR-SP-X-15-S-2 – Commercial Retail, Specific Plan (Orangecrest), Building Stories (Two-Story Maximum) and Building Setback (15 feet from Van Buren Boulevard) Overlay Zones, on 0.87 acres located southerly of Van Buren Boulevard, westerly of Little Court, northerly of Blue Sky Street, and easterly of Alta Cresta Avenue, and also construct a 6,412 square foot commercial building which includes 3,804 square feet of retail area, and 2,608 square foot drive-thru restaurant. The site is located within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, non-residential intensity is not restricted.

The elevation of Runway 14-32 at March Air Reserve Base/Inland Port Airport is approximately 1,535 feet above mean sea level (AMSL) at its northerly terminus. At a distance of 22,900 feet from the project to the nearest point on the runway, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with an elevation at top of roof exceeding 1,764 feet AMSL. The site's finished floor elevation is 1,606 feet AMSL, and the proposed building height is 28 feet, resulting in a top point elevation of 1,634 feet AMSL. Therefore, FAA OES review for height/elevation reasons was not required.

ALUC Director Simon Housman issued a determination of consistency for this project on November 16, 2020.

ZAP1442MA20 (March Air Reserve Base/Inland Port Airport Zone E) pertains to the City of Riverside Case Nos. P19-0926 (General Plan Amendment), P19-0943 (Rezone), P19-0958 (Design Review), P19-0959 (Variance), a proposal to amend 1.33 acres (of 1.40 acre) site's General Plan land use designation from LDR (Low Density Residential) to C (Commercial), and rezone the site's 1.40 acres from R-1-7000 (Single Family Residential) and CG (Commercial General) Zone to CG-CS (Commercial General and Commercial Storage Overlay Zones), located southerly of Ivy Street, easterly of State Route 91, and westerly of the AT & SF railroad, and to expand an existing self-storage facility on an adjacent 1.40 acre parcel, including the new construction of 13,400 square feet of self-storage area, located southerly of Ivy Street, easterly of State Route 91, and westerly of the AT & SF railroad. A variance is required to allow walls and fences to exceed municipal code standards. The site is located within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, non-residential intensity is not restricted.

Although the project is located within the March Air Reserve Base/Inland Port Airport Influence Area, the nearest runway is actually Runway 9-27 at Riverside Municipal Airport. The elevation of Runway 9-27 at Riverside Municipal Airport is approximately 815.8 feet above mean sea level (AMSL) at its southeasterly terminus. At a distance of 16,980 feet from the project to the nearest point on the runway, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with an elevation at top of roof exceeding 985 feet AMSL. The site's finished floor elevation is 912 feet AMSL, and the proposed maximum building height is 12 feet, resulting in a top point elevation of 924 feet AMSL. Therefore, FAA OES review for height/elevation reasons was not required.

ALUC Director Simon Housman issued a determination of consistency for this project on December 10, 2020.

5.2 Update March Air Reserve Base Compatibility Use Study (CUS)
Presentation by ALUC Director Simon Housman or his designee.

5.3 Standardizing Conditions of Approval
Presentation by ALUC Director Simon Housman or his designee.

5.4 Correspondence from the March Air Reserve Base
During the ALUC review of submitted application ZAP1439MA20, a proposal to develop the D1 Gateway Aviation Center Project including a 201,200 square foot gateway air freight cargo warehouse building and a 69,620 square foot maintenance building located in Zone B2 of the March Air Reserve Base/Inland Port Airport Influence Area westerly of Heacock Street and northerly of Oleander Avenue, ALUC staff received written correspondence from the March Air Reserve Base, stating their concerns that the project could impact airfield operations, environment and utilities, and safety security and traffic of the Base. The application has been continued off-calendar at the request of the applicant.

Presentation by ALUC Director Simon Housman or his designee.

AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY



November 26, 2020

Mr. Robert Rodriguez, Planning Director
City of Cathedral City Planning Department
68-700 Avenida Lalo Guerrero
Cathedral City, CA 92234

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Russell Betts
Desert Hot Springs

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Palm Springs

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Director
Simon A. Housman

Paul Rull
Barbara Santos

County Administrative Center
4080 Lorton St., 4th Floor
Riverside, CA 92501
(951) 955-6132

www.rcaluc.org

RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW – DIRECTOR'S DETERMINATION

File No.: ZAP1090PS20
Related File No.: DR20-002 (Design Review)
APN: 680-260-031

Dear Mr. Rodriguez:

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to Policy 1.5.2(d) of the Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan, staff reviewed City of Cathedral City Case No. DR20-002 (Design Review), a proposal to construct a 58-unit apartment complex on 3.55 acres, located northerly of Corral Road, westerly of Canyon Vista Road, southerly of Ramon Road, and easterly of Cathedral Canyon Drive.

Pursuant to the Palm Springs Airport Land Use Compatibility Plan, the project site is located within Compatibility Zone D of the Palm Springs Airport Influence Area, where Additional Compatibility Policy #2.3 restricts residential densities to either 0.2 dwelling units per acre or above 3.0 dwelling units per acre. The project proposes 58 dwelling units on 3.55 gross acres, resulting in a density of 16.4 dwelling units per acre, which is consistent with the Zone D residential density criteria. Pursuant to the Palm Springs International Airport Land Use Compatibility Plan, the project site is located within Compatibility Zone D, which restricts non-residential intensity to an average of 100 people per acre and a maximum of 300 people in any given single acre. The project includes amenities such as a 698 square foot swimming pool and 2,047 pool deck area, and a recreation building that includes 523 square feet of recreation area, 183 square feet of storage area, and 198 square feet of office area, which would accommodate an occupancy of 164 people, resulting in an average intensity of 46 people per acre, and a single acre intensity of 164 people, both of which are consistent with the Compatibility Zone D intensity criteria.

Land use practices that attract or sustain hazardous wildlife populations on or near airports significantly increase the potential of Bird Aircraft Strike Hazards (BASH). The FAA strongly recommends that storm water management systems located within 5,000 or 10,000 feet of the Airport Operations Area, depending on the type of aircraft, be designed and operated so as not to create above-ground standing water. To facilitate the control of hazardous wildlife, the FAA recommends the use of steep-sided, rip-rap lined, narrow, linearly shaped water detention basins. All vegetation in and around detention basins that provide food or cover for hazardous wildlife

AIRPORT LAND USE COMMISSION

should be eliminated. (FAA Advisory Circular 5200-33B). The nearest portion of the project is located 9,600 feet from the runway, and therefore would be subject to the above requirement. The project utilizes underground detention systems which will not contain surface water or attract wildlife and, therefore, would not constitute a hazard to flight. The project has been conditioned to be consistent with these standards in order to reduce bird attractant: 1) new basins are to be designed so as to provide for a maximum 48-hour detention period following the conclusion of a storm event, and to remain totally dry between rainfalls, and 2) any landscaping proposed in the detention basin shall be in accordance with the ALUC "Landscaping Near Airports" and "Airports, Wildlife and Stormwater Management" brochures.

The elevation of Runway 13R-31L at Palm Springs International Airport at its southerly terminus is approximately 395.5 feet above mean sea level (AMSL). At a distance of approximately 9,600 feet from the runway to the project property line, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with top point exceeding 491.5 feet AMSL. The site's elevation is 356 feet AMSL, and the height of the tallest proposed building is 28 feet, resulting in a maximum top point elevation of 384 feet AMSL. Therefore, review by the FAA OES was not required.

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2005 Palm Springs Airport Land Use Compatibility Plan, provided that the City of Cathedral City applies the following recommended conditions:

CONDITIONS:

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)

AIRPORT LAND USE COMMISSION

- (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.**
 - (e) Highly noise-sensitive outdoor nonresidential uses and hazards to flight.**
- 3. The attached disclosure notice shall be provided to all potential purchasers, lessees, and/or tenants of the property, and shall be recorded as a deed notice.**
- 4. Detention basin(s) shall be designed so as to provide a maximum 48-hour detention period for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention basin that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the detention basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.**

Landscaping in the detention basin, if not rip-rap, should be in accordance with the guidance provided in ALUC “LANDSCAPING NEAR AIRPORTS” brochure, and the “AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT” brochure available at RCALUC.ORG which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist. The infiltration basin shall be designed in accordance with all parameters identified in the Wildlife Hazard Management at Riverside County Airports: Background and Policy.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: “There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes”. The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin.

- 5. The evaluated project amenities consists of a 698 square foot swimming pool and 2,047 pool deck area, and a recreation building that includes 523 square feet of recreation area, 183 square feet of storage area, and 198 square feet of office area. Any increase in building area or change in use to assembly occupancies will require an amended review by the Airport Land Use Commission.**
- 6. The ALUC overflight informational brochure shall be provided to prospective purchasers showing the locations of aircraft flight patterns, the frequency of overflights, the typical altitudes of the aircraft, and the range of noise levels that can be expected from individual aircraft overflights, as well as Compatibility Factors exhibit from the Palm Springs International Airport Land Use Compatibility Plan.**

If you have any questions, please contact Paul Rull, ALUC Principal Planner, at (951) 955-6893.

AIRPORT LAND USE COMMISSION

Sincerely,
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

Simon A. Housman

Simon A. Housman, ALUC Director

Attachments: Notice of Airport in Vicinity

cc: PVG Architects, David Drake (applicant/representative)
Neelem Salhorta (property owner)
Ulises Aguirre, Interim Airport Manager, Palm Springs International Airport
ALUC Case File

Y:\AIRPORT CASE FILES\Palm Springs\ZAP1090PS20\ZAP1090PS20.LTR.doc

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

NOTICE

**THERE IS AN AIRPORT NEARBY.
THIS STORM WATER BASIN IS DESIGNED TO HOLD
STORM WATER FOR ONLY 48 HOURS AND
NOT TO ATTRACT BIRDS**

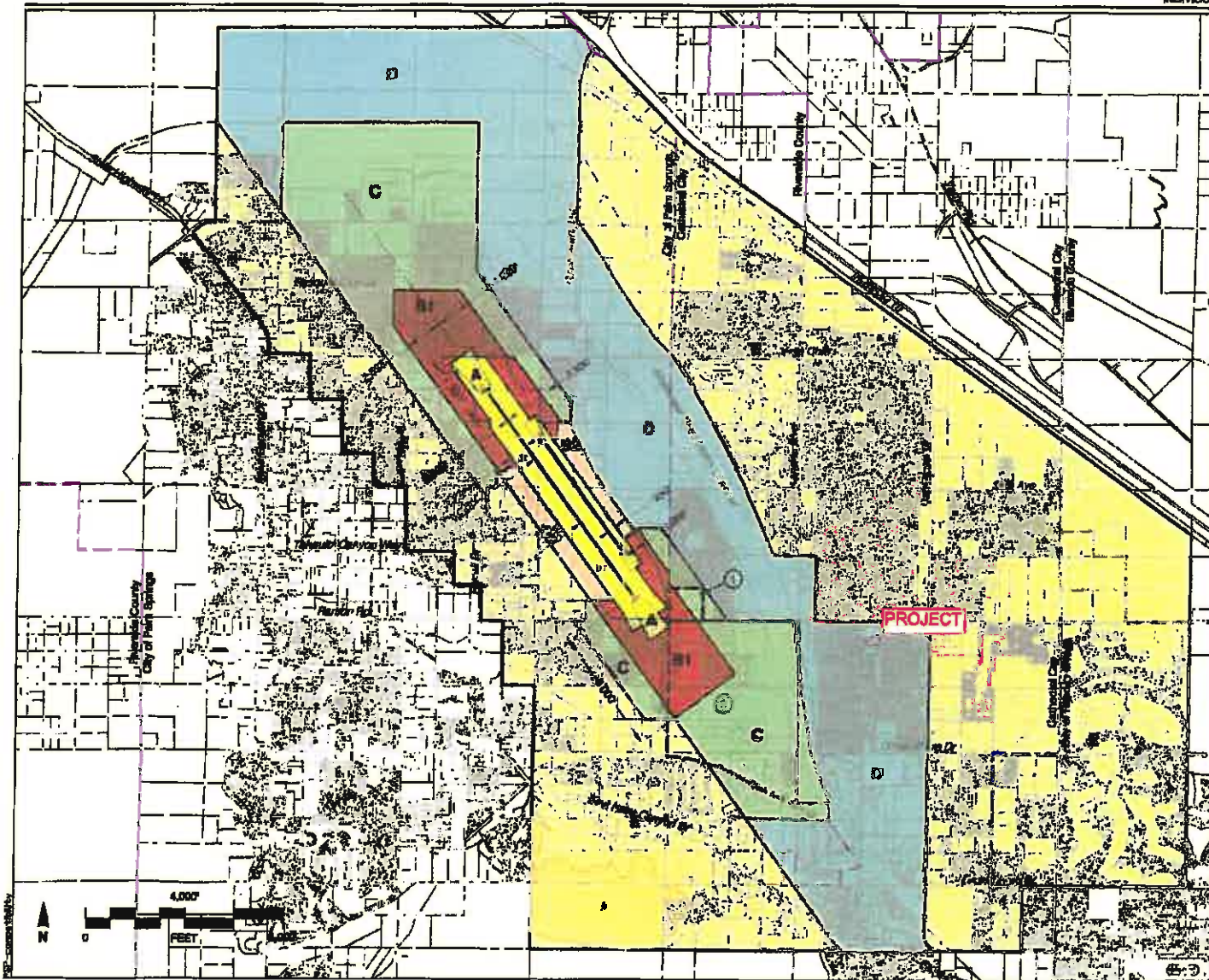
**PROPER MAINTENANCE IS NECESSARY TO AVOID
BIRD STRIKES**



IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:

Name: _____

Phone: _____



Legend

Compatibility Zones

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E
- Height Review Overlay Zone

Boundary Lines

- Airport Property Line
- City Limits

Notes

All dimensions measured from runway ends and centerlines.

DT = Displaced Threshold

See Chapter 2, Table 2A for compatibility criteria associated with this map.

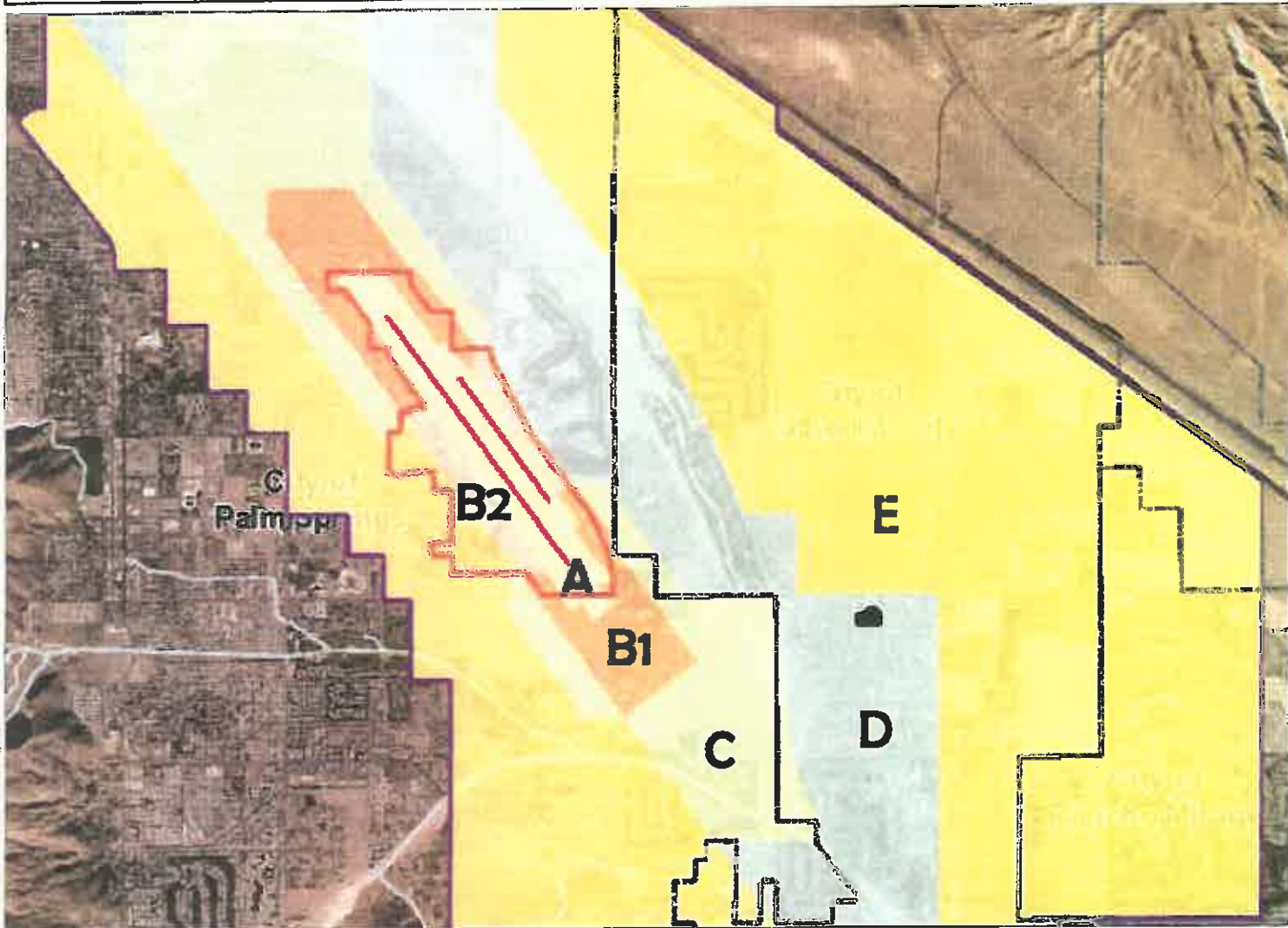
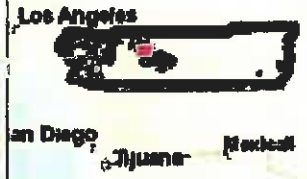
See Policy PB.2.1.

Riverside County
 Airport Land Use Commission
 Riverside County
 Airport Land Use Compatibility Plan
 Policy Document
 (Adopted March 2008)

Map PB-1

Compatibility Map
 Palm Springs International Airport

Map My County Map



- Legend**
- Runways
 - Airports
 - Airport Influence Areas
 - Airport Compatibility Zones
 - ▨ OTHER COMPATIBILITY ZONE
 - A
 - A-EXC1
 - B1
 - B1-APZ I
 - B1-APZ I-EXC1
 - B1-APZ II
 - B1-APZ II-EXC1
 - B1-EXC1
 - B2
 - B2-EXC1
 - C
 - C1
 - C1-EXC1
 - C1-EXC3
 - C1-EXC4
 - C1-HIGHT
 - C2
 - C2-EXC1
 - C2-EXC2
 - C2-EXC3
 - C2-EXC5
 - C2-EXC8



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

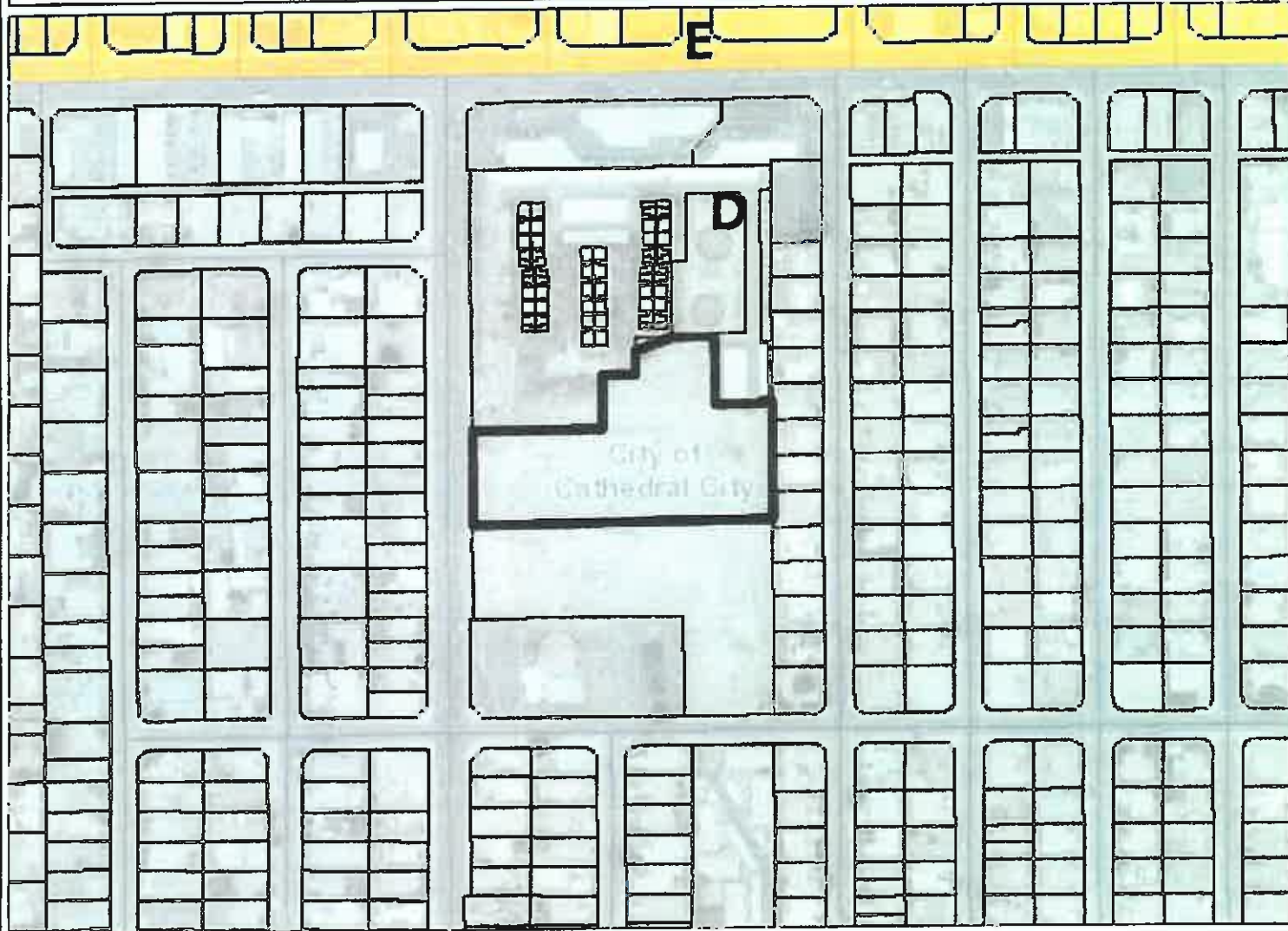
Notes



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Map My County Map



Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5



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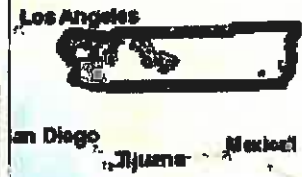
Notes

0 376 752 Feet






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Map My County Map



Legend

-  Parcels
-  County Centerlines
-  Blueline Streams
-  City Areas
-  World Street Map



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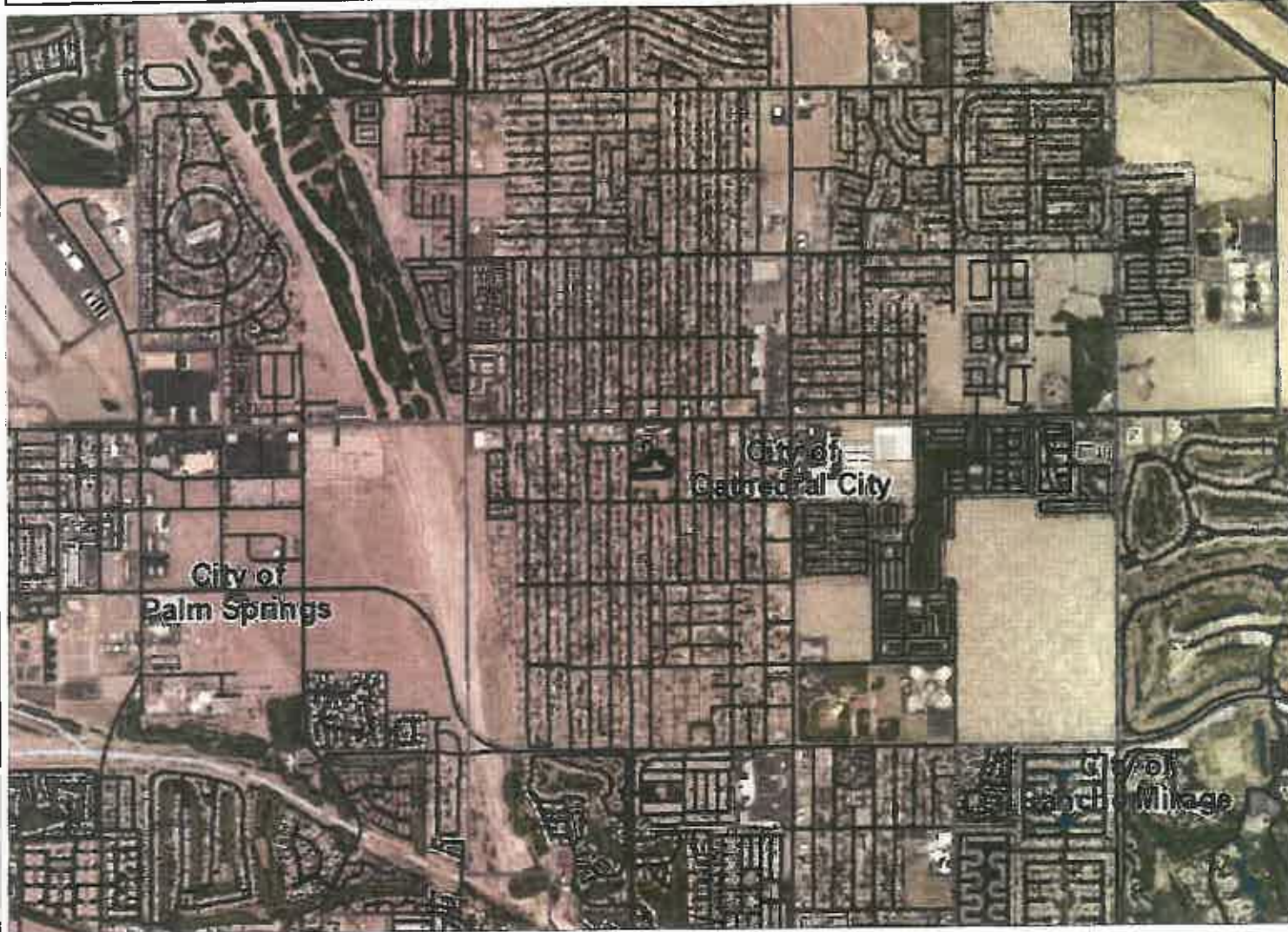
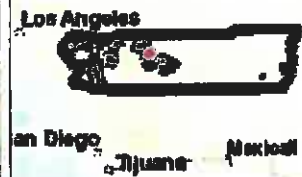
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Notes

Map My County Map



Legend

- County Centerlines
- Blue/line Streams
- City Areas
- World Street Map



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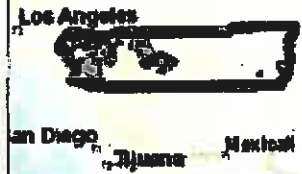
0 3 6,019 Feet

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Notes

Map My County Map



Legend

- Parcels
- County Centerlines
- Blueline Streams
- City Areas
- World Street Map



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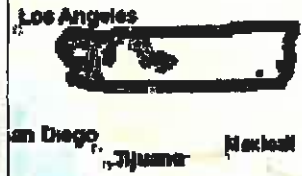
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Notes

Map My County Map



Legend

- Parcels
- County Centerline Names
- County Centerlines
- Blueline Streams
- City Areas
- World Street Map

Notes



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CHAPTER 17 BACKSOUND DATA: PALM SPRINGS INTERNATIONAL AIRPORT AND ENVIRONS

| GENERAL INFORMATION | AIRPORT PLANNING DOCUMENTS |
|--|--|
| <ul style="list-style-type: none"> ➤ Airport Ownership: City of Palm Springs ➤ Year Opened: 1989 ➤ Property Size <ul style="list-style-type: none"> • Fee title: 692 acres • Argillite easements: 10 acres ➤ Airport Characteristics: Primary Commercial Service ➤ Airport Elevation: 474-foot MSL | <ul style="list-style-type: none"> ➤ Airport Master Plan <ul style="list-style-type: none"> • Adopted by City Council, May 2013 ➤ Airport Layout Plan Drawing <ul style="list-style-type: none"> • Last updated, May 2009 ➤ FAP Part 150 Airport Noise Compatibility Program <ul style="list-style-type: none"> • Approved by FAA June 1994 |
| PLANNING/TERRAIN DESIGN | TRAFFIC PATTERNS AND APPROACH PROCEDURES |
| <p>Runway 24R-24L</p> <ul style="list-style-type: none"> ➤ Critical Aircraft: DC-90, B-747 ➤ Airport Reference Code: D-IV ➤ Dimensions: 13,000 ft. long, 152 ft. wide ➤ Runway 24R and displaced 3,000 ft. ➤ Runway 24L and displaced 1,800 ft. ➤ Pavement Strength: (from existing gear configuration) <ul style="list-style-type: none"> • 125,000 lbs (single wheel) • 200,000 lbs (dual wheel) • 300,000 lbs (dual tandem wheel) • 400,000 lbs (dual tri-axle wheel) ➤ Average Gradient: 0.5% (sloping to north) ➤ Runway Lighting: High-intensity edge lights (HIEL) ➤ Primary Taxiway: Full-width parallel on both sides <p>Runway 9R-9L</p> <ul style="list-style-type: none"> ➤ Critical Aircraft: Medium Jet ➤ Airport Reference Code: E-II ➤ Dimensions: 4,582 ft. long, 75 ft. wide ➤ Pavement Strength: (from existing gear configuration) <ul style="list-style-type: none"> • 12,500 lbs (single wheel) • 40,000 lbs (dual wheel) ➤ Average Gradient: 0.0% (sloping to north) ➤ Runway Lighting: Medium-intensity edge lights (MIEL) ➤ Primary Taxiway: Full-width parallel on east side | <ul style="list-style-type: none"> ➤ Airplane Traffic Patterns <ul style="list-style-type: none"> • Runway 24R: 100% left traffic • Runway 24L: 80% Right traffic • Helo: 1,000 ft. AGL, small to med. AGL, others ➤ Instrument Approach Procedures (lowest minimum) <ul style="list-style-type: none"> • Runway 24L: VOR or GPS-B <ul style="list-style-type: none"> • Ceiling (1 NM) visibility, 1,000 ft. descent (holding) ➤ Standard Inst. Approach Procedures (initial approach) <ul style="list-style-type: none"> • Runway 24R: Climb to 1,000 ft. to 24R* • Runway 24L: Climb to 1,000 ft. to 24L* ➤ Visual Approach/Aid <ul style="list-style-type: none"> • Runway 24R: VASI (3-4); HIEL • Runway 24L: PAPI (3-4); HIEL • Runway 9R: PAPI (3-4); HIEL • Runway 9L: PAPI (3-4); HIEL ➤ Operational Procedures / Noise Abatement Procedures <ul style="list-style-type: none"> • Call to taxi: Use Runway 24 • Noise sensitive area of quiet route use quiet flight procedures • Runway 24R: 24L: *noise vis displaced for noise abatement |
| BUILDING AREA | PLANNED FACILITY IMPROVEMENTS |
| <ul style="list-style-type: none"> ➤ Location: South side and southeast along property line ➤ Aircraft Parking Capacity <ul style="list-style-type: none"> • Hangar spaces: 76 (includes FBO, Sigwest hangar) • Tiedowns: 80 ➤ Other Major Facilities <ul style="list-style-type: none"> • Air traffic control tower • Flight lounge ➤ Services <ul style="list-style-type: none"> • Fuel: 100LL, Jet A (full tank 6:00 a.m. to 10:00 p.m.) • Commercial tie-down service • Other: Aircraft rental & inspection, aircraft maintenance & overhauls, aircraft leasing | <ul style="list-style-type: none"> ➤ ATIS <ul style="list-style-type: none"> • Add approach light system to Runway 24L • Extend Runway 24L Cat. I precision inst. approach ➤ Safety Area <ul style="list-style-type: none"> • Repave a 1-mile oval area • Expand terminal apron ➤ Property <ul style="list-style-type: none"> • No planned acquisition |

Exhibit PB-1

Airport Features Summary
Palm Springs International Airport

BACKSOUND DATA: PALM SPRINGS INTERNATIONAL AIRPORT AND ENVIRONS

| BASED AIRCRAFT | Current ^a 2002 data | Future ^b 2025 | TIME OF DAY DISTRIBUTION | Current ^c | Future ^b |
|----------------------------------|-----------------------------------|-----------------------------|--|----------------------|---------------------|
| Aircraft Type | | | Atmos | | |
| Single-Eng on | 69 | 182 | Day | 77% | 76% |
| Twin-Engine Piston | 12 | 25 | Evening | 14% | 18% |
| Turboprop | 4 | 18 | Night | 9% | 6% |
| Turbopiston | 2 | 11 | Other Airplane | | no change |
| Helicopter | 2 | 1 | Day | 78% | no change |
| Total | 127 | 220 | Evening | 16% | no change |
| | | | Night | 7% | |
| | | | Helicopter | | no change |
| | | | Day | 66% | no change |
| | | | Evening | 10% | no change |
| | | | Night | 2% | |
| AIRLINE ACTIVITY | Current ^a 2002 data | Future ^b 2025 | PLANNING USE DISTRIBUTION | Current ^c | Future ^b |
| Enplaned Passengers | 642,498 | 1,700,000 | General Aviation, Local | | |
| Deplaned Passengers | 26,789 | 26,482 | Taxi-in & Landings | | |
| | | | Runway 24R | 85% | no change |
| | | | Runway 24L | 0% | no change |
| | | | Runway 9R | 0% | no change |
| | | | Runway 9L | 0% | no change |
| AIRCRAFT OPERATIONS | Current ^a 2002 data | Future ^b 2025 | General Aviation, Remote | | |
| Total | 185,844 | 170,820 | Taxi-in & Landings | | |
| Annual | 304 | 473 | Runway 24R | 17% | no change |
| Average Day | | | Runway 24L | 82% | no change |
| | | | Runway 9R | 0% | no change |
| | | | Runway 9L | 0% | no change |
| | | | Runway 24L | 89% | |
| Distribution by Aircraft Type | | | Business Jet & Government Airline | | |
| Single-Eng on | 87% | 46% | Taxi-in & Landings | | |
| Twin-Engine | | | Runway 24R | 4% | no change |
| Piston & Turboprop | 4% | 5% | Runway 24L | 0% | no change |
| Small turboprop | 0% | 1% | Runway 9R | 0% | no change |
| Helicopter | 2% | 3% | Runway 9L | 0% | no change |
| Air-to-air, Jet & Turboprop | 3% | 3% | Runway 24L | 85% | |
| Distribution by Type of Operator | | | Air Carrier | | |
| Local | 14% | 14% | Taxi-in & Landings | | |
| (incl. bush-and-gate) | | | Runway 24R | 0% | no change |
| Remote | 86% | 86% | Runway 24L | 0% | no change |
| | | | Runway 9R | 0% | no change |
| | | | Runway 9L | 0% | no change |
| | | | Runway 24L | 85% | |
| FLIGHT TRACK USES* | | | Current and Future | | |
| | | | ➤ Approaches generally straight to airport for long and go | | |
| | | | ➤ Departures run southeast to use of residential areas and San Jacinto Mountains | | |

Notes

- ^a Source: Airport management records
- ^b Source: 2025 Airport Master Plan forecast for 2025 based on 2025 for compatibility planning purposes
- ^c Source: 2002 Airport Master Plan estimate

Exhibit PB-3

Airport Activity Data Summary
Palm Springs International Airport

**Presence of Aircraft Overflight:
Palm Springs International Airport**

EXPANDED BUYER AWARENESS MEASURES

As stipulated in the Riverside County Airport Land Use Compatibility Plan (ALUCP) for Palm Springs International Airport, any new single-family or multi-family residential development within the Palm Springs International Airport Influence Area (except those portions in Compatibility Zone E) shall be provided measures intended to ensure that prospective buyers or renters are informed about the presence of aircraft overflights of the property.

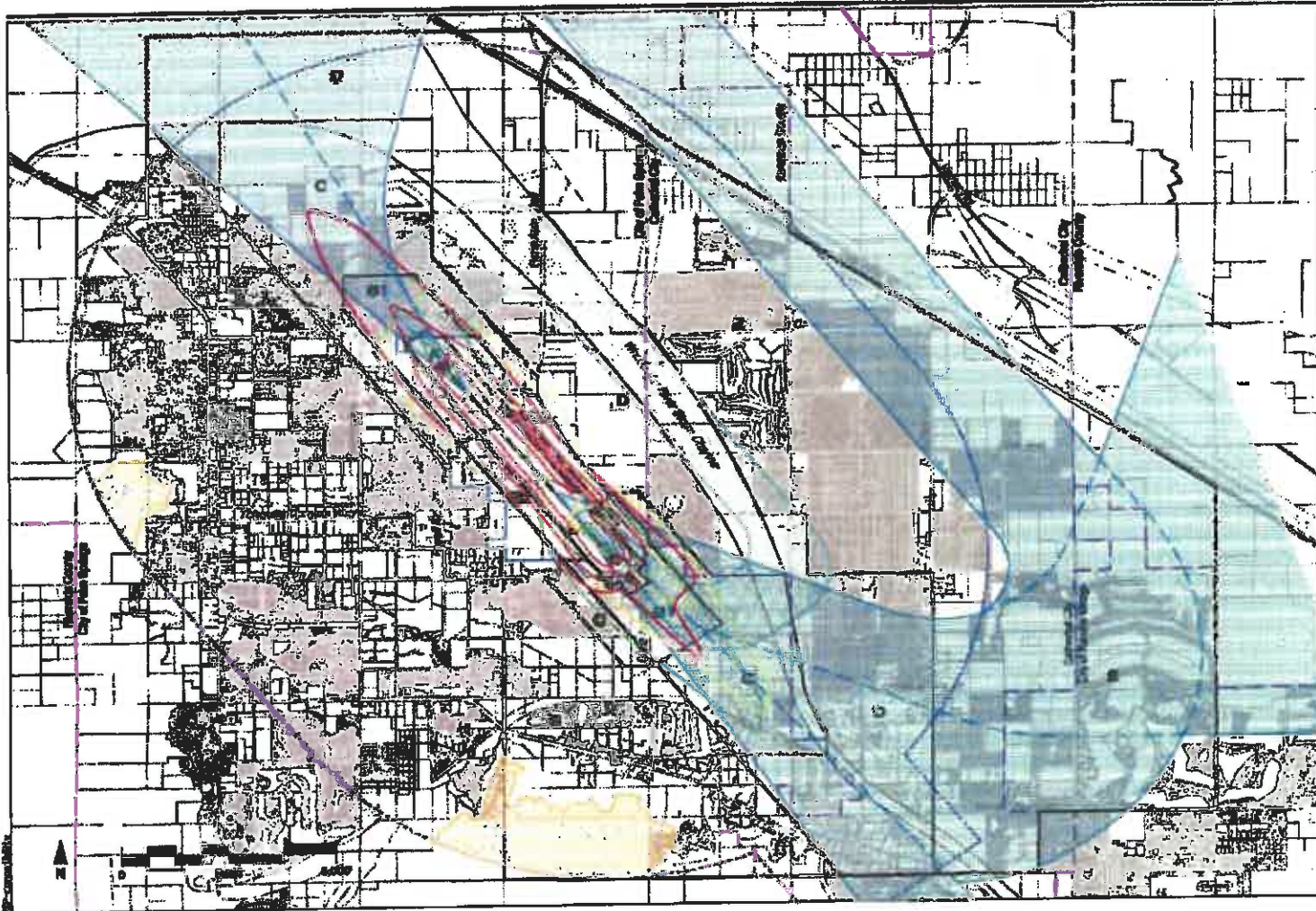
This brochure provides buyers or renters with information showing the locations of aircraft flight patterns, frequency of overflights, typical altitudes of the aircraft, and range of noise levels that can be expected from individual aircraft overflight.



For more information contact us:
Airport Land Use Commission
(951) 855-5132
www.rcaluc.org



FIND YOUR NEIGHBORHOOD ON THIS MAP



Legend

Compatibility Zones

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E

Noise and Overflight Compatibility Factors

- 75 dB CNEL } Composite of
- 70 dB CNEL } Landing and Takeoff
- 65 dB CNEL } Average Annual Day
- 60 dB CNEL } Contour Not Shown

— General Traffic Pattern Routes (approximately 50% of aircraft overflights estimated to occur within these limits)

Safety and Airspace Compatibility Factors

- Aircraft Departure Accident Risk Intensity Contour* (Shown only for Takeoffs to the Northwest)
- Aircraft Approach Accident Risk Intensity Contour* (Shown only for Landings from the Southeast; offset 1,000 feet to reflect displacement in ground on primary runway)
- FAA Part 77 Geared Surface Limits
- Terrain Penetration of FAA Part 77 Surfaces

Boundary Lines

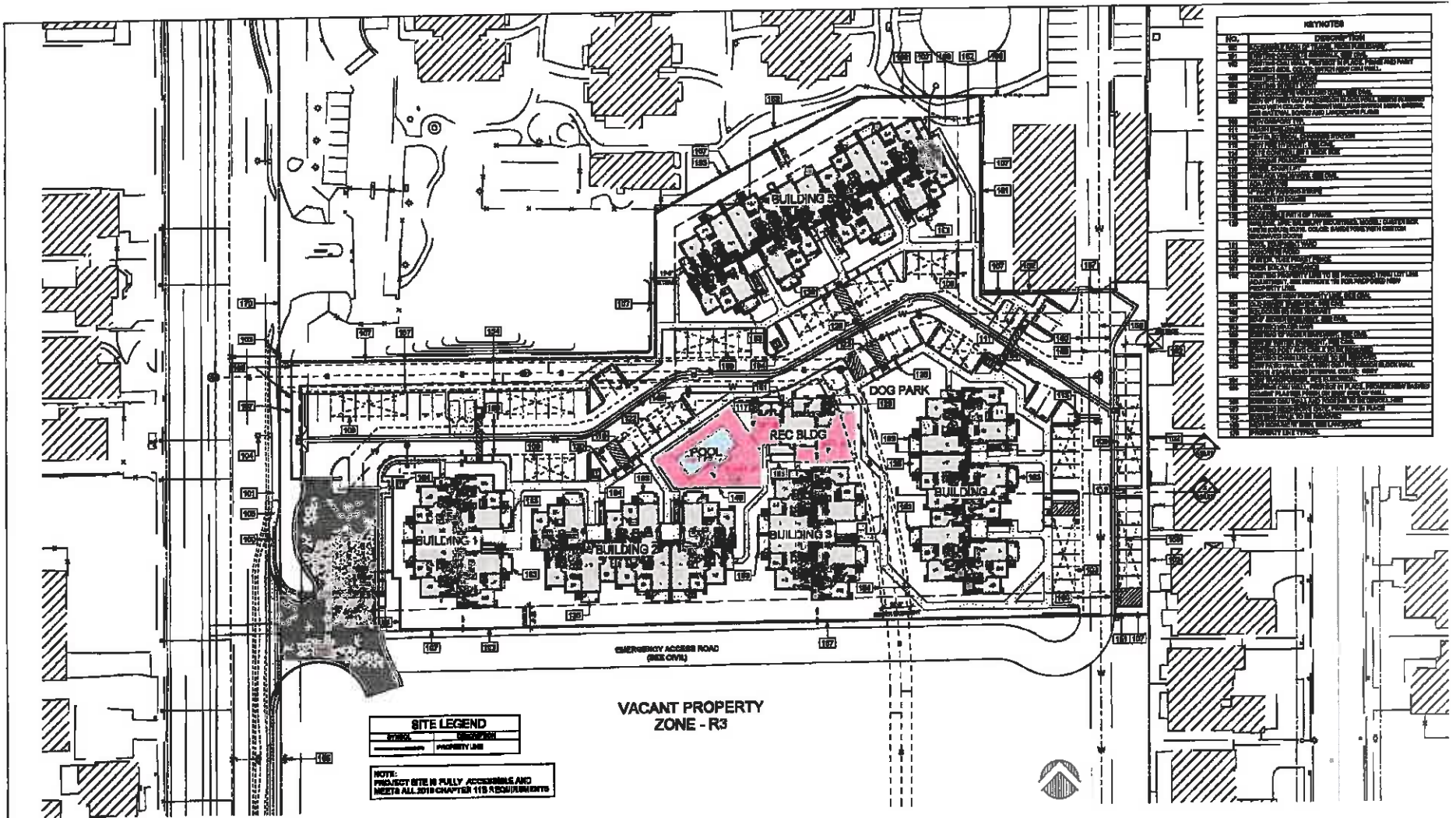
- Airport Property Line
- City Limits

* Aircraft accident risk intensity contours are derived from nationwide accident location data in California Division of Aeronautics database. The contours show relative international (flight concentrations) of non-airport accidents in 20% increments. The contour shapes represent a wide range of general aviation airports and have not been modified to reflect the flight profile for this airport.

Riverside County
Airport Land Use Commission
Riverside County
Airport Land Use Compatibility Plan
East County Airports Background Data
(March 2007)

Exhibit PB-7

Compatibility Factors Map Palm Springs International Airport



CANYON SPRINGS VILLAS
 CATHEDRAL CANYON DR - CATHEDRAL CITY, CALIFORNIA

SITE LEGEND
 SYMBOL DESCRIPTION
 PROPERTY LINE
 NOTE:
 PROJECT SITE IS FULLY ACCESSIBLE AND
 MEETS ALL 2018 CHAPTER 11B REQUIREMENTS

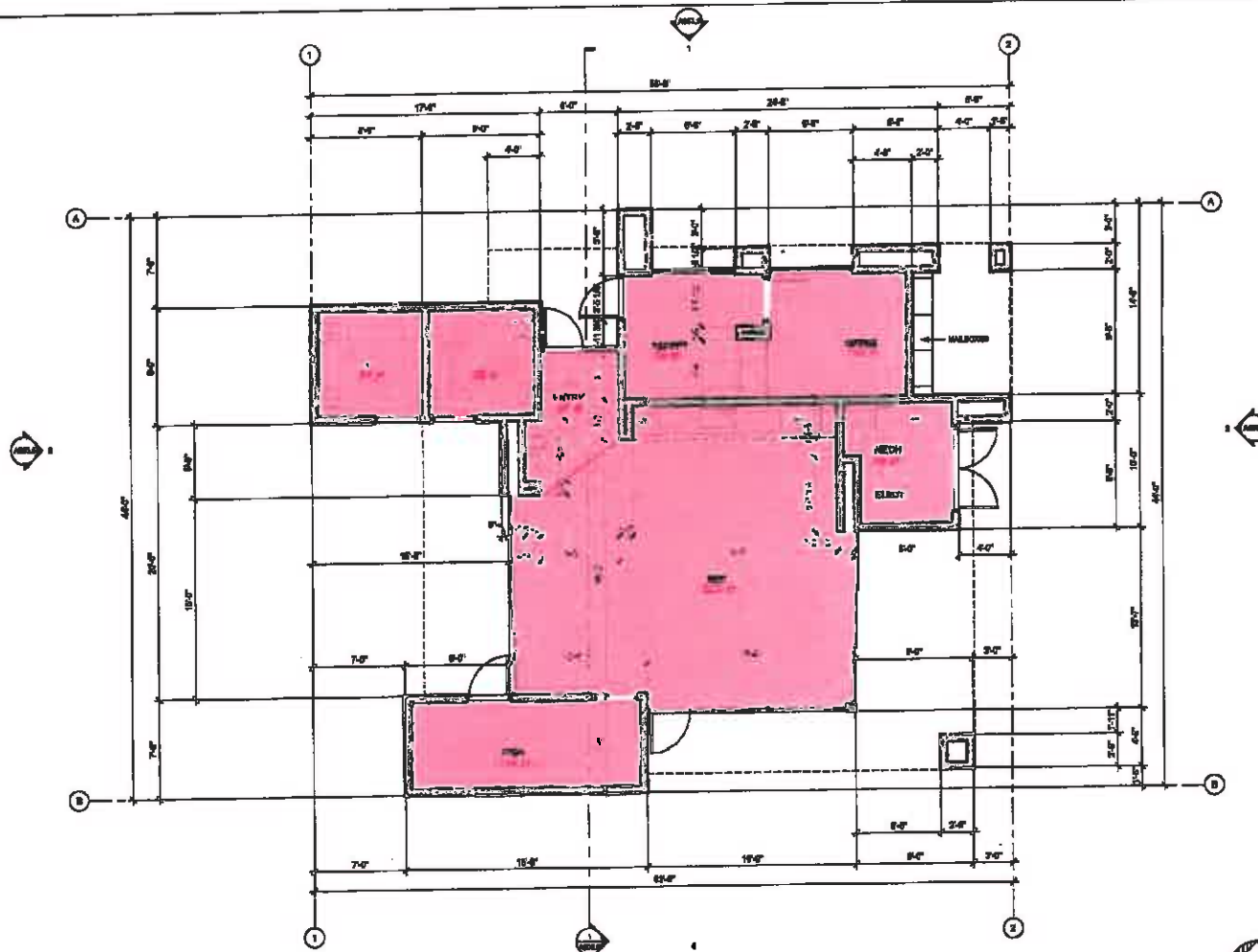
VACANT PROPERTY
 ZONE - R3



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 4081 SAN PABLO AVE. STE 200 | PALM DESERT, CA 92260
 WWW.PVGNARCHITECTS.COM
 INFO@PVGNARCHITECTS.COM | 760.799.4887

SITE PLAN
 Scale: As Indicated
 Sheet: **A010.0**

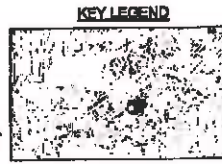
DATE: 10/20/2023
 BY: [Signature]
 CHECKED: [Signature]



CANYON SPRINGS VILLAS
 CATHEDRAL CANYON DR - CATHEDRAL CITY, CALIFORNIA



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 951.999.8070 | 760.207.8851



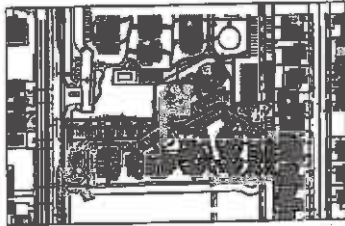
REC BUILDING
FLOOR PLAN

Scale: 1/8" = 1'-0"
 Sheet: **A601.6**

August 28th, 2009



VICINITY MAP
N.T.S.



CANYON SPRINGS VILLAS

CATHEDRAL CANYON DR - CATHEDRAL CITY, CALIFORNIA

PROJECT INFORMATION

OWNER
MELISSA BALHOUTA
CANYON SPRINGS VILLAS, LLC
87-728 TWENTY NINE PALMS HWY, SUITE 401
YUCCA VALLEY, CA 92284
(760) 232-7071
MBALHOUTA@GMAIL.COM

APPLICANT
PREST VUKSIC GREENWOOD ARCHITECTS
JOHN VUKSIC
4400 SAN PABLO AVE., STE. 208
PALM DESERT, CA 92260
DVAUC@PVGARCHITECTS

ASSESSOR'S PARCEL
624-260-021

LEGAL DESCRIPTION
PPROJ. 2 OF L.A. 8 88-374 RECORDED AS BMT.
8 8889-21248 DATED 04-19-1988, OFFICIAL
RECORDS OF RIVERSIDE COUNTY, CA
REFERENCE TRACT 18799-1, MS 141-71-74

PROJECT DESCRIPTION/LOCATION
MULTI-FAMILY HOUSING
CATHEDRAL CANYON DRIVE, CATHEDRAL CITY, CA

EXISTING ZONING
R3 MULTIPLE - FAMILY RESIDENTIAL DISTRICT

BUILDING DATA
CORPORATE (APARTMENTS) 0/0
TYPE OF CONSTRUCTION V/S
SPRAWLED YES
NO. OF STORIES 2
GREATEST HEIGHT ALLOWED 37 A.F.F.

PARKING REQUIREMENTS

STANDARD SPACES 12 SPACES
ADA COMPLIANT SPACES: 8 EA (ALL-VAH SPACES)
ELECTRIC VEHICLE SPACES: 11 EA (11.9%)
ADA COMPLIANT ELECTRIC VEHICLE SPACES: 2 EA
COVERED SPACES: 29 EA (24.7%)
TOTAL PARKING SPACES PROVIDED: 67 EA (1,874 SQ FT)
TOTAL PARKING SPACES REQUIRED: 57 EA (1,549 SQ FT)
LOADING ZONE: 1 EA

CONSULTANTS

ARCHITECT
PREST VUKSIC GREENWOOD ARCHITECTS
JOHN VUKSIC
4400 SAN PABLO AVE., STE. 208
PALM DESERT, CA 92260
760.778.8063

LANDSCAPE
ONE FUTURE DESIGN INC.
GUSTAVO MAGANA
P.O. BOX 8703
LA CRUZ, CA 92033
760.916.7473

CIVIL
BRIAN ENGINEERING
BRIAN @BRIANENGINEER
77-470 S PRINCIPAL LANE, SUITE 107
PALM DESERT, CA 92211
760.372.8400

BUILDING SQUARE FOOTAGES

| | | | |
|-----------------------|------------|--|------------|
| BUILDING 1 | | | |
| 1ST FLOOR | 4,791 SF. | | |
| 2ND FLOOR | 4,792 SF. | | |
| BUILDING 2 | | | |
| 1ST FLOOR | 7,208 SF. | | |
| 2ND FLOOR | 7,208 SF. | | |
| BUILDING 3 | | | |
| 1ST FLOOR | 4,791 SF. | | |
| 2ND FLOOR | 4,792 SF. | | |
| BUILDING 4 | | | |
| 1ST FLOOR | 7,208 SF. | | |
| 2ND FLOOR | 7,208 SF. | | |
| BUILDING 5 | | | |
| 1ST FLOOR | 10,476 SF. | | |
| 2ND FLOOR | 10,476 SF. | | |
| PREL. BUILDING | | | |
| 1ST FLOOR | 1,886 SF. | | |
| TOTAL | | | 76,527 SF. |

APARTMENT UNIT BREAKDOWN

| BUILDING | UNIT | BEDS | BATHS | TOTALS |
|----------------------------|-------------|------|-------|------------------|
| BUILDING 1 | UNIT TYPE A | 4 | 2 | 4 BEDS 8 BATHS |
| | UNIT TYPE B | 4 | 2 | 4 BEDS 8 BATHS |
| BUILDING 2 | UNIT TYPE A | 4 | 2 | 4 BEDS 8 BATHS |
| | UNIT TYPE B | 8 | 2 | 12 BEDS 12 BATHS |
| | UNIT TYPE C | 2 | 3 | 6 BEDS 6 BATHS |
| BUILDING 3 | UNIT TYPE A | 4 | 2 | 4 BEDS 8 BATHS |
| | UNIT TYPE B | 4 | 2 | 4 BEDS 8 BATHS |
| BUILDING 4 | UNIT TYPE A | 4 | 2 | 4 BEDS 8 BATHS |
| | UNIT TYPE B | 8 | 2 | 12 BEDS 12 BATHS |
| | UNIT TYPE C | 2 | 3 | 6 BEDS 6 BATHS |
| BUILDING 5 | UNIT TYPE A | 8 | 2 | 16 BEDS 16 BATHS |
| | UNIT TYPE B | 2 | 3 | 20 BEDS 16 BATHS |
| | UNIT TYPE C | 1 | 1 | 2 BEDS 2 BATHS |
| GRAND TOTAL | | | | |
| 118 BEDROOMS | | | | 118 BEDROOMS |
| 118 BATHROOMS | | | | 118 BATHROOMS |
| 1 BEDROOM UNIT TYPE D | | | | 2 UNITS |
| 2 BEDROOMS UNIT TYPE A & B | | | | 82 UNITS |
| 3 BEDROOMS UNIT TYPE C | | | | 4 UNITS |
| TOTAL | | | | 86 UNITS |

| DEVELOPMENT STANDARDS | | |
|-----------------------|----------|----------|
| REQUIREMENT | PROPOSED | REQUIRED |
| LOT SETBACK | 5' | 5' |
| FRONT SETBACK | 5' | 5' |
| REAR SETBACK | 5' | 5' |
| SIDE SETBACK | 5' | 5' |
| MIN. COVER | 5' | 5' |
| MIN. COVER | 5' | 5' |
| MIN. COVER | 5' | 5' |
| MIN. COVER | 5' | 5' |
| MIN. COVER | 5' | 5' |
| MIN. COVER | 5' | 5' |
| MIN. COVER | 5' | 5' |
| MIN. COVER | 5' | 5' |

| OPEN SPACE TABULATION | | |
|-----------------------|----------|----------|
| REQUIREMENT | PROPOSED | REQUIRED |
| MIN. LOT | 5% | 5% |
| MIN. LOT | 5% | 5% |
| MIN. LOT | 5% | 5% |
| MIN. LOT | 5% | 5% |
| MIN. LOT | 5% | 5% |
| MIN. LOT | 5% | 5% |
| MIN. LOT | 5% | 5% |
| MIN. LOT | 5% | 5% |
| MIN. LOT | 5% | 5% |
| MIN. LOT | 5% | 5% |



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SHEET INDEX

Sheet: _____
Block: _____

A001

JULY 27, 2023

SITE PLAN

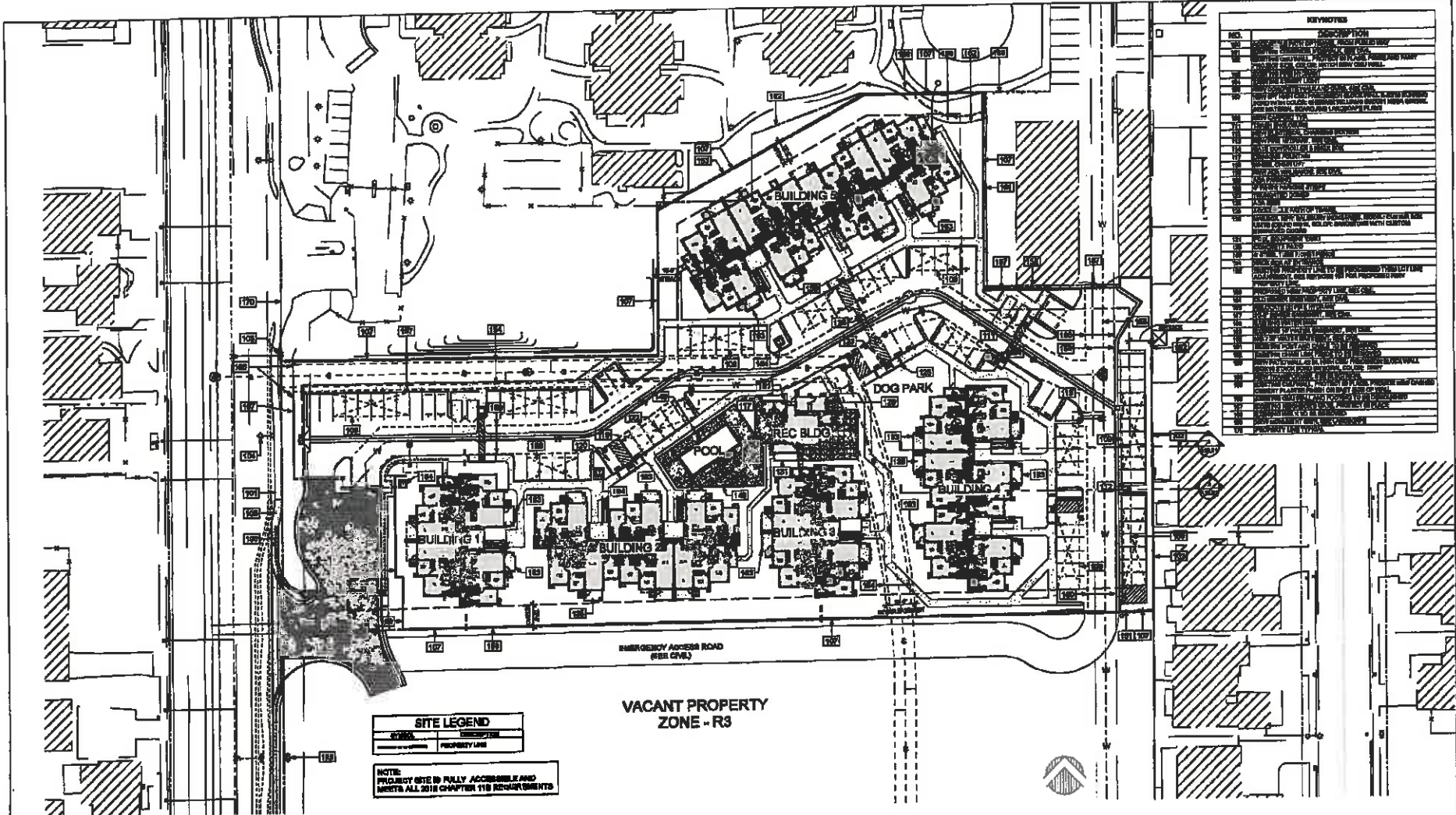
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CATHEDRAL CANYON DR - CATHEDRAL CITY, CALIFORNIA



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WWW.PVGSICARCHITECTS.COM
INFO@PVGSICARCHITECTS.COM | 951.779.4881

JULY 27, 2009



KEYNOTES

| NO. | DESCRIPTION |
|-----|------------------------------|
| 01 | EXISTING LOT LINES |
| 02 | EXISTING BUILDING FOOTPRINTS |
| 03 | EXISTING DRIVEWAYS |
| 04 | EXISTING SIDEWALKS |
| 05 | EXISTING UTILITIES |
| 06 | EXISTING EROSION CONTROL |
| 07 | EXISTING LANDSCAPE PLANS |
| 08 | EXISTING TREES |
| 09 | EXISTING FENCES |
| 10 | EXISTING SIGNAGE |
| 11 | EXISTING LIGHTING |
| 12 | EXISTING PAVEMENT |
| 13 | EXISTING CONCRETE |
| 14 | EXISTING ASPHALT |
| 15 | EXISTING GRAVEL |
| 16 | EXISTING SAND |
| 17 | EXISTING DIRT |
| 18 | EXISTING VEGETATION |
| 19 | EXISTING SOIL |
| 20 | EXISTING WATER |
| 21 | EXISTING SEWER |
| 22 | EXISTING GAS |
| 23 | EXISTING ELECTRIC |
| 24 | EXISTING TELEPHONE |
| 25 | EXISTING CABLE |
| 26 | EXISTING FIBER OPTIC |
| 27 | EXISTING RAILROAD |
| 28 | EXISTING HIGHWAY |
| 29 | EXISTING AIRPORT |
| 30 | EXISTING CANAL |
| 31 | EXISTING DAM |
| 32 | EXISTING BRIDGE |
| 33 | EXISTING TUNNEL |
| 34 | EXISTING ELEVATOR |
| 35 | EXISTING ESCALATOR |
| 36 | EXISTING STAIR |
| 37 | EXISTING RAMP |
| 38 | EXISTING CURB |
| 39 | EXISTING GUTTER |
| 40 | EXISTING DRAIN |
| 41 | EXISTING MANHOLE |
| 42 | EXISTING VALVE |
| 43 | EXISTING PUMP |
| 44 | EXISTING TOWER |
| 45 | EXISTING ANTENNA |
| 46 | EXISTING SIGN |
| 47 | EXISTING LIGHT |
| 48 | EXISTING METER |
| 49 | EXISTING BOX |
| 50 | EXISTING ENCLOSURE |
| 51 | EXISTING FENCE |
| 52 | EXISTING GATE |
| 53 | EXISTING WALL |
| 54 | EXISTING COLUMN |
| 55 | EXISTING BEAM |
| 56 | EXISTING JOIST |
| 57 | EXISTING RAFTER |
| 58 | EXISTING TRUSS |
| 59 | EXISTING ROOF |
| 60 | EXISTING FLOOR |
| 61 | EXISTING CEILING |
| 62 | EXISTING WALL |
| 63 | EXISTING DOOR |
| 64 | EXISTING WINDOW |
| 65 | EXISTING PORCH |
| 66 | EXISTING PATIO |
| 67 | EXISTING DECK |
| 68 | EXISTING BALCONY |
| 69 | EXISTING TERRACE |
| 70 | EXISTING DRIVEWAY |
| 71 | EXISTING SIDEWALK |
| 72 | EXISTING STAIR |
| 73 | EXISTING RAMP |
| 74 | EXISTING CURB |
| 75 | EXISTING GUTTER |
| 76 | EXISTING DRAIN |
| 77 | EXISTING MANHOLE |
| 78 | EXISTING VALVE |
| 79 | EXISTING PUMP |
| 80 | EXISTING TOWER |
| 81 | EXISTING ANTENNA |
| 82 | EXISTING SIGN |
| 83 | EXISTING LIGHT |
| 84 | EXISTING METER |
| 85 | EXISTING BOX |
| 86 | EXISTING ENCLOSURE |
| 87 | EXISTING FENCE |
| 88 | EXISTING GATE |
| 89 | EXISTING WALL |
| 90 | EXISTING COLUMN |
| 91 | EXISTING BEAM |
| 92 | EXISTING JOIST |
| 93 | EXISTING RAFTER |
| 94 | EXISTING TRUSS |
| 95 | EXISTING ROOF |
| 96 | EXISTING FLOOR |
| 97 | EXISTING CEILING |
| 98 | EXISTING WALL |
| 99 | EXISTING DOOR |
| 100 | EXISTING WINDOW |
| 101 | EXISTING PORCH |
| 102 | EXISTING PATIO |
| 103 | EXISTING DECK |
| 104 | EXISTING BALCONY |
| 105 | EXISTING TERRACE |
| 106 | EXISTING DRIVEWAY |
| 107 | EXISTING SIDEWALK |
| 108 | EXISTING STAIR |
| 109 | EXISTING RAMP |
| 110 | EXISTING CURB |
| 111 | EXISTING GUTTER |
| 112 | EXISTING DRAIN |
| 113 | EXISTING MANHOLE |
| 114 | EXISTING VALVE |
| 115 | EXISTING PUMP |
| 116 | EXISTING TOWER |
| 117 | EXISTING ANTENNA |
| 118 | EXISTING SIGN |
| 119 | EXISTING LIGHT |
| 120 | EXISTING METER |
| 121 | EXISTING BOX |
| 122 | EXISTING ENCLOSURE |
| 123 | EXISTING FENCE |
| 124 | EXISTING GATE |
| 125 | EXISTING WALL |
| 126 | EXISTING COLUMN |
| 127 | EXISTING BEAM |
| 128 | EXISTING JOIST |
| 129 | EXISTING RAFTER |
| 130 | EXISTING TRUSS |
| 131 | EXISTING ROOF |
| 132 | EXISTING FLOOR |
| 133 | EXISTING CEILING |
| 134 | EXISTING WALL |
| 135 | EXISTING DOOR |
| 136 | EXISTING WINDOW |
| 137 | EXISTING PORCH |
| 138 | EXISTING PATIO |
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| 140 | EXISTING BALCONY |
| 141 | EXISTING TERRACE |
| 142 | EXISTING DRIVEWAY |
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| 144 | EXISTING STAIR |
| 145 | EXISTING RAMP |
| 146 | EXISTING CURB |
| 147 | EXISTING GUTTER |
| 148 | EXISTING DRAIN |
| 149 | EXISTING MANHOLE |
| 150 | EXISTING VALVE |
| 151 | EXISTING PUMP |
| 152 | EXISTING TOWER |
| 153 | EXISTING ANTENNA |
| 154 | EXISTING SIGN |
| 155 | EXISTING LIGHT |
| 156 | EXISTING METER |
| 157 | EXISTING BOX |
| 158 | EXISTING ENCLOSURE |
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| 162 | EXISTING COLUMN |
| 163 | EXISTING BEAM |
| 164 | EXISTING JOIST |
| 165 | EXISTING RAFTER |
| 166 | EXISTING TRUSS |
| 167 | EXISTING ROOF |
| 168 | EXISTING FLOOR |
| 169 | EXISTING CEILING |
| 170 | EXISTING WALL |
| 171 | EXISTING DOOR |
| 172 | EXISTING WINDOW |
| 173 | EXISTING PORCH |
| 174 | EXISTING PATIO |
| 175 | EXISTING DECK |
| 176 | EXISTING BALCONY |
| 177 | EXISTING TERRACE |
| 178 | EXISTING DRIVEWAY |
| 179 | EXISTING SIDEWALK |
| 180 | EXISTING STAIR |
| 181 | EXISTING RAMP |
| 182 | EXISTING CURB |
| 183 | EXISTING GUTTER |
| 184 | EXISTING DRAIN |
| 185 | EXISTING MANHOLE |
| 186 | EXISTING VALVE |
| 187 | EXISTING PUMP |
| 188 | EXISTING TOWER |
| 189 | EXISTING ANTENNA |
| 190 | EXISTING SIGN |
| 191 | EXISTING LIGHT |
| 192 | EXISTING METER |
| 193 | EXISTING BOX |
| 194 | EXISTING ENCLOSURE |
| 195 | EXISTING FENCE |
| 196 | EXISTING GATE |
| 197 | EXISTING WALL |
| 198 | EXISTING COLUMN |
| 199 | EXISTING BEAM |
| 200 | EXISTING JOIST |
| 201 | EXISTING RAFTER |
| 202 | EXISTING TRUSS |
| 203 | EXISTING ROOF |
| 204 | EXISTING FLOOR |
| 205 | EXISTING CEILING |
| 206 | EXISTING WALL |
| 207 | EXISTING DOOR |
| 208 | EXISTING WINDOW |
| 209 | EXISTING PORCH |
| 210 | EXISTING PATIO |
| 211 | EXISTING DECK |
| 212 | EXISTING BALCONY |
| 213 | EXISTING TERRACE |
| 214 | EXISTING DRIVEWAY |
| 215 | EXISTING SIDEWALK |
| 216 | EXISTING STAIR |
| 217 | EXISTING RAMP |
| 218 | EXISTING CURB |
| 219 | EXISTING GUTTER |
| 220 | EXISTING DRAIN |
| 221 | EXISTING MANHOLE |
| 222 | EXISTING VALVE |
| 223 | EXISTING PUMP |
| 224 | EXISTING TOWER |
| 225 | EXISTING ANTENNA |
| 226 | EXISTING SIGN |
| 227 | EXISTING LIGHT |
| 228 | EXISTING METER |
| 229 | EXISTING BOX |
| 230 | EXISTING ENCLOSURE |
| 231 | EXISTING FENCE |
| 232 | EXISTING GATE |
| 233 | EXISTING WALL |
| 234 | EXISTING COLUMN |
| 235 | EXISTING BEAM |
| 236 | EXISTING JOIST |
| 237 | EXISTING RAFTER |
| 238 | EXISTING TRUSS |
| 239 | EXISTING ROOF |
| 240 | EXISTING FLOOR |
| 241 | EXISTING CEILING |
| 242 | EXISTING WALL |
| 243 | EXISTING DOOR |
| 244 | EXISTING WINDOW |
| 245 | EXISTING PORCH |
| 246 | EXISTING PATIO |
| 247 | EXISTING DECK |
| 248 | EXISTING BALCONY |
| 249 | EXISTING TERRACE |
| 250 | EXISTING DRIVEWAY |

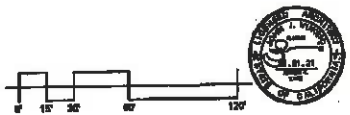
SITE LEGEND

| SYMBOL | DESCRIPTION |
|----------------|-----------------|
| (Hatched Area) | VACANT PROPERTY |
| (Solid Line) | PROPERTY LINE |

NOTE:
PROJECT SITE IS FULLY ACCESSIBLE AND
MEETS ALL 2018 CHAPTER 11B REQUIREMENTS.

VACANT PROPERTY
ZONE - R3

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SITE PLAN
Scale: As Indicated
Sheet: **A010.0**

JULY 27, 2024

BUILDING 1 & 3

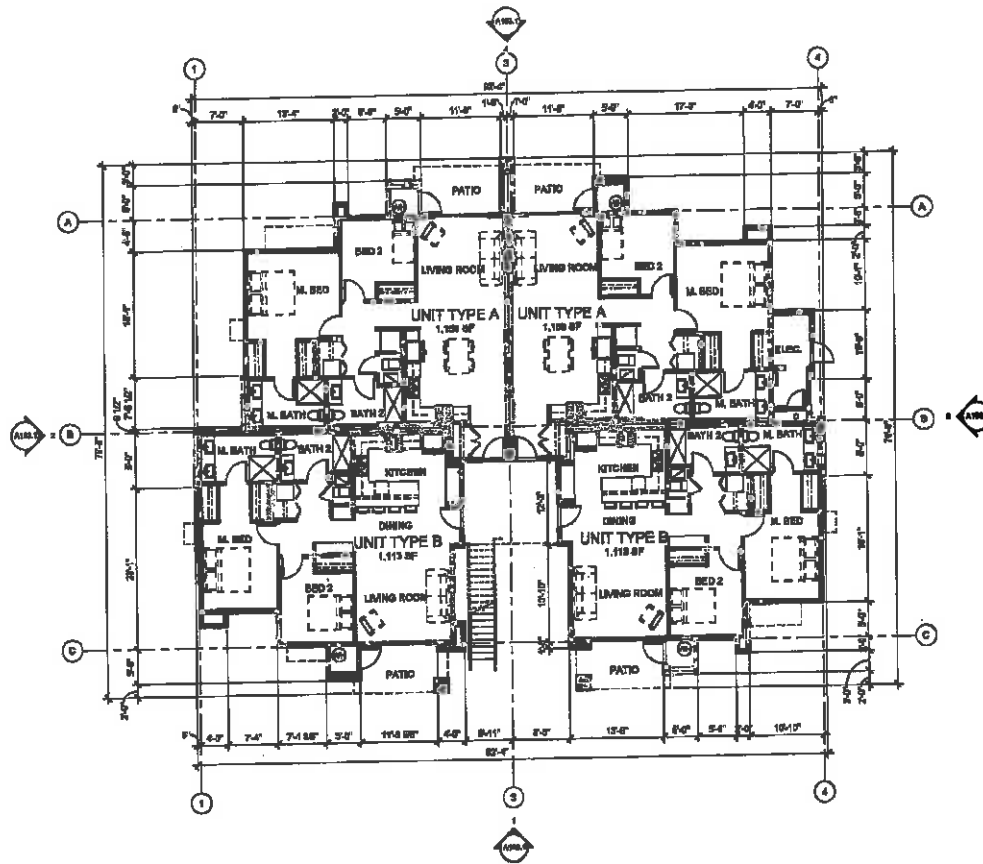
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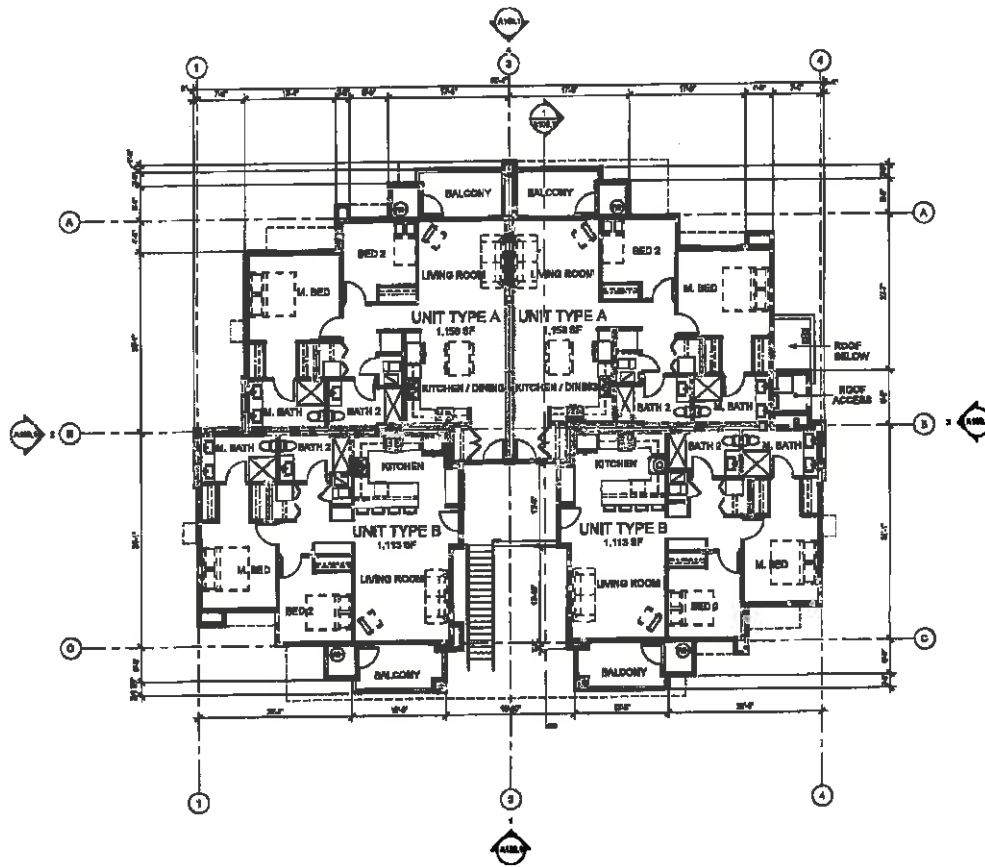


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BUILDINGS 1 & 3
1ST FLOOR PLAN
 Scale: 1/4" = 1'-0"
 Sheet: **A101.1**

DATE: 08/20/2009



KEY LEGEND



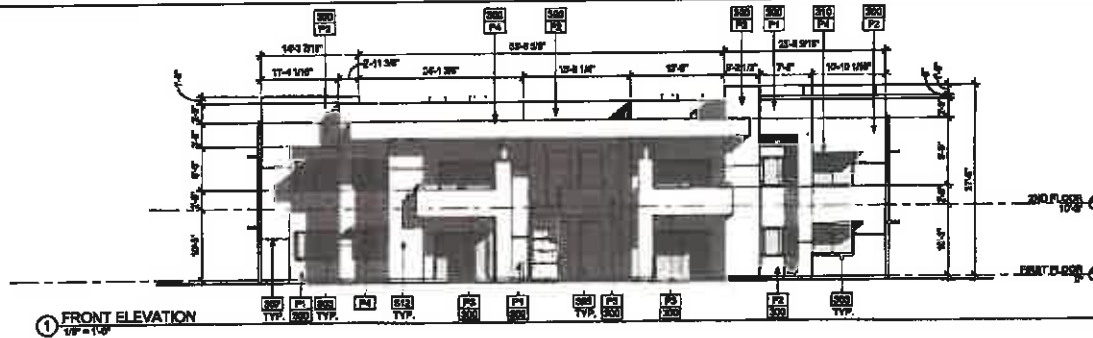
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 4430 BEN PARKS AVE. #200 | PALM SPRING, CA 92260
 WWW.PVIAARCHITECTS.COM
 INFO@PVIAARCHITECTS.COM | 760.779.4091

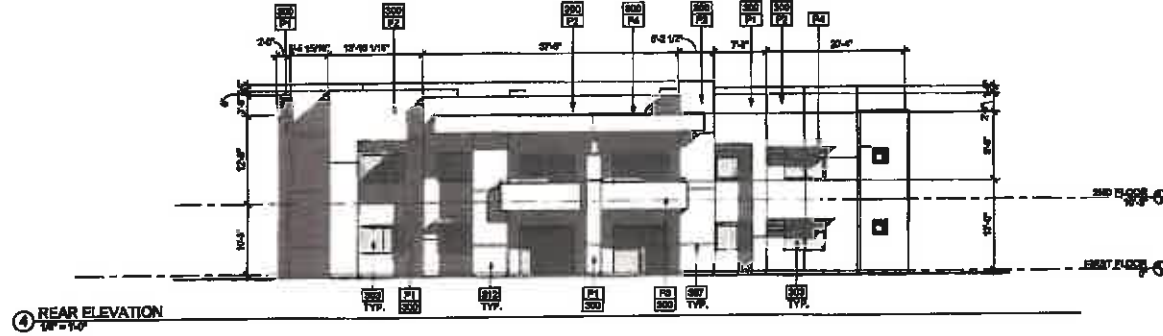
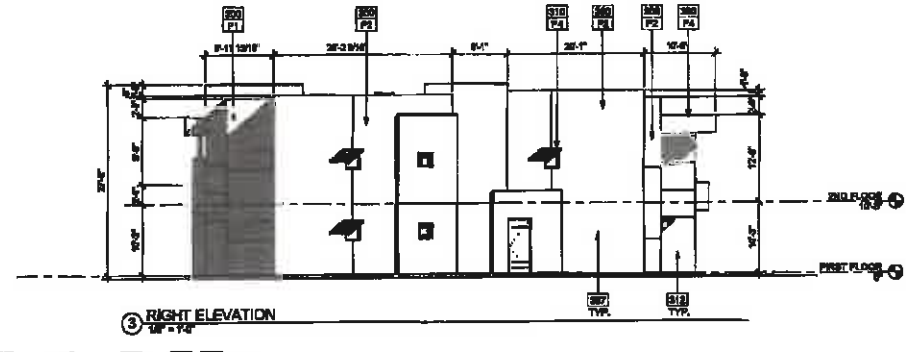
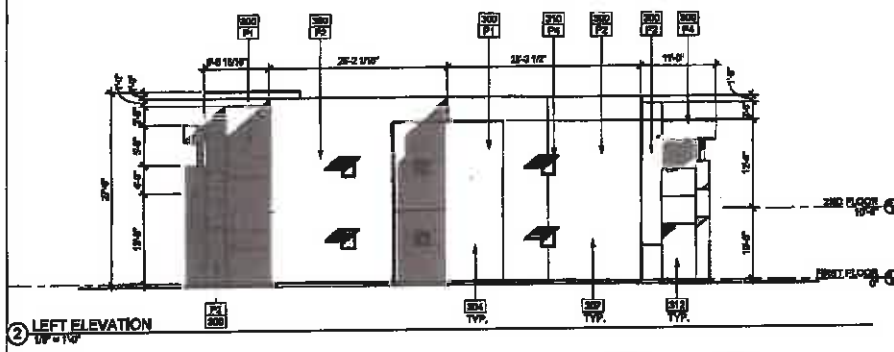
BUILDINGS 1 & 3
2ND FLOOR PLAN
 Scale: 1/8" = 1'-0"
 Sheet: **A102.1**

09/15/17 JXD



| KEYNOTE LEGEND | |
|----------------|---|
| NO. | DESCRIPTION |
| P1 | 1/2" (12.5mm) GRANITE PORTLAND CEMENT PLASTER WITH 1/2" (12.5mm) POLYMER PORTLAND CEMENT SAND FINISH OVER 1/2" (12.5mm) GYP BOARD |
| P2 | 1/2" (12.5mm) GRANITE PORTLAND CEMENT PLASTER WITH 1/2" (12.5mm) POLYMER PORTLAND CEMENT SAND FINISH OVER 1/2" (12.5mm) GYP BOARD |
| P3 | 1/2" (12.5mm) GRANITE PORTLAND CEMENT PLASTER WITH 1/2" (12.5mm) POLYMER PORTLAND CEMENT SAND FINISH OVER 1/2" (12.5mm) GYP BOARD |
| P4 | 1/2" (12.5mm) GRANITE PORTLAND CEMENT PLASTER WITH 1/2" (12.5mm) POLYMER PORTLAND CEMENT SAND FINISH OVER 1/2" (12.5mm) GYP BOARD |
| P5 | 1/2" (12.5mm) GRANITE PORTLAND CEMENT PLASTER WITH 1/2" (12.5mm) POLYMER PORTLAND CEMENT SAND FINISH OVER 1/2" (12.5mm) GYP BOARD |
| P6 | 1/2" (12.5mm) GRANITE PORTLAND CEMENT PLASTER WITH 1/2" (12.5mm) POLYMER PORTLAND CEMENT SAND FINISH OVER 1/2" (12.5mm) GYP BOARD |
| P7 | 1/2" (12.5mm) GRANITE PORTLAND CEMENT PLASTER WITH 1/2" (12.5mm) POLYMER PORTLAND CEMENT SAND FINISH OVER 1/2" (12.5mm) GYP BOARD |
| P8 | 1/2" (12.5mm) GRANITE PORTLAND CEMENT PLASTER WITH 1/2" (12.5mm) POLYMER PORTLAND CEMENT SAND FINISH OVER 1/2" (12.5mm) GYP BOARD |
| P9 | 1/2" (12.5mm) GRANITE PORTLAND CEMENT PLASTER WITH 1/2" (12.5mm) POLYMER PORTLAND CEMENT SAND FINISH OVER 1/2" (12.5mm) GYP BOARD |
| P10 | 1/2" (12.5mm) GRANITE PORTLAND CEMENT PLASTER WITH 1/2" (12.5mm) POLYMER PORTLAND CEMENT SAND FINISH OVER 1/2" (12.5mm) GYP BOARD |

| PAINT LEGEND | |
|--------------|---|
| NO. | DESCRIPTION |
| P1 | 1/2" (12.5mm) GRANITE PORTLAND CEMENT PLASTER WITH 1/2" (12.5mm) POLYMER PORTLAND CEMENT SAND FINISH OVER 1/2" (12.5mm) GYP BOARD |
| P2 | 1/2" (12.5mm) GRANITE PORTLAND CEMENT PLASTER WITH 1/2" (12.5mm) POLYMER PORTLAND CEMENT SAND FINISH OVER 1/2" (12.5mm) GYP BOARD |
| P3 | 1/2" (12.5mm) GRANITE PORTLAND CEMENT PLASTER WITH 1/2" (12.5mm) POLYMER PORTLAND CEMENT SAND FINISH OVER 1/2" (12.5mm) GYP BOARD |
| P4 | 1/2" (12.5mm) GRANITE PORTLAND CEMENT PLASTER WITH 1/2" (12.5mm) POLYMER PORTLAND CEMENT SAND FINISH OVER 1/2" (12.5mm) GYP BOARD |

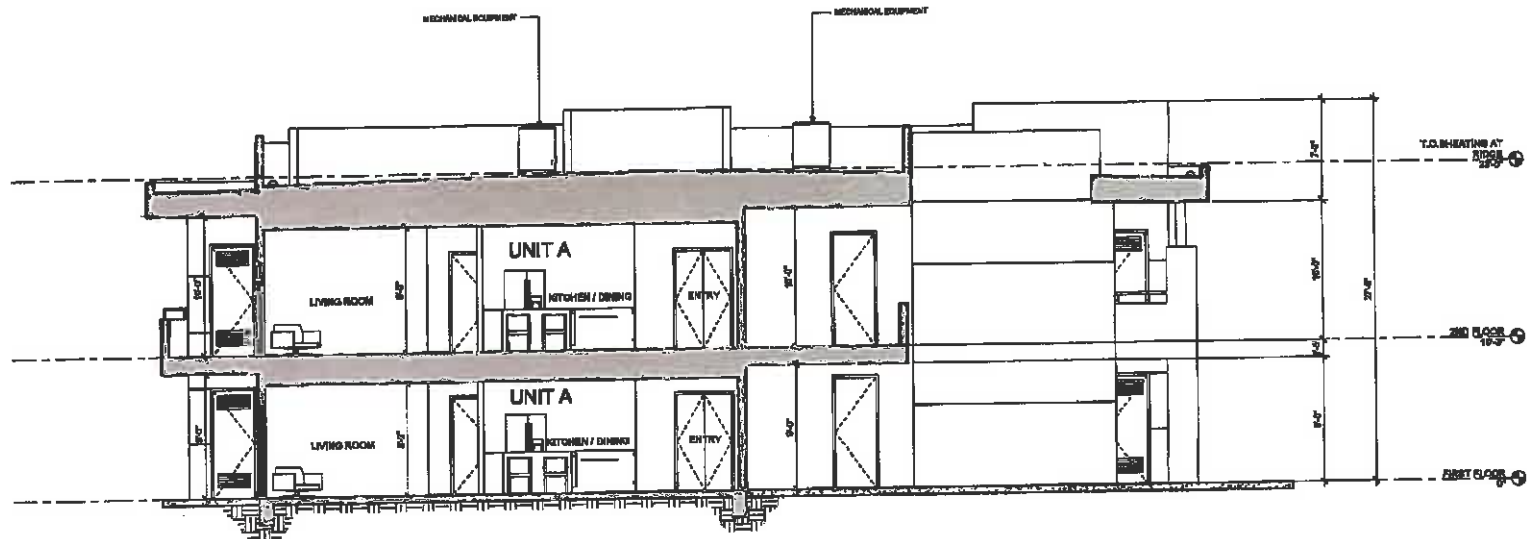


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 WWW.PVGMARCHITECTS.COM
 INFO@PVGMARCHITECTS.COM | 714.771.1887

BUILDINGS 1 & 3
EXTERIOR ELEVATIONS
 Scale: 1/8" = 1'-0"
 Sheet: **A103.1**
 DATE: 07/18/2020



1 BUILDING SECTION 1

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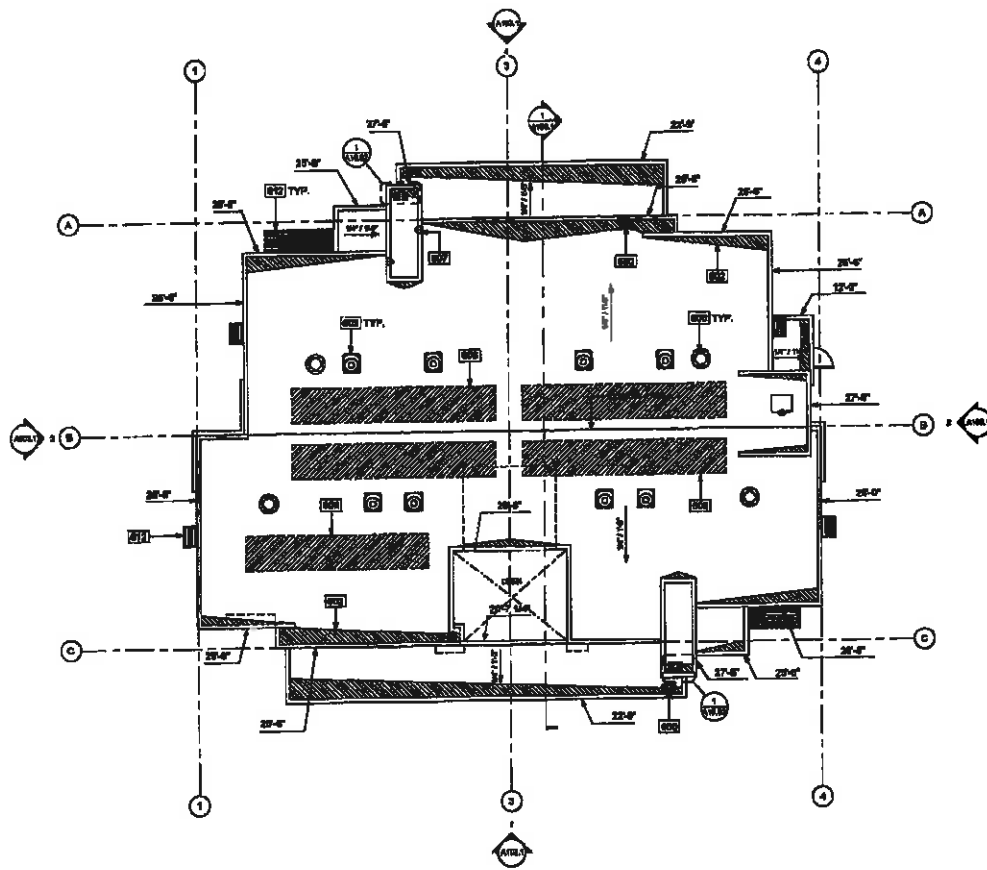
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4432 SAN PABLO AVENUE | SAN MARINO, CA 91108
 WWW.PVGAARCHITECTS.COM
 916.279.4000 | 916.279.4001

BUILDINGS 1 & 3
BUILDING SECTION

Scale: 1/4" = 1'-0"
 Date: **A106.1**

DATE PLOTTED: 11/11/2011 10:58 AM



| KEYNOT & LEGEND | |
|-----------------|--|
| NO. | DESCRIPTION |
| 1 | ROOF DRAINAGE SYSTEM, DOWN SLOPE DRAIN TO UNDERGROUND DRAINAGE SYSTEM AND DRAINAGE TO STREET |
| 2 | ROOF DRAINAGE SYSTEM, DOWN SLOPE DRAIN TO STREET |
| 3 | ROOF DRAINAGE SYSTEM, DOWN SLOPE DRAIN TO STREET |
| 4 | ROOF DRAINAGE SYSTEM, DOWN SLOPE DRAIN TO STREET |
| 5 | ROOF DRAINAGE SYSTEM, DOWN SLOPE DRAIN TO STREET |
| 6 | ROOF DRAINAGE SYSTEM, DOWN SLOPE DRAIN TO STREET |
| 7 | ROOF DRAINAGE SYSTEM, DOWN SLOPE DRAIN TO STREET |
| 8 | ROOF DRAINAGE SYSTEM, DOWN SLOPE DRAIN TO STREET |
| 9 | ROOF DRAINAGE SYSTEM, DOWN SLOPE DRAIN TO STREET |
| 10 | ROOF DRAINAGE SYSTEM, DOWN SLOPE DRAIN TO STREET |

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 951-497-0000 | FAX: 951-381-1111

BUILDINGS 1 & 3
ROOF PLAN
 Scale: 1/8" = 1'-0"
 Sheet: **A107.1**

JUNE 18, 2020

BUILDING 2 & 4

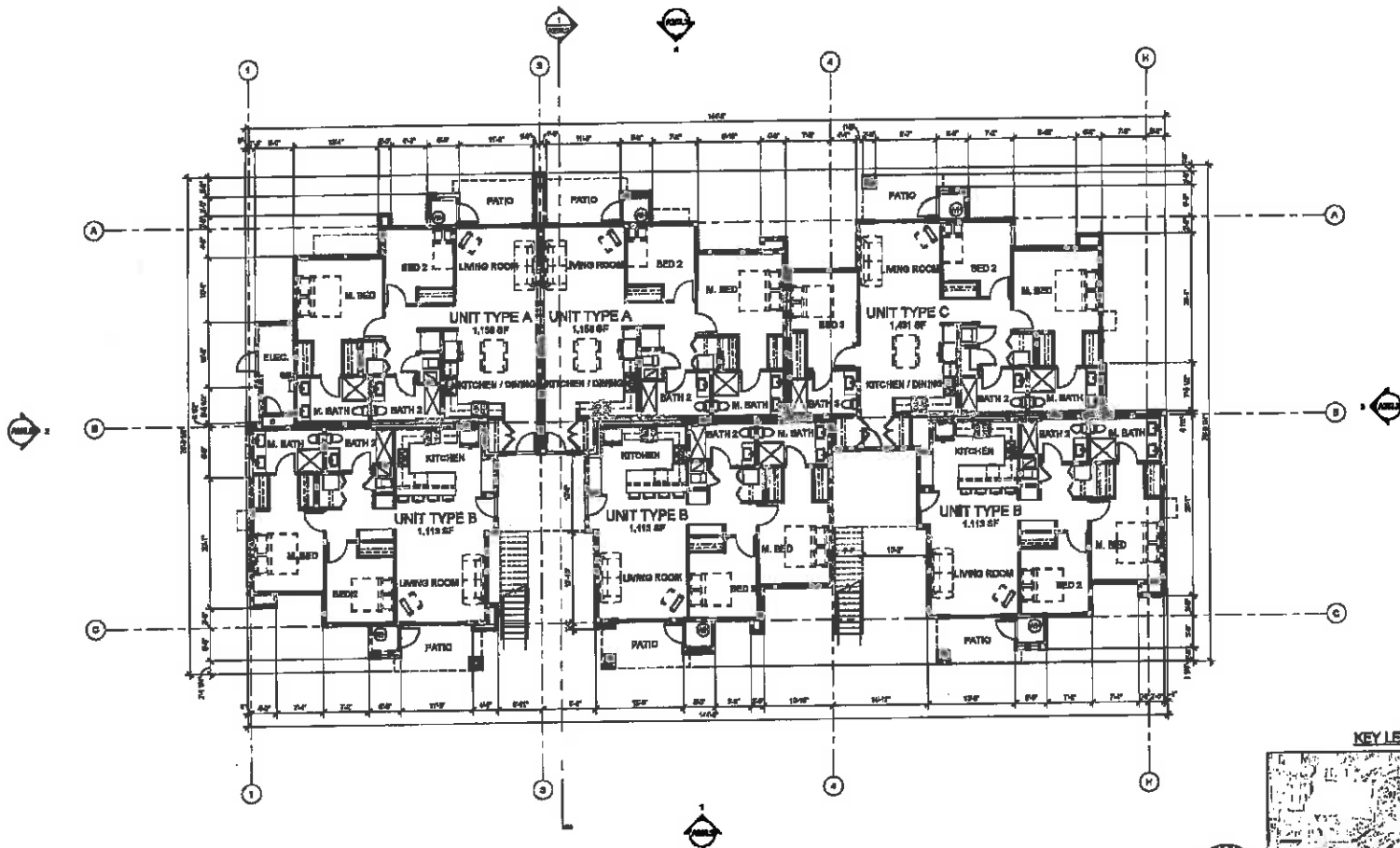
CANYON SPRINGS VILLAS
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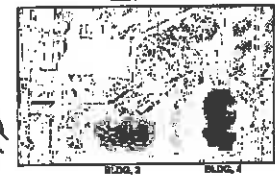
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JULY 27, 2010



KEY LEGEND



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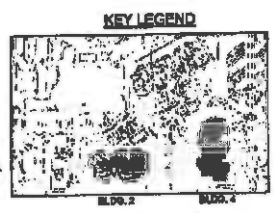
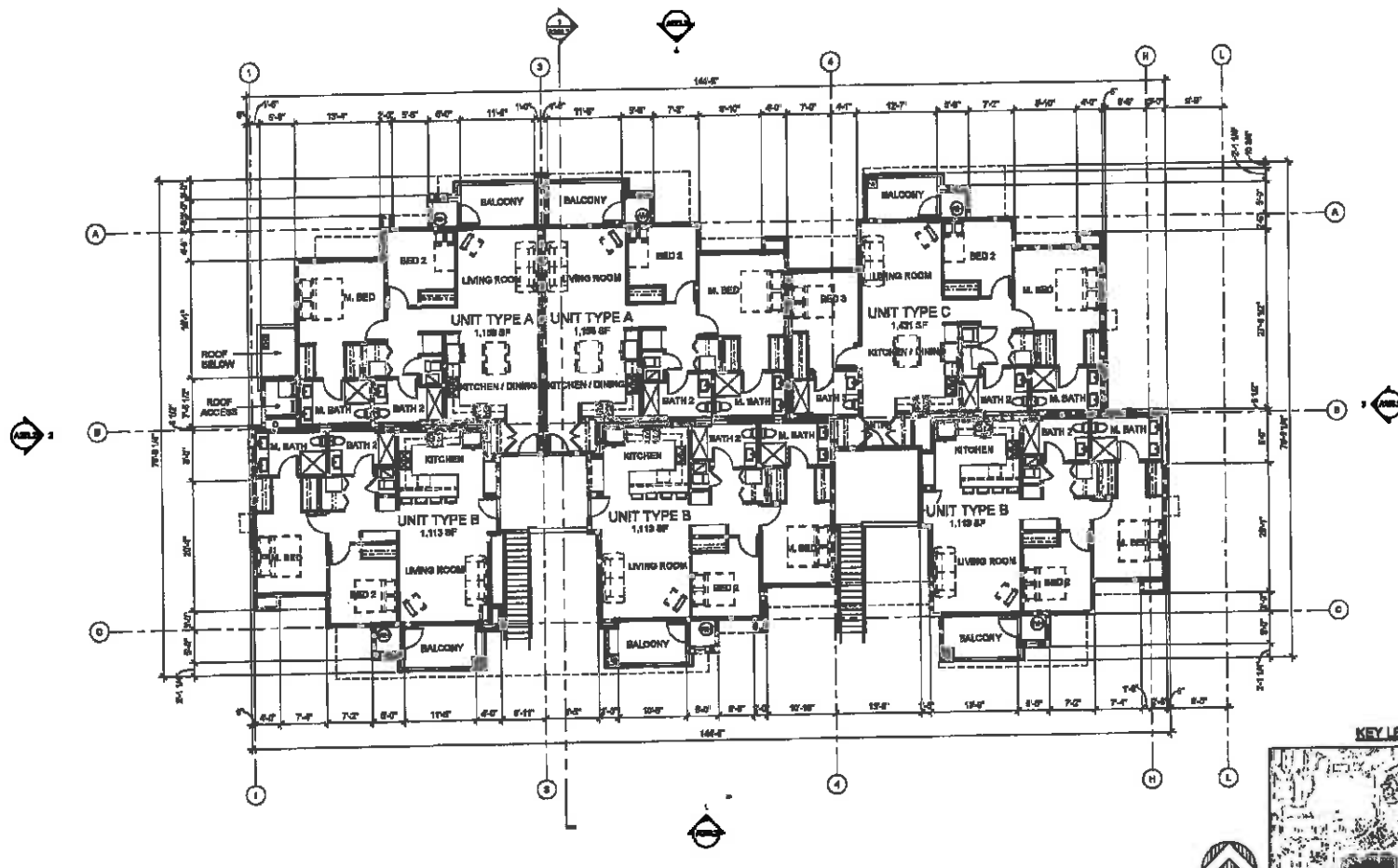


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 951.974.8200 | 760.777.0017

BUILDINGS 2 & 4
1ST FLOOR PLAN

Scale: 1/8" = 1'-0"
 Sheet: **A201.2**

08/11/17 - 08/11/17



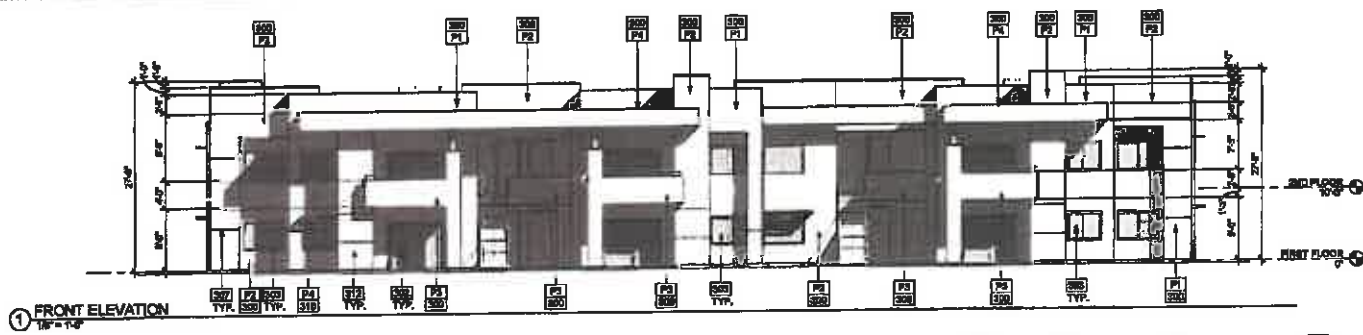
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 4128 SANTA FE AVE, SUITE 200 | PALMDALE, CA 93550
 WWW.PVWGARCHITECTS.COM
 PHONE: 760.977.2931

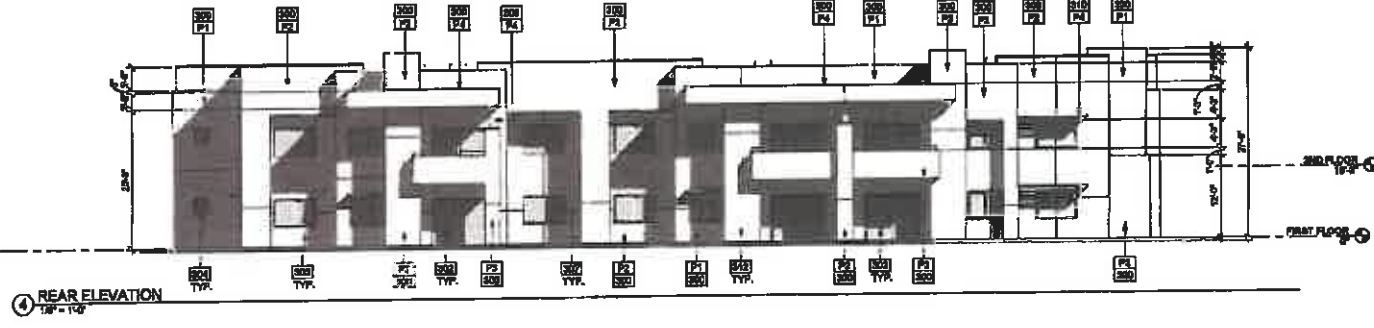
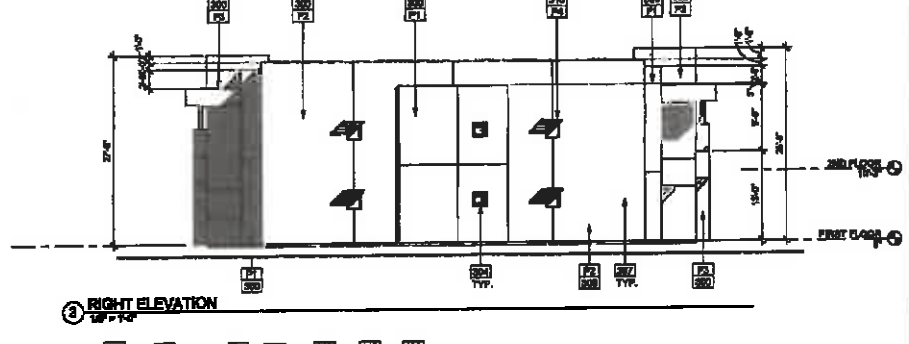
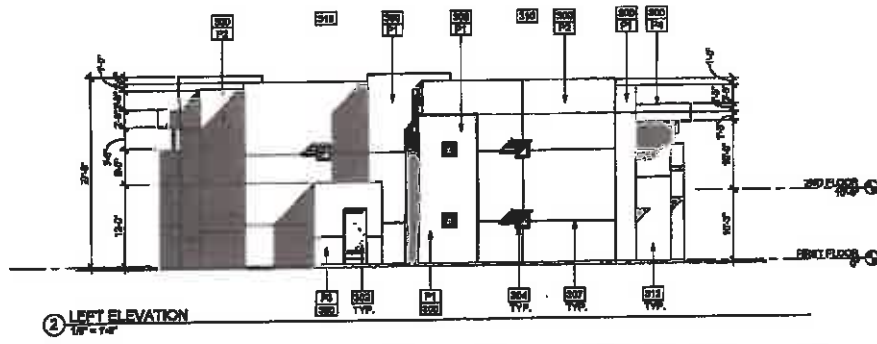
BUILDINGS 2 & 4
2ND FLOOR PLAN
 Scale: 1/8" = 1'-0"
 Date: **A202.2**

MAY 18, 2023



| KEYNOTE LEGEND | |
|----------------|--|
| NO. | DESCRIPTION |
| 100 | 2" X 4" CONCRETE BLOCK, PRECAST CONCRETE PLASTER WITH COATED METAL SAND FINISH, 1/8" PAINT ACCESSIBLE FOR COAT |
| 200 | 3/4" X 1 1/4" SLIP RESISTANT ALUM. DOOR, ANODIZED BRASS BRONZE FINISH, 1/8" PAINT ACCESSIBLE FOR COAT |
| 300 | 1/2" X 1 1/4" SLIP RESISTANT ALUM. DOOR, ANODIZED BRASS BRONZE FINISH, 1/8" PAINT ACCESSIBLE FOR COAT |
| 400 | 1/2" X 1 1/4" SLIP RESISTANT ALUM. DOOR, ANODIZED BRASS BRONZE FINISH, 1/8" PAINT ACCESSIBLE FOR COAT |
| 500 | 1/2" X 1 1/4" SLIP RESISTANT ALUM. DOOR, ANODIZED BRASS BRONZE FINISH, 1/8" PAINT ACCESSIBLE FOR COAT |
| 600 | 1/2" X 1 1/4" SLIP RESISTANT ALUM. DOOR, ANODIZED BRASS BRONZE FINISH, 1/8" PAINT ACCESSIBLE FOR COAT |
| 700 | 1/2" X 1 1/4" SLIP RESISTANT ALUM. DOOR, ANODIZED BRASS BRONZE FINISH, 1/8" PAINT ACCESSIBLE FOR COAT |
| 800 | 1/2" X 1 1/4" SLIP RESISTANT ALUM. DOOR, ANODIZED BRASS BRONZE FINISH, 1/8" PAINT ACCESSIBLE FOR COAT |
| 900 | 1/2" X 1 1/4" SLIP RESISTANT ALUM. DOOR, ANODIZED BRASS BRONZE FINISH, 1/8" PAINT ACCESSIBLE FOR COAT |
| 1000 | 1/2" X 1 1/4" SLIP RESISTANT ALUM. DOOR, ANODIZED BRASS BRONZE FINISH, 1/8" PAINT ACCESSIBLE FOR COAT |
| 1100 | 1/2" X 1 1/4" SLIP RESISTANT ALUM. DOOR, ANODIZED BRASS BRONZE FINISH, 1/8" PAINT ACCESSIBLE FOR COAT |
| 1200 | 1/2" X 1 1/4" SLIP RESISTANT ALUM. DOOR, ANODIZED BRASS BRONZE FINISH, 1/8" PAINT ACCESSIBLE FOR COAT |
| 1300 | 1/2" X 1 1/4" SLIP RESISTANT ALUM. DOOR, ANODIZED BRASS BRONZE FINISH, 1/8" PAINT ACCESSIBLE FOR COAT |
| 1400 | 1/2" X 1 1/4" SLIP RESISTANT ALUM. DOOR, ANODIZED BRASS BRONZE FINISH, 1/8" PAINT ACCESSIBLE FOR COAT |
| 1500 | 1/2" X 1 1/4" SLIP RESISTANT ALUM. DOOR, ANODIZED BRASS BRONZE FINISH, 1/8" PAINT ACCESSIBLE FOR COAT |
| 1600 | 1/2" X 1 1/4" SLIP RESISTANT ALUM. DOOR, ANODIZED BRASS BRONZE FINISH, 1/8" PAINT ACCESSIBLE FOR COAT |
| 1700 | 1/2" X 1 1/4" SLIP RESISTANT ALUM. DOOR, ANODIZED BRASS BRONZE FINISH, 1/8" PAINT ACCESSIBLE FOR COAT |
| 1800 | 1/2" X 1 1/4" SLIP RESISTANT ALUM. DOOR, ANODIZED BRASS BRONZE FINISH, 1/8" PAINT ACCESSIBLE FOR COAT |
| 1900 | 1/2" X 1 1/4" SLIP RESISTANT ALUM. DOOR, ANODIZED BRASS BRONZE FINISH, 1/8" PAINT ACCESSIBLE FOR COAT |
| 2000 | 1/2" X 1 1/4" SLIP RESISTANT ALUM. DOOR, ANODIZED BRASS BRONZE FINISH, 1/8" PAINT ACCESSIBLE FOR COAT |

| PAINT LEGEND | |
|--------------|--------------------------------|
| NO. | DESCRIPTION |
| P1 | 1/8" PAINT ACCESSIBLE FOR COAT |
| P2 | 1/8" PAINT ACCESSIBLE FOR COAT |
| P3 | 1/8" PAINT ACCESSIBLE FOR COAT |
| P4 | 1/8" PAINT ACCESSIBLE FOR COAT |
| P5 | 1/8" PAINT ACCESSIBLE FOR COAT |
| P6 | 1/8" PAINT ACCESSIBLE FOR COAT |
| P7 | 1/8" PAINT ACCESSIBLE FOR COAT |
| P8 | 1/8" PAINT ACCESSIBLE FOR COAT |
| P9 | 1/8" PAINT ACCESSIBLE FOR COAT |
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| P55 | 1/8" PAINT ACCESSIBLE FOR COAT |
| P56 | 1/8" PAINT ACCESSIBLE FOR COAT |
| P57 | 1/8" PAINT ACCESSIBLE FOR COAT |
| P58 | 1/8" PAINT ACCESSIBLE FOR COAT |
| P59 | 1/8" PAINT ACCESSIBLE FOR COAT |
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| P63 | 1/8" PAINT ACCESSIBLE FOR COAT |
| P64 | 1/8" PAINT ACCESSIBLE FOR COAT |
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| P66 | 1/8" PAINT ACCESSIBLE FOR COAT |
| P67 | 1/8" PAINT ACCESSIBLE FOR COAT |
| P68 | 1/8" PAINT ACCESSIBLE FOR COAT |
| P69 | 1/8" PAINT ACCESSIBLE FOR COAT |
| P70 | 1/8" PAINT ACCESSIBLE FOR COAT |
| P71 | 1/8" PAINT ACCESSIBLE FOR COAT |
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| P74 | 1/8" PAINT ACCESSIBLE FOR COAT |
| P75 | 1/8" PAINT ACCESSIBLE FOR COAT |
| P76 | 1/8" PAINT ACCESSIBLE FOR COAT |
| P77 | 1/8" PAINT ACCESSIBLE FOR COAT |
| P78 | 1/8" PAINT ACCESSIBLE FOR COAT |
| P79 | 1/8" PAINT ACCESSIBLE FOR COAT |
| P80 | 1/8" PAINT ACCESSIBLE FOR COAT |
| P81 | 1/8" PAINT ACCESSIBLE FOR COAT |
| P82 | 1/8" PAINT ACCESSIBLE FOR COAT |
| P83 | 1/8" PAINT ACCESSIBLE FOR COAT |
| P84 | 1/8" PAINT ACCESSIBLE FOR COAT |
| P85 | 1/8" PAINT ACCESSIBLE FOR COAT |
| P86 | 1/8" PAINT ACCESSIBLE FOR COAT |
| P87 | 1/8" PAINT ACCESSIBLE FOR COAT |
| P88 | 1/8" PAINT ACCESSIBLE FOR COAT |
| P89 | 1/8" PAINT ACCESSIBLE FOR COAT |
| P90 | 1/8" PAINT ACCESSIBLE FOR COAT |
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| P97 | 1/8" PAINT ACCESSIBLE FOR COAT |
| P98 | 1/8" PAINT ACCESSIBLE FOR COAT |
| P99 | 1/8" PAINT ACCESSIBLE FOR COAT |
| P100 | 1/8" PAINT ACCESSIBLE FOR COAT |



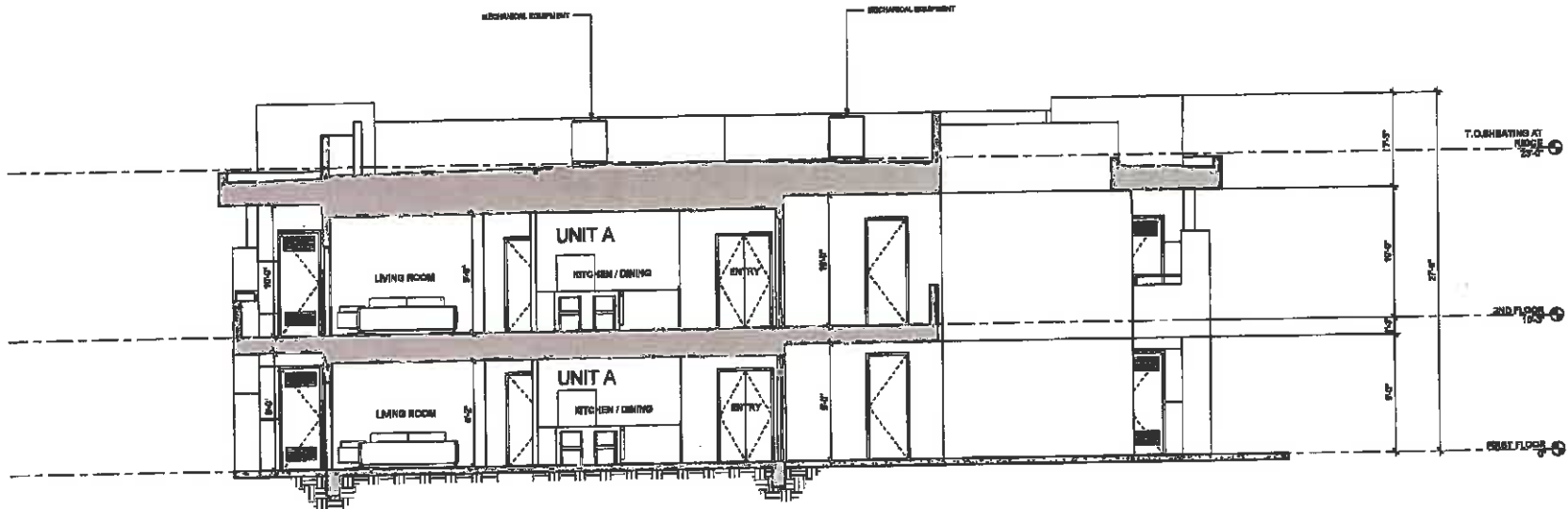
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BUILDINGS 2 & 4
EXTERIOR ELEVATIONS
Scale: 1/8" = 1'-0"
Sheet: **A203.2**

DATE: 07/20/2023



① BUILDING SECTION
1/4" = 1'-0"

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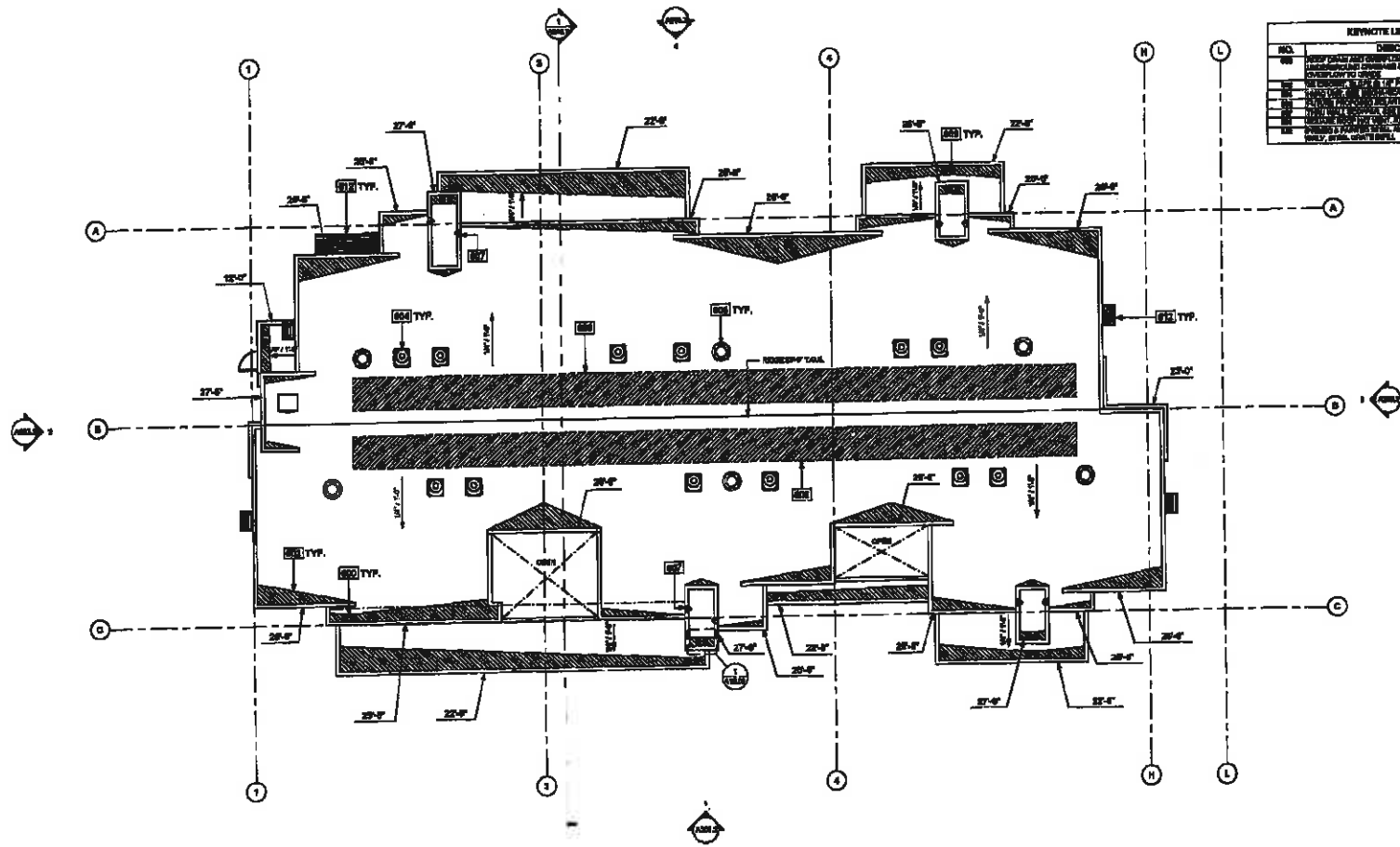
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WWW.PVAGARCHITECTS.COM
INFO@PVAGARCHITECTS.COM | TEL: 714.488.4881

BUILDINGS 2 & 4
BUILDING SECTION

Scale: 1/4" = 1'-0"
Sheet: **A206.2**

DATE: 05/11/2016



| KEYNOTE LEGEND | |
|----------------|--|
| NO. | DESCRIPTION |
| 50 | ROOF CHAIR AND BRACKET WITH VERTICAL ROOF DRAINAGE |
| 51 | ROOF DRAINAGE THROUGH ROOF AND DRAINAGE |
| 52 | ROOF DRAINAGE THROUGH ROOF AND DRAINAGE |
| 53 | ROOF DRAINAGE THROUGH ROOF AND DRAINAGE |
| 54 | ROOF DRAINAGE THROUGH ROOF AND DRAINAGE |
| 55 | ROOF DRAINAGE THROUGH ROOF AND DRAINAGE |
| 56 | ROOF DRAINAGE THROUGH ROOF AND DRAINAGE |
| 57 | ROOF DRAINAGE THROUGH ROOF AND DRAINAGE |
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| 76 | ROOF DRAINAGE THROUGH ROOF AND DRAINAGE |
| 77 | ROOF DRAINAGE THROUGH ROOF AND DRAINAGE |
| 78 | ROOF DRAINAGE THROUGH ROOF AND DRAINAGE |
| 79 | ROOF DRAINAGE THROUGH ROOF AND DRAINAGE |
| 80 | ROOF DRAINAGE THROUGH ROOF AND DRAINAGE |

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BUILDINGS 2 & 4
ROOF PLAN
 Scale: 1/8" = 1'-0"
A207.2

DATE: 05.14.19

BUILDING 5

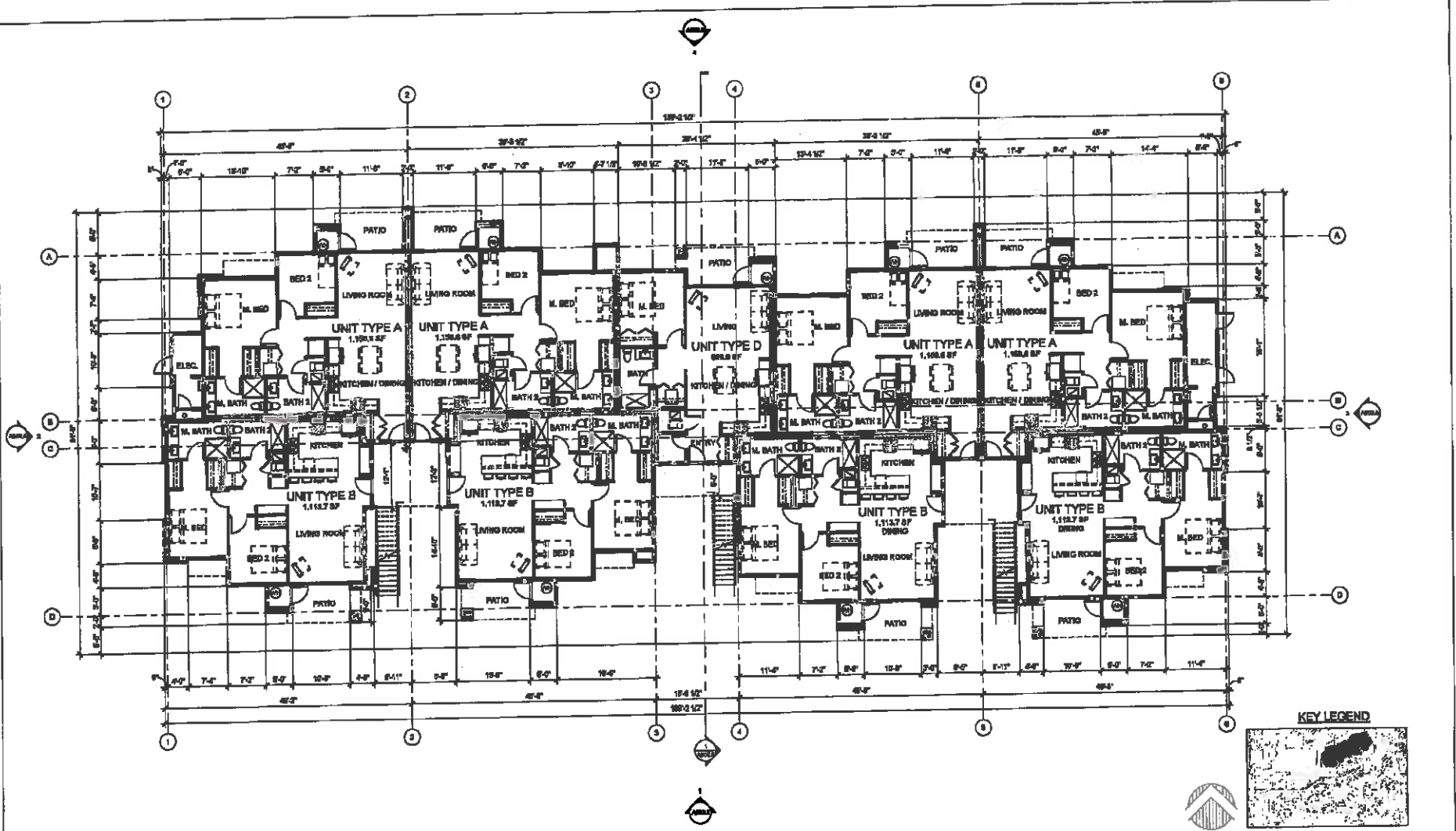
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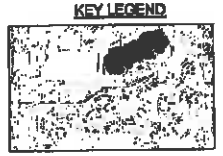
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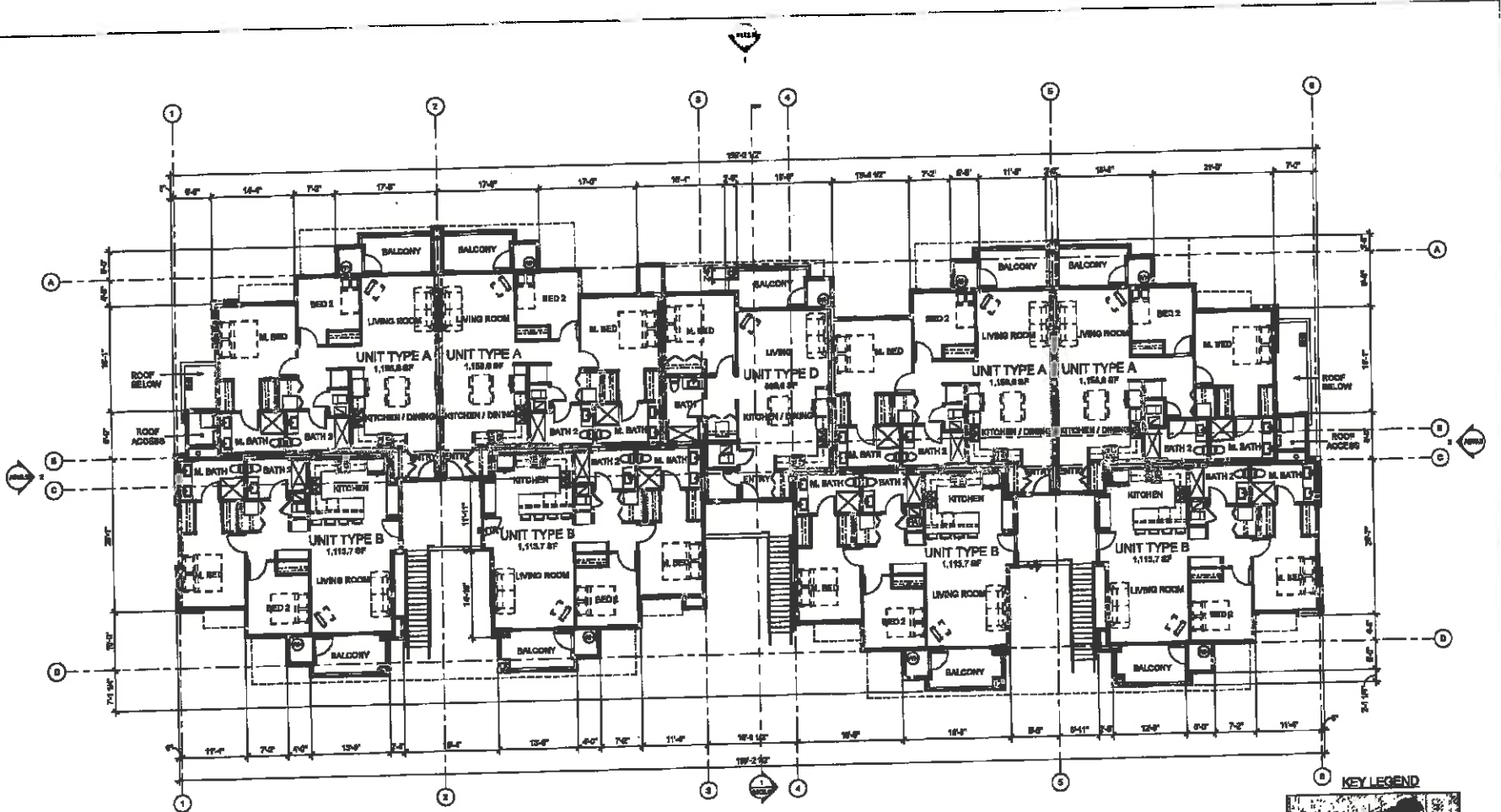


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 516.874.0000 | 714.777.4837

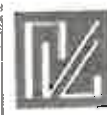


BUILDING 5
1ST FLOOR PLAN
 Scale: 1/8" = 1'-0"
A501.5

MARCH 17, 2020



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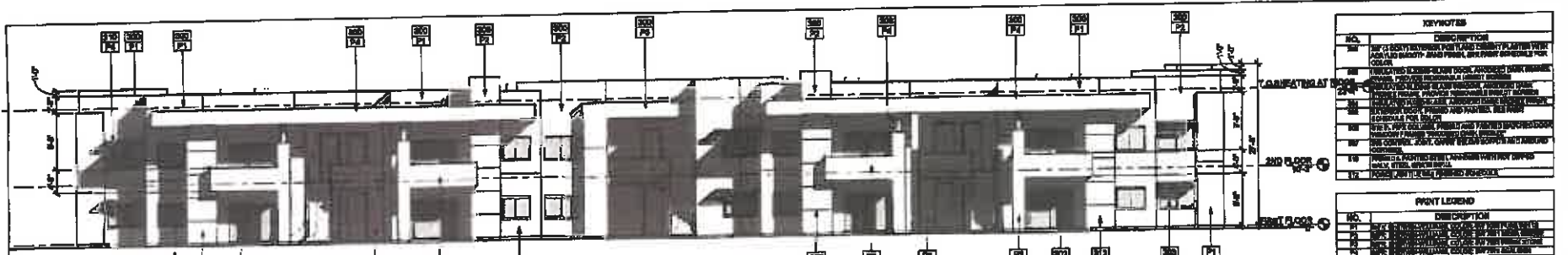


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 WWW.PVGIARCHITECTS.COM
 813.974.0000 | 714.779.8897



BUILDING 5
2ND FLOOR PLAN
 Scale: 1/8" = 1'-0"
A502.5

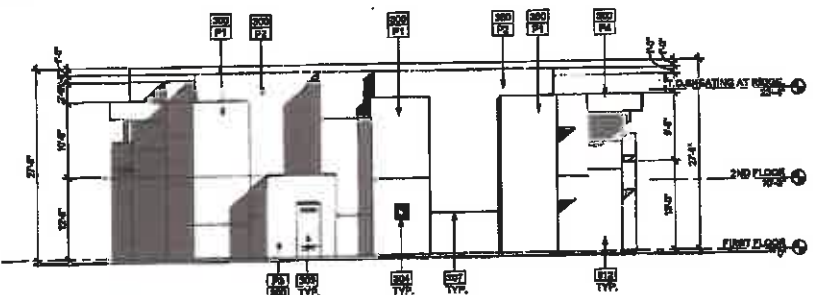
MARCH 17, 2009



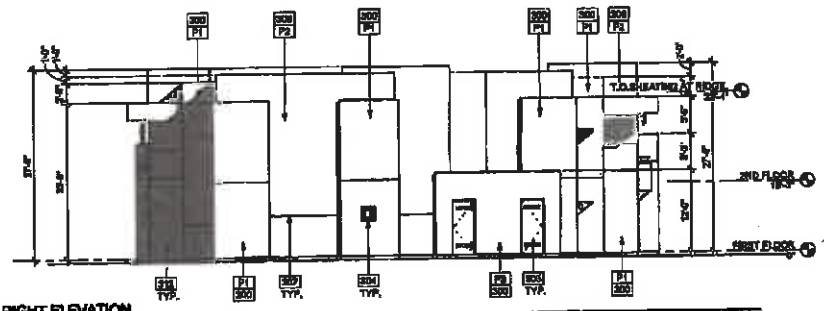
| KEYNOTES | |
|----------|--|
| NO. | DESCRIPTION |
| 1 | SEE 2D EXTERIOR FINISH SCHEDULE FOR FINISHES |
| 2 | SEE 2D EXTERIOR FINISH SCHEDULE FOR FINISHES |
| 3 | SEE 2D EXTERIOR FINISH SCHEDULE FOR FINISHES |
| 4 | SEE 2D EXTERIOR FINISH SCHEDULE FOR FINISHES |
| 5 | SEE 2D EXTERIOR FINISH SCHEDULE FOR FINISHES |
| 6 | SEE 2D EXTERIOR FINISH SCHEDULE FOR FINISHES |
| 7 | SEE 2D EXTERIOR FINISH SCHEDULE FOR FINISHES |
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| 10 | SEE 2D EXTERIOR FINISH SCHEDULE FOR FINISHES |
| 11 | SEE 2D EXTERIOR FINISH SCHEDULE FOR FINISHES |

| FONT LEGEND | |
|-------------|--|
| NO. | DESCRIPTION |
| 1 | SEE 2D EXTERIOR FINISH SCHEDULE FOR FINISHES |
| 2 | SEE 2D EXTERIOR FINISH SCHEDULE FOR FINISHES |
| 3 | SEE 2D EXTERIOR FINISH SCHEDULE FOR FINISHES |
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| 11 | SEE 2D EXTERIOR FINISH SCHEDULE FOR FINISHES |

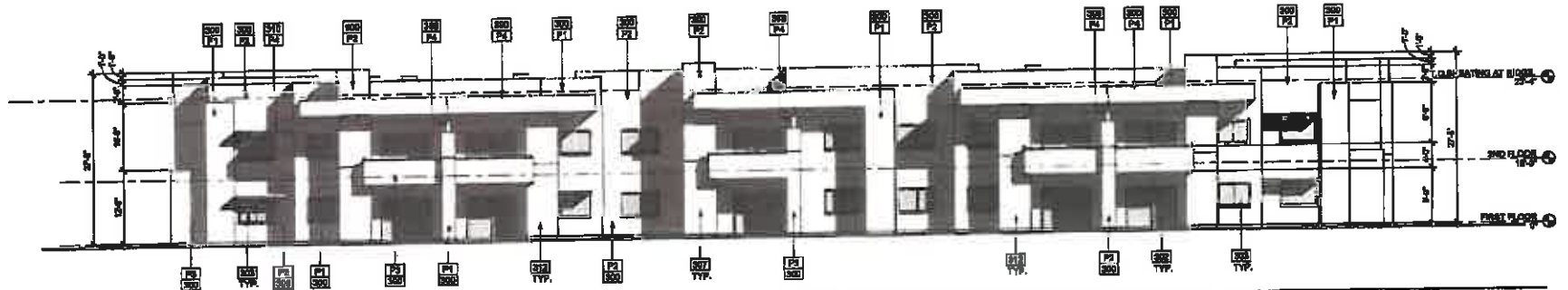
1 FRONT ELEVATION
10'-10"



2 LEFT ELEVATION
10'-10"



3 RIGHT ELEVATION
10'-10"



4 REAR ELEVATION
10'-10"

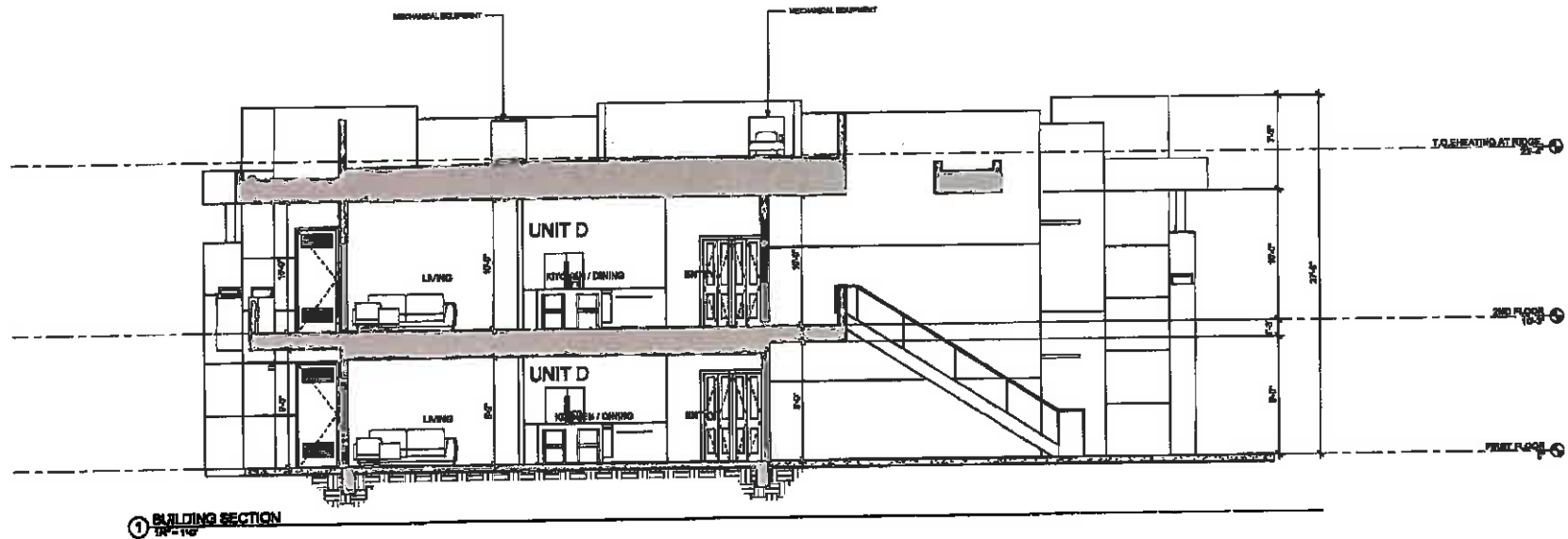
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BUILDING 5
EXTERIOR ELEVATIONS
Scale: 1/8" = 1'-0"
Sheet: **A503.5**

MARCH 17, 2020



① BUILDING SECTION
1/4" = 1'-0"

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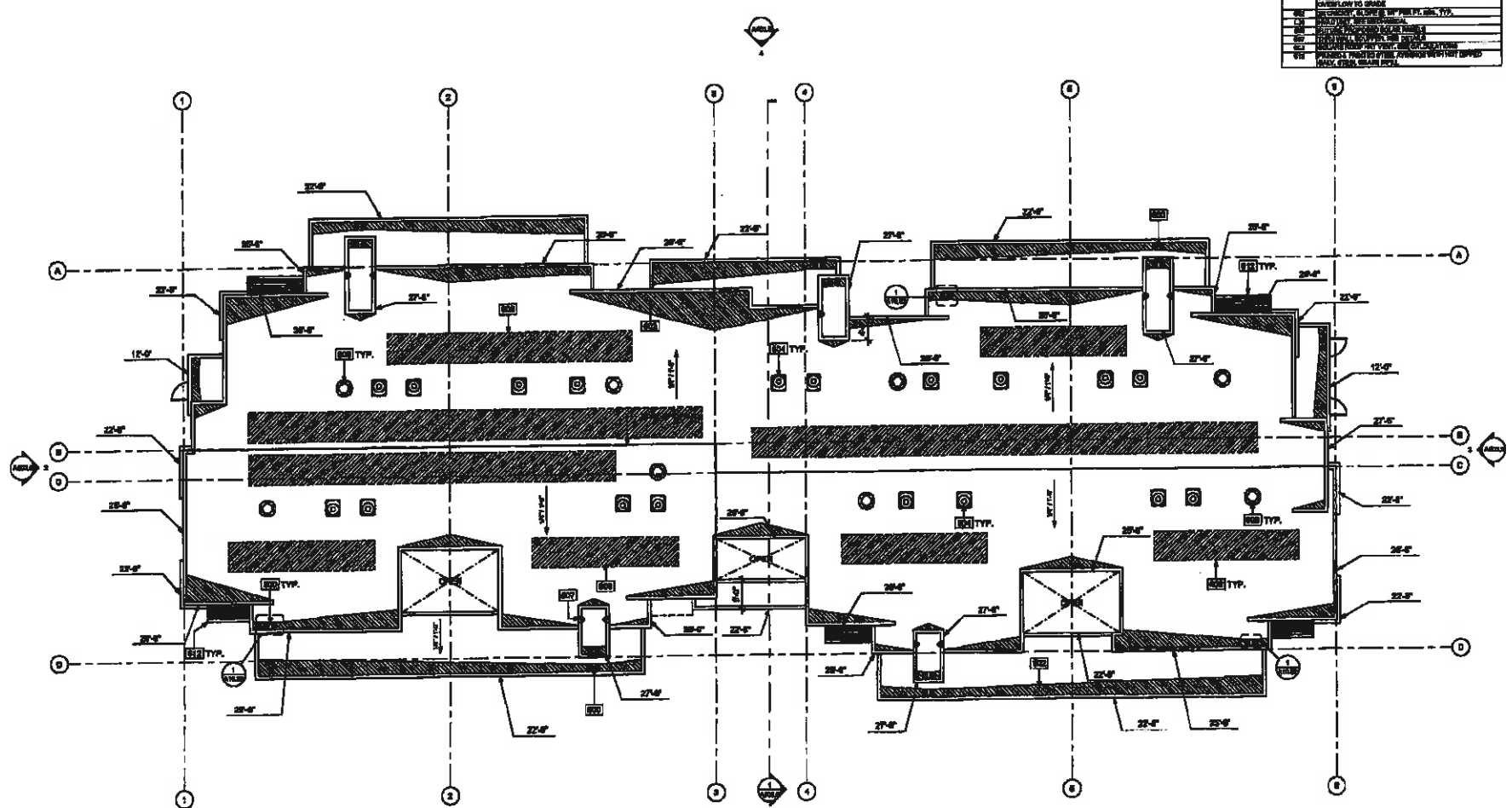
305 - 854-6666 | 310 - 777 - 1174

BUILDING 5
BUILDING SECTION

Scale: 1/4" = 1'-0"
Sheet: **A506.5**

SHEET 15 OF 20

| KEYNOTE LEGEND | |
|----------------|--|
| 102 | DESCRIPTION |
| 00 | ROOM FINISH / FLOOR FINISH, EXCEPT INDICATED TO OTHERWISE AND FINISH ELEVATION AND CHG. LIST |
| 01 | CONCRETE FLOOR TO GRADE |
| 02 | MECHANICAL ROOMS SUPPLY AIR, TYP. |
| 03 | MECHANICAL ROOMS EXHAUST AIR, TYP. |
| 04 | MECHANICAL ROOMS EXHAUST AIR, TYP. |
| 05 | MECHANICAL ROOMS EXHAUST AIR, TYP. |
| 06 | MECHANICAL ROOMS EXHAUST AIR, TYP. |
| 07 | MECHANICAL ROOMS EXHAUST AIR, TYP. |
| 08 | MECHANICAL ROOMS EXHAUST AIR, TYP. |
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| 98 | MECHANICAL ROOMS EXHAUST AIR, TYP. |
| 99 | MECHANICAL ROOMS EXHAUST AIR, TYP. |
| 100 | MECHANICAL ROOMS EXHAUST AIR, TYP. |



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BUILDING 5
ROOF PLAN
 Scale: 1/8" = 1'-0"
A507.5

DATE: 11.14.2018

REC BUILDING

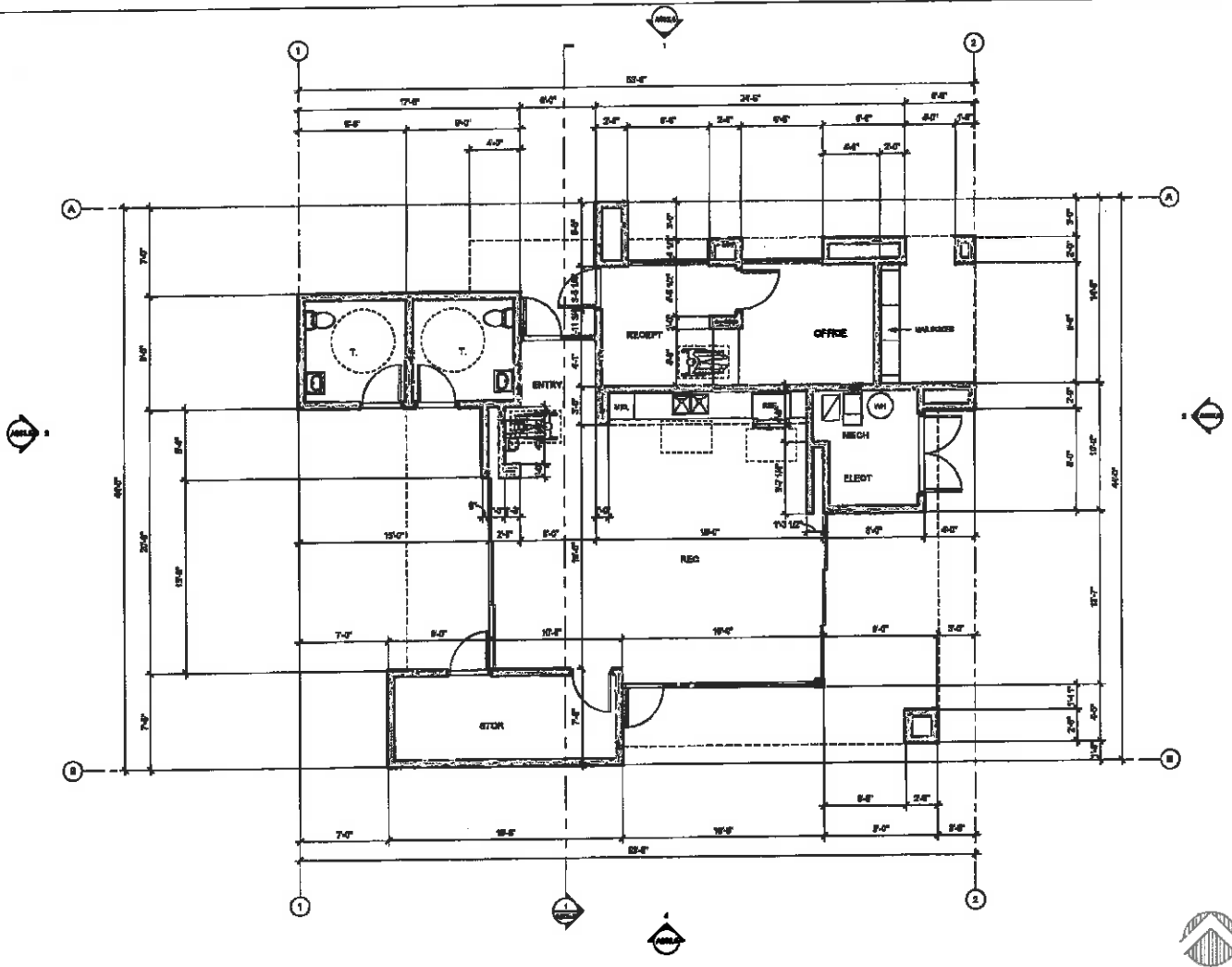
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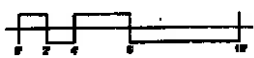
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WWW.PVGAARCHITECTS.COM
AP08PV0000-01001.COM | 760.777.4801

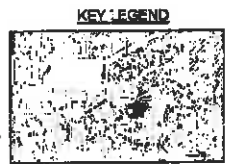
11/22 11.6.17



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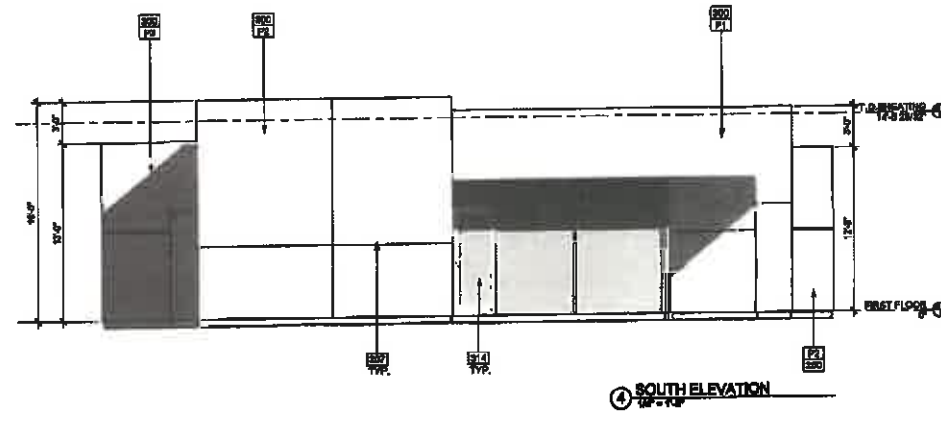
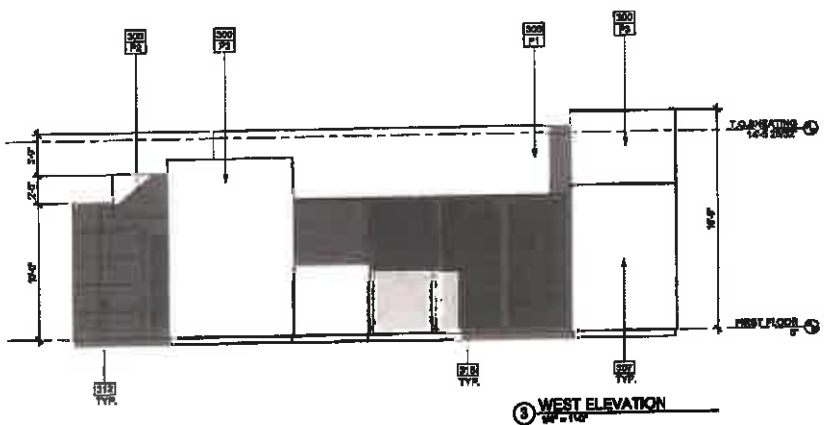
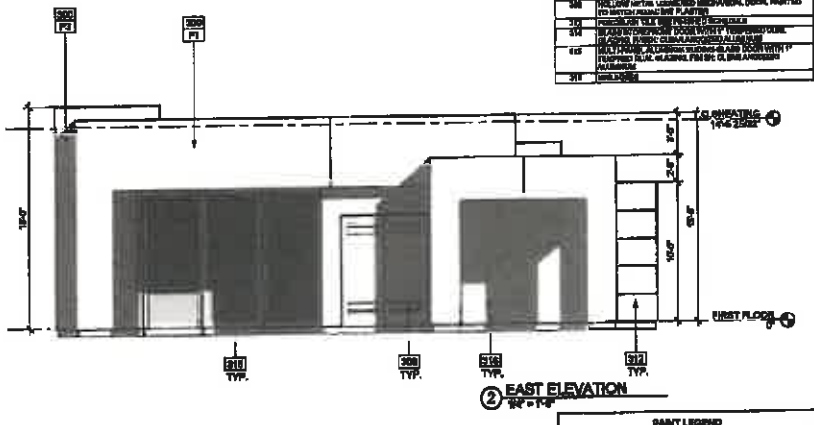
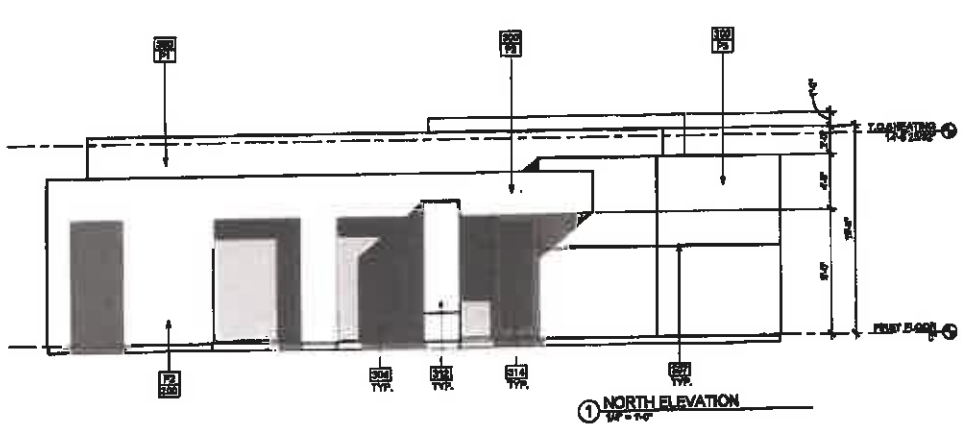


REC BUILDING
FLOOR PLAN
 Scale: 1/8" = 1'-0"
 Sheet: **A601.6**

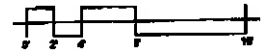
1 AUGUST 2010, 2008

| KEYNOTE LEGEND | |
|----------------|---|
| NO. | DESCRIPTION |
| 300 | 1/2" (12.7mm) THICK POLYURETHANE COATED GIP BOARD WITH 1/2" (12.7mm) THICK GYPSUM BOARD AND GROUT JOINTS AND FINISH WITH 1/8" (3.2mm) GROUT |
| 301 | 1/2" (12.7mm) THICK POLYURETHANE COATED GIP BOARD WITH 1/2" (12.7mm) THICK GYPSUM BOARD AND GROUT JOINTS AND FINISH WITH 1/8" (3.2mm) GROUT |
| 302 | 1/2" (12.7mm) THICK POLYURETHANE COATED GIP BOARD WITH 1/2" (12.7mm) THICK GYPSUM BOARD AND GROUT JOINTS AND FINISH WITH 1/8" (3.2mm) GROUT |
| 303 | 1/2" (12.7mm) THICK POLYURETHANE COATED GIP BOARD WITH 1/2" (12.7mm) THICK GYPSUM BOARD AND GROUT JOINTS AND FINISH WITH 1/8" (3.2mm) GROUT |
| 304 | 1/2" (12.7mm) THICK POLYURETHANE COATED GIP BOARD WITH 1/2" (12.7mm) THICK GYPSUM BOARD AND GROUT JOINTS AND FINISH WITH 1/8" (3.2mm) GROUT |
| 305 | 1/2" (12.7mm) THICK POLYURETHANE COATED GIP BOARD WITH 1/2" (12.7mm) THICK GYPSUM BOARD AND GROUT JOINTS AND FINISH WITH 1/8" (3.2mm) GROUT |
| 306 | 1/2" (12.7mm) THICK POLYURETHANE COATED GIP BOARD WITH 1/2" (12.7mm) THICK GYPSUM BOARD AND GROUT JOINTS AND FINISH WITH 1/8" (3.2mm) GROUT |
| 307 | 1/2" (12.7mm) THICK POLYURETHANE COATED GIP BOARD WITH 1/2" (12.7mm) THICK GYPSUM BOARD AND GROUT JOINTS AND FINISH WITH 1/8" (3.2mm) GROUT |
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| 311 | 1/2" (12.7mm) THICK POLYURETHANE COATED GIP BOARD WITH 1/2" (12.7mm) THICK GYPSUM BOARD AND GROUT JOINTS AND FINISH WITH 1/8" (3.2mm) GROUT |
| 312 | 1/2" (12.7mm) THICK POLYURETHANE COATED GIP BOARD WITH 1/2" (12.7mm) THICK GYPSUM BOARD AND GROUT JOINTS AND FINISH WITH 1/8" (3.2mm) GROUT |
| 313 | 1/2" (12.7mm) THICK POLYURETHANE COATED GIP BOARD WITH 1/2" (12.7mm) THICK GYPSUM BOARD AND GROUT JOINTS AND FINISH WITH 1/8" (3.2mm) GROUT |
| 314 | 1/2" (12.7mm) THICK POLYURETHANE COATED GIP BOARD WITH 1/2" (12.7mm) THICK GYPSUM BOARD AND GROUT JOINTS AND FINISH WITH 1/8" (3.2mm) GROUT |
| 315 | 1/2" (12.7mm) THICK POLYURETHANE COATED GIP BOARD WITH 1/2" (12.7mm) THICK GYPSUM BOARD AND GROUT JOINTS AND FINISH WITH 1/8" (3.2mm) GROUT |
| 316 | 1/2" (12.7mm) THICK POLYURETHANE COATED GIP BOARD WITH 1/2" (12.7mm) THICK GYPSUM BOARD AND GROUT JOINTS AND FINISH WITH 1/8" (3.2mm) GROUT |
| 317 | 1/2" (12.7mm) THICK POLYURETHANE COATED GIP BOARD WITH 1/2" (12.7mm) THICK GYPSUM BOARD AND GROUT JOINTS AND FINISH WITH 1/8" (3.2mm) GROUT |

| PAINT LEGEND | |
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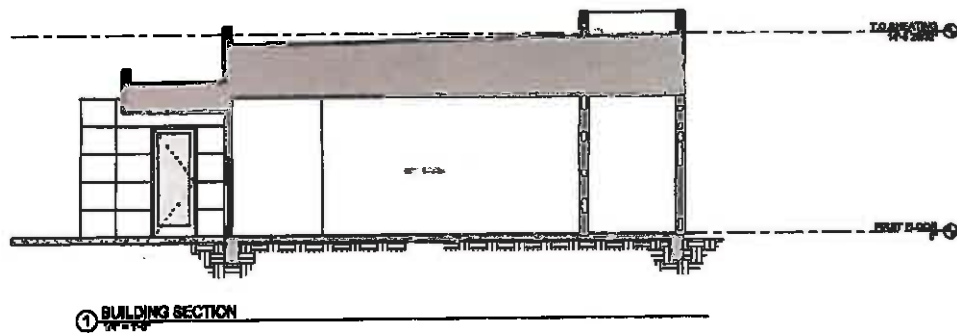
CANYON SPRINGS VILLAS
 CATHEDRAL CANYON DR - CATHEDRAL CITY, CALIFORNIA



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 WWW.PVGARCHITECTS.COM
 INFO@VUGARCHITECTS.COM | 704.779.4887

REC BUILDING
 EXTERIOR ELEVATIONS
 Scale: 1/4" = 1'-0"
 Sheet: **A602.6**

AUGUST 2010, 2010



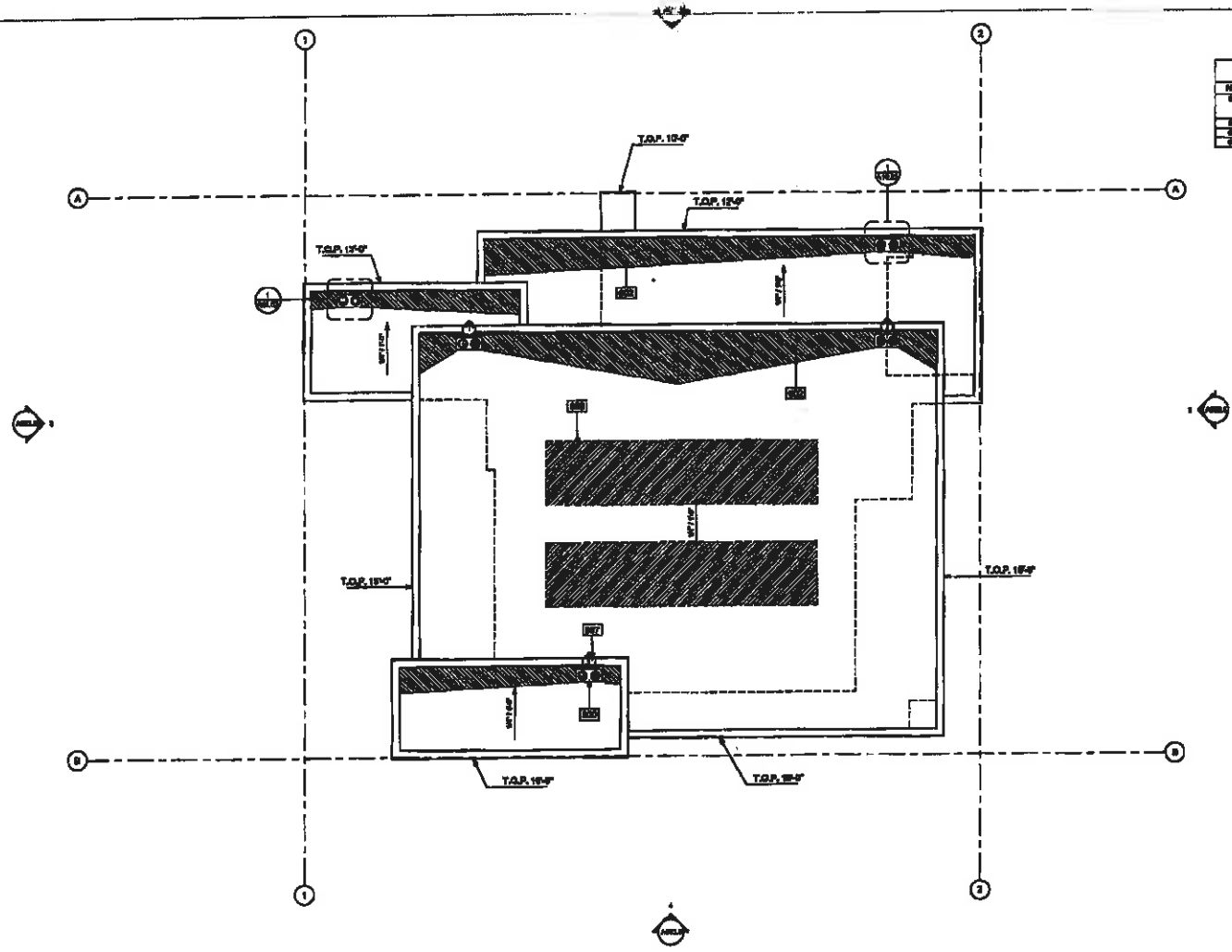
CANYON SPRINGS VILLAS
 CATHEDRAL CANYON DR - CATHEDRAL CITY, CALIFORNIA



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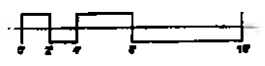
REC BUILDING
BUILDING SECTION
 Scale: 1/4" = 1'-0"
A605.6

January 2015 5/10



| KEYNOTE LEGEND | |
|----------------|---|
| NO. | DESCRIPTION |
| 01 | ROOF FLOOR AND EXTERIOR CONCRET ROOF DECK |
| 02 | 1\"/> |

CANYON SPRINGS VILLAS
 CATHEDRAL CANYON DR - CATHEDRAL CITY, CALIFORNIA



PRESTI YUKSIC I GREENWOOD
 ARCHITECTS + INTERIORS
 4000 BUFFALO AVE. SUITE 200 | PASADENA, CA 91107
 WWW.PYGARCHITECTS.COM
 951.799.1921

REC BUILDING
ROOF PLAN
 Scale: 1/8" = 1'-0"
 Sheet: **A607.6**

ARCHITECT: PRELIM, 2008

PAGE BREAK





AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY

November 16, 2020

CHAIR
Russell Butts
Desert Hot Springs

VICE CHAIR
Steven Stewart
Palm Springs

COMMISSIONERS

Arthur Butler
Riverside

John Lyon
Riverside

Steve Manos
Lake Elsinore

Richard Stewart
Moreno Valley

Gary Youmans
Temecula

STAFF

Director
Simon A. Houseman

Paul Rull
Barbara Santos

County Administrative Center
4080 Lemoor St., 14th Floor
Riverside, CA 92501
(951) 955-5132

www.rcaluc.org

Mr. H.P. Kang, Community Development Director
City of Hemet Planning Division
445 East Florida Avenue
Hemet CA 92543

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW –
DIRECTOR’S DETERMINATION**

File No.: ZAP1064HR20
Related File No.: GPA20-001 (General Plan Amendment), ZOA20-002 (Zoning Ordinance Amendment)
APN: Citywide

Dear Mr. Kang:

As authorized by the Riverside County Airport Land Use Commission (ALUC) pursuant to its Resolution No. 2011-02, as ALUC Director, I have reviewed City of Hemet amendments to the General Plan increasing the maximum floor area ratio for certain allowable uses in the Community Commercial land use designation from 0.4 to 1.0 FAR, and amendments to the Zoning Ordinance granting exceptions to maximum height and floor area ratios standards for hotel, medical facility and unique projects located in the commercial zones.

The City’s proposed General Plan Amendment, and its companion application Zoning Ordinance Amendment seeks to provide a mechanism for hotel, medical facility, or unique development projects located in the Community Commercial General Plan land use designation, to request exceptions to the floor area ratio (FAR) standard of 0.4 to a maximum 1.0 FAR, as well as granting exceptions to building height standards. Note that FAR is a tool used to design building’s floor area in relation to the size of the underlying lot or parcel. FAR does not identify the occupancy of the building, which is a requirement of the Airport Land Use Compatibility Plan.

The City’s General Plan was found consistent with the 2017 Hemet-Ryan Airport Land Use Compatibility Plan by the ALUC on May 9, 2019. As such, the City is required to apply all (airport) zoning compatibility intensity criteria to these projects that are seeking a FAR exception as outlined in this amendment. In addition, the City is also required to review project’s heights for Federal Aviation Administration Part 77 obstruction standards to ensure that buildings and structures do not result in a hazard to air navigation.

The proposed amendment does not involve changes in development standards or allowable land uses that would increase residential density or non-residential intensity. Therefore, these amendments have no possibility for having an impact on the safety of air navigation within

AIRPORT LAND USE COMMISSION

airport influence areas located within the unincorporated areas of Riverside County.

As ALUC Director, I hereby find the above-referenced project CONSISTENT with the 2017 Hemet-Ryan Airport Land Use Compatibility Plan.

This determination of consistency relates to airport compatibility issues and does not necessarily constitute an endorsement of the proposed amendments.

If you have any questions, please contact Paul Rull, ALUC Principal Planner, at (951) 955-6893.

Sincerely,
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Simon A. Housman, ALUC Director

cc: ALUC Case File

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**CITY OF HEMET
Hemet, California**

**PLANNING COMMISSION
RESOLUTION BILL NO. 20-017**

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF HEMET, CALIFORNIA RECOMMENDING THAT THE CITY COUNCIL APPROVE AN ORDINANCE AMENDING SECTION 90-45.3 OF THE HEMET MUNICIPAL CODE TO CLARIFY THE APPROVAL AUTHORITY FOR ADMINISTRATIVE ADJUSTMENT APPLICATIONS AND SECTION 90-895(d) TO ESTABLISH PARAMETERS, PROCEDURES, AND FINDINGS FOR THE GRANTING OF EXCEPTIONS TO MAXIMUM HEIGHT AND FLOOR AREA RATIO STANDARDS FOR HOTEL, MEDICAL FACILITY, OR UNIQUE PROJECTS LOCATED IN THE COMMERCIAL ZONES, AND APPROVAL OF AN ENVIRONMENTAL EXEMPTION PURSUANT TO SECTION 15081(b)(3) OF THE CEQA GUIDELINES.

WHEREAS, the City desires to encourage and facilitate innovative and contemporary site design by introducing flexibility into the evaluation of commercial development projects; and

WHEREAS, the proposed zoning ordinance amendment establishes parameters, procedures, and findings for the approval of exceptions to maximum floor area ratio and height standards for hotel, medical facility, or unique projects in the commercial zones; and

WHEREAS, approval of the proposed zoning ordinance amendment will not detrimentally affect the health, safety, or welfare of residents of the City of Hemet; and

WHEREAS, the Planning Commission has the authority to make a recommendation to the City Council regarding the proposed Zoning Ordinance Amendment 20-002 in accordance with the Government Code Sections 65353, 65355, and 65090, and the Hemet Municipal Code; and

**Planning Commission Resolution Bill No. 20-017
ZOA20-002**

Page 1 of 4

1 **WHEREAS**, on September 4, 2020, the City placed a notice in the Press
2 Enterprise of the holding of a public hearing at which the proposed Zoning Ordinance
3 Amendment 20-002 would be considered by the Planning Commission; and
4

5 **WHEREAS**, on September 15, 2020, the Planning Commission held the noticed
6 public hearing at which interested persons had an opportunity to testify in support of, or
7 opposition to, the proposed Zoning Ordinance Amendment, and at which the Planning
8 Commission considered the proposed Zoning Ordinance Amendment.
9

10 **NOW, THEREFORE**, the Planning Commission of the City of Hemet does
11 Resolve, Determine, Find and Order as follows:
12

13 **SECTION 1: ENVIRONMENTAL FINDINGS**
14

15 The Planning Commission, in light of the whole record before it, including but not limited
16 to, the City's Local CEQA Guidelines and Thresholds of Significance, the direction of
17 the Planning Commission at its meeting on September 15, 2020 and documents
18 incorporated therein by reference, and any other evidence (within the meaning of Public
19 Resources Code Sections 21080(e) and 21082.2) within the record or provided at the
20 public hearing of this matter, hereby finds and determines as follows:

21 **CEQA**: The City has analyzed this proposed project and has determined that it is
22 exempt from the California Environmental Quality Act ("CEQA") under section
23 15061(b)(3) of the CEQA Guidelines which provides that CEQA only applies to projects
24 that have the potential for causing a significant effect on the environment. The
25 amendments proposed by this Ordinance do not relate to any physical project and will
26 not result in any physical change to the environment. Therefore, it can be seen with
27 certainty that there is no possibility that this Ordinance may have a significant adverse
28 effect on the environment, and therefore the adoption of this Ordinance is exempt from
29 CEQA pursuant to Section 15061(b)(3) of the CEQA Guidelines.
30

31 **SECTION 2: ZONING ORDINANCE AMENDMENT FINDINGS**
32

33 Pursuant to Hemet Municipal Code Section 90-41.5(a), the Planning Commission
34 makes the following findings with respect to this zoning ordinance amendment:
35

- 36 1. The proposed Zoning Ordinance Amendment is in conformance with the latest
37 adopted General Plan for the City because it meets the following General Plan
38 goals and policies:
39
- 40 a. *Policy LU-1.12 Flexibility Over Time*. Require development to occur within
41 the designated range of density and intensity, but allow for flexibility in the
42 types of uses to account for changes in industrial and employment
43 markets, retail commercial enterprises, and housing needs and
44 characteristics"
45

Planning Commission Resolution Bill No. 20-017
 ZOA20-002

 Page 2 of 4

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b. *Policy LU-11.1 Attract New Business.* Support existing businesses and seek to attract new businesses and industries that strengthen and diversify Hemet's tax revenue base, improve wage and salary levels, increase the variety of job opportunities, and employ the resident labor force.

c. *Implementation Program LU-P-16 Development Review Process.* Continue to evaluate and improve the development review process for increased efficiency and effectiveness and revise applications, procedures, and informational materials as needed.

2. The proposed Zoning Ordinance Amendment will protect the public health, safety, and welfare because it is a text amendment only and, therefore, will not have a direct effect on people or the environment. Additionally, by requiring projects that are seeking an exception to development standards to obtain a conditional use permit or an administrative adjustment permit, the proposed amendment ensures that the projects will be evaluated on a case-by-case basis and will be subject to conditions of approval intended.

SECTION 3: PLANNING COMMISSION ACTIONS

The Planning Commission hereby takes the following action:

The Planning Commission approves Resolution Bill No. 20-017 recommending that the City Council adopt the proposed Ordinance which is attached hereto and incorporated herein by reference as Exhibit "A."

PASSED, APPROVED, AND ADOPTED this 15th day of September, 2020 by the following vote:

AYES:
NOES:
ABSTAIN:
ABSENT:

Vince Overmyer, Chairman
Hemet Planning Commission

ATTEST:

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Gabriela Hernandez, Records Secretary
Hemet Planning Commission

Planning Commission Resolution Bill No. 20-017
ZOA20-002

Page 4 of 4



**CITY OF HEMET
Hemet, California**

ORDINANCE NO.

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF HEMET, CALIFORNIA APPROVING ZONING ORDINANCE AMENDMENT NO. 20-002 AMENDING SECTION 90-45.3 OF THE HEMET MUNICIPAL CODE TO CLARIFY THE APPROVAL AUTHORITY FOR ADMINISTRATIVE ADJUSTMENT APPLICATIONS AND SECTION 90-895(d) TO ESTABLISH PARAMETERS, PROCEDURES, AND FINDINGS FOR THE GRANTING OF EXCEPTIONS TO MAXIMUM HEIGHT AND FLOOR AREA RATIO STANDARDS FOR HOTEL, MEDICAL FACILITY, OR UNIQUE PROJECTS LOCATED IN THE COMMERCIAL ZONES, AND APPROVAL OF AN ENVIRONMENTAL EXEMPTION PURSUANT TO SECTION 15061(b)(3) OF THE CEQA GUIDELINES.

WHEREAS, the City desires to encourage and facilitate innovative and contemporary site design by introducing flexibility into the evaluation of certain commercial development projects; and

WHEREAS, the proposed zoning ordinance amendment establishes parameters, procedures, and findings for the approval of exceptions to maximum floor area ratio and height standards for hotel, medical facility, and unique projects in the commercial zones; and

WHEREAS, approval of the proposed zoning ordinance amendment will not detrimentally affect the health, safety, or welfare of residents of the City of Hemet; and

WHEREAS, on September 15, 2020, subsequent to a public hearing, the Planning Commission adopted Planning Commission Resolution Bill No. 20-017

**CITY COUNCIL ORDINANCE
ZOA20-002 – Height and FAR Exceptions in the Commercial Zones**

[1]

1 recommending that the City Council approve Zoning Ordinance Amendment 20-002;
2 and

3 WHEREAS, on _____, the Hemet City Council considered the
4 Ordinance, the Planning Commission's findings, and the record of information
5 regarding Zoning Ordinance Amendment No. 20-002 at a duly noticed public hearing,
6 at which time interested parties had an opportunity to provide testimony on this matter.

7 NOW, THEREFORE, the City Council of the City of Hemet does hereby
8 resolve, determine, find and order as follows:

9 **SECTION 1: AMENDMENT OF SUBSECTION (d) (HEIGHT EXCEPTIONS) OF**
10 **SECTION 90-895 (SITE DEVELOPMENT REQUIREMENTS) OF ARTICLE XXVI**
11 **(COMMERCIAL ZONES) OF CHAPTER 90 (ZONING) OF THE HEMET MUNICIPAL**
12 **CODE.**

13 Subsubsection 90-895(d) is hereby modified to read as follows:

14 (d) Height and Floor Area Ratio Exceptions.

15 **SECTION 2: AMENDMENT OF SUBSUBSECTION (2) OF SUBSECTION (d)**
16 **(HEIGHT AND FLOOR AREA RATIO EXCEPTIONS) OF SECTION 90-895 (SITE**
17 **DEVELOPMENT REQUIREMENTS) OF ARTICLE XXVI (COMMERCIAL ZONES) OF**
18 **CHAPTER 90 (ZONING) OF THE HEMET MUNICIPAL CODE.**

19 Subsubsection 90-895(d)(2) is hereby modified to read as follows:

20 (2) Buildings that exceed the maximum height of the zone in which the use is
21 proposed by 20 percent or less may be considered with the processing of an
22 administrative adjustment permit pursuant to section 90-45, et al of this chapter.
23 Buildings that exceed the maximum height by more than 20 percent may be
24 permitted up to 50 feet in height with the approval of a conditional use permit
25 pursuant to section 90-42 of this chapter. In addition to the findings required to
26 grant a conditional use permit, the planning commission must also find that:

- 1 a. The proposed use is a hotel, medical facility, or unique use for which
2 the proposed height is consistent with industry standards.
- 3 b. The proposed height does not exceed 50 feet and will not result in a
4 building or site design that detrimentally impacts the sun exposure, air
5 circulation, or privacy of adjacent land uses.
- 6 c. The proposed height exception is appropriate and necessary for site
7 development and contributes to the commercial development goals of
8 the city.

9 **SECTION 3: ADDITION OF SUBSUBSECTION (3) TO SUBSECTION (d) (HEIGHT**
10 **AND FLOOR AREA RATIO EXCEPTIONS) OF SECTION 90-895 (SITE**
11 **DEVELOPMENT REQUIREMENTS) OF ARTICLE XXVI (COMMERCIAL ZONES) OF**
12 **CHAPTER 90 (ZONING) OF THE HEMET MUNICIPAL CODE.**

13 Subsubsection 90-895(d)(3) is hereby added to read as follows:

14 (3) A development project that proposes to increase the maximum allowable floor
15 area ratio by 10 percent or less may be considered with the processing of an
16 administrative adjustment permit pursuant to section 90-45, et al of this chapter.
17 A project that proposes to exceed the floor area ratio by more than 10 percent
18 to a floor area ratio maximum of 1.0 may be permitted with the approval of a
19 conditional use permit pursuant to section 90-42 of this chapter and subject to
20 the following supplemental findings:

- 21 a. The proposed use is a hotel, medical facility, or unique use for which
22 the proposed floor area ratio is consistent with industry standards.
- 23 b. The proposed project is located within the community commercial land
24 use designation of the general plan and the proposed floor area ratio
25 does not exceed 1.0.

- 1 c. The proposed floor area ratio will not negatively impact infrastructure,
2 municipal services, or the character of the area surrounding the project
3 site.

4 **SECTION 4: AMENDMENT OF SUBSECTION 90-45.3 (REVIEW OF APPLICATIONS**
5 **AND APPROVING AUTHORITY) OF SECTION 90-45 (ADMINISTRATIVE**
6 **ADJUSTMENT APPLICATIONS) OF DIVISION 1 (APPLICATIONS) OF ARTICLE II**
7 **(ADMINISTRATIVE REGULATIONS) OF CHAPTER 90 (ZONING) OF THE HEMET**
8 **MUNICIPAL CODE.**

9 Subsection 90-45.3 is hereby amended to read as follows:

10 Sec. 90-45.3. - Review of applications and approving authority.

11 (a) *Administrative adjustments.* Administrative adjustment determinations by the director
12 are administrative actions and generally will not require notice and a public hearing
13 unless the project may affect the property rights or enjoyment of neighboring
14 properties as determined by the director.

15 (b) *Review authority.* The director shall be responsible for the review and approval of
16 administrative adjustments. However, the Planning Commission shall review and
17 approve administrative adjustment applications that accompany a development
18 project under consideration by the Planning Commission.

19 (c) *Effective date.* The administrative adjustment shall become effective ten days
20 following the date of approval by the applicable approving authority, unless
21 appealed pursuant to section 90-45.5.

22 **SECTION 5. FINDINGS**

23 1. The proposed Zoning Ordinance Amendment is in conformance with the latest
24 adopted General Plan for the City because it meets the following General Plan goals
25 and policies:

1 A. *Policy LU-1.12 Flexibility Over Time.* Require development to occur within
2 the designated range of density and intensity, but allow for flexibility in the types of uses
3 to account for changes in industrial and employment markets, retail commercial
4 enterprises, and housing needs and characteristics”

5 B. *Policy LU-11.1 Attract New Business.* Support existing businesses and
6 seek to attract new businesses and industries that strengthen and diversify Hemet’s tax
7 revenue base, improve wage and salary levels, increase the variety of job opportunities,
8 and employ the resident labor force.

9 C. *Implementation Program LU-P-16 Development Review Process.*
10 Continue to evaluate and improve the development review process for increased
11 efficiency and effectiveness and revise applications, procedures, and informational
12 materials as needed.

13 2. The proposed Zoning Ordinance Amendment will protect the public health,
14 safety, and welfare because it is a text amendment only and, therefore, will not have a
15 direct effect on people or the environment. Additionally, by requiring projects that are
16 seeking an exception to development standards to obtain a conditional use permit or an
17 administrative adjustment permit, the proposed amendment ensures that the projects
18 will be evaluated on a case-by-case basis and will be subject to conditions of approval
19 intended to protect the public health, safety, and welfare.

20 **SECTION 6: CEQA FINDINGS.**

21 The City has analyzed this proposed project and has determined that it is exempt
22 from the California Environmental Quality Act (“CEQA”) under section 15061(b)(3) of the
23 CEQA Guidelines, which provides that CEQA only applies to projects that have the
24 potential for causing a significant effect on the environment. The proposed Zoning
25 Ordinance Amendment does not relate to any physical project and will not result in any
26 physical change to the environment. Therefore, it can be seen with certainty that there

1 is no possibility that this Zoning Ordinance Amendment may have a significant adverse
2 effect on the environment, and therefore the adoption of this Zoning Ordinance
3 Amendment is exempt from CEQA pursuant to Section 15061(b)(3) of the CEQA
4 Guidelines.

5 **SECTION 7: SEVERABILITY.**

6 If any section, subsection, subdivision, sentence, clause, phrase, or portion of
7 this Ordinance is, for any reason, held to be invalid or unconstitutional by the decision of
8 any court of competent jurisdiction, such decision shall not affect the validity of the
9 remaining portions of this Ordinance. The City Council hereby declares that it would
10 have adopted this Ordinance, and each section, subsection, subdivision, sentence,
11 clause, phrase, or portion thereof, irrespective of the fact that any one or more sections,
12 subsections, subdivisions, sentences, clauses, phrases, or portions thereof be declared
13 invalid or unconstitutional.

14 **SECTION 8: EFFECTIVE DATE.**

15 This Ordinance shall take effect thirty (30) days from its passage by the City
16 Council of the City of Hemet.

17 **SECTION 9: PUBLICATION.**

18 The City Clerk is authorized and directed to cause this Ordinance to be published
19 within fifteen (15) days after its passage in a newspaper of general circulation and
20 circulated within the City in accordance with Government Code Section 36933(a) or, to
21 cause this Ordinance to be published in the manner required by law using the
22 alternative summary and pasting procedure authorized under Government Code
23 Section 39633(c).

24 INTRODUCED at the regular meeting of Hemet City Council on the ___ day of _____,
25 2020.

26 APPROVED AND ADOPTED this _____ day of _____, 2020

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Russ Brown, Mayor

ATTEST:

APPROVED AS TO FORM:

Clay James, Deputy City Clerk

Eric S. Vail, City Attorney

ATTEST:

APPROVED AS TO FORM:

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State of California)
County of Riverside)
City of Hemet)

I, Clay James, Deputy City Clerk of the City of Hemet, do hereby certify that the foregoing Ordinance is the actual Ordinance adopted by the City Council of the City of Hemet and was passed at a regular meeting of the City Council on the ____ day of _____, 2020 by the following vote:

AYES:
NOES:
ABSTAIN:
ABSENT:

Clay James, Deputy City Clerk

Shown below are the proposed amendments to the Hemet Municipal Code. The proposed additions are shown in redline and the proposed deletions are shown in strike-through. No other sections of the Municipal Code are affected by ZOA20-02.

Article XXVI Commercial Zones

Sec. 90-895. - Site development requirements.

- (a) The commercial design guidelines as established by resolution of the city council are applicable to all new commercial development to complement the mandatory standards in this article. Where there is a conflict or lack of clarity, the provisions of this article shall apply.
- (b) *Measurement of yards.* A required yard or other open space around an existing or proposed building shall not be used to meet a required yard or other open space for any other building on an adjoining lot or building site.
- (c) *Building additions.* Subject to site development review pursuant to section 90-48, additions to an existing, legally constructed structure or center may be permitted. The building addition shall not exceed 40 percent of the floor area of the existing structure or center and the coverage requirements of the zone.
- (d) *Height and Floor Area Ratio exceptions.*
 - (1) Structures permitted above a specified height limit may be erected as follows: Structures or walls for the housing of elevators, stairways, tanks, ventilating fans or similar structures, skylights, towers, steeples, flagpoles, chimneys, smokestacks, wireless masts, radio and television masts, water tanks, silos or similar structures, provided that no roof structure, as listed in this subsection, or any space above the height limit specifically prescribed for particular zones, shall be allowed or used for the purpose of providing usable floor space in excess of that reasonably required to maintain such structures and shall not be used for signage.
 - (2) Buildings that exceed the maximum height of the zone in which the use is proposed by 20 percent or less may be considered with the processing of an administrative adjustment permit pursuant to section 90-45, et al of this chapter. Buildings that exceed the maximum height by more than 20 percent may be permitted with the approval of a conditional use permit pursuant to section 90-42 of this chapter. In addition to the findings required to grant a conditional use permit, the planning commission must also find that:
 - (a) The proposed use is a hotel, medical facility, or unique use for which the proposed height is consistent with industry standards.
 - (b) The proposed height does not exceed 50 feet and will not result in a building or site design that detrimentally impacts the sun exposure, air circulation, or privacy of adjacent land uses.
 - (c) The proposed height exception is appropriate and necessary for site development and contributes to the commercial development goals of the city.
 - (3) A development project that proposes to increase the maximum allowable floor area ratio by 10 percent or less may be considered with the processing of an administrative adjustment permit pursuant to section 90-45, et al of this chapter. A project that proposes to exceed the floor area ratio by more than 10 percent to a floor area ratio maximum of 1.0 may be permitted with the approval of a conditional use permit

pursuant to section 90-42 of this chapter and subject to the following supplemental findings:

- (a) The proposed use is a hotel, medical facility, or unique use for which the proposed floor area ratio is consistent with industry standards.
 - (b) The proposed project is located within the community commercial land use designation of the general plan and the proposed floor area ratio does not exceed 1.0.
 - (c) The proposed floor area ratio exception will not negatively impact infrastructure, municipal services, or the character of area surrounding the project site.
- (e) *Accessory structures.*
-

Article II. – Administrative Regulations

Division 1. - Applications

Sec. 90-45. - Administrative adjustment applications.

Sec. 90-45.3. - Review of applications and approving authority.

- (a) *Administrative adjustments.* Administrative adjustment determinations by the director are administrative actions and generally will not require notice and a public hearing unless the project may affect the property rights or enjoyment of neighboring properties as determined by the director.
- (b) *Review **authority** by director.* The director shall be responsible for the review and approval of administrative adjustments. **However, the Planning Commission shall review and approve administrative adjustment applications that accompany a development project under consideration by the Planning Commission.**
- (c) *Effective date.* The administrative adjustment shall become effective ten days following the date of approval by the applicable approving authority, unless appealed pursuant to section 90-45.5.



**CITY OF HEMET
Hemet, California**

RESOLUTION NO. 20-_____

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF HEMET, CALIFORNIA APPROVING GENERAL PLAN AMENDMENT 20-001 AMENDING TABLE 2.1 (LAND USE DENSITY AND INTENSITY) OF CHAPTER 2 (LAND USE ELEMENT) OF THE HEMET GENERAL PLAN TO INCREASE THE MAXIMUM FLOOR AREA RATIO FOR CERTAIN ALLOWABLE USES IN THE COMMUNITY COMMERCIAL LAND USE DESIGNATION FROM 0.4 TO 1.0 SUBJECT TO THE PROCESSING OF AN ADMINISTRATIVE ADJUSTMENT OR CONDITIONAL USE PERMIT WITH SUPPLEMENTAL FINDINGS PURSUANT TO SECTION 90-895(d)(3) OF THE HEMET MUNICIPAL CODE, AND FINDING THE PROJECT EXEMPT PURSUANT TO SECTION 15061(b)(3) OF THE CEQA GUIDELINES.

WHEREAS, Table 2.1 (Land Use Intensity and Density) of Chapter 2 (Land Use Element) of the Hemet General Plan establishes the minimum, maximum, and target density and intensity for each land use designation of the General Plan; and

WHEREAS, non-residential uses measure intensity by floor area ratio which is calculated by dividing the square footage of the proposed buildings by the square footage of the project site; and

WHEREAS, the City desires to increase the maximum floor area ratio for hotels, medical facilities, or unique uses located within the community commercial land use designation from 0.4 to 1.0 subject to the processing of an administrative adjustment or conditional use permit with supplemental findings; and

**CITY COUNCIL RESOLUTION NO. 20-_____
GPA NO. 20-001 – FAR Exception in the CC Land Use Designation**

[1]

1 **WHEREAS**, California state law allows general plans to be amended up to four
2 times annually and the proposed amendment is part of Cycle 1 of 2020 Amendments to
3 the Hemet General Plan; and

4 **WHEREAS**, on date subsequent to a public hearing, the Planning Commission
5 adopted Planning Commission Resolution No. 20-____ recommending that the City
6 Council approve General Plan Amendment 20-001; and

7 **WHEREAS**, in accordance with Government Code Sections 65353,
8 65355, and 65090 on date the City placed a notice in The Press Enterprise of the
9 holding of a public hearing at which the proposed General Plan Amendment No. 20-001
10 would be considered by the City Council; and

11 **WHEREAS**, on _____, the Hemet City Council conducted a duly
12 noticed public hearing at which interested persons had an opportunity to testify in
13 support of, or opposition to, General Plan Amendment No. 20-001, and at which time
14 the City Council considered General Plan Amendment 20-001.

15 **NOW, THEREFORE**, the City Council of the City of Hemet does hereby
16 resolve, determine, find and order as follows:

17 **SECTION 1: AMENDMENT OF THE GENERAL PLAN LAND USE ELEMENT**

18 The notes in Table 2.1 (Land Use Density and Intensity) in Chapter 2 (Land Use
19 Element) of the General Plan are hereby modified as follows:

- 20 1. Subscript 1 is added at the Intensity Range column heading to read as follows,
21 "Intensity Range¹."
- 22 2. Subscript 2 replaces subscript 1 at the Target Intensity column heading to read
23 as follows: "Target Intensity²."
- 24 3. Subscript 3 is added to the Community Commercial land use designation in the
25 Land Use column to read as follows: "CC – Community Commercial³."
- 26 4. The Notes are modified to read as follows:

CITY COUNCIL RESOLUTION NO. 20-____
GPA NO. 20-001 – FAR Exception in the CC Land Use Designation

[2]

1 **Notes:**

2 ¹ du/ac = dwelling units per acre; FAR = floor area ratio; NA = not applicable.

3 ² Target Intensity is a range used in the traffic model prepared for the General
4 Plan's environmental impact report and represents a "reasonable worst case" analysis.

5 ³ A maximum FAR of 1.0 may be applied to certain development projects in the
6 CC land use designation through an administrative adjustment or conditional use permit
7 process.

8 **SECTION 2. FINDINGS**

9 1. The proposed General Plan Amendment is in the public interest because it
10 encourages and facilitates innovative and contemporary commercial site design by
11 introducing flexibility into the evaluation of hotel, medical facility and unique
12 development projects in the community commercial land use designation. The
13 provisions of Section 90-895(d)(3) in the Hemet Municipal Code ensure project
14 accountability by requiring a review of each request for a floor area ratio exception on a
15 case-by-case basis through the processing of an administrative adjustment permit or a
16 conditional use permit with supplemental findings.

17 2. The proposed General Plan Amendment is not detrimental to public health,
18 safety, and welfare because it is a text amendment only and, therefore, will not have a
19 direct effect on people or the environment. Additionally, the proposal requires projects
20 that are seeking a floor area ratio exception to obtain an administrative adjustment
21 permit or a conditional use permit which requires, among other things, demonstration
22 that the project "will not be detrimental to the public health, safety, or welfare, or
23 materially injurious to properties or improvements in the vicinity."

24 3. The General Plan Amendment is consistent with the overall vision, goals, and
25 objectives of the General Plan because it implements the following goals and policies:

1 A. *Section 1.2 A City in Transition.* At Hemet's core is a strong community
2 willing to invest in opportunities for future and make forward-thinking decisions to meet
3 the pressing needs of a changing economy and a growing and diverse population.

4 B. *Policy LU-1.12 Flexibility Over Time.* Require development to occur within
5 the designated range of density and intensity, but allow for flexibility in the types of uses
6 to account for changes in industrial and employment markets, retail commercial
7 enterprises, and housing needs and characteristics"

8 C. *Policy LU-4.3 Infill Development and Re-Use.* Actively promote the
9 adaptive reuse and infill of economically underutilized, obsolete, and dilapidated
10 commercial and industrial sites"

11 D. *Goal LU-8 Florida Corridor.* Revitalize and enhance the land uses and
12 appearance of the Florida-Devonshire-Acacia Corridors to create an integrated mix of
13 commercial, office, hospitality, and residential uses.

14 E. *LU-11.1 Attract New Business.* Support existing businesses and seek to
15 attract new businesses and industries that strengthen and diversify Hemet's tax revenue
16 base, improve wage and salary levels, increase the variety of job opportunities, and
17 employ the resident labor force.

18 **SECTION 3: CEQA FINDINGS.**

19 The City has analyzed this proposed project and has determined that it is exempt
20 from the California Environmental Quality Act ("CEQA") under section 15061(b)(3) of the
21 CEQA Guidelines, which provides that CEQA only applies to projects that have the
22 potential for causing a significant effect on the environment. The proposed General
23 Plan Amendment does not relate to any physical project and will not result in any
24 physical change to the environment. Therefore, it can be seen with certainty that there
25 is no possibility that this General Plan Amendment may have a significant adverse

1 effect on the environment, and therefore the adoption of this General Plan Amendment
2 is exempt from CEQA pursuant to Section 15061(b)(3) of the CEQA Guidelines.

3 **SECTION 4 CITY COUNCIL ACTION.**

4 Based on the foregoing findings, and on substantial evidence in the whole of the
5 record, the City Council approves General Plan Amendment No. 20-001.

6 **APPROVED AND ADOPTED** this ____ day of _____, 2020

7
8 **PASSED, APPROVED, AND ADOPTED** this ____ day of _____, 2020.

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Russ Brown, Mayor

ATTEST:

APPROVED AS TO FORM:

Clay James, Deputy City Clerk

Eric S. Vail, City Attorney

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State of California)
County of Riverside)
City of Hemet)

I, Clay James, Deputy City Clerk of the City of Hemet, do hereby certify that the foregoing Resolution is the actual Resolution adopted by the City Council of the City of Hemet and was passed at a regular meeting of the City Council on the ____ day of _____, 2020 by the following vote:

AYES:
NOES:
ABSTAIN:
ABSENT:

Clay James, Deputy City Clerk

Shown below are the proposed amendments to Table 2.1 of the Hemet General Plan. The proposed additions are shown in **redline** and the proposed deletions are shown in **strike-through**. No other sections of the General Plan are affected by GPA20-XXX.

| Table 2.1 Land Use Density and Intensity | | |
|---|---|-------------------------------|
| Land Use | Intensity Range ¹ (min. and max.) | Target Intensity ² |
| RESIDENTIAL | | |
| RR – Rural Residential | 0-2.0 du/ac | 1.0 du/ac |
| RR 2.5 | 2.5 acre min | 1.0 du/2.5 acre |
| RR 5 | 5.0 acre min | 1.0 du/5.0 acre |
| HR – Hillside Residential | 0-0.5 du/ac | 0.5 du/ac |
| HR-10 | 1 du/10 acres | 1 du/10 acres |
| LDR – Low Density Residential | 2.1-5.0 du/ac | 3.5 du/ac |
| LMDR – Low Med. Density Residential | 5.1-8.0 du/ac | 6.5 du/ac |
| MDR – Medium Density Residential | 8.1-18 du/ac | 14 du/ac |
| HDR – High Density Residential | 18.1-30 du/ac | 22 du/ac |
| VHDR – Very High Density Residential | 30.1-45 du/ac | 35 du/ac |
| COMMERCIAL | | |
| NC – Neighborhood Commercial | FAR 0.35 | FAR 0.25 |
| CC – Community Commercial ³ | FAR 0.40 | FAR 0.30 |
| RC – Regional Commercial | FAR 0.50 | FAR 0.40 |
| OP – Office Professional/Medical | FAR 1.0 | FAR 0.50 |
| MU – Mixed Use | Varies | |
| INDUSTRIAL | | |
| ARPT – Airport/Support Uses | Varies | |
| BP – Business Park | FAR 0.60 | FAR 0.35 |
| I – Industrial | FAR 0.45 | FAR 0.40 |
| PUBLIC AND OPEN SPACE | | |
| PF – Public Facility | Varies | |
| P – Park/Outdoor Recreation | NA | |
| OS – Open Space/Natural Resources | NA | |
| A – Agricultural | NA | |
| SCH – School | NA | |
| QP – Quasi Public | Varies | |
| OVERLAY DESIGNATIONS | | |
| SP – Specific Plan | Varies | |
| EM – Environmental Management | NA | |

Notes:

¹ du/ac = dwelling units per acre; FAR = floor area ratio; NA = not applicable.

² Target Intensity is a range used in the traffic model prepared for the General Plan’s environmental impact report and represents a “reasonable worst case” analysis.

³ A maximum FAR of 1.0 may be applied to certain development projects in the CC land use designation through an administrative adjustment or conditional use permit process.



**CITY OF HEMET
Hemet, California**

**PLANNING COMMISSION
RESOLUTION BILL NO. 20-016**

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF HEMET, CALIFORNIA RECOMMENDING THAT THE CITY COUNCIL APPROVE A RESOLUTION AMENDING TABLE 2.1 (LAND USE DENSITY AND INTENSITY) OF CHAPTER 2 (LAND USE ELEMENT) OF THE HEMET GENERAL PLAN TO INCREASE THE MAXIMUM FLOOR AREA RATIO FOR CERTAIN ALLOWABLE USES IN THE COMMUNITY COMMERCIAL LAND USE DESIGNATION FROM 0.4 TO 1.0 SUBJECT TO THE PROCESSING OF AN ADMINISTRATIVE ADJUSTMENT OR CONDITIONAL USE PERMIT WITH SUPPLEMENTAL FINDINGS PURSUANT TO SECTION 90-895(d)(3) OF THE HEMET MUNICIPAL CODE, AND FINDING THE PROJECT EXEMPT PURSUANT TO SECTION 15061(b)(3) OF THE CEQA GUIDELINES.

WHEREAS, Table 2.1 (Land Use Intensity and Density) of Chapter 2 (Land Use Element) of the Hemet General Plan establishes the minimum, maximum, and target density and intensity for each land use designation of the General Plan; and

WHEREAS, non-residential uses measure intensity by floor area ratio which is calculated by dividing the square footage of the proposed buildings by the square footage of the project site; and

WHEREAS, the City desires to increase the maximum floor area ratio for hotel, medical facility, and unique uses located within the Community Commercial Land Use designation from 0.4 to 1.0 subject to the processing of an administrative adjustment or conditional use permit and supplemental findings; and

WHEREAS, California state law allows general plans to be amended up to four times annually and the proposed amendment is part of Cycle 1 of 2020 Amendments to the Hemet General Plan; and

**Planning Commission Resolution Bill No. 20-016
GPA20-001**

Page 1 of 4

1 **WHEREAS**, the Planning Commission has the authority to make a
2 recommendation to the City Council regarding the proposed General Plan Amendment
3 20-001 in accordance with the Government Code Sections 65353, 65355, and 65090,
4 and the Hemet Municipal Code; and
5

6 **WHEREAS**, on date, the City placed a notice in the Press Enterprise of the
7 holding of a public hearing at which the proposed General Plan Amendment No. 20-001
8 would be considered by the Planning Commission; and
9

10 **WHEREAS**, on date, the Planning Commission held the noticed public hearing at
11 which interested persons had an opportunity to testify in support of, or opposition to, the
12 proposed General Plan Amendment, and at which the Planning Commission considered
13 the proposed General Plan Amendment.
14

15 **NOW, THEREFORE**, the Planning Commission of the City of Hemet does
16 Resolve, Determine, Find and Order as follows:
17

18 **SECTION 1: ENVIRONMENTAL FINDINGS**
19

20 The Planning Commission, in light of the whole record before it, including but not limited
21 to, the City's Local CEQA Guidelines and Thresholds of Significance, the direction of
22 the Planning Commission at its meeting on date 2020 and documents incorporated
23 therein by reference, and any other evidence (within the meaning of Public Resources
24 Code Sections 21080(e) and 21082.2) within the record or provided at the public
25 hearing of this matter, hereby finds and determines as follows:

26 **CEQA:** The City has analyzed this proposed project and has determined that it is
27 exempt from the California Environmental Quality Act ("CEQA") under section
28 15061(b)(3) of the CEQA Guidelines which provides that CEQA only applies to projects
29 that have the potential for causing a significant effect on the environment. The
30 amendments proposed by this Ordinance do not relate to any physical project and will
31 not result in any physical change to the environment. Therefore, it can be seen with
32 certainty that there is no possibility that this Ordinance may have a significant adverse
33 effect on the environment, and therefore the adoption of this Ordinance is exempt from
34 CEQA pursuant to Section 15061(b)(3) of the CEQA Guidelines.
35

36 **SECTION 2: FINDINGS FOR APPROVAL OF THE GENERAL PLAN AMENDMENT**
37

38 The Planning Commission recommends approval of General Plan Amendment 20-001
39 on the following findings:
40

- 41 1. *The proposed general plan amendment is in the public interest. By allowing the*
42 *maximum floor area ratio to range from 0.4 to 1.0, GPA20-001 introduces*
43 *flexibility into the evaluation of hotel, medical facility, and unique development*
44 *projects in the Community Commercial land use designation, which encourages*
45 *and enables innovative and current commercial site design. The Planning*

1 Commission has the authority to consider each project on a case-by-case basis
2 ensuring project accountability and quality.
3

- 4 2. *The proposed amendment is not detrimental to public health, safety, and welfare.*
5 GPA20-001 is a text amendment only and, therefore, will not have a direct effect
6 on public health, safety, and welfare. Additionally, GPA20-001 requires hotel,
7 medical facility and unique projects that are seeking a floor area ratio exception
8 to obtain an administrative use permit or a conditional use permit which requires,
9 among other things, demonstration that the project "will not be detrimental to the
10 public health, safety, or welfare, or materially injurious to properties or
11 improvements in the vicinity."
12
- 13 3. *The proposed amendment is consistent with the overall vision, goals, and*
14 *objectives contained in the General Plan.* GPA20-001 meets the following
15 components of the General Plan:
16 a. *Section 1.2 A City in Transition.* At Hemet's core is a strong community
17 willing to invest in opportunities for future and make forward-thinking
18 decisions to meet the pressing needs of a changing economy and a
19 growing and diverse population.
20 b. *Policy LU-1.12 Flexibility Over Time.* Require development to occur within
21 the designated range of density and intensity, but allow for flexibility in the
22 types of uses to account for changes in industrial and employment
23 markets, retail commercial enterprises, and housing needs and
24 characteristics . . . "
25 c. *Policy LU-4.3 Infill Development and Re-Use.* Actively promote the
26 adaptive reuse and infill of economically underutilized, obsolete, and
27 dilapidated commercial and industrial sites . . . "
28 d. *Goal LU-8 Florida Corridor.* Revitalize and enhance the land uses and
29 appearance of the Florida-Devonshire-Acacia Corridors to create an
30 integrated mix of commercial, office, hospitality, and residential uses.
31 e. *Policy LU-11.1 Attract New Business.* Support existing businesses and
32 seek to attract new businesses and industries that strengthen and
33 diversify Hemet's tax revenue base, improve wage and salary levels,
34 increase the variety of job opportunities, and employ the resident labor
35 force.
36

37 **SECTION 3: PLANNING COMMISSION ACTIONS**

38
39 The Planning Commission hereby recommends that the City Council adopt Resolution
40 Bill No. 20-016 approving General Plan Amendment 20-001, which is attached hereto
41 and incorporated herein by reference as Exhibit "1."
42

43 **PASSED, APPROVED, AND ADOPTED** this 15th day of September, 2020 by the
44 following vote:
45

Planning Commission Resolution Bill No. 20-016
GPA20-001

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AYES:
NOES:
ABSTAIN:
ABSENT:

Vince Overmyer, Chairman
Hemet Planning Commission

ATTEST:

Gabriela Hernandez, Records Secretary
Hemet Planning Commission

Planning Commission Resolution Bill No. 20-016
GPA20-001

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PAGE BREAK





AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY

December 10, 2020

Ms. Lauren Sotelo, Senior Planner
March Joint Powers Authority
14205 Meridian Parkway, Suite 140
Riverside CA 92518

CHAIR
Russell Batts
Desert Hot Springs

VICE CHAIR
Steven Stewart
Palm Springs

COMMISSIONERS

Arthur Butler
Riverside

John Lyon
Riverside

Steve Mazzeo
Lake Elsinore

Richard Stewart
Moreno Valley

Gary Youmans
Temecula

STAFF

Director
Simon A. Houseman

Paul Rull
Barbara Santos

County Administrative Center
4080 Lemon St., 14th Floor.
Riverside, CA 92501
(951) 955-5132

www.aluc.org

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW –
DIRECTOR'S DETERMINATION**

File No.: ZAP1441MA20
Related File No.: GPA20-01 (General Plan Amendment), SP20-01 (Specific Plan Amendment, No. 8) (updated exhibits)
APN: Multiple

Dear Ms. Sotelo:

As authorized by the Riverside County Airport Land Use Commission (ALUC) pursuant to its Resolution No. 2011-02, as ALUC Director, I have reviewed March Joint Powers Authority (MJPA) (updated) cases GPA20-01 (General Plan Amendment), SP20-01 (Specific Plan Amendment), a proposal to amend the MJPA General Plan and the March Business Center Specific Plan SP-1, Amendment No. 8 to rename Bandit Boulevard to Gless Ranch Road, and to update truck routes in the South Campus portion of the plan, **identifying Caroline Way as a truck route. The original project was found consistent by ALUC on September 24, 2020.**

The proposed amendment will revise text, maps, plans, figures, and tables, within the MJPA General Plan and the March Business Center Specific Plan SP-1, Amendment No. 8, to correctly identify Gless Ranch Road (previously referred to as Bandit Boulevard), and to correctly reflect the approved truck route within the March Business Center. **The proposed updated exhibits now show Caroline Way as a truck route.**

The proposed amendment does not involve changes in development standards or allowable land uses that would increase residential density or non-residential intensity. Therefore, this amendment has no possibility for having an impact on the safety of air navigation within airport influence areas located within the unincorporated areas of Riverside County.

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan.

This determination of consistency relates to airport compatibility issues and does not necessarily constitute an endorsement of the proposed amendment.

If you have any questions, please contact Paul Rull, ALUC Principal Planner, at (951) 955-6893.

AIRPORT LAND USE COMMISSION

Sincerely,
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

Simon A. Housman

Simon A. Housman, ALUC Director

cc: ALUC Case File

Y:\AIRPORT CASE FILES\March\ZAP1441MA20\ZAP1441MA20.LTR.doc

MARCH BUSINESS CENTER - SPECIFIC PLAN AMENDMENT



Legend:
 ——— Truck Route
 - - - - - Bike Route

Notes:

1. Existing or planned off-site facilities will be constructed and maintained by others.
2. The class II bike lane on Cactus Avenue will transition to a class III facility as it approaches the eastern edge of the property.
3. Route applies to commercial vehicles having a gross weight in excess of five tons.
4. Physical counter-measures to prevent southbound to westbound truck movements at Meridian Pkwy. and Opportunity Way Intersections with Van Buren Blvd.

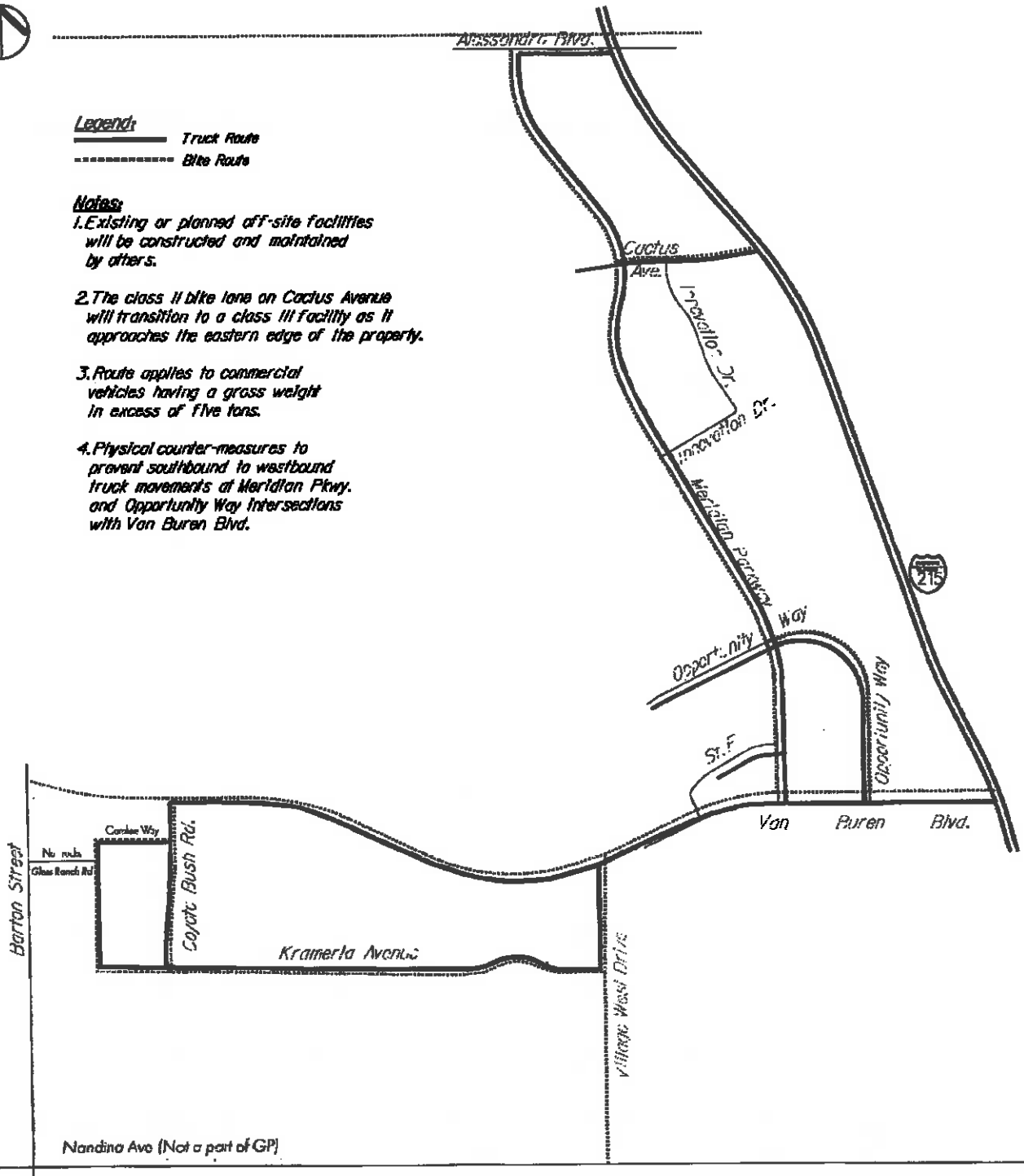


FIGURE V-6

TRANSPORTATION SYSTEMS PLAN

MARCH BUSINESS CENTER - GENERAL PLAN AMENDMENT

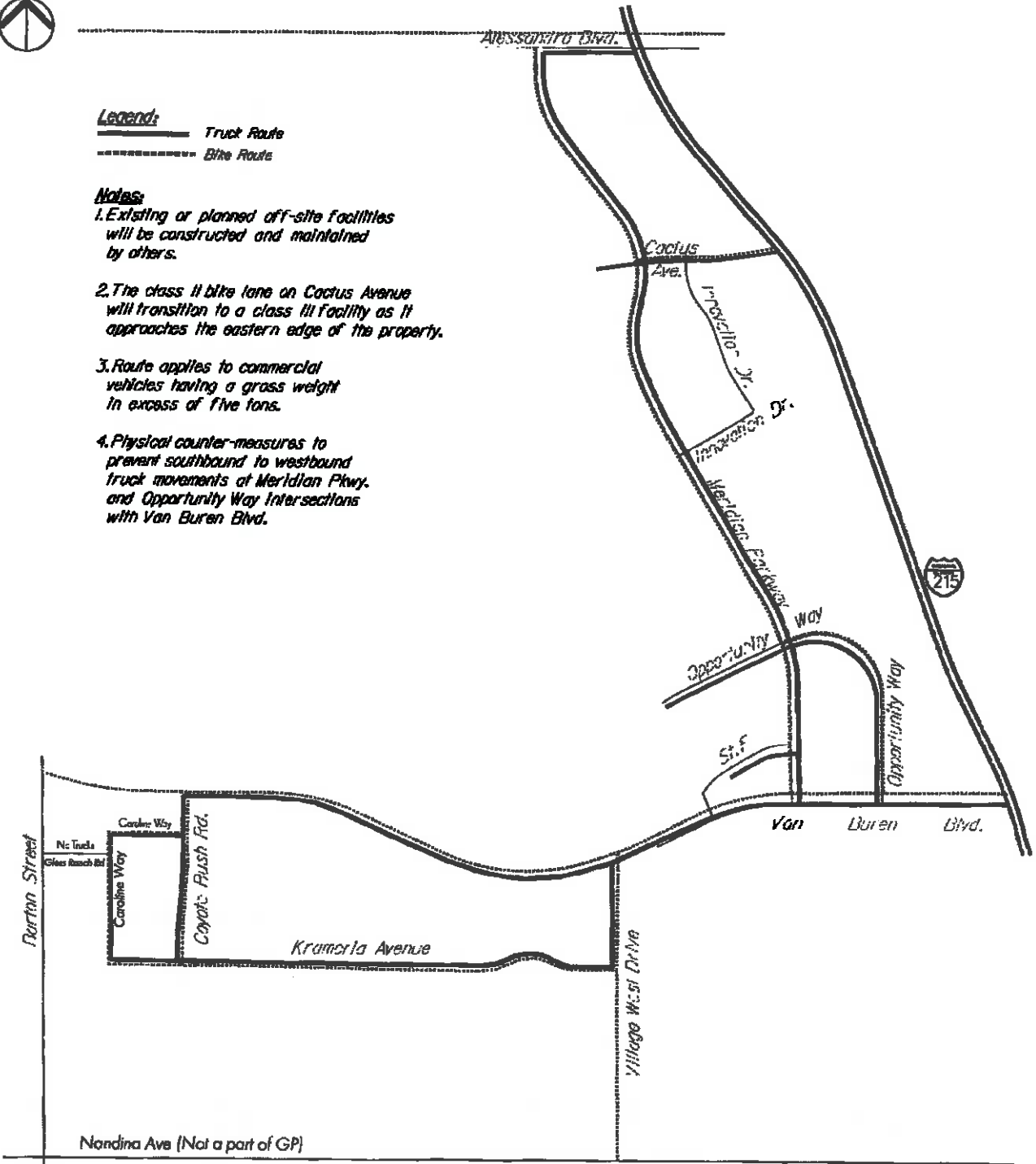


Legend:

- Truck Route
- Bike Route

Notes:

1. Existing or planned off-site facilities will be constructed and maintained by others.
2. The class II bike lane on Cactus Avenue will transition to a class III facility as it approaches the eastern edge of the property.
3. Route applies to commercial vehicles having a gross weight in excess of five tons.
4. Physical counter-measures to prevent southbound to westbound truck movements at Meridian Pkwy. and Opportunity Way Intersections with Van Buren Blvd.



K:\Meridian\South Campus\Specific Plan Amendment\Exhibits\Figures\DWG\X-FigV-8-Transportation Plan.dwg 8-14-17-12:48 PM

FIGURE 2-3B

PROPOSED TRANSPORTATION SYSTEMS PLAN

Specific Plan



SP-1 Amendment #8

May 2020

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I. PROJECT SUMMARY

BACKGROUND OF THE SPECIFIC PLAN AMENDMENT SP-1 A78

The March Business Center project was the subject of a Specific Plan and a Focused Environmental Impact Report (FEIR) certified by the March Joint Powers Commission in February 2003 (SCH# 2002071089). The March Business Center Specific Plan is here forward referred to the previously adopted Specific Plan. The previously adopted Specific Plan described an industrial business park to be developed on approximately 1,290 acres in the northwestern portion of Riverside County, California, on land that was formerly a part of the March Air Force Base (MAFB). Figure I-1 depicts the location of the project in a regional context. The previously adopted Specific Plan area is shown in Figure I-2. Land use authority for the Specific Plan area resides with the March Joint Powers Authority (JPA). The March JPA General Plan was approved in 1999 and divided the MAFB property into planning subareas. The March Business Center is located in the northern portion of West March Planning Subarea. This Specific Plan guides the development of the March Business Center within the framework of the March JPA General Plan.

The previously adopted Specific Plan was divided into two elements. The northern portion, referred to as the North Campus, comprises approximately 662 acres, and is bounded by Alessandro Boulevard to the north, I-215 to the east, and Van Buren Boulevard to the south. It should be noted, North Campus previously adopted SP-5 in 2010 which reconfigured a portion of North Campus' developable lots. The lots that were not included in that specific plan amendment are still a part of this Specific Plan. As such, this Specific Plan Amendment focuses on revisions to the South Campus which encompasses approximately 628 acres, and is located south of Van Buren Boulevard and west of an existing golf course and cemetery. The March Business Center development footprint would include 962 acres on 153 developable lots. Approximately 328 acres will be allocated for parks, landscape buffers, drainage basins, and roads.

The following land use types specified in the March JPA General Plan will be developed as part of the March Business Center project:

- **Business Park (BP):** including administrative, financial, light manufacturing, and commercial services.
- **Industrial (IND):** including manufacturing, warehousing, and associated uses.
- **Office (OF):** commercial office building accommodating professional and/or administrative services.
- **Mixed Use (MU):** complementary uses, including commercial retail, office, research and development, industrial, and others.
- **Commercial (COM):** retail and service oriented land uses.
- **Park/Recreation/Open Space (P/R/OS):** primarily passive open spaces and recreational areas.
- **Public facilities (PF):** wide range of public, quasi-public, and private uses such as schools, public cultural and historical facilities, government administrative offices and facilities, public utilities, and major transportation corridors.

Following FEIR certification and permitting, development commenced on the North Campus. Numerous parcels were developed and are now occupied. South Campus, is currently under construction with mass graded lots, utilities, roadway improvements, and traffic devices underway. Specific Plan Amendment

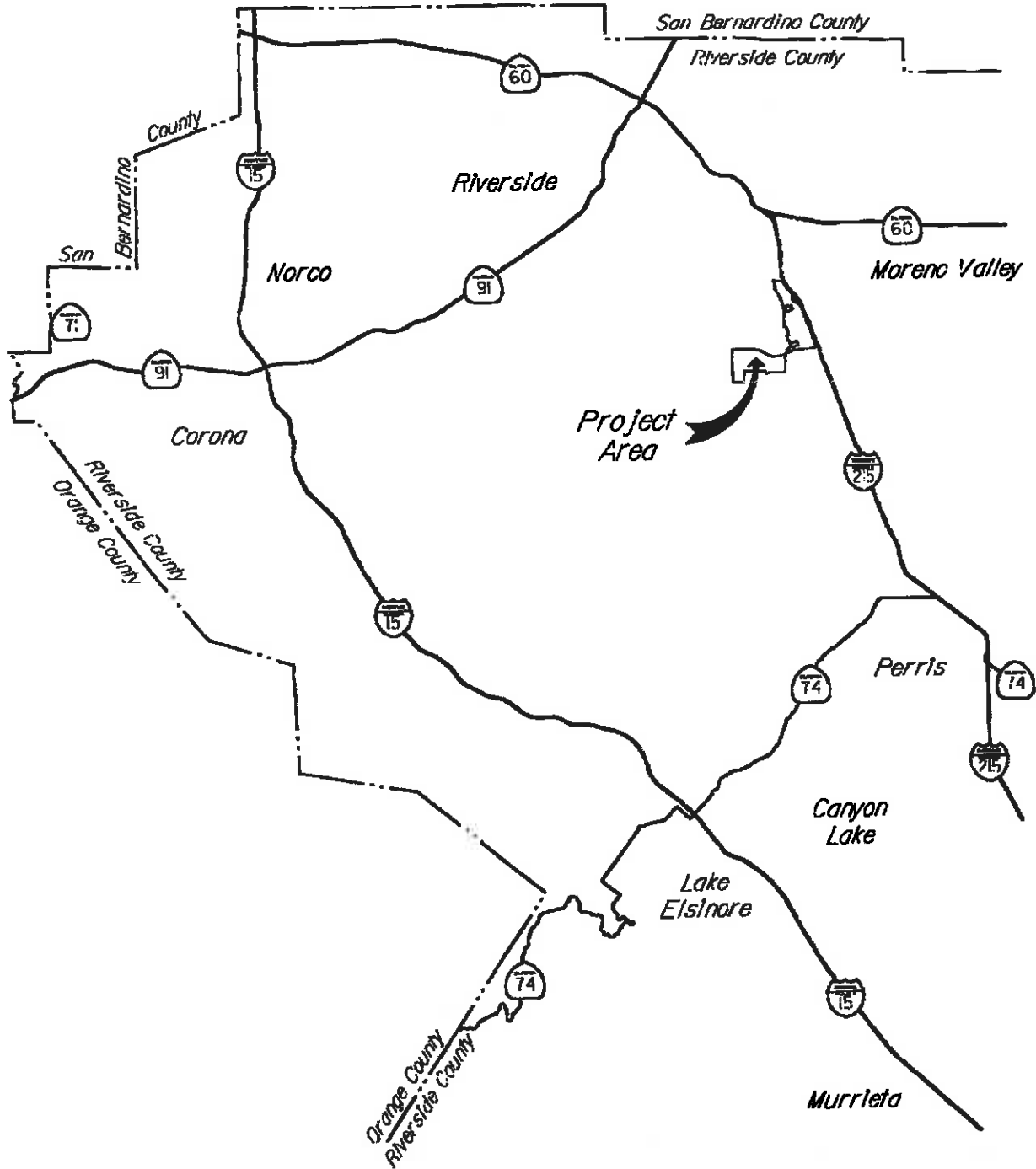
SP-1 A78 will shift land uses and reconfigure street and lot layout in four Specific Plan areas to more efficiently accommodate needed uses in the Meridian Park South Campus as follows:

- ~~Change 5.84 acres of Commercial to Business Park located just slightly southwest of the corner of Van Buren Boulevard and Village West Drive on Parcel 28.~~
- ~~Change 3.7 acres (3.4 net acres) of Parcel 74 from Office to Commercial, located just southeast of the intersection of Orange Terrace Parkway and Van Buren Boulevard (leaving a remaining 5.72 acres designated as Office).~~
- ~~Change 10.77 acres of Business Park to Office, at the southwest corner of Van Buren and Coyote Bush Road on Parcel 73.~~
- ~~Change 22.93 acres of land at the northeast corner of Krameria and Coyote Bush (Parcels 4/5) from Business Park to Industrial, while changing the approximate 19.84 acres located further to the east (Parcels 5/6) from Industrial to Business Park.~~

- Increase of 15.3 acres of Parks/Open Space
- Increase of 65.8 acres of Industrial
- Increase of 17.1 acres of Commercial
- Increase of 4.5 acres of Mixed Use
- Increase of 0.9 acres of Public Facilities
- Reduction of 27.4 acres of Office
- Reduction of 61.3 acres of Business Park
- Remove Street K, Street O, Street T, and Street U
- Reconfigure Street Y and rename as Caroline Way
- Reconfigure Street P and rename as Gless Ranch Road
- Prohibition of trucks on Gless Ranch Road
- Extension of Village West Drive to the south to provide a 54-foot wide roadway with two through lanes, a center striped median, a bike lane, and sidewalks on each side of the roadway between Van Buren Boulevard to the north and Nandina Avenue to the south.
- Modification to the definition of Business Enterprise in Appendix A, Land Use Definitions, to apply to enclosed buildings that occupy 200,000 square feet or less of divisible building space within the South Campus.
- Modifications to the definitions of Wholesale, Storage and Distribution – Medium and Wholesale, Storage and Distribution – Heavy in Appendix A, Land Use Definitions, to accommodate cold storage use.
- Addition of a definition of Grocery Store in Appendix A, Land Use Definitions.
- Addition of Grocery Store under Commercial Uses in Table III-1 as a permitted use for the Commercial land use designation.
- Revision to footnote 7 of Table III-1 to exclude Grocery Stores from the requirement to obtain a use permit. Alcohol sales at Grocery Stores would still require a use permit.

The increase in Parks/Open Space, Industrial, Commercial, Mixed Use, and Public Facilities land uses results in an increase of 103.6 acres, while the decrease in Office and Business Park results in a decrease of 88.7 acres. This discrepancy of acreage is a result of the addition of 10 acres comprising a portion of Lot 31 that were previously not a part of the Project, as well as the inclusion of 4.9 additional acres resulting from the removal of Street K, Street O, Street T, and Street U.

MARCH BUSINESS CENTER - SPECIFIC PLAN AMENDMENT

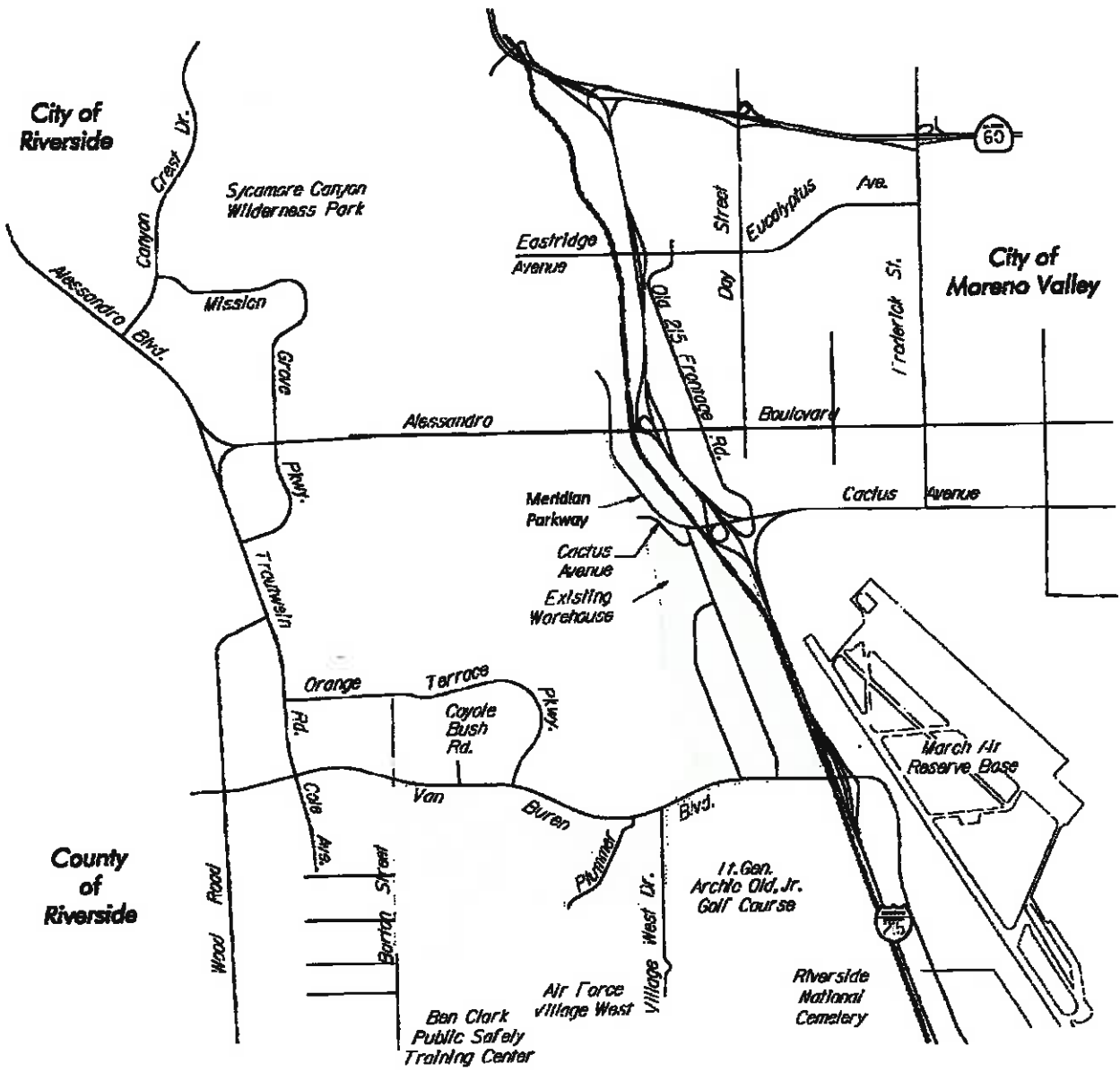


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FIGURE I-1

REGIONAL LOCATION MAP

MARCH BUSINESS CENTER - SPECIFIC PLAN AMENDMENT



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FIGURE I-2

LOCAL VICINITY MAP

The previously adopted Specific Plan, Design Guidelines, FEIR, Design Implementation Review Committee, Statutory Development Agreement, Settlement Agreement and other official Joint Powers Authority documents, ordinances and resolutions refer to the overall project as the “March Business Center” project. However, the project has been marketed and branded under the name “Meridian” and has come to be known by that name over the past years. References to existing approved plans and reports in this Specific Plan Amendment use the original name of the project (i.e., March Business Center), which is consistent with the titles of these reports and plans. In the interest of clarity, the following terminology is used to differentiate this Specific Plan Amendment from the previously adopted Specific Plan:

- *“the previously adopted Specific Plan”*: the 2003 March Business Center Specific Plan.
- *“this Specific Plan Amendment”*: the land use, development regulation and other amendments to the previously adopted Specific Plan contained in this document.

This Specific Plan Amendment presents all revisions in a strike-through/underline format so the reader can quickly identify all changes from the previously adopted Specific Plan. In addition to changes resulting from this Specific Plan Amendment, other revisions have been incorporated to address changed circumstances or new information that has come about since the previously adopted Specific Plan was approved in 2003. For example, current street names have been incorporated (i.e., the former Street Z is now called Meridian Parkway). In addition, editorial changes (such as using consistent verb tenses) have been incorporated to improve the clarity of this Specific Plan Amendment document.

Regional access to and from the Specific Plan area will be provided via the I-215 freeway and interchanges with Alessandro Boulevard, Cactus Avenue and Van Buren Boulevard. This Specific Plan Amendment will construct a collector and arterial street network to facilitate access to and from the areas to be developed. Project internal streets will be public roadways to be maintained by the March Joint Powers Authority.

II. INTRODUCTION

A. LAND USE OVERVIEW

This Specific Plan Amendment has been prepared to establish guidelines for development accommodating Business Park, Industrial, Office, Mixed Use, and Commercial land uses. The objective of the Specific Plan is to guide and regulate the development of the March Business Center in accordance with the March JPA General Plan. The Specific Plan fulfills both planning and regulatory functions. As such, this document contains the regulations, procedures, and development standards necessary to accomplish both objectives.

The March Business Center Specific Plan document was prepared under the authority granted to the March JPA by the California Government Code Title 7, Division 1, Article 8, Section 65450. The Notice of Preparation (NOP) for the Draft Environmental Impact Report (EIR) was issued by the March JPA as the lead agency. The State of California encourages agencies to adopt Specific Plans whether by resolution (to establish a policy document) or by ordinance (to establish a regulatory document). The Specific Plan document is a regulatory document and is therefore subject to adoption by ordinance.

All future development plans, tentative parcel and/or tract map(s), or other similar entitlements for properties located within the boundaries of this Specific Plan shall be consistent with the regulations set forth in this document and with all other applicable March JPA policies and regulations. All regulations, conditions, and programs contained in this document shall be deemed separate, distinct and independent provisions of the March Business Center Specific Plan. In the event that any such provision is held invalid or unconstitutional by a state or federal court of competent jurisdiction, the validity of all remaining provisions of this Specific Plan shall not be affected.

A Focused EIR (SCH No. 2002071089) was prepared for the Specific Plan in accordance with the California Environmental Quality Act (CEQA). The EIR evaluated the land use plan, circulation, and infrastructure improvements to be provided under the March Business Center Specific Plan and the potential impacts associated with their implementation. The EIR also identified actions to mitigate potential impacts. Many mitigation measures have been incorporated into this document. A Master EIR (MEIR) (SCH No. 97071095) for the entire General Plan area was certified in 1999. The Specific Plan implements the policies of the General Plan.

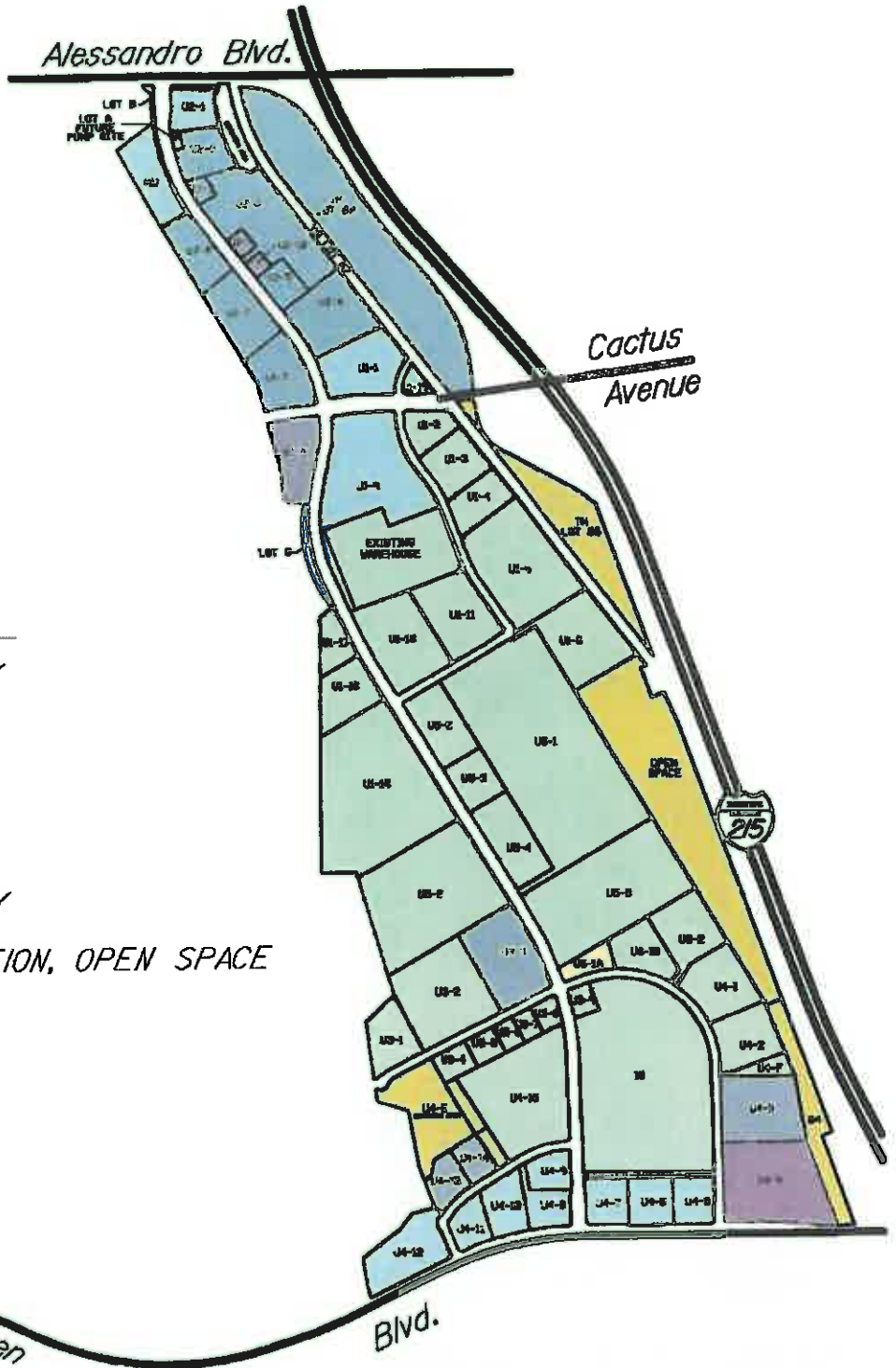
The General Plan area encompasses the 6,500 acres of the former March Air Force Base, including 4,400 acres identified for disposal and reuse by the Department of Defense. The development intensity of the March Business Center is substantially less than what was assumed for the project in the MEIR. In terms of traffic generation, the General Plan EIR assumed 131,400 daily trips. The previously approved Specific Plan would generate 88,100 daily trips. This reflects a daily traffic reduction of more than 33 percent.

B. LOCATION AND ACCESS




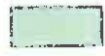



The March Business Center Specific Plan approved on February 26, 2003 is located within the southern portion of the West March Planning Subarea. The March Business Center Specific Plan applies to both the North and South Campuses; however, modifications proposed by Specific Plan Amendment SP-1 A78 are limited to the South Campus. This Specific Plan Amendment includes revisions to land uses, ultimate roadway configuration, modifies the definitions of Business Enterprise, Wholesale, Storage and

Distribution – Medium, and Wholesale, Storage and Distribution - Heavy, and adds a definition for Grocery Store, thus effects the entire—approximately 628 acres—located within the South Campus, located south of Van Buren Boulevard between Barton Street and Village West Drive. Figures II-1A and II-1B illustrates the location of the North Campus and the South Campus. Figure II-2 is an aerial photograph showing the project location and the boundaries of adjacent developments.

MARCH BUSINESS CENTER - SPECIFIC PLAN AMENDMENT



LEGEND:

-  BUSINESS PARK
-  COMMERCIAL
-  MIXED USE
-  INDUSTRIAL
-  PUBLIC FACILITY
-  PARK, RECREATION, OPEN SPACE
-  OFFICE

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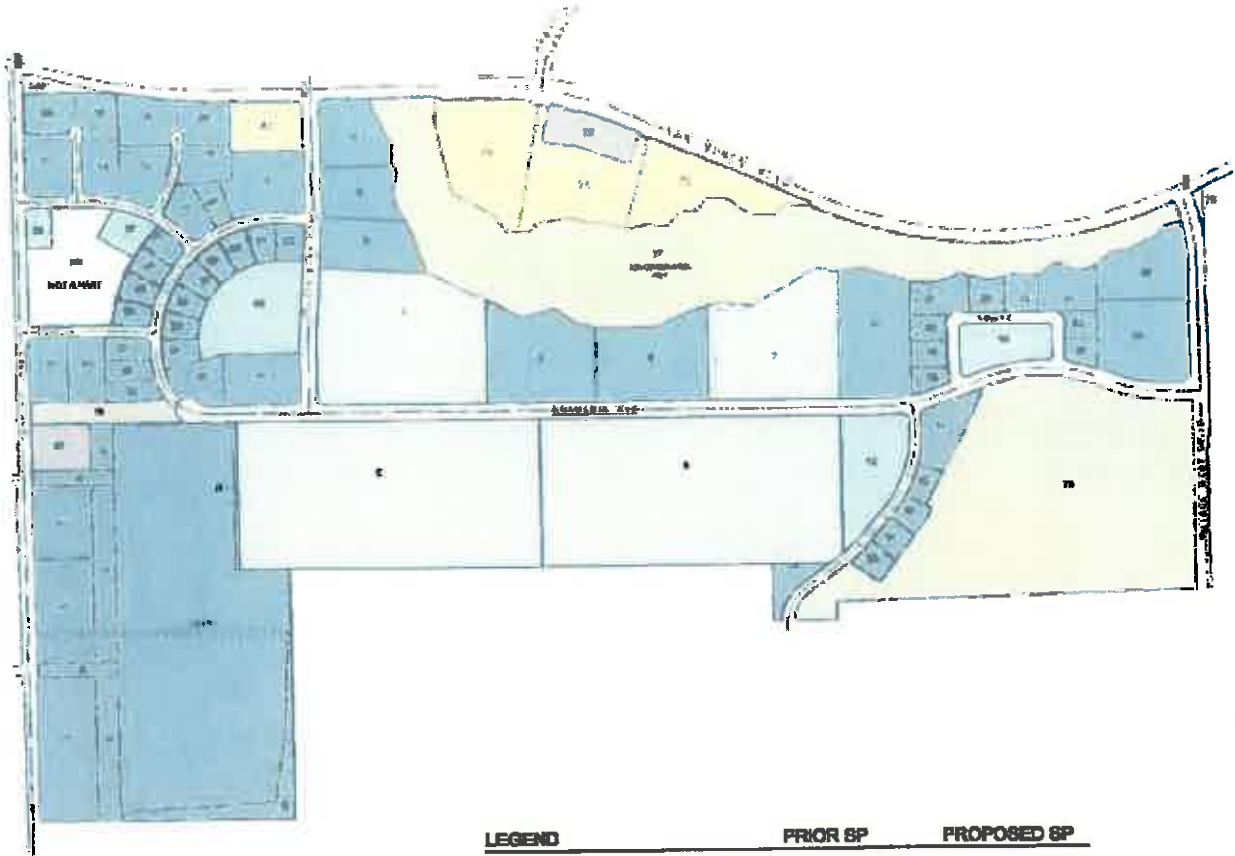
FIGURE II-1A

EXISTING SPECIFIC PLAN: NORTH CAMPUS

MARCH BUSINESS CENTER - SPECIFIC PLAN AMENDMENT

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| LEGEND | PRIOR SP | PROPOSED SP |
|--------------------------|-----------------|-----------------|
| OFFICE | 32.3 AC | 32.0 AC |
| COMMERCIAL | 6.8 AC | 6.4 AC |
| MIXED USE | 23.3 AC | 23.3 AC |
| BUSINESS PARK | 231.6 AC | 201.7 AC |
| INDUSTRIAL | 134.5 AC | 134.5 AC |
| PARK / OPEN SPACE | 125.0 AC | 125.0 AC |
| NOT A PART | 0.0 AC | 0.0 AC |
| TOTAL NET ACREAGE | 563.9 AC | 663.3 AC |

FIGURE II-1B

EXISTING SPECIFIC PLAN: SOUTH CAMPUS

MARCH BUSINESS CENTER - SPECIFIC PLAN AMENDMENT

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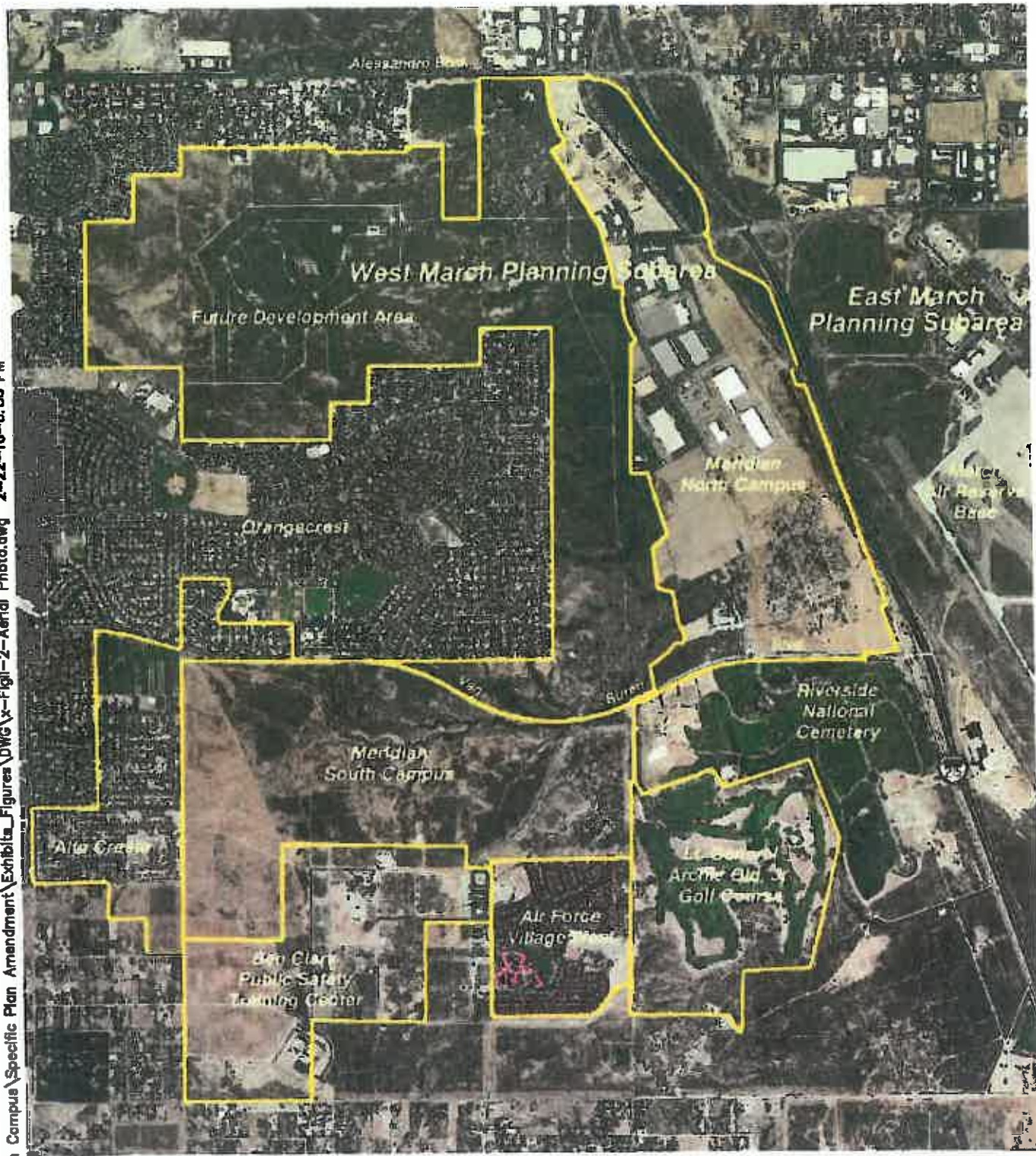


FIGURE II-2
AERIAL PHOTO

In the South Campus, Krameria Avenue will terminate at its intersection with Caroline Way, in a cul-de-sac west of its intersection with Coyote Bush Road. The South Campus will take access to/from Van Buren Boulevard via Coyote Bush Road and Village West Drive. Gless Ranch Road Streets Y, Q, and P will provide access for passenger vehicles to and from Barton Street south of Van Buren Boulevard; trucks will not be permitted on Gless Ranch Road east of Barton Street. The project roadway network will be public roadways, to be maintained by the March Joint Powers Authority.

II-C. BACKGROUND AND HISTORY

Since 1988, the federal government closed and realigned military bases throughout the United States. In order to limit the economic disruption caused by base closures, the California State Legislature authorized the formation of joint powers authorities to regulate the redevelopment of closed/realigned military installations. Joint powers authorities are empowered to activate a redevelopment agency for each base to be closed. In 1993, the federal government called for the realignment of MAFB and for a substantial reduction in its military use. In April 1996, March Air Force Base was re-designated as an Air Reserve Base (ARB). The communities of Moreno Valley, Perris, the City of Riverside, and the County of Riverside formed the March JPA pursuant to Article 1, Chapter 5, Division 7, Title 1 (commencing with Section 6500 et seq.). The JPA prepared a number of planning, policy and regulatory documents to guide the redevelopment of the former MAFB. These documents include:

- Final Environmental Impact Statement: Disposal of Portions of March Air Force Base (February 1996)
- Final Environmental Impact Report for the March Air Force Base Redevelopment Project (June 1996)
- Redevelopment Plan for the March Air Force Base Redevelopment Project (June 1996)
- March Joint Powers Authority Development Code (July 1997)
- General Plan of the March Joint Powers Authority (September 1999)
- Master Environmental Impact Report for the General Plan of the March Joint Powers Authority (September 1999)
- March Business Center Statutory Development Agreement (2003)
- March JPA General Plan Amendment (February 2003)
- March Business Center Design Guidelines (2003)
- March Business Center Focused Environmental Impact Report (2003)
 - Addenda to the certified 2003 FEIR, including:
 - Tentative Tract Map 30857 Amendment (April 2007)
 - Ordinance #JPA 08-01: An Ordinance of the March Joint Powers Commission of the March Joint Powers Authority Amending the Meridian Specific Plan to Remove the Arnold Heights School Overlay Zone (June 2008)
 - March Business Center Unit 1, Lot 2, Addendum to the FEIR (January 2009)
 - Meridian Specific Plan Amendment, SP-5, An Amendment to the North Campus of the Meridian Specific Plan, (also known as the March Business Center Specific Plan), March JPA, July 2010
 - General Plan Amendment GP 15-01 (2016)
 - Specific Plan Amendment SP-1, A5 (2016)
 - Tentative Tract Map 30857, Amendment #3 (2016)
 - The Subsequent Environmental Impact Report for the Meridian Specific Plan (SP-5), (State Clearinghouse #2009071069)
 - Addendum to the Certified FEIR (2016)
 - Specific Plan Amendment SP-1, A6 (2017)

- General Plan Amendment GP 18-01 (2017)
- Tentative Tract Map 30857, Amendment #4 (2017)
- Addendum to the Certified FEIR (2017)
- Specific Plan Amendment SP-1, A7 (2018)

A vast majority of the 1,178-acre area situated along the western boundary of the North Campus has been placed into conservation easements. The portion of this area outside of the conservation easements could be developed in the future but is not the subject of any current development plans. The previously adopted Specific Plan infrastructure has been planned for the development within this area in accordance with the land uses identified in the General Plan.

D. PLANNING CONTEXT

Figure II-3 depicts the land use designations in the Specific Plan area as shown in the existing General Plan. Figure II-4 illustrates General Plan land uses that account for the March Business Center as proposed under this Specific Plan. The March JPA General Plan has been amended concurrently with this Specific Plan to rectify minor differences in land use designations within the Specific Plan area. Figure II-5A and 5B depict zoning for the North and South Campus.

E. FOCUSED ENVIRONMENTAL IMPACT REPORT

A Focused EIR was previously prepared in accordance with the provisions under CEQA. The EIR evaluated the land use plan, circulation and infrastructure improvements associated with the March Business Center Specific Plan and the potential impacts that would result from their implementation. The EIR serves as a project-wide environmental document for the March Business Center Specific Plan Area. Review and permit approval by the Air Quality management district is required for all stationary source emissions other than natural gas and electricity, for projects with the possibility of emitting air pollutants. Also, any project which deviates or has impacts not considered in the Focused EIR shall require additional environmental documentation. Together, the March Business Center Specific Plan, Tentative Map and EIR provide a path to properly develop the project site, taking into account policies, goals, objectives and environmental considerations of the March JPA General Plan. The Focused EIR was previously certified by resolution. The changes proposed for this Specific Plan Amendment are being evaluated under a separate EIR (SCH2020059028). Environmental review under NEPA will occur concurrently for the Village West Drive Extension easement.

F. DISCRETIONARY ACTIONS

The following discretionary actions will be required as part of SP-1 A78 project:

1. General Plan Amendment

~~General Plan Amendment No. 20-0118-01 modifies the General Plan Circulation and Land Use Maps to reflect the changes to land uses and ultimate roadway configuration as identified below, such that the total amount of acres designated for commercial and office remain comparable to what is currently designated (all acreages in gross acres).~~

- Increase of 15.3 acres of Parks/Open Space
- Increase of 65.8 acres of Industrial
- Increase of 17.1 acres of Commercial
- Increase of 4.5 acres of Mixed Use
- Increase of 0.9 acres of Public Facilities
- Reduction of 27.4 acres of Office
- Reduction of 61.3 acres of Business Park
- Remove Street K, Street O, Street T, and Street U
- Reconfigure Street Y and rename as Caroline Way
- Reconfigure Street P and rename as Gless Ranch Road
- Prohibition of trucks on Gless Ranch Road
- Extension of Village West Drive to the south to provide a 54-foot wide roadway with two through lanes, a center striped median, a bike lane, and sidewalks on each side of the roadway between Van Buren Boulevard to the north and Nandina Avenue to the south.
- Modification to the definition of Business Enterprise in Appendix A, Land Use Definitions, to apply to enclosed buildings that occupy 200,000 square feet or less of divisible building space within the South Campus.
- Modifications to the definitions of Wholesale, Storage and Distribution – Medium and Wholesale, Storage and Distribution – Heavy in Appendix A, Land Use Definitions, to accommodate cold storage use.
- Addition of a definition of Grocery Store in Appendix A, Land Use Definitions.
- Addition of Grocery Store under Commercial Uses in Table III-1 as a permitted use for the Commercial land use designation.
- Revision to footnote 7 of Table III-1 to exclude Grocery Stores from the requirement to obtain a use permit. Alcohol sales at Grocery Stores would still require a use permit.

The increase in Parks/Open Space, Industrial, Commercial, Mixed Use, and Public Facilities land uses results in an increase of 103.6 acres, while the decrease in Office and Business Park results in a decrease of 88.7 acres. This discrepancy of acreage is a result of the addition of 10 acres comprising a portion of Lot 31 that were previously not a part of the Project, as well as the inclusion of 4.9 additional acres resulting from the removal of Street K, Street O, Street T, and Street U.

- ~~▪ Change 6.57 gross acres (5.84 net acres due to roadway dedications) of land in Parcel 28 designated as Commercial, located just southwest of the corner of Van Buren Boulevard and Village West Drive, to Business Park.~~
- ~~▪ Change 3.7 gross acres (3.49 net acres due to roadway dedications) of land in the northern portion of Parcel 74, located just southeast of the intersection of Orange Terrace Parkway and Van Buren Boulevard, from Office to Commercial (leaving a remaining 9.42 gross acres designated as Office).~~
- ~~▪ Change approximately 25.77 gross acres of land at the northeast corner of Krameria and Coyote Bush (Parcels 4/5) from Business Park to Industrial, while changing the approximate 21.78 gross acres located further to the east (Parcels 5/6) from Industrial to Business Park.~~
- ~~▪ Change the current land use designation from Business Park to Office on approximately 4.11 gross acres (3.65 net acres due to roadway dedications) on Parcel 41 at the southwest corner of Coyote Bush and Van Buren Boulevard.~~

2. Specific Plan Amendment

The following Specific Plan changes would result in a revised March Business Center Specific Plan (SP-1, Amendment #78) through modification of the land use designation and zoning of several areas and will specifically modify Figure II-1A, Figure II-1B, Figure II-3, Figure II-4, Figure II-5A, Figure II-5B, Figure II-5C, Figure III-1A, Figure III-1B, Figure V-1, Figure V-2, Figure V-3, Figure V-4, Figure V-5A, Figure V-5B, Figure V-5C, Figure V-5C1, Figure V-5E, V-5E1, Figure V-5G, Figure V-6, Figure VI-1, Figure VI-2, Figure VI-3, and Figure VI-4 H-1B, Figure II-3, Figure II-4, Figure II-5B, Figure II-5C, and Figure III-1 of the March Business Center Specific Plan (SP-1).

- Increase of 15.3 acres of Parks/Open Space
- Increase of 65.8 acres of Industrial
- Increase of 17.1 acres of Commercial
- Increase of 4.5 acres of Mixed Use
- Increase of 0.9 acres of Public Facilities
- Reduction of 27.4 acres of Office
- Reduction of 61.3 acres of Business Park
- Remove Street K, Street O, Street T, and Street U
- Reconfigure Street Y and rename as Caroline Way
- Reconfigure Street P and rename as Gless Ranch Road
- Prohibition of trucks on Gless Ranch Road
- Extension of Village West Drive to the south to provide a 54-foot wide roadway with two through lanes, a center striped median, a bike lane, and sidewalks on each side of the roadway between Van Buren Boulevard to the north and Nandina Avenue to the south.
- Modification to the definition of Business Enterprise in Appendix A, *Land Use Definitions*, to apply to enclosed buildings that occupy 200,000 square feet or less of divisible building space within the South Campus.
- Modifications to the definitions of Wholesale, Storage and Distribution – Medium and Wholesale, Storage and Distribution – Heavy in Appendix A, *Land Use Definitions*, to accommodate cold storage use.
- Addition of a definition of Grocery Store in Appendix A, *Land Use Definitions*.
- Addition of Grocery Store under Commercial Uses in Table III-1 as a permitted use for the Commercial land use designation.
- Revision to footnote 7 of Table III-1 to exclude Grocery Stores from the requirement to obtain a use permit. Alcohol sales at Grocery Stores would still require a use permit.

The increase Parks/Open Space, Industrial, Commercial, Mixed Use, and Public Facilities land uses results in an increase of 103.6 acres, while the decrease in Office and Business Park results in a decrease of 88.7 acres. This discrepancy of acreage is a result of the addition of 10 acres comprising a portion of Lot 31 that were previously not a part of the Project, as well as the inclusion of 4.9 additional acres resulting from the removal of Street K, Street O, Street T, and Street U.

- ~~Change 5.84 acres of Commercial to Business Park located just slightly southwest of the corner of Van Buren Boulevard and Village West Drive on Parcel 28.~~

~~▪ Change 3.7 acres (3.49 net acres) of Parcel 74 from Office to Commercial, located just southeast of the intersection of Orange Terrace Parkway and Van Buren Boulevard (leaving a remaining 5.72 acres designated as Office).~~

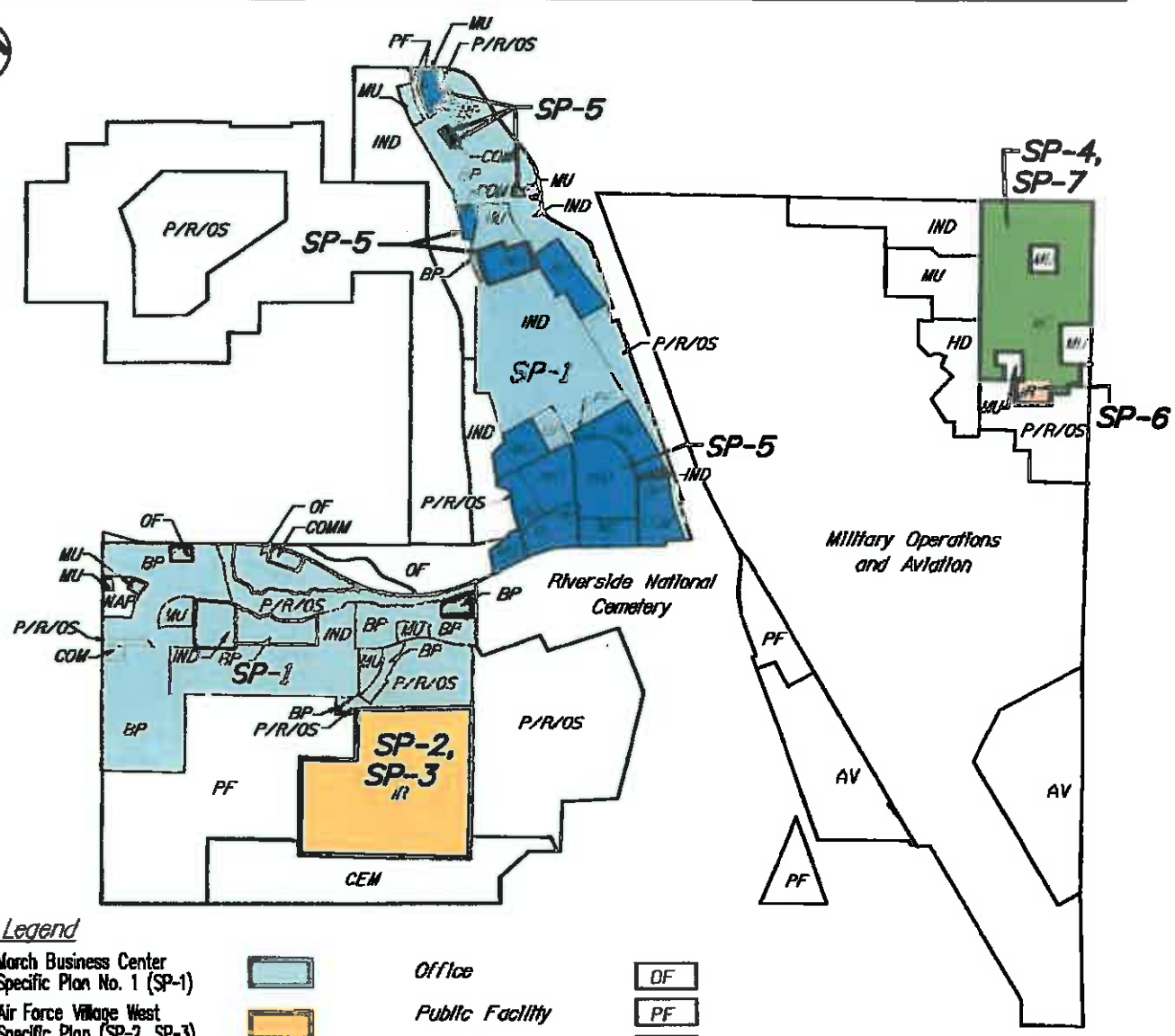
~~▪ Change 10.77 acres of Business Park to Office, at the southwest corner of Van Buren and Coyote Bush Road on Parcel 73.~~

~~Change 22.93 acres of land at the northeast corner of Krameria and Coyote Bush (lots 4/5) from Business Park to Industrial, while changing the approximate 19.84 acres located further to the east (lots 5/6) from Industrial to Business Park.~~

~~Thus, the total acres of land within these seven parcels are currently designated 134.6 acres Industrial, 8.8 acres Commercial, and 230.0 acres Business Park in the existing Specific Plan. Under the proposed Project, the totals would be 134.5 acres Industrial, 6.4 acres Commercial, 32.0 of Office, and 232.1 acres Business Park. Thus the total acres of land for the seven land use designations within the South Campus of the March Business Center are identified below. The Any minor variations to acreages have to do with rounding numbers up and down accordingly.~~

- ~~▪ Parks/Open Space: 140.3 acres within the South Campus of the March Business Center~~
- ~~▪ Industrial: 200.3 acres within the South Campus of the March Business Center~~
- ~~▪ Commercial: 23.5 acres within the South Campus of the March Business Center~~
- ~~▪ Office: 4.6 acres within the South Campus of the March Business Center~~
- ~~▪ Mixed Use: 27.8 acres within the South Campus of the March Business Center~~
- ~~▪ Business Park: 170.8 acres within the South Campus of the March Business Center~~
- ~~▪ Public Facilities: 0.9 acres within the South Campus of the March Business Center~~

MARCH BUSINESS CENTER - SPECIFIC PLAN AMENDMENT



Legend

| | | | |
|--|--|----------------------------|--|
| March Business Center Specific Plan No. 1 (SP-1) | | Office | |
| Air Force Village West Specific Plan (SP-2, SP-3) | | Public Facility | |
| March LifeCare Specific Plan (SP-4, SP-7) | | Park/Recreation Open Space | |
| Meridian North Campus Specific Plan (SP-5) | | AFVW Expansion | |
| US Vets Transitional Housing Plan Program Specific Plan (SP-6) | | Cemetery | |
| Business Park | | Aviation MIP | |
| Commercial | | Historic District | |
| Industrial | | Medical Campus | |
| Mixed Use | | Institutional-Residential | |
| | | Not a Part | |

FIGURE II-3

EXISTING GENERAL PLAN LAND USES

MARCH BUSINESS CENTER - SPECIFIC PLAN AMENDMENT



Alessandro Blvd.

Cactus Avenue

LEGEND:

- BUSINESS PARK
- COMMERCIAL
- MIXED USE
- INDUSTRIAL
- PUBLIC FACILITY
- PARK, RECREATION, OPEN SPACE
- OFFICE

Van Buren Blvd.

Bld.

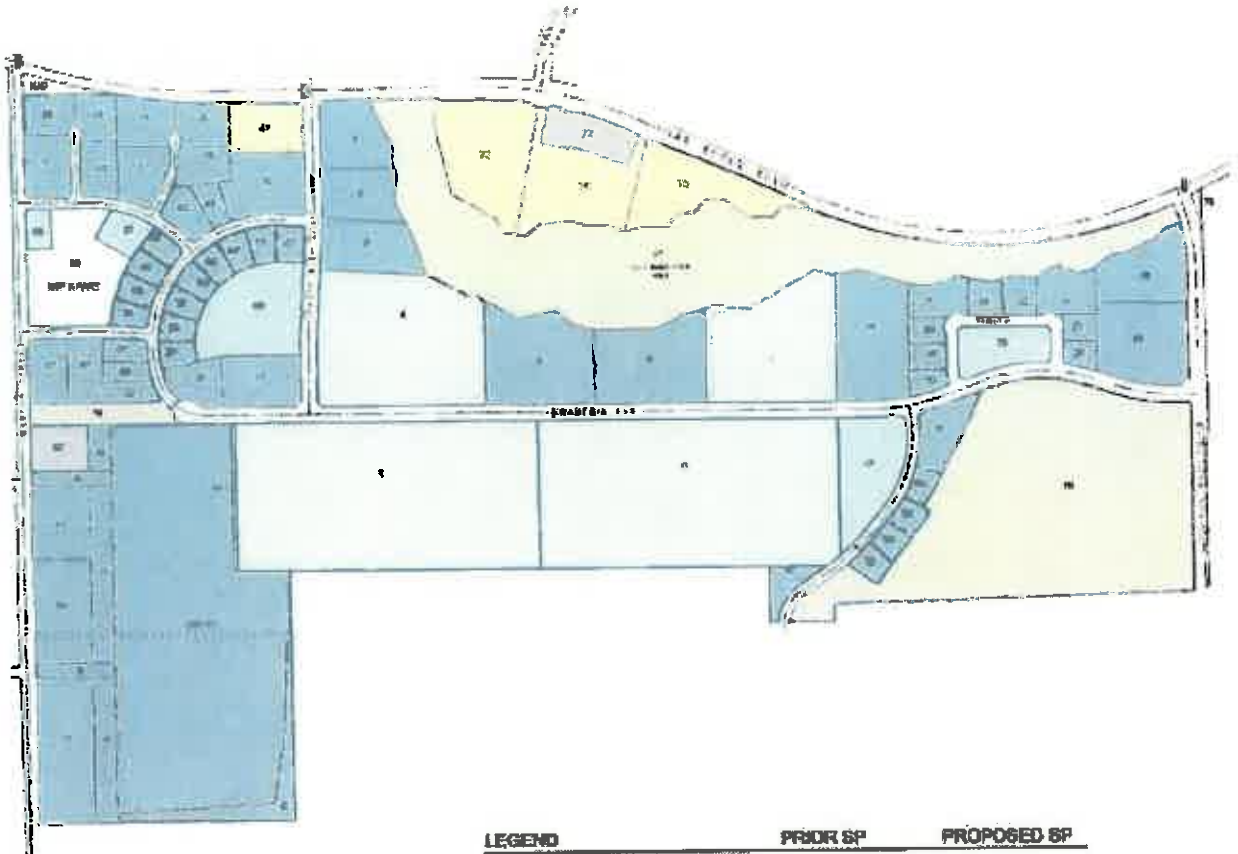


FIGURE II-5A

ZONING MAP FOR NORTH CAMPUS

MARCH BUSINESS CENTER - SPECIFIC PLAN AMENDMENT

\\Sndfp01\ca_and\Meridian\South Campus\Specific Plan Amendment\Exhibits\Figures\DWG\2018.06.05 -- SC Revisions\Existing\y-Fig1-1B-Specific Plan Amendment.dwg

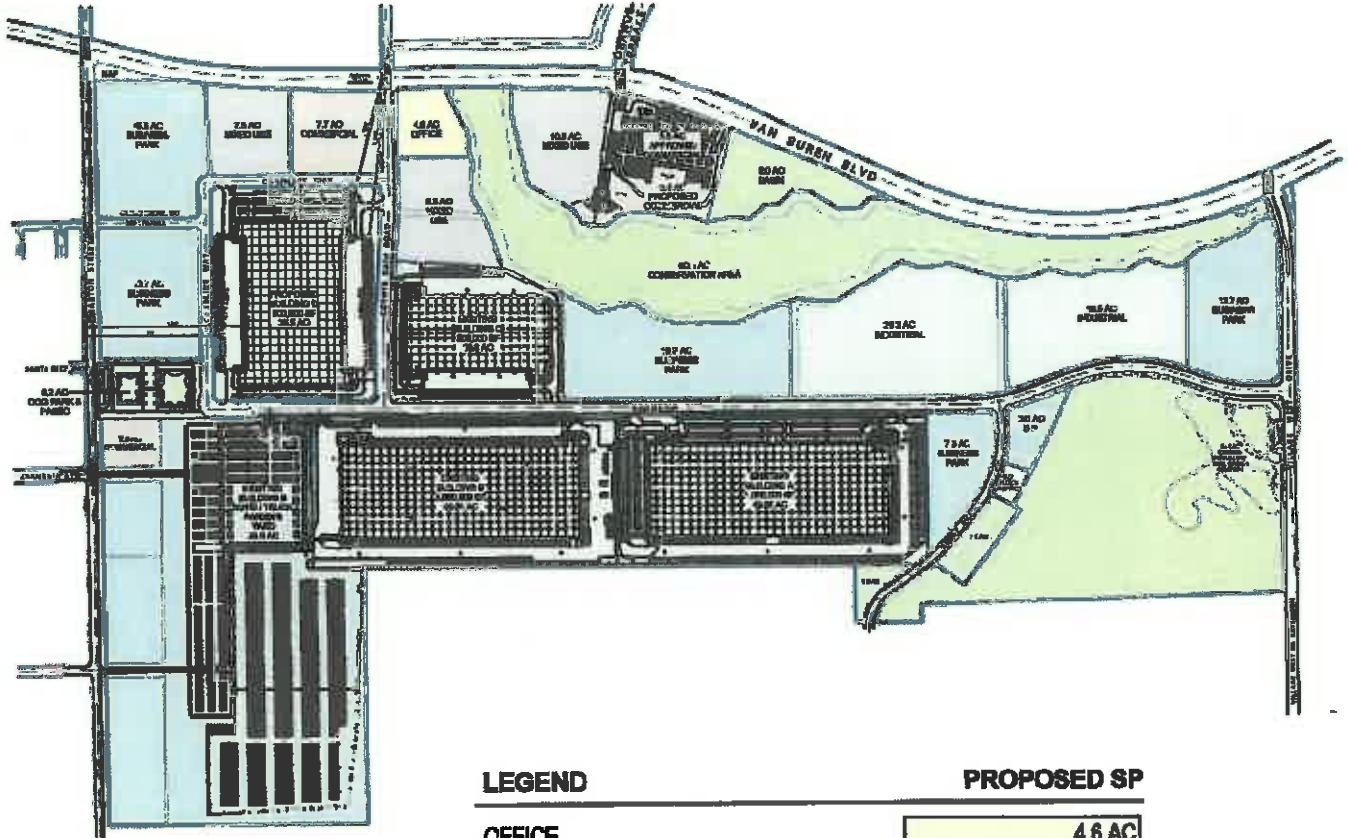


| LEGEND | PRIOR SP | PROPOSED SP |
|--------------------------|-----------------|-----------------|
| OFFICE | 32.3 AC | 32.0 AC |
| COMMERCIAL | 8.8 AC | 6.4 AC |
| MIXED USE | 23.3 AC | 23.3 AC |
| BUSINESS PARK | 252.8 AC | 232.1 AC |
| INDUSTRIAL | 134.5 AC | 134.5 AC |
| PARK / OPEN SPACE | 125.0 AC | 125.0 AC |
| NOT A PART | 0.0 AC | 0.0 AC |
| TOTAL NET ACREAGE | 553.9 AC | 553.9 AC |

FIGURE II-5B

EXISTING ZONING MAP FOR SOUTH CAMPUS

MARCH BUSINESS CENTER - SPECIFIC PLAN AMENDMENT



| LEGEND | PROPOSED SP |
|--------------------------|-----------------|
| OFFICE | 4.6 AC |
| COMMERCIAL | 23.6 AC |
| MIXED USE | 27.8 AC |
| BUSINESS PARK | 170.8 AC |
| INDUSTRIAL | 200.3 AC |
| PUBLIC FACILITIES | 0.9 AC |
| PARK / OPEN SPACE | 140.3 AC |
| TOTAL NET ACREAGE | 568.2 AC |

FIGURE II-5C

PROPOSED ZONING MAP FOR SOUTH CAMPUS

3. Master Plot Plan Applications

Upon approval of the General Plan Amendment and Specific Plan Amendment, ~~a Master Plot Plan Applications would be submitted~~ are also proposed to allow the construction of the following: the construction of a new 500,000 square foot Industrial building and a commercial and office development. The plot plan for the proposed Industrial building is shown in Figure 7, Building C Plot Plan. The proposed commercial and office development is shown in Figure 8, Commercial and Office Plot Plan. The following are included within the Master Plot Plan Application:

~~In addition to the General Plan and Specific Plan entitlements, the Project consists of development of Industrial, Commercial and Office uses. Development would consist of the following:~~

- ~~▪ Development of a 500,000 square foot Industrial building on the 22.93 acre Parcel 4 (Building C)~~
- ~~▪ Development of 14,267 square feet of the following Commercial uses on the new 3.7 acre Parcel 72:~~
 - ~~▪ 3,300 square feet of fast food convenience restaurant with a drive through facility on Pad A site;~~
 - ~~▪ 7,560 square feet (7,905 gross square feet) of shops;~~
 - ~~▪ Gas station with 12 vehicle fueling positions and a 3,062 square foot convenience market;~~
 - ~~▪ 150 parking spaces provided.~~
- ~~▪ Development of 140,570 square feet of Office uses and 1,015 parking stalls on 20.19 acres comprised of Parcel 73 and Parcel 74~~
- ~~▪ Development of an 8.24 acre stormwater basin on a Parcel 75~~
- Plot Plan (PP 20-03) - Commercial: 61,336-square foot Commercial development with a total of 345 parking spaces on the southern 9.4 acres of an existing Commercial parcel. This plot plan would include the extension of Village West Drive to the south to provide a 54-foot wide roadway with two through lanes, a center striped median, a bike lane, and sidewalks on each side of the roadway between Van Buren Boulevard to the north and Nandina Avenue to the south.
- Plot Plan (PP 20-04) - Building D: 800,000 square foot industrial warehouse on a 36.5-acre parcel bound by Caroline Way, Krameria Avenue and Coyote Bush Road. This plot plan would include construction of Caroline Way street improvements.
- Plot Plan (PP 20-05) - Dog Park and Pasco development on 6.2 acres.

~~The 2003 Final Focused EIR evaluated a mix of land uses within the South Campus, which was considered to be Phase 3 of the March Business Center project implementation. Table 1 below identifies each of the proposed development areas (transit area zone or TAZ), the proposed land use, and the assumed square footages.~~

| TABLE II-1 SOUTH CAMPUS BUILD-OUT ASSUMPTIONS | | |
|--|------------------------|----------------|
| TAZ | USE | SQUARE FOOTAGE |
| 5A | Industrial Park | 294,870 |
| 5B | Research & Development | 84,620 |
| 5B | Industrial Park | 87,870 |
| 5C | Research & Development | 195,150 |
| 5C | Industrial Park | 52,280 |
| 5D | Research & Development | 152,460 |
| 5D | Industrial Park | 121,160 |
| 5E | Industrial Park | 105,270 |
| 5E | Shopping Center | 54,450 |
| 5E | Warehousing | 97,570 |
| 5F | Research & Development | 161,300 |
| 5F | Large Industrial Park | 524,460 |
| 5F | Research & Development | 240,430 |
| 5G | Warehousing | 398,040 |
| 5G | Manufacturing | 215,620 |
| 5H | Industrial Park | 228,700 |
| 5H | Warehousing | 618,830 |
| 5H | Research & Development | 76,230 |
| 5H | Large Industrial Park | 438,430 |
| 6 | Office | 140,570 |
| 6 | Research & Development | 421,700 |
| 7A | Warehousing | 157,010 |
| 7A | Large Industrial Park | 999,270 |
| 7A | Large Industrial Park | 1,306,800 |
| 7A | Research & Development | 164,500 |
| 7A | Industrial Park | 39,200 |
| 7B | Industrial Park | 205,260 |
| 7B | Park | FBD |
| 7C | Research & Development | 39,210 |
| 7C | Industrial Park | 149,660 |
| 7C | Warehousing | 107,790 |
| 7D | Shopping Center | 54,450 |
| 7D | Research & Development | 152,460 |

Source: Table B-1, Land Use Inventory and Traffic Generation, Entire Project, Appendix B to the Traffic Circulation and Phasing Study (2002).

Based on the information in Table II-1 above, the following square footages of each respective land use category were evaluated in the Traffic Circulation and Phasing Study, and thus in the 2003 Focused EIR for the South Campus.

As shown in Table II-1, the proposed Project would result in the following changes to the Specific Plan land use categories as compared to the 2003 South Campus and Current South Campus.

| | <u>2003 South Campus (acres)</u> | <u>Current South Campus (acres)</u> | <u>Proposed South Campus (acres)</u> | <u>Change from Current South Campus (acres)</u> |
|------------------------|--------------------------------------|---|--|---|
| Office | 43.9 | 32.0 | 4.6 | -27.4 |
| Commercial | 12.5 | 6.4 | 23.5 | +17.1 |
| Mixed Use | 48.5 | 23.3 | 27.8 | +4.5 |
| Business Park | 263.2 | 232.1 | 170.8 | -61.3 |
| Industrial | 146.8 | 134.5 | 200.3 | +65.8 |
| Park/Open Space | 111.6 | 125.0 | 140.3 | +15.3 |
| Public Facilities | 0 | 0 | 0.9 | +0.9 |
| Total Net Acres | 626.5 | 553.3 | 568.2 | +14.9* |

*change in total net acres due to reconfiguration of internal road system, addition of 10 acres of Lot 31, and rounding differences

| <u>LAND USE CATEGORY</u> | <u>DEVELOPMENT SQUARE FOOTAGE</u> |
|------------------------------|---------------------------------------|
| Industrial Park | 1,284,270 SF |
| Research & Development (R&D) | 1,688,060 SF |
| Shopping Center | 108,900 SF |
| Warehousing | 1,379,240 SF |
| Large Industrial Park | 3,268,960 SF |
| Manufacturing | 215,620 SF |
| Office | 140,570 SF |
| Park | TBD |

Based on the approved land use development categories and square footages outlined in Table II-2 above, a summary of what has been constructed and/or approved for construction by March JPA within the South Campus is shown in Table II-3 below. As shown in Table II-2 below, therein, the previously approved and/or built projects total approximately 2,514,267 2,000,000 square feet. The proposed Project would develop an additional 861,336 654,837 square feet. Therefore, the remaining square footage not yet built-out would be 4,710,017 5,401,783 square feet, including up to 700,000 square feet of cold storage. The proposed Project's development falls well within the assumed build-out parameters evaluated in the 2003 Focused EIR for the March Business Center, and specifically the South Campus.

| Component | Land Use | Status | Tenant | Square Footage |
|------------|------------|-------------|--------------|---------------------|
| Building A | Industrial | Constructed | Amazon | 1,000,000 SF |
| Building B | Industrial | Constructed | UPS | 1,000,000 SF |
| Building C | Industrial | Constructed | Safavieh | 500,000 SF |
| Commercial | Commercial | Entitled | TBD | 14,267 SF |
| | | | TOTAL | 2,514,267 SF |

| LAND USE | ORIGINALLY APPROVED | APPROVED/ BUILT | PROPOSED | REMAINING | TOTALS |
|-----------------------|---------------------|---------------------|-------------------|---------------------|---------------------|
| Industrial Park | 1,284,270 SF | N/A | | 1,284,270 SF | 1,284,270 SF |
| R&D | 1,688,060 SF | N/A | | 1,688,060 SF | 1,688,060 SF |
| Shopping Center | 108,900 SF | N/A | 14,267 SF | 94,633 SF | 108,900 SF |
| Warehousing | 1,379,240 SF | N/A | | 1,379,240 SF | 1,379,240 SF |
| Large Industrial Park | 3,268,960 SF | 2,000,000 SF | 500,000 SF | 768,960 SF | 3,268,960 SF |
| Manufacturing | 215,620 SF | N/A | | 215,620 SF | 215,620 SF |
| Office | 140,570 SF | N/A | 140,570 SF | 0 SF | 140,570 SF |
| Park | TBD | N/A | | TBD | TBD |
| Total | 8,085,620 SF | 2,000,000 SF | 654,837 SF | 5,401,783 SF | 8,085,620 SF |

4. Tentative Tract Map Amendment 30857, Amendment No. 6 #6 and Lot Merger

Tentative Subdivision Map 30857 was approved on February 19, 2003. The Tentative Map 30857, Amendment No. 6 and Lot Merger would include the following minor lot changes to accommodate the proposed land use changes:

- Split Parcel 1 - Creates Parcel 1, 90, 91 (1.0 ac+1.2 ac+2.4 ac=4.6)
- Moved Parcel line between Parcel 2 & 3 north approximately 21 feet. (Lot 2=4.0 ac, Lot 3=5.5 ac)
- Merged Parcel 5 & 6 together. Kept as Parcel 5. (19.8 acres)
- Eastern Parcel Line of Parcel 10 moved approximately 53.4' to the east. Added Parcel 7 & 10 together. Kept as Parcel 7 (26.3 acres)
- Merged Parcel 18, 19, 20, 21, 22, 23, 24, 25, 26 & 27 together. Removed Street K. Created Parcel 10. (18.5 acres)
- Parcels 28 and 29 split into lots 27, 28 & 29. (6.3+4.1+3.3=13.7 acres)
- Parcels 13, 14, 15 combined. Kept as Parcel 13. (3.1 acres)
- Parcels 30-48, 51-66, 79 removed. Replaced with 30-32, 34-37 & 79
- Split Parcel 72 – Creates Parcels 72, 80 81 (1.16 ac + 0.89 ac + 1.44 ac = 3.49 ac)
- Split Parcel 74 – Creates Parcels 74, 92, 93 (3.06 ac + 5.0 ac +1.40 ac = 9.46 ac)
- Removed Letter Lots A, B, C & D per approved Tract Map. No. 30857-8

- ~~Consolidate the existing 4.42-acre Parcel 5 into Parcel 4 through a lot merger, for a combined acreage of 22.93 acres, all having the land use designation of Industrial. (Note that differences are slight because streets, pads and landscaped areas have been constructed so measurements are slightly different along Parcel edges and therefore we will process a tentative parcel map to clean up the acreages consistent with this project description.)~~
- ~~Through lot line adjustments, create a new Parcel 5 (10.33 acres) in the western portion of Parcel 6 (9.51 acres), and change the existing Industrial designation for the revised Parcel 5 and all of Parcel 6, to a designation of Business Park. (Note that differences are slight because streets, pads and landscaped areas have been constructed so measurements are slightly different along Parcel edges and therefore we will process a tentative parcel map to clean up the acreages consistent with this project description.)~~
- ~~Reduce Parcel 41 to 3.65 acres.~~
- ~~Consolidate the southern portion of Parcel 41 as well as all of Parcels 49 and 50 to become a revised Parcel 48 with a total acreage of 5.03 acres.~~
- ~~Reduce Parcel 72 to 3.7 acres with a net acreage of 3.49 per the tentative map which excludes the dedication of roadway.~~
- ~~Increase Parcel 73 to 10.77 acres.~~
- ~~Increase Parcel 74 to 9.42 acres.~~

- ~~Increase Parcel 75 to 8.24 acres with a net acreage 8.01 acres per the tentative map which excludes the dedication of roadway.~~

Table II-3, Land Use Designations summarizes the changes in land use designations and acreages to the affected parcels. As shown in Table II-3, the overall acreage for Park/Open Space would increase from 125.0 acres to 140.3 acres, Industrial would increase from 134.5 acres to 200.3 acres, Commercial would increase from 6.4 acres to 23.5 acres, Mixed Use would increase from 23.3 acres to 27.8 acres, Public Facilities would increase from 0 acres to 0.9 acres, Office would decrease from 32 acres to 4.6 acres, and Business Park would decrease from 232.1 acres to 170.8 acres.

Table 1, Land Use Designations summarizes the changes in land use designations and acreages to the affected parcels. As shown in Table 1, the overall acreage for Industrial use would decrease from 24.7 to 22.9 acres, Business Park use would decrease from 23.7 to 19.8 acres, Commercial would decrease from 5.8 to 3.7 acres, and Office use would increase from 32.2 to 37.9 acres.

| TABLE II-3 LAND USE DESIGNATIONS | | | | | |
|-------------------------------------|-------------------------------|---------------------------|-------------------------------|---------------------------|---------------------|
| PARCEL NUMBER | EXISTING LAND USE DESIGNATION | EXISTING LAND USE ACREAGE | PROPOSED LAND USE DESIGNATION | PROPOSED LAND USE ACREAGE | CHANGE IN ACREAGE |
| 1 | Business Park | 4.62 | Office | 1.00 | -3.62 |
| 90 | Business Park | 0 | Office | 1.19 | +1.19 |
| 91 | Business Park | 0 | Office | 2.37 | +2.37 |
| 2 | Business Park | 4.21 | Mixed Use | 3.94 | -.27 |
| 3 | Business Park | 5.24 | Mixed Use | 5.51 | +.27 |
| 72 | Commercial | 3.49 | Commercial | 1.16 | -2.33 |
| 80 | Commercial | 0 | Commercial | 0.89 | +0.89 |
| 81 | Commercial | 0 | Commercial | 1.44 | +1.44 |
| 73 | Office | 10.77 | Mixed Use | 10.77 | 0 |
| 74 | Office | 9.46 | Commercial | 3.06 | -6.40 |
| 92 | Office | 0 | Commercial | 5.00 | +5.00 |
| 93 | Office | 0 | Commercial | 1.40 | +1.40 |
| 75 | Office | 8.0 | Parks and Open Space | 8.0 | 0 |
| 5 | Business Park | 10.33 | Business Park | 19.84 | +9.51 |
| 6 | Business Park | 9.51 | Business Park | 0 | -9.51 |
| 7 | Industrial | 15.61 | Industrial | 26.27 | +10.66 ¹ |
| 10 | Business Park | 9.7 | Industrial | 0 | -9.7 |
| 10 | Business Park | 0 | Industrial | 18.44 | +18.44 ² |
| 18-24, 26 & 27 | Business Park | 12.62 | Industrial | 0 | -12.62 |
| 25 | Mixed Use | 4.16 | Industrial | 0 | -4.16 |
| 27 | Business Park | 0 | Business Park | 6.27 | +6.27 |
| 28 | Business Park | 5.84 | Business Park | 4.15 | -1.69 |
| 29 | Business Park | 7.84 | Business Park | 3.28 | -4.56 |

| TABLE II-3 LAND USE DESIGNATIONS | | | | | |
|--|---|---------------------------|-------------------------------|---------------------------|-------------------|
| PARCEL NUMBER | EXISTING LAND USE DESIGNATION | EXISTING LAND USE ACREAGE | PROPOSED LAND USE DESIGNATION | PROPOSED LAND USE ACREAGE | CHANGE IN ACREAGE |
| 11 | Business Park | 1.56 | Parks and Open Space | 1.56 | 0 |
| 12 | Mixed Use | 7.31 | Business Park | 7.31 | 0 |
| 13 | Business Park | 1.06 | Parks and Open Space | 3.07 | +2.01 |
| 14 | Business Park | 1.05 | Parks and Open Space | 0 | -1.05 |
| 15 | Business Park | 0.97 | Parks and Open Space | 0 | -0.97 |
| 16 | Business Park | 0.93 | Public Facilities | | |
| 30-32, 34, 35, 38-40, 42-48 51-59, 61-66 | Business Park | 52.34 | N/A | 0 | -52.34 |
| 33 | Mixed Use | 1.04 | N/A | 0 | -1.04 |
| 36 | Not a part of prior SP | 0 | N/A | 0 | 0 |
| 37 | Mixed Use | 1.72 | N/A | 0 | -1.72 |
| 41 | Office | 3.65 | N/A | 0 | -3.65 |
| 60 | Mixed Use | 9.02 | N/A | 0 | -9.02 |
| 79 | Park/Open Space | 3.66 | N/A | 0 | -3.66 |
| 30 | N/A | 0 | Business Park | 15.32 | +15.32 |
| 31 | N/A | 0 | Business Park | 13.68 | +13.68 |
| 32 | N/A | 0 | Mixed Use | 7.42 | +7.42 |
| 34 | N/A | 0 | Industrial | 36.5 | +36.5 |
| 35 | N/A | 0 | Commercial | 1.89 | +1.89 |
| 36 | N/A | 0 | Commercial | 2.81 | +2.81 |
| 37 | N/A | 0 | Commercial | 2.99 | +2.99 |
| 79 | N/A | 0 | Parks/Open Space | 6.19 | +6.19 |
| A, B, C, D | Removed Letter Lots A, B, C & D per approved Tract Map, No. 30857-8 | | | | |

Notes:

1. Additional acreage result of eastern parcel line of parcel 10 moving approximately 53.4' to the east.
2. Additional acreage result of removal of K Street.

| TABLE II-4 LAND USE DESIGNATIONS | | | | | |
|-------------------------------------|-------------------------------|---------------------------|-------------------------------|---------------------------|-------------------|
| PARCEL NUMBER | EXISTING LAND USE DESIGNATION | EXISTING LAND USE ACREAGE | PROPOSED LAND USE DESIGNATION | PROPOSED LAND USE ACREAGE | CHANGE IN ACREAGE |
| 4 | Business Park | 18.1 ¹ | Industrial | 22.9 | +4.8 |
| 5 | Industrial | 4.4 | Business Park | 10.3 | +5.9 |
| 6 | Industrial | 20.3 | Business Park | 9.5 | -10.8 |
| 28 | Commercial | 5.8 | Office | 5.8 | 0 |
| 41 | Business Park | 5.6 | Office | 3.7 | -1.9 |

| TABLE II-4 LAND-USE DESIGNATIONS | | | | | |
|-------------------------------------|-------------------------------|---------------------------|-------------------------------|---------------------------|-------------------|
| PARCEL NUMBER | EXISTING LAND-USE DESIGNATION | EXISTING LAND-USE ACREAGE | PROPOSED LAND-USE DESIGNATION | PROPOSED LAND-USE ACREAGE | CHANGE IN ACREAGE |
| 72 | Office | 11.2 | Commercial | 3.7 | -7.5 |
| 73 | Office | 9.7 | Office | 10.8 | +1.1 |
| 74 | Office | 7.9 | Office | 9.4 | +1.5 |
| 75 | Office | 3.4 | Office | 8.2 | +4.8 |

⁺ Acreages have been rounded to the nearest tenth.

This amended Tentative Map details project land development, and is in accordance with guidelines and development intensities presented in this Specific Plan Amendment, the State Subdivision Map Act and March JPA requirements. The amended Tentative Map complies with Schedule "E" Parcel Map Division as per March Joint Powers Authority Development Code Section 9.14.100 (J).

5. Conditional Use Permit

A Conditional Use Permit is requested to allow for alcohol sales at the 45,000 sf Grocery Store proposed to be developed southeast of the Van Buren Boulevard and Orange Terrace intersection.

III. LAND USE

The previously adopted Specific Plan helped to implement some of the regional land use/transportation goals outlined in the General Plan. Specifically, the previously adopted Specific Plan facilitated development of a large employment center in a portion of the County that is largely residential. Improving the balance of housing and jobs in this area. This provides an opportunity for residents to work locally, rather than commute to Los Angeles or Orange Counties. Jobs/housing balance provides a transportation capacity benefit due to a reduced concentration of work trips on public streets during the peak hour/peak direction of travel.

A. Land Use Overview

This section identifies the types of land uses to be allowed in this Specific Plan Amendment and provides regulations and standards to govern future development. In accordance with the General Plan, the Specific Plan will accommodate land uses that will support future growth and development in the area. The March Business Center Specific Plan Land Use Element references the following policies, regulations, and guidelines:

- March JPA General Plan
- March JPA Development Code
- March Business Center Design Guidelines

This section specifies broad land use categories that will guide the development of the Specific Plan area. Within each broad category, specific land uses are identified, together with an indication of whether such uses are permitted, subject to a conditional use permit, or not allowed. In addition, development regulations that will govern the development of the individual projects comprising the March Business Center are described.

B. Purpose and Applicability

The following items describe the relationship of the Specific Plan land use regulations in the context of other land use documents developed by the JPA.

1. Terms used in these regulations and guidelines shall have the same definitions as given in the March JPA Development Code (“Development Code”) and the General Plan of the March JPA (“General Plan”) unless otherwise defined in the Specific Plan.
2. Any details or issues not specifically covered in these regulations shall be subject to the regulations of the Development Code.
3. The Specific Plan Land Use regulations are adopted pursuant to Section 65450 of the State of California Government Code et seq. It is specifically intended by such adoption that the development standards herein shall regulate all development within the project area.

C. Land Use Compatibility

This Specific Plan establishes development patterns to limit the potential for land use conflicts, both within the March Business Center and in relation to other uses in the project vicinity. A key

consideration guiding the development is the proximity of Air Reserve Base Runway 14/32. The Riverside County Airport Land Use Commission published an Airport Land Use Plan in 1984. This plan established land use restrictions within Airport Influenced Areas, which consist of imaginary surfaces extending outward from an airport's runway. In 1998, an Air Installation Compatible Use Zone (AICUZ) Study was completed to identify land use restrictions and height limitations within the airfield influence area

This Specific Plan creates an overlay zone to limit the types of uses within a quarter-mile radius of the Tomas Rivera Elementary (off-site) based on noise and hazardous materials. (See Section E of this chapter for additional discussion.). Additionally, Arnold Heights Elementary School was located in the North Campus at the time of the original March Business Center Specific Plan approval in 2003. The school has since been demolished; therefore, there are no compatibility issues with the Specific Plan and this school.¹

Another potential land use conflict involves the Commercial land uses proposed within the Specific Plan. The intent of these uses is to serve retail demand generated within the March Business Center only. They are not intended to attract customers or clientele from outside the Specific Plan area. This Specific Plan identifies land use types, intensities, and locations that will limit the potential competition between project Commercial uses and shopping centers and other retail uses in surrounding communities.

D. Objectives of Development Districts

This section of the Specific Plan identifies the following ~~six~~ seven land use districts: Business Park, Industrial, Office, Mixed Use, Commercial, ~~and Park, and Public Facility.~~²

The land use designations are summarized below:

1. Business Park

Business park uses include administrative, financial, governmental, and community support services; research and development centers; light manufacturing; parcel delivery terminal on no less than 60-acres; vocational education and training facilities; business and trade schools; and emergency services. Business Park areas are generally served by arterial roadways, providing automobile and transit access. These areas are characterized as major employment concentrations. Development in this category, except for warehousing, is generally within a campus-like setting or cluster development pattern. Outdoor storage is prohibited.

¹ Arnold Heights Elementary School was removed from the Specific Plan in accordance with Ordinance #JPA 08-01.

² The land use descriptions are taken directly from the General Plan, amended to apply to this Specific Plan.

2. Industrial

Industrial may support a wide range of manufacturing and non-manufacturing uses from warehouse and distribution facilities to industrial activities. Uses supported include warehousing/distribution and assemblage of non-hazardous products and materials or retailing related to manufacturing activity; and parcel delivery terminal on no less than 60 acres. Uses may include open storage, office/industrial park; light industry; manufacturing; research and development centers; maintenance shops; and emergency services centers. The area devoted to outdoor storage may not exceed the building area.

3. Office

Office uses include business activities associated with professional or administrative services. Activities can consist of corporate offices, cultural and community facilities, financial institutions, legal and medical offices, and other similar uses, which together represent major concentrations of community and employment activities. Uses may include office parks, office buildings, and educational and vocational training facilities. Development in this category is generally within a campus-like setting. Office development is typically located on arterial roadways for convenient automobile access and transit service.

4. Mixed-Use

Mixed uses include a variety of complementary land uses; including commercial, business park, office, medical, educational and vocational, research and development, and services. Industrial, warehousing, and outdoor storage is prohibited.

5. Commercial

Commercial/Service uses within the designation include retail and service oriented business serving the Planning Area. Commercial uses include retail establishments (shopping centers), administrative, financial, service and government offices. Development in this category generally occurs at key intersections of major arterial roads or at major off-ramps from Interstate 215. In addition to being accessible to automobiles and pedestrians, commercial developments may also be served by public transit. Commercial land uses within the Specific Plan are intended to serve the Specific Plan area only; they will not serve a regional demand.

6. Park/Recreation/Open Space

~~Park/Recreation/Open Space uses include all passive and active park or recreation areas whether private or public in the Planning Area. Active recreation activities include recreation facilities, equestrian centers, golf courses/driving ranges, indoor/outdoor athletic facilities, and public parklands. Passive activities include natural preserves, along with designated arid natural open space. A 52-acre park is proposed within the planning area in the South Campus, adjacent to the existing golf course and Air Force Village West. A portion of the park is located on a closed and capped sanitary landfill. The park would be passive in nature, with limited athletic facilities and other improvements.~~

7. Public Facility

Public facilities uses include a wide range of public, quasi-public, and private uses such as schools, public cultural and historical facilities, government administrative offices and facilities, public utilities, and major transportation corridors. However, land uses determined to be sensitive to, or incompatible with aviation operations shall be excluded.

E. Overlay Districts

The Specific Plan area contains overlay zones within its boundaries. Figure III-1 depicts the location of the runway Clear Zone (CZ), Accident Potential Zones (APZ), and the Elementary School Buffer zone. The CZ, together with both APZs, comprise Airport Influenced Area I, as defined in the *Riverside County Airport Land Use Plan (ALUP)*. The remainder of the site is included in Airport Influenced Area II. The standards for military operations are as follows:

1. **Clear Zone (CZ):** The clear zone (CZ) is a 3,000-foot by 3,000-foot area closest to the end of each runway and along the approach and departure flight path. These areas must remain clear and free of any infrangible obstacles. The project would provide drainage facilities and underground utilities within the CZ.
2. **Accident Potential Zone (APZ) I:** The accident potential zones (APZs) are areas beyond the CZ, which possess a significant potential for accidents. APZ I extends from the outer edge of the CZ for an additional 5,000 feet. According to the AICUZ study, APZ I is compatible with a variety of industrial/manufacturing, transportation, and other uses. Uses that create dense concentrations of people are not recommended within this zone. See the current AICUZ standards for restrictions within the APZ I zone.
3. **Accident Potential Zone (APZ) II:** This APZ extends from APZ I for an additional 7,000 feet in length. The potential for accidents in this zone are lower than APZ I; accordingly, some low-density residential could be provided. However, multi-story buildings and places of assembly (theaters, churches, schools, restaurants, etc.) are not considered appropriate. See the current AICUZ standards for restrictions within the APZ II zone.

In order to conform to the ALUP, this Specific Plan provides land use regulations relating to safety (both for air navigation and for people within the March Business Center), noise impacts, and building heights. The following paragraphs summarize these regulations.

1. Aviation Safety Regulations

Figure III-2 shows the location of Airport Influenced Areas I and II. The ALUP has the following policies related to project land uses:

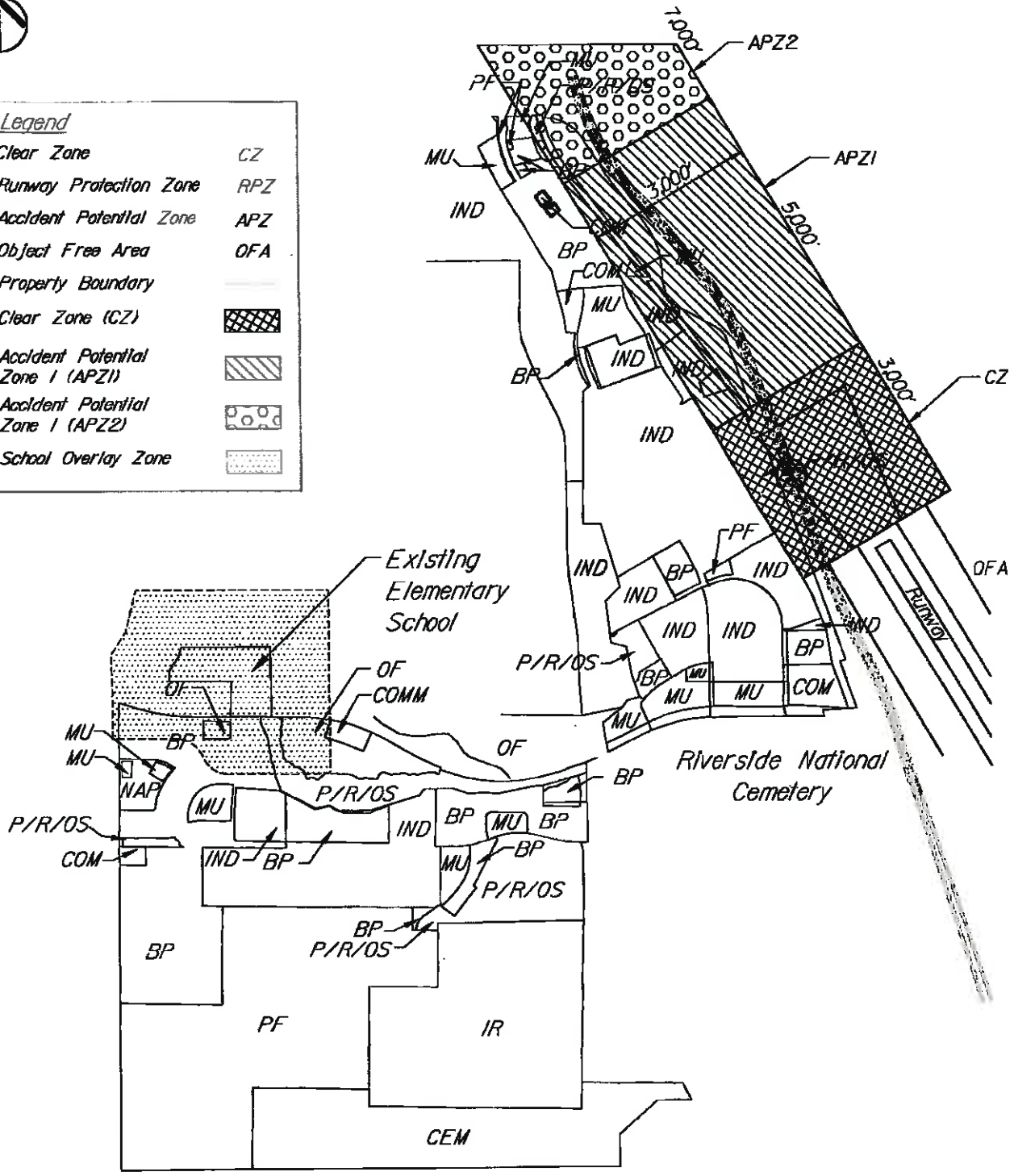
Policy 1: Area I shall be kept free of all high-risk land uses. High-risk land uses are defined as having a high concentration of people; having a critical facility (such as a telephone exchange); or having explosive or flammable materials.

In order to comply with the requirements of the ALUP, the following land uses are prohibited from Airport Influenced Area I:

MARCH BUSINESS CENTER - SPECIFIC PLAN AMENDMENT



| Legend | |
|----------------------------------|-----|
| Clear Zone | CZ |
| Runway Protection Zone | RPZ |
| Accident Potential Zone | APZ |
| Object Free Area | OFA |
| Property Boundary | --- |
| Clear Zone (CZ) | |
| Accident Potential Zone 1 (APZ1) | |
| Accident Potential Zone 1 (APZ2) | |
| School Overlay Zone | |



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FIGURE III-1A

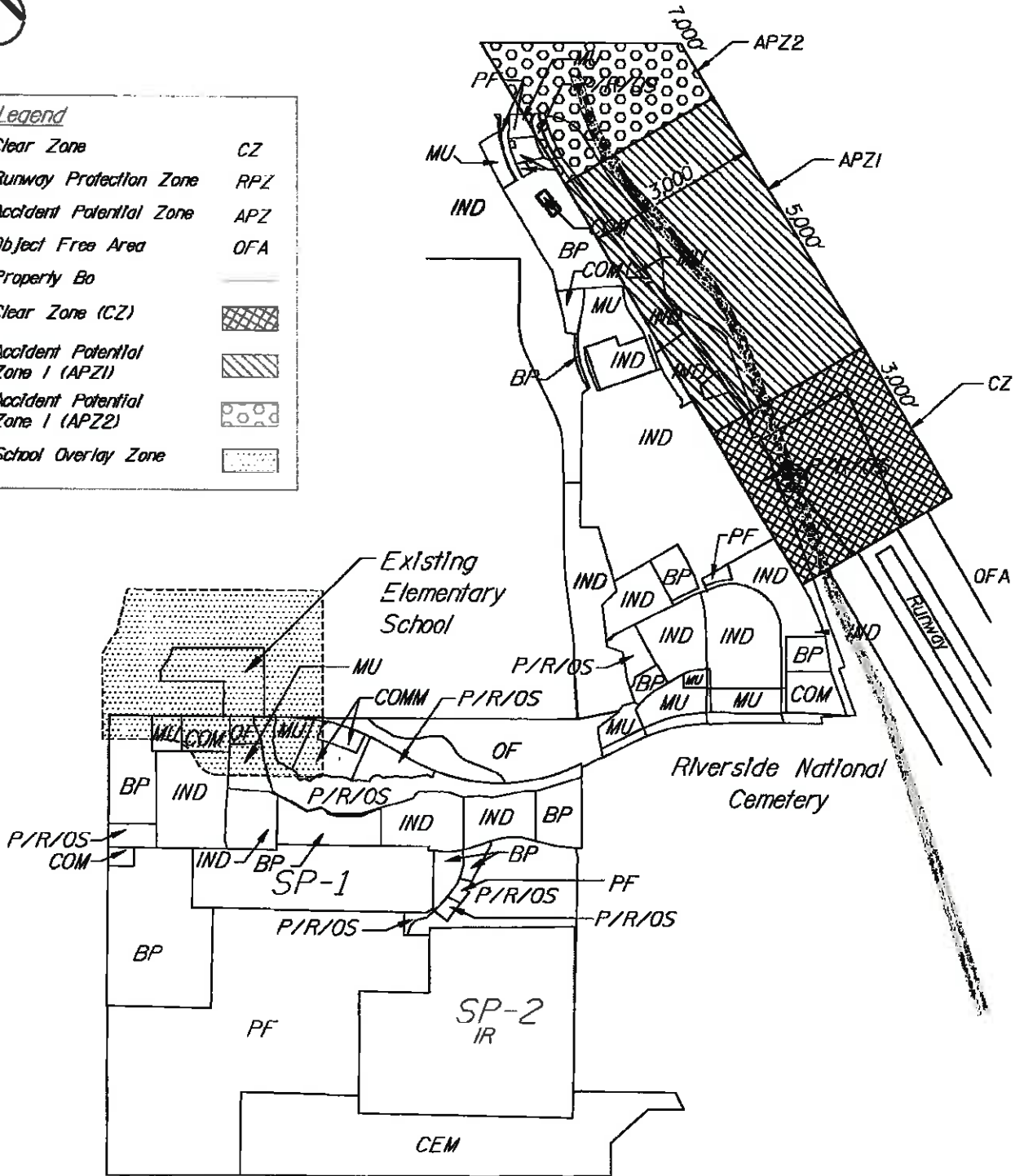
EXISTING OVERLAY ZONES

MARCH BUSINESS CENTER - SPECIFIC PLAN AMENDMENT

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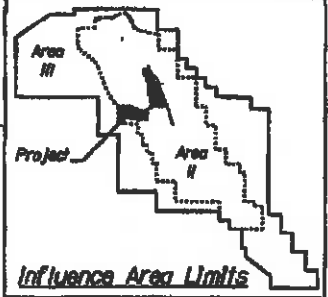
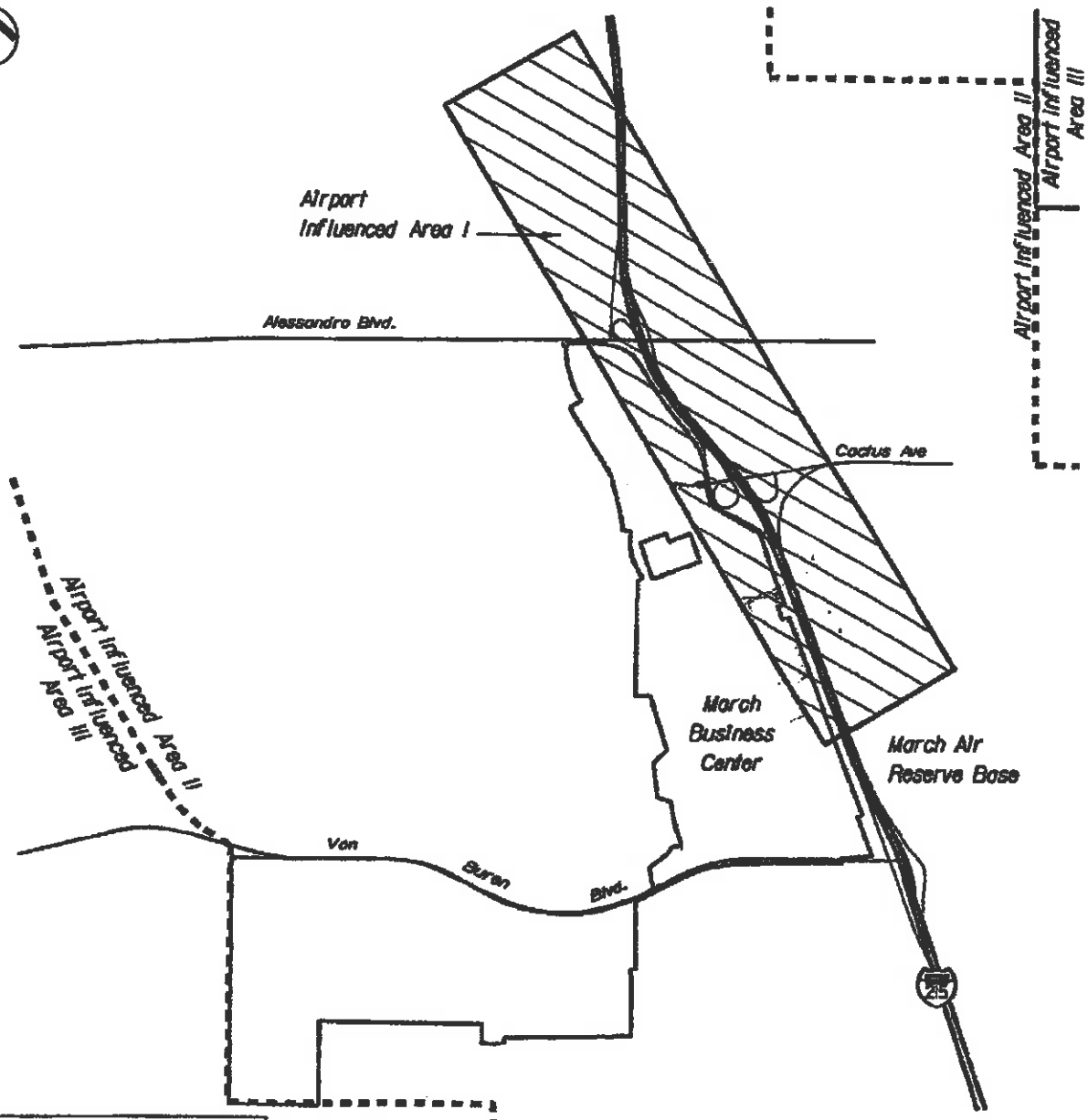


| Legend | |
|----------------------------------|-----|
| Clear Zone | CZ |
| Runway Protection Zone | RPZ |
| Accident Potential Zone | APZ |
| Object Free Area | OFA |
| Property Bo | |
| Clear Zone (CZ) | |
| Accident Potential Zone 1 (APZ1) | |
| Accident Potential Zone 1 (APZ2) | |
| School Overlay Zone | |



**FIGURE III-1B
EXISTING OVERLAY ZONES
WITH PROPOSED LAND USE CHANGES**

MARCH BUSINESS CENTER - SPECIFIC PLAN AMENDMENT



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FIGURE III-2

AIRPORT INFLUENCE AREAS

Land Uses Prohibited in Airport Influenced Area I

- Assembly and Entertainment
- Child Care Facilities
- Churches and Places of Religious Assembly
- Energy Generation and Distribution Facilities
- Hospitals, Intermediate Care Facilities and Nursing Facilities
- Hotel/Motel
- Major Transmission, Relay or Communications Switching Stations
- Nightclubs and Bars
- Petroleum Products Storage
- Private Clubs, Lodges, and Fraternal Organizations
- Restaurant (fast food)
- Restaurant (sit down)
- Sidewalk Cafes
- Theaters
- Trade Schools

The remainder of March Business Center is located within Airport Influenced Area II. Within this area, a number of land uses are prohibited or discouraged because of their proximity to the airport. Discouraged uses must demonstrate to the satisfaction of the March JPA Commission that development outside the Airport Influenced Area II is not feasible. The following uses are prohibited within Airport Influenced Area II:

Land Uses Prohibited in Airport Influenced Area II

- Child Care Facilities (Commercial facilities as defined in accordance with state law; noncommercial facilities ancillary to a place of business are generally allowed)
- Hospitals, Intermediate Care Facilities and Nursing Facilities
- Above-ground storage of explosives or flammable gases or liquids shall be prohibited, except gas and diesel of 500 gallons or less

Land Uses Discouraged in Airport Influenced Area II

- Churches and Places of Religious Assembly or Public Assembly
- Energy Generation and Distribution Facilities
- Hotel/Motel
- Petroleum Products Storage
- Private Clubs, Lodges, and Fraternal Organizations
- Trade Schools

Any discouraged use must be reviewed by the ALUC, and is required to obtain a use permit in accordance with Table III-1.

Regulations relating to the safety of air navigation are as follows:

- The Final Map shall convey an aviation easement to the JPA
- Lighting Plans for any development shall be reviewed and approved by the Airport Authority and the Air Force Reserve
- Uses that would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft during initial climb or final approach shall be prohibited
- Uses that would cause sunlight to be reflected toward an aircraft during initial climb or final approach shall be prohibited
- Uses that generate smoke or water vapor which would affect safe air navigation shall be prohibited
- Uses that generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation shall be prohibited
- Detention facilities within the Specific Plan will be designed to drain within six to 12 hours. These basins shall be monitored on a regular basis to identify if waterfowl and other birds are attracted to the basins. If such birds begin to be attracted to the basins, a plan to harass or exclude potential hazardous wildlife shall be developed and implemented.

2. Aviation Noise Regulations

An acoustical analysis shall be required for any noise sensitive uses. These uses include the following:

- Churches and Places of Religious Assembly
- Hotel/Motel
- Museums
- Private Clubs, Lodges, and Fraternal Organizations
- Radio and television studios
- Trade Schools

This analysis will include components necessary to achieve an indoor noise reduction level of 25 and 30 decibels for each of the project's components with noise sensitive uses, and will include all surrounding noise sources (e.g., transportation and industrial) at their ultimate design and capacity.

3. Aviation Building Height Regulations

A further limitation on site development is the height of structures in the vicinity of the runway. Federal Aviation Regulations (FAR) Part 77 defines a variety of imaginary surfaces around airports, including a horizontal surface and a conical surface. FAR Part 77 is not an absolute height limit. Instead, it is a guideline used by the FAA to identify structures that may constitute a hazard to air navigation. Any construction or alteration of greater height than an imaginary surface extending upward and outward at a 100 to 1 slope from the nearest point of the runway (see FAR §77.13.2.i) will require the preparation of FAA Notice of Proposed Construction or Alteration (form 7460-1). If a hazard to air navigation is identified, then the FAA will issue a determination of hazard to air navigation. However, the FAA does not have the authority to prevent encroachment; it is up to the local land use authority to enforce the recommendation.

Figure B-1 in Appendix B is a land use compatibility map that shows FAR Part 77 surfaces adjacent to MARB. As shown in this exhibit, terrain elevations (without structures) penetrate the conical surface to the west of the North Campus and into the South Campus, east of Barton Street. Depending on the elevation of the finished grade and height of the proposed structure, future development in March

Business Center may penetrate the Part 77 surfaces. Development proposals in March Business Center will file form 7460-1 as appropriate based on §77.13.2.i.

Figure B-1 in Appendix B also shows the location of the Height Caution Zone. This zone is defined as the area within Airport Influenced Area II where the maximum allowable building height plus the rough grading plan elevation penetrates the Part 77 surface. Within the Height Caution Zone, objects up to 50 feet tall are acceptable, and do not require ALUC review for the purposes of height factors. ALUC review will be required for any proposed object taller than 50 feet within the Height Caution Zone.

4. **Elementary School Buffer:** This zone extends 0.25 miles outward from Tomas Rivera Elementary (located north of Van Buren Boulevard). The objective of this zone is to limit the potential for incompatible uses within nearby Business Park, Office, Industrial and Mixed Use zones and to provide additional screening for the school through landscaping. This zone provides for the following:

1. Abutting lots shall be required to provide 40 feet of landscaping adjacent to the school
2. The following permitted or conditionally permitted uses are not allowed within the Buffer zone:
 - Automotive Service Stations
 - Hazardous Waste Treatment Facilities
 - Manufacturing (Custom, Light, Medium, Heavy)
 - Trucking/Transportation Terminals
 - Automotive/Truck Repair (Major and Minor)
 - Energy Generation and Distribution Facilities
3. Development projects not listed above that are proposed within the Buffer zone shall provide studies of noise and hazardous materials impacts on the school.

Development (if allowed) within each aviation-related overlay district shall be consistent with the guidelines and standards established in the Airport Land Use Plan and the AICUZ study.

F. Land Use Table

Table III-1 is a matrix indicating the status of specific land use types within the development districts described in preceding paragraphs. For each specific land use, a "P" indicates that it is permitted and a "C" indicates that a conditional use permit is required. A blank space indicates that the use is not allowed.

| TABLE III-1 MARCH BUSINESS CENTER SPECIFIC PLAN LAND USE TABLE | | | | | |
|---|------------------------------|-------------------------|--------|------------------------|-------------------------|
| USES | BUSINESS PARK ^{3,4} | INDUSTRIAL ⁵ | OFFICE | MIXED USE ⁶ | COMMERCIAL ⁷ |
| INDUSTRIAL | | | | | |
| Hazardous Waste Treatment Facility | | | | | |
| Bio-Medical Waste Treatment Facility | | C | | | |
| Manufacturing – Custom | P | P | | | |
| Manufacturing – Light | P | P | | | |
| Manufacturing – Medium | | P | | | |
| Manufacturing – Heavy | | C | | | |
| Mining & Extractive Industries | | | | | |
| Newspaper Publishing Plants | P | P | | | |
| Parcel Delivery Terminal | P | P | | | |
| Research & Development | P | P | P | P | |
| Trucking/Transportation Terminals | | P | | | |
| Wrecking & Dismantling of Motor Vehicles | | | | | |
| WHOLESALE STORAGE/DISTRIBUTION | | | | | |
| Public storage/Mini-warehouse (indoor) | C | C | | | |
| Business Enterprise | P | P | | P | |
| Warehouse, Storage & Distribution – Medium | | P | | | |
| Warehouse, Storage & Distribution – Heavy | | P | | | |
| OFFICE | | | | | |
| Financial Institutions | P | | P | P | P |
| Government | P | | P | P | P |
| Medical Clinics | P | P | P | P | |
| Offices, Business & Professional | P | | P | P | P |
| Regional & Corporate Headquarters | P | | P | P | P |
| COMMERCIAL | | | | | |
| Agricultural Equipment Repair Shops | | P | | | |
| Agricultural/Nursery Supplies & Service | C | C | | | C |
| Alcoholic Beverage Outlets | | | | | C |
| Animal Care/Pet Hotels | P | P | | | P |

³ Within the Business Park zone, a use permit is required for uses that provide outdoor storage in excess of 10% of the building area

⁴ Parcel Delivery Terminal is allowed within the Business Park and Industrial zones on parcels that are 60 acres in size or larger

⁵ Within the Industrial zone, a use permit is required for uses that provide outdoor storage in excess of the building area

⁶ The Mixed Use designation shall have a maximum of 25 percent retail uses.

⁷ Within the Commercial zoning district, a use permit shall be required for single uses above 25,000 square feet of gross floor area, with the exception of grocery stores. A use permit is required for alcohol sales at grocery stores.

| TABLE III-1 MARCH BUSINESS CENTER SPECIFIC PLAN LAND USE TABLE | | | | | |
|---|-------------------------------|-------------------------|--------|---------------------------|-------------------------|
| USES | BUSINESS PARK ¹ | INDUSTRIAL ² | OFFICE | MIXED USE ⁶ | COMMERCIAL ⁷ |
| Assembly & Entertainment | | | | C | |
| Automotive Parts and Accessory Sales | | | | | P |
| Automotive Fleet Storage | | C | | | |
| Automotive Service Stations | | | | | C |
| Automotive/Truck Repair-major | | P | | | |
| Automotive/Truck Repair-minor | | P | | | C |
| Building & Site Maintenance Services | P | P | | P | |
| Building Contractor's Storage yard | | P | | | |
| Building Material & Equipment Sales | P | | | P | P |
| Business Supply/Equip Sales/Rentals | C | C | | | P |
| Business Support Services ² | P | P | P | P | |
| Child Care Facilities | C | | C | C | C |
| Churches & Places of Religious Assembly | | | | C | |
| Communication Facilities, Antennas & Satellite Dishes | C | C | | | |
| Consumer Goods, Furniture, Appliances, Equipment Sales | | | | P | P |
| Convenience Sales | | | | | C |
| Energy Generation & Distribution Facilities | | C | | | |
| Equestrian Show & Exhibition Facilities | | | | | |
| Exhibit Halls & Convention Facilities | | | | C | |
| Fairgrounds | | | | | |
| Food and Beverage Sales | | | | P | P |
| Funeral & Mortuary Services | C | | | | |
| General Retail Establishments | | | | | P |
| Golf Courses, Driving Ranges and Pitch & Putt Courses | | | | | P |
| Grocery Stores | | | | | C |
| Health Club | | | | C | C |
| Heavy Equipment Sales and Rentals with outside merchandising | | C | | C | |
| Horticulture Nurseries & Greenhouses | C | P | | | C |
| Hospitals, Intermediate Care Facilities & Nursing Facilities | | | | | |
| Hotel/Motel | | | | C | C |
| Instructional Studios | P | P | P | P | |

| TABLE III-1 MARCH BUSINESS CENTER SPECIFIC PLAN LAND USE TABLE | | | | | |
|---|---------------------------------|-------------------------|--------|---------------------------|-------------------------|
| USES | BUSINESS PARK ^{3,4} | INDUSTRIAL ⁵ | OFFICE | MIXED USE ⁶ | COMMERCIAL ⁷ |
| Interpretive Centers | P | P | P | P | |
| Laundry Services | P | P | | C | |
| Maintenance & Repair | P | P | | P | |
| Major Transmission, Relay or Communications Switching Stations | P | P | | | |
| Museums | | | P | P | P |
| Bar & Grill | | | | C | P |
| Open Air Markets for the Sale of Agriculture- related Products & Flowers | C | | | C | C |
| Outdoor Commercial | | C | | | C |
| Outpatient Medical Clinics | P | | P | P | P |
| Parking Facilities as a <i>Primary Use</i> | P | | | P | P |
| Personal Services | | | | | |
| Petroleum Products Storage | | | | C | P |
| Pets & Pet Supplies | | | | C | |
| Private Clubs, Lodges & Fraternal Organizations | | | | P | |
| Radio & Television Studios | P | P | | C | C |
| Recreational Facilities | | | | | |
| Recycling Facilities (outdoor storage not to exceed building area) | | P | | | |
| Repair Services | P | P | | P | |
| Restaurant (fast food) | | | | C | P |
| Restaurant (sit down) | | | C | P | P |
| Sidewalk Cafes | | | | | P |
| Social Service Institutions | P | | P | P | |
| Sundries, Pharmaceutical & Convenience Sales | | | | | P |
| Swap Meets & Other Large Outdoor Retail Facilities | | C | | | |
| Theaters | | | | | |
| Trade Schools | C | | | | C |
| Vehicle, Boat and Trailer Sales | C | | | C | C |
| Vehicle Storage | | C | | | |
| Veterinary Clinics & Animal Hospitals | P | | | P | P |
| Zoological Parks | | | | | |

G. Development Regulations

No building or portion thereof shall be erected, constructed, converted, established, altered, enlarged, nor shall any legal lot or premises be used unless the legal lot or premises and building comply with the following regulations and standards:

(a) Lot Development

- (1) Two adjoining lots which have a common interior side or rear lot line may be developed with zero side yard setbacks on the common lot line, provided that the opposite side yard setback is not less than 30 feet.
- (2) Any construction or alteration of greater height than an imaginary surface extending upward and outward at a 100 to 1 slope from the nearest point of the runway (see FAR §77.13.2.i) will require the preparation of FAA Notice of Proposed Construction or Alteration (form 7460-1).
- (3) Construction of objects taller than 50 feet in the Height Caution Zone (see Figure B-1), will require review by the Airport Land Use Commission

| TABLE III-2 LOT AND YARD DIMENSIONS BY LAND USE CATEGORY | | | | | |
|---|---|---|---|---|---|
| Dimensions | Business Park | Industrial | Office | Mixed Use | Commercial |
| Area (minimum) | 30,000 sq. ft. | 30,000 sq. ft. | 30,000 sq. ft. | 30,000 sq. ft. | 30,000 sq. ft. |
| Street Frontage (minimum) | 100 ft. * | 100 ft. * | 100 ft. | 100 ft. | 100 ft. |
| Lot Width (minimum) | 100 ft. | 100 ft. | 100 ft. | 100 ft. | 100 ft. |
| Minimum Yards | | | | | |
| Front Yard Setback | 20 ft. | 20 ft. | 25 ft. | 20 ft. | 25 ft. |
| Interior Side Yard Setback | 0 ft. | 0 ft. | 0 ft. | 0 ft. | 0 ft. |
| (Abutting Residential Zone) | 30 ft. | 30 ft. | 30 ft. | 30 ft. | 30 ft. |
| Street Side Yard Setback | 20 ft. | 20 ft. | 15 ft. | 15 ft. | 15 ft. |
| Rear Yard Setback | 25 ft. | 25 ft. | 10 ft. | 10 ft. | 10 ft. |
| (Abutting Residential Zone) | 50 ft. | 50 ft. | 40 ft. | 40 ^a ft. | 40 ft. |
| Building Height | 35' ² / ₂ stories ^{**} | 35' ² / ₂ stories ^{**} | 50' ³ / ₃ stories | 50' ³ / ₃ stories | 50' ³ / ₃ stories |
| Floor Area Ratio | 0.45 | 0.50 | 0.35 | 0.35 | 0.25 |
| Site Landscaping ^a | 10% | 10% | 20% | 20% | 20% |

* Any lot which fronts on a turnaround or curving street having a radius of curvature of less than 100 feet, the minimum frontage shall be 60 feet.

** Increased height up to 80 feet is permitted where all building setbacks meet or exceed the proposed building height.

^a May be reduced through the use of colored pavers or other decorative pavement treatments under certain conditions. See item (b) landscaping.

(b) Landscaping

Landscaping design for development in the March Business Center Specific Plan Amendment SP-1 A78 shall be consistent with the March Business Center Design Guidelines. A 15-foot landscaped setback, measured from the public right-of-way, will be required for all front and side yards adjacent to public streets. The following two exceptions apply:

- (1) The use of colored pavers or other decorative pavement treatments within public right-of-ways or public areas in the Specific Plan Amendment Area may reduce the site landscaping requirements for Office, Mixed-use and Commercial land uses by up to a maximum of two percent.
- (2) The use of colored pavers or other decorative pavement treatments within public right-of-ways or public areas in the Specific Plan Amendment Area may reduce the site landscaping requirement for Industrial land uses on lots or development greater than 20 acres by up to a maximum of two percent.

(c) Driveway Widths and Locations

Driveway width and spacing shall be in conformance with the March JPA Development Code or as approved by the March IPA Civil Engineer.

(d) Off-street Loading Facilities

Loading or unloading facilities shall be so sized and located so that they do not require trucks to be located in required front or street side yards during loading and unloading activities.

(e) Special Regulations

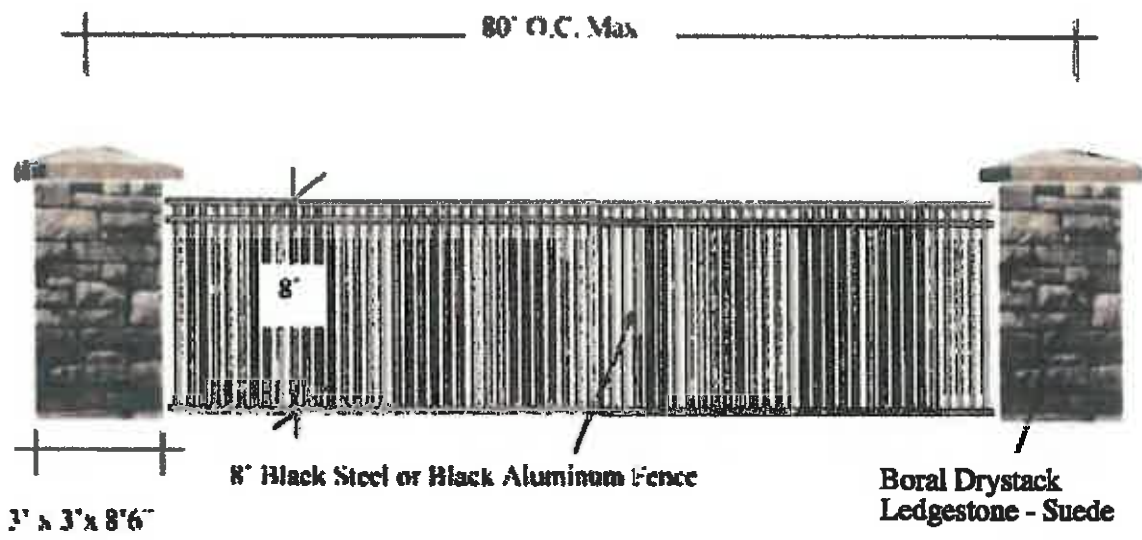
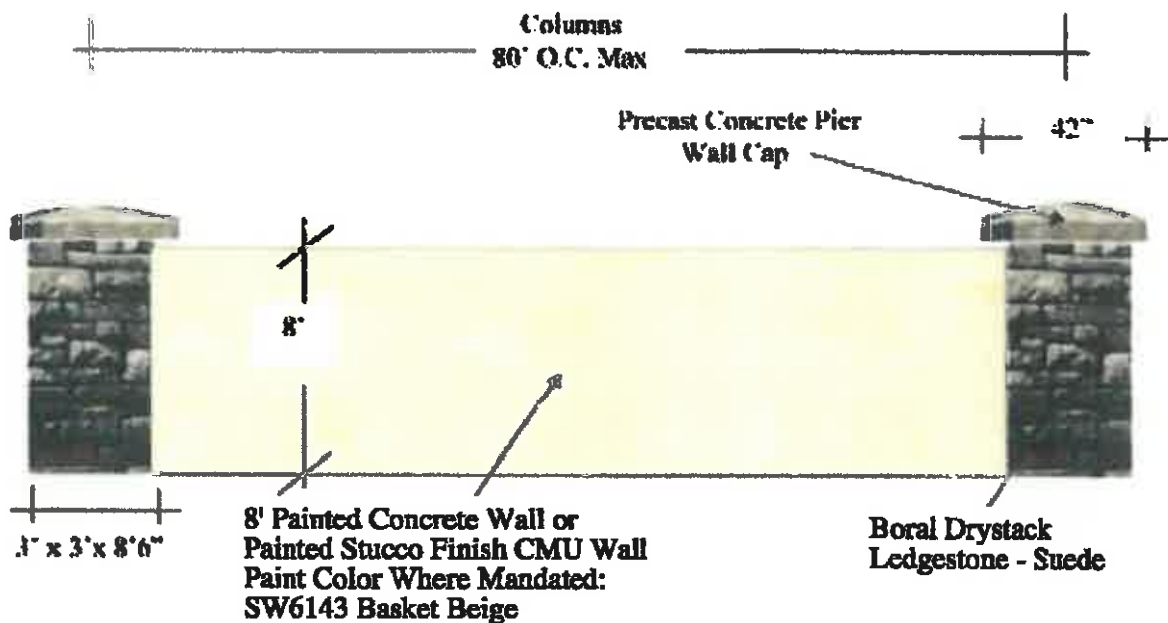
All uses, except storage, loading and outdoor work, shall be conducted entirely within an enclosed building. Outdoor work; storage of merchandise, material, and equipment is permitted in interior side or rear yards, provided the area is completely enclosed by sight obscuring walls, fences, or a combination thereof.

Fences and Walls: The design and location of fences and walls shall be the same as set forth in the March Business Center Design Guidelines.

In addition to the above, the following regulations apply:

- (1) Chain link fences shall not be used within 100 feet of a public right-of-way. Where used, chain link fences shall be vinyl coated.
- (2) Coiled, spiraled, or rolled fencing such as razor wire or concertina wire shall not be permitted.
- (3) All walls or fences within 100 feet of a public right-of-way or visible from residential development shall match wall details in Figure III-3 (higher walls may be necessary to screen trucks and outdoor storage, consistent with the approved screening plan). All walls facing residential development and along the outside boundaries of the Meridian South Campus Specific Plan Area shall be painted the designated color as shown Figure III-3. All other site walls may be painted to be consistent with the approved building color scheme.

MARCH BUSINESS CENTER - SPECIFIC PLAN AMENDMENT



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FIGURE III-3

WALL DETAIL

(f) Off-street Parking Regulations

Transportation Element Policy 2.7 of the March JPA General Plan indicates that on-street parking should be de-emphasized in order to both increase vehicle capacity and to accommodate bicycle access. Table III-3 summarizes Specific Plan parking ratios. It is acknowledged that certain land uses will have unique parking characteristics, based on building utilization, workforce composition, and other considerations. In these cases, the March JPA Commission may review a use permit application to reduce required parking through a detailed parking analysis. All uses shall provide a rack that can accommodate at least three bikes.

| TABLE III-3 PARKING RATIOS BY LAND USE | |
|--|--|
| Use | Parking Spaces Per 1,000 Square Feet of Gross Floor Area (Unless Otherwise Noted) |
| INDUSTRIAL | |
| Light, Medium & Heavy Manufacturing | 2.0 |
| Research & Development | 3.0 |
| Other Industrial Uses | 1.0 - 1.5 |
| WHOLESALE STORAGE & DISTRIBUTION | |
| Public Storage/Mini-Warehouse | 1 per 100 storage spaces & 2 per caretaker residence |
| Other Wholesale Storage/Distribution | 1.0 per 1,000 sq. ft. |
| 0 - 50,000 sq. ft. | 50 spaces + (0.33 per ksf > 50,000 sq. ft.) |
| 50,000 - 200,000 sq. ft. | 100 spaces + (0.20 per ksf > 200,000 sq. ft.) |
| 200,000 sq. ft. or greater | |
| OFFICE | |
| Medical Clinics, Hospitals, and Medical Offices | 4.0 |
| Other Office | 3.3 |
| COMMERCIAL | |
| Agricultural Equipment Repair Shops | 2.5 |
| Agricultural/Nursery Supplies | 3.3 |
| Alcoholic Beverage Outlets | 2.5 |
| Animal Care/Pet Hotels | 2.5 |
| Assembly and Entertainment | 3.3 |
| Automotive Parts and Accessory Sales | 3.3 |
| Automotive Service Stations | 3/station + 2/service bay |
| Automotive/Truck Repair-Major | 3/station + 2/service bay |
| Automotive/Truck Repair-Minor | 3/station + 2/service bay |
| Business Support Services | 4.0 |
| Churches and Places of Religious Assembly | 1/3 seats, 1/60 inches of pew, 30.0 if no seating |
| Energy Generation & Distribution Facilities | 2.0 |
| Equestrian Show & Exhibition Facilities | 1/3 seats |
| Funeral & Mortuary Services | 1/3 seats, 30.0 for assembly area if no fixed seating |
| Interpretive Centers | 3.3 |
| Major Transmission, Relay or Communications Switching Stations | 2.0 |
| Bar & Grill | 8.0 |
| Outdoor Recreation and Swap Meet Facilities | (to be determined through use permit) |
| Private Clubs, Lodges and Fraternal Organization | 4.0 |
| Radio and Television Studios | 3.3 |
| Restaurants (sit down) | 8.0 |
| Restaurants (fast food) | 8.0 |
| Theaters | 1-3 screens: 1/3 seats 4+ screens: 1/3.3 seats Per assembly area if no fixed seating: 50 |
| Vehicle & Vehicular Equipment Sales and Service | 1 per each 20 display cars (minimum of 5 spaces) |
| Vehicle Storage | 2.5 |
| Veterinary Clinic & Animal Hospitals | 2.5 |
| Other Commercial | 3.5 |

(g) Settlement Agreement Provisions

A Settlement and General Release Agreement for Development of March Business Center within the March Joint Powers Authority was signed by the March JPA, the Master Developer and community groups in 2003 as the result of litigation following March JPA approval of the previously adopted Specific Plan. The Settlement Agreement contained a number of land use requirements and limitations related to “logistics”⁹ warehouse type facilities, including the prohibition of such facilities to the west of Meridian Parkway. This Specific Plan Amendment is consistent with this restriction.

H. Special Treatment Areas

1. Riparian Areas

Riparian areas found within the project area are considered prime habitat for the endangered least bell vireo. The riparian vegetation community is located along several of the brief and intermittent drainages within the project area. These areas are concentrated in the north portion of the South Campus and the south portion of the North Campus. Projects adjacent to designated riparian areas will not be allowed to encroach within one hundred feet of the drainage channel. In addition, storm water runoff from adjacent development is to be intercepted by applicable non-source pollution control best management practices (BMP) prior to discharging off-site.

2. Adjacent to SKR Management Area

As part of the on-going management within the Stephens’ kangaroo rat (SKR) set-aside area, development of lots directly adjacent to this area will be subject to brush removal activities along their western boundary. Suitable SKR habitat requires minimal vegetation. This is provided with frequent controlled burns. No encroachment of grading or improvements within the SKR management area are allowed without March JPA or USF&WS approval. Projects which are adjacent to the SKR management area will be required to design their sites in such a way as to limit the potential for controlled burns crossing into the property. These measures may include placing parking along the western edge of the site and use of less-flammable plants for landscaping.

⁹ Defined in the Settlement Agreement as any structure over 500,000 square feet that is designed to have more than 95% of its ground floor area dedicated to the storage of finished goods that are received at the facility specifically to be stored for a time and then shipped to various locales.

IV. OPEN SPACE

A. Conservation Easements

The previously adopted Specific Plan and this Specific Plan Amendment include jurisdictional waters of the U.S. and State waters, which consist of unnamed ephemeral, intermittent and perennial channels, in-channel wetlands and wetlands. The previously adopted Specific Plan and this Specific Plan Amendment impact 4.06 acres that are waters of the U.S. and State waters under the jurisdiction of the U.S. Army Corps of Engineers (ACOE) and California Department of Fish and Game (CDFG). The total impact to waters of the U.S. is 2.36 acres, and the total impact to State waters is 1.70 acres.

The previously adopted Specific Plan and this Specific Plan Amendment will mitigate on-site and within the adjacent vacant land for impacts to jurisdictional waters of the U.S./State waters and LBV habitat. The purpose of the mitigation is to replace lost functional values of avian habitat, water quality and aesthetics. The mitigation will include the following:

- Create 4.2 acres of habitat (Riparian Woodland, Southern Willow Scrub and Mulefat Scrub) suitable for occupation by LBV in realigned and restored drainages.
- Create 1.9 acres and restore 1.4 acres of wetland waters of the U.S. and State waters.

All newly created and existing adjacent habitat and wetlands will be overlaid with a conservation easement for management and monitoring in perpetuity. The boundary of the easements will be extended past the limits of the habitat to include a "buffer" zone. This "buffer" will serve as additional protection of wetlands and riparian habitat. Also, this area provides the potential for wildlife to find refuge in a conserved and maintained habitat. The Conservation Easement is 185.9 acres and is located to the north and south of Van Buren Boulevard and west of the North Campus.

B. Park

An approximately 59-acre park area will be located near the southern boundary of the South Campus, south of Krameria Avenue. Approximately 16 acres of the designated park area is impacted by a closed and capped landfill. This area is subject to certain Deed Restrictions as directed by the Air Force and regulates/restricts access to this area. This 16-acre area will generally remain in its natural vegetation state. Approximately 6 acres of the park, adjacent to and accessed via Village West Drive is shown to be a picnic type area with turf, shade trees, benches, picnic tables, restrooms, parking areas, etc. This could be used by employees within March Business Center and/or local residents. Discussions have been held that may see the balance of the usable area of the designated park be utilized as an "interpretive style" memorial park incorporating the historical significance of the former March Air Force Base. Preliminary plans indicate that this area would incorporate approximately 11 acres. In addition, a 3-46.2-acre pocket dog park and paseo will be located at the west terminus of Krameria Avenue, on the eastern side of Barton Street across from the Santa Inez Way and Barton Street intersection. The dog park and paseo would extend to Caroline Way and provide an open space connection to Krameria Avenue. Additional natural buffers surrounding the developed portions of the park areas would amount to approximately 19 acres.

The park is shown in the third phase of the project. Subject to the availability of construction capital and ongoing maintenance responsibilities being defined, the park could be constructed prior to

commencement of the third phase of the development without causing significant impacts to the area. Potential sources of funding for the construction and maintenance of the park include county and local funds, grants, bonds, tax increment financing or private sources. The park will have the following acreages:

| | |
|------------------------|--------------------------------|
| Capped landfill | 16 AC |
| Peeket Park | 3.4 AC |
| <u>Dog Park</u> | <u>6.2 AC</u> |
| Picnic area/parking | 6 AC |
| Interpretive park | 11 AC |
| Buffer/open space | <u>26 AC</u> |
| TOTAL: | 62.4 <u>65.2</u> AC |

C. Scenic Corridor Landscape

Landscaping easements will be provided along the project frontages along segments of Van Buren Boulevard, Cactus Avenue, and Alessandro Boulevard. These easements will be 20 feet wide and the landscaping will be maintained by a Landscaping and Lighting Assessment District. A 30-foot landscape easement will be provided along the eastern side of Barton Street along the project frontage. In addition, a nine-foot landscaping easement will be provided along the entire length of Meridian Parkway (formerly Street Z). Supplemental landscaping shall also be provided adjacent to the Elementary School in accordance with the Elementary School Buffer Zone. The March Business Center Design Guidelines establish the landscaping criteria.

D. Drainage Basins

Drainage detention facilities will be provided along the eastern portion of the North Campus and the southwestern portion of the South Campus. The southern North Campus basin will encompass the portion of the project that is included in the CZ. ~~Currently, there is a new Drainage Master Plan underway, which will revise the need for dual 8' x 8' box culverts at the Project's Van Buren terminus of the San Jacinto River Watershed. This revision to the Drainage Master Plan incorporates an increase of volume to the existing basins which can accommodate South Campus' post-development run-off contribution into this system.~~ The northern facility is south of Alessandro Boulevard in APZ II. The detention facilities shall be maintained by either the Riverside County Flood Control and Water Conservation District (RCFC & WCD) or an assessment district. Storm water detention basins would also be provided south of Van Buren Boulevard and east of Orange Terrace to detain storm water related to the Van Buren Boulevard widening improvements. Storm water would be collected by parkway drains leading to rip rap energy dissipators that spill into the basins. Water would percolate into the soil and then collected by a 6-inch perforated storm drain line located under each basin, then routed to an 18-inch high-density polyethylene (HDPE) storm drain line that leads to the existing storm drain under Van Buren Boulevard.

V. TRANSPORTATION

A. Existing Transportation Issues

Local and regional access to project area is provided by I-215, Alessandro Boulevard, Cactus Avenue, Barton Street, and Van Buren Boulevard. A Burlington Northern and Santa Fe (BNSF) Railway line runs parallel to I-215, along the west side of the freeway. Existing transportation network issues are summarized below:

- The Van Buren Boulevard/Barton Street intersection is characterized by LOS F conditions during the morning peak.
- An off-site segment of Krameria Avenue, west of Barton Street, will need to be paved in order to accommodate future traffic growth. This improvement will be constructed by the adjacent subdivider.

While the Specific Plan requires a General Plan amendment due to land use changes, it should be noted that the Specific Plan land uses will have a substantially lower traffic generation than previously approved uses evaluated in the MEIR. The MEIR assumed a development scenario for the entire March JPA area that totaled 220,000 trips per day. Of this, 131,400 trips per day were evaluated for the 1,290 acres comprising March Business Center. As discussed in the *Traffic Circulation and Phasing Study*, the March Business Center will generate 88,100 total daily trips, including 74,900 external trips. This is a 33 percent reduction from approved General Plan land uses. The *Traffic Circulation and Phasing Study* was prepared to fulfill the following objectives:

1. To provide the baseline for future traffic monitoring updates
2. To specify the internal circulation network for the March Business Center Specific Plan
3. To determine the timing of off-site transportation improvements with respect to Specific Plan development phases
4. To confirm whether or not the transportation improvements identified in the 1998 Transportation Study are still needed, given updated land use information for the Specific Plan and the surrounding area

B. Traffic Circulation Plan

An internal roadway network, consisting of a hierarchy of local, collector and arterial streets, will be constructed to provide access to and from the parcels comprising the March Business Center. The internal street network will be public roadways to be maintained by the County of Riverside. Off-site transportation improvements will be provided to ensure there is sufficient capacity to accommodate future traffic. The improvements associated with each development phase will be assured to the satisfaction of the JPA prior to the occupancy of that phase.

1. Project Development Phasing

~~South Campus of~~ The March Business Center will be constructed in phases. Figure I-3 on page I-4 of this Specific Plan illustrates the boundaries of each of the three phases. The purpose of phasing the project is to divide the project into smaller phases that deliver transportation facilities based on when the

project needs these improvements. As shown in this figure, the first development phase encompasses the North Campus area between Alessandro Boulevard and the to-be-demolished former military housing project. Figure V-1 illustrates the transportation improvements associated with this development phase. Phase 2 includes the area formerly occupied by the demolished housing development. The improvements for Phase 2 are shown in Figure V-2. Phase 3 includes the South Campus. The improvements for this phase are shown in Figure V-3.

2. Street Sizing and Landscaping

The internal street network will accommodate traffic from the March Business Center, traffic generated by new development in the project vicinity, and existing trips diverted to internal streets from parallel routes. Figure V-4 illustrates the classifications of internal roadways and Van Buren Boulevard. Figure V-5A – V-5G depicts typical cross-sections for internal roadways based on the *Riverside County Road Improvement Standards and Specifications*. The project's circulation network has been designed to be consistent with the Riverside County Integrated Plan (RCIP) recommended additional right-of-way allocated for landscaping. Secondary Highways will be consistent with RCIP guidelines. However, additional landscaping on Van Buren Boulevard in accordance with RCIP is not practical for the following reasons:

- Existing improvements are provided adjacent to the Orangecrest development and Riverside National Cemetery
- Van Buren Boulevard will traverse sensitive habitat west of the North Campus

However, the design of Van Buren Boulevard is consistent with the intent of the RCIP because additional landscaping will be provided along this roadway adjacent to the project, including a 50-foot landscaping easement on the south side of Van Buren Boulevard, east of Barton Street.

County standards typically provide 12-foot travel lanes and eight-foot shoulders. In cases where a Class II bike lane is recommended, the shoulder will be replaced by a six-foot striped bike lane, with the remaining two feet added to the outer travel lane (i.e., this lane would be 14 feet wide). This configuration is desirable to accommodate both bicyclists and trucks or other heavy vehicles.

3. Traffic Monitoring

One of the MEIR mitigation measures requires traffic monitoring every five years. Because the Transportation Study prepared in support of the MEIR was published in 1998, an updated *Transportation Circulation and Phasing Study* has been completed for March Business Center. This document forms the basis for future traffic monitoring updates. This document has established a traffic generation "budget" in order to ensure that the transportation network has sufficient capacity to accommodate project traffic. This budget is 74,900 daily external trips. Over the course of project buildout, the traffic generation characteristics of the site will be monitored and compared to the traffic generation assumed in the *Transportation Circulation and Phasing Study*. If future increases in density result in traffic generation in excess of the established budget, then the traffic impacts and mitigation identified in the study must be revisited as part of the traffic monitoring update process.

MARCH BUSINESS CENTER - SPECIFIC PLAN AMENDMENT

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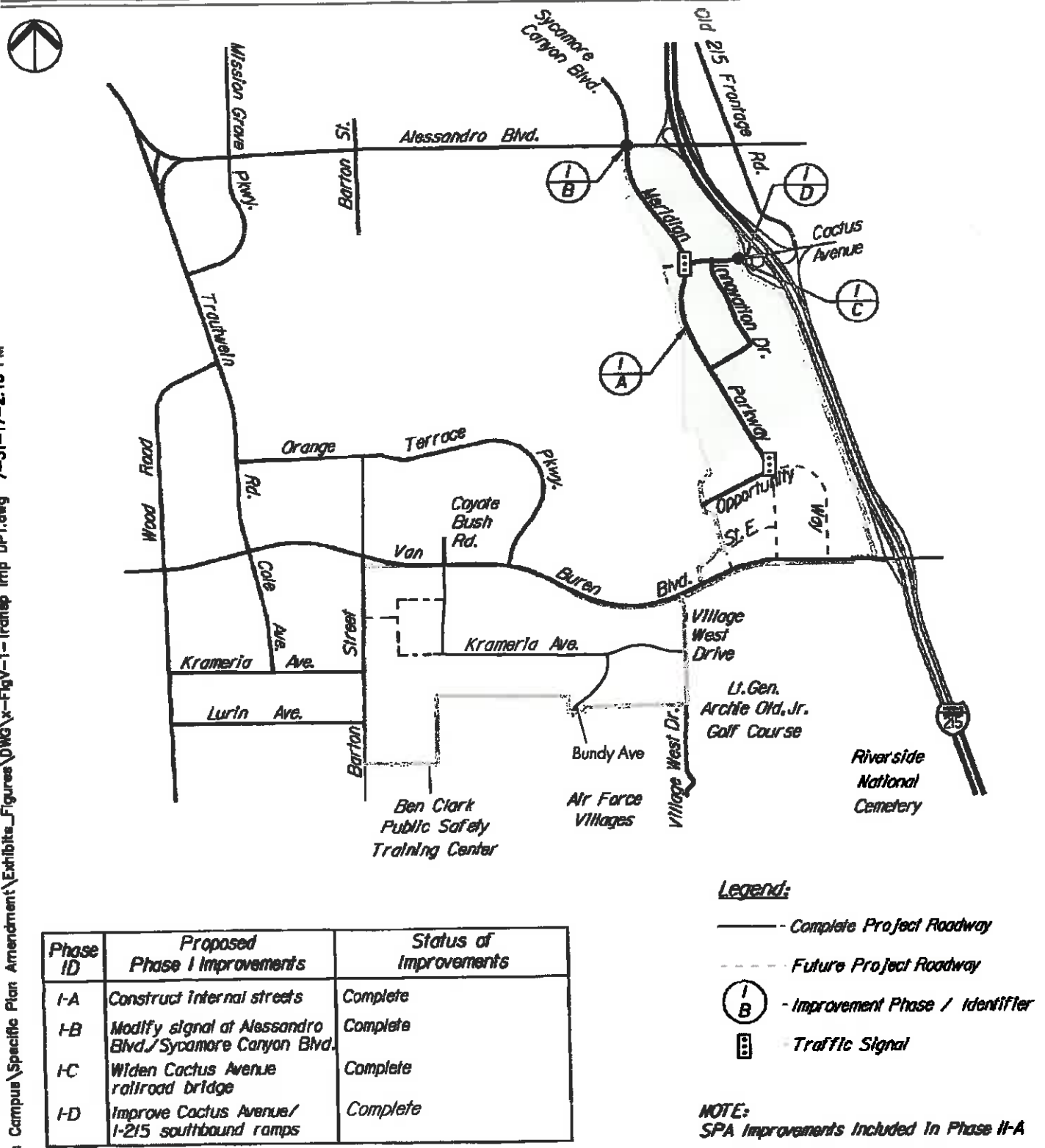
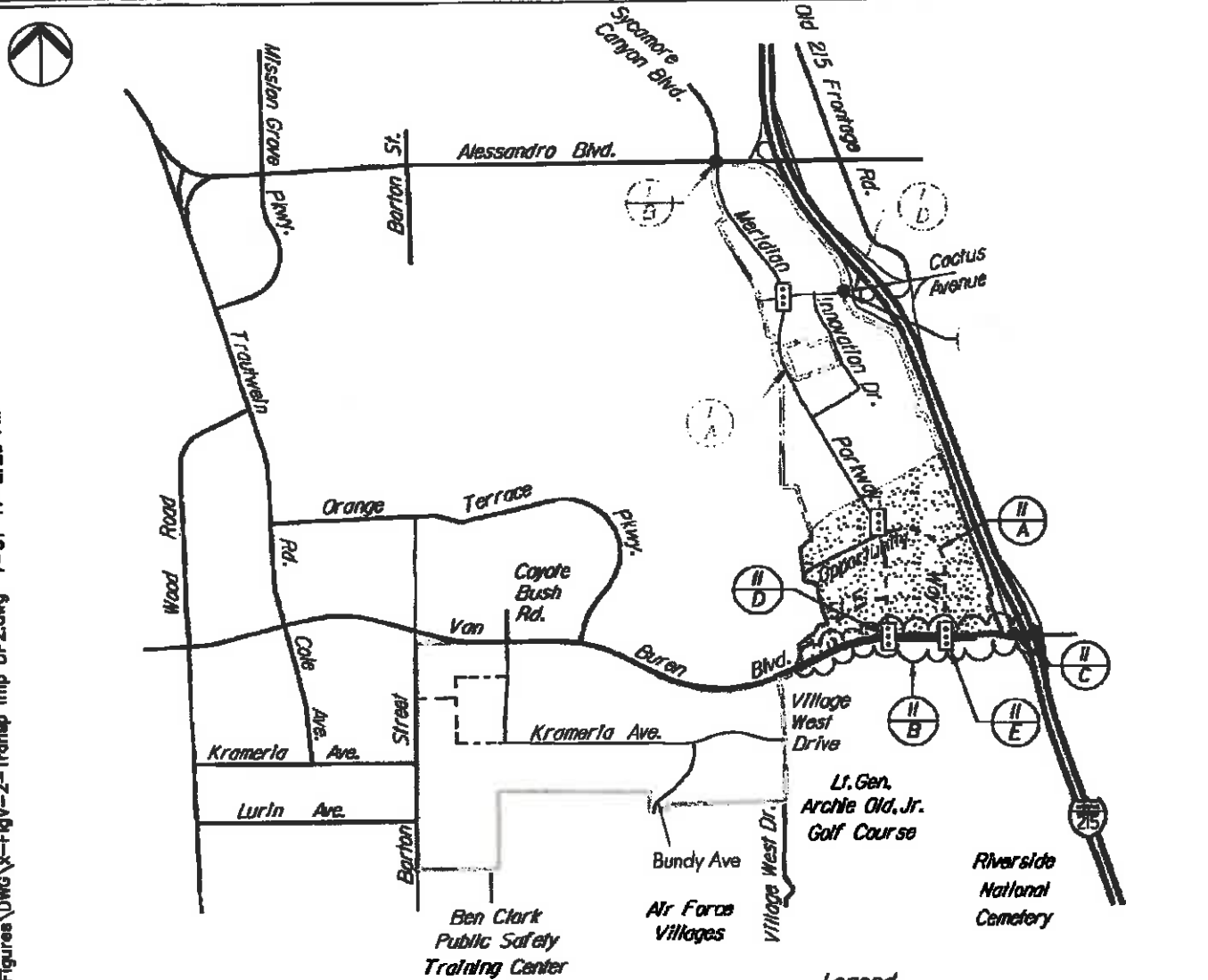


FIGURE V-1
TRANSPORTATION IMPROVEMENTS,
DEVELOPMENT PHASE I

MARCH BUSINESS CENTER - SPECIFIC PLAN AMENDMENT

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Legend:

- - - Future Project Roadway
- ⊙ I / B - Improvement Phase / Identifier
- ▭ - Traffic Signal
- ▨ - Phase II

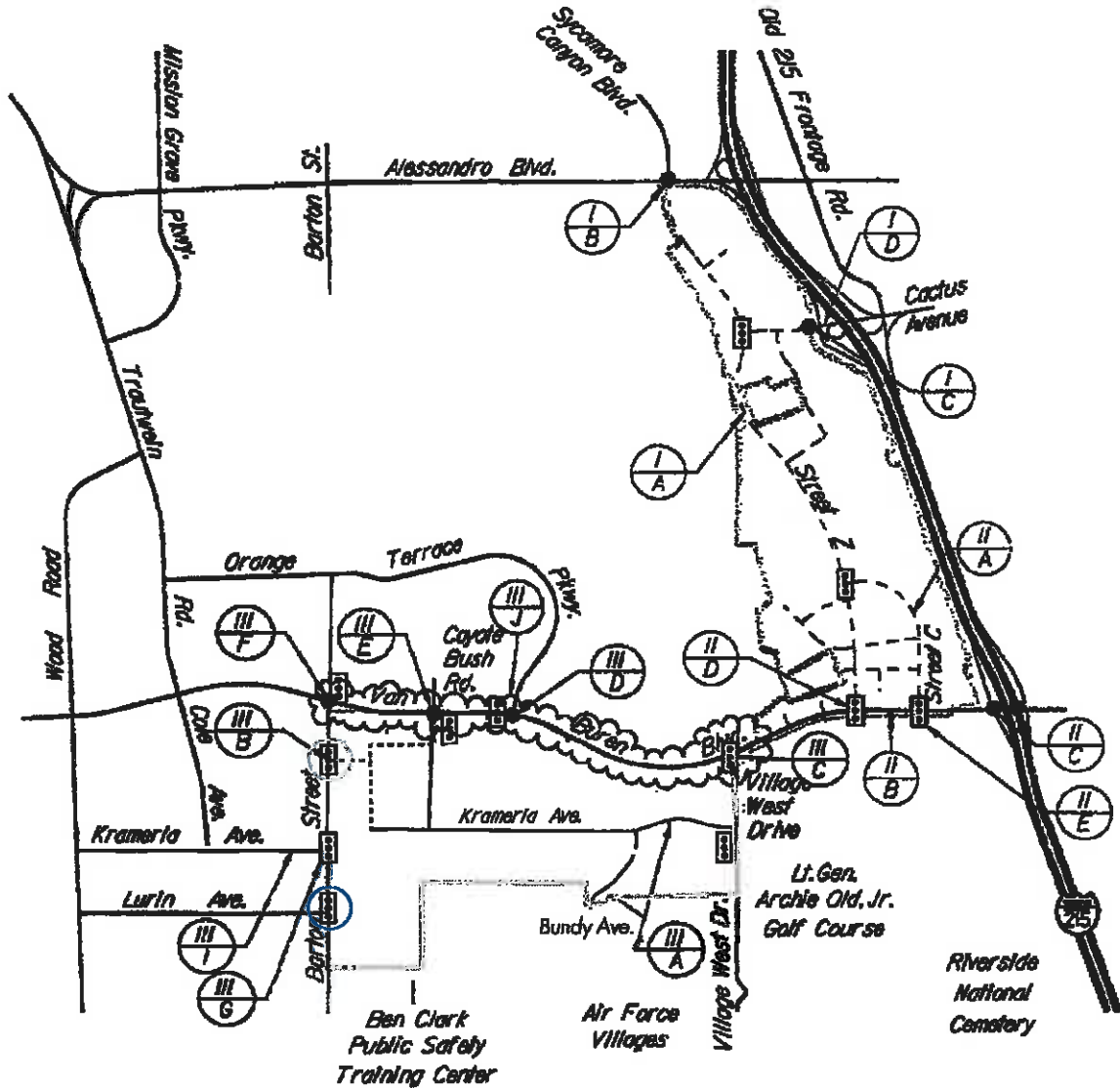
| Phase ID | Proposed Phase 2 Improvements | Status of Improvements * |
|----------|--|--------------------------|
| II-A | Construct Internal streets | Complete |
| II-B | Widen Van Buren Boulevard | In Construction |
| II-C | Improve Van Buren Blvd / I-215 Interchange | Complete |
| II-D | Provide signal control | Complete |
| II-E | Provide signal control | Complete |

NOTE:
SPA Improvements Included In Phase II-A

* Improvements to be Constructed or Assured upon Issuance of occupancy permit for any use within Phase 1, 2, or 2-A after traffic generated by this use exceeds 44,966 ADT.

**FIGURE V-2
TRANSPORTATION IMPROVEMENTS,
DEVELOPMENT PHASE II**

MARCH BUSINESS CENTER - SPECIFIC PLAN AMENDMENT



| Phase ID | Proposed Phase 3 Improvements |
|------------------|--|
| III-A | Construct Internal streets |
| III-B | Half-width Improvements on Barton Street |
| III-C Thru III-G | Provide or modify traffic signal control |
| III-H | Complete |
| III-J | Widen Van Buron Boulevard |

Legend:

- Future Project Roadway
- Future Roadway By Others
- (I/B) - Improvement Phase / Identifier
- [T] - Traffic Signal
- [T] - Traffic Signal (if warranted)

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FIGURE V-3
TRANSPORTATION IMPROVEMENTS,
DEVELOPMENT PHASE III

MARCH BUSINESS CENTER - SPECIFIC PLAN AMENDMENT

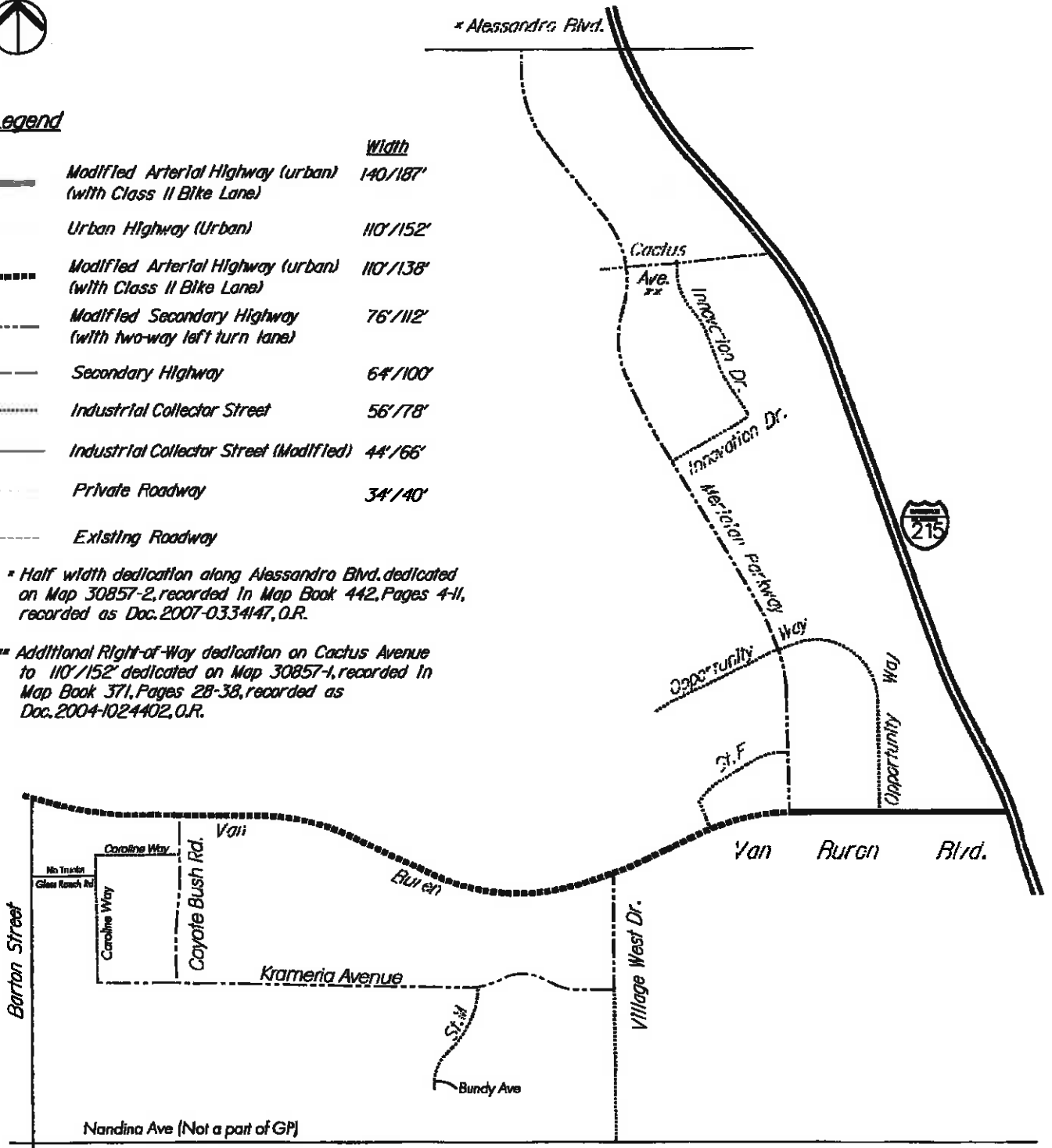


Legend

| | <i>Width</i> |
|--|--|
| | Modified Arterial Highway (urban) (with Class II Bike Lane) 140'/187' |
| | Urban Highway (Urban) 110'/152' |
| | Modified Arterial Highway (urban) (with Class II Bike Lane) 110'/138' |
| | Modified Secondary Highway (with two-way left turn lane) 76'/112' |
| | Secondary Highway 64'/100' |
| | Industrial Collector Street 56'/78' |
| | Industrial Collector Street (Modified) 44'/66' |
| | Private Roadway 34'/40' |
| | Existing Roadway |

* Half width dedication along Alessandra Blvd. dedicated on Map 30857-2, recorded in Map Book 442, Pages 4-II, recorded as Doc. 2007-0334147, O.R.

** Additional Right-of-Way dedication on Cactus Avenue to 110'/152' dedicated on Map 30857-1, recorded in Map Book 371, Pages 28-38, recorded as Doc. 2004-1024402, O.R.



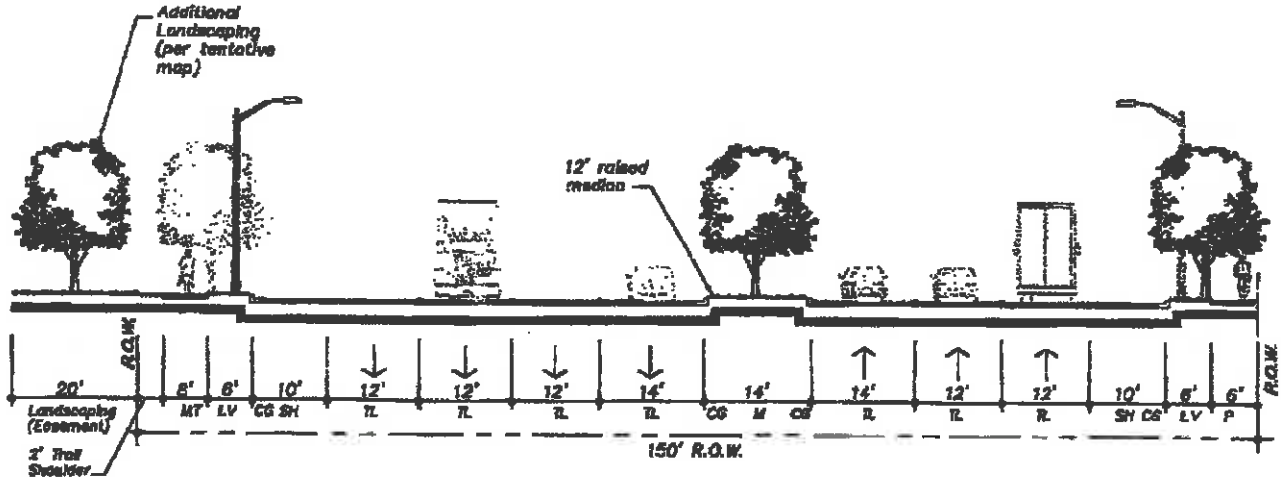
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FIGURE V-4

TRANSPORTATION PLAN

MARCH BUSINESS CENTER - SPECIFIC PLAN AMENDMENT

NOT TO SCALE



Legend

- TL = Travel Lane
- M = Median
- LV = Landscape Verge
- P = Pedestrian Way
- CG = Curb/Gutter
- SH = Shoulder
- MT = Multi-use Trail

Notes:

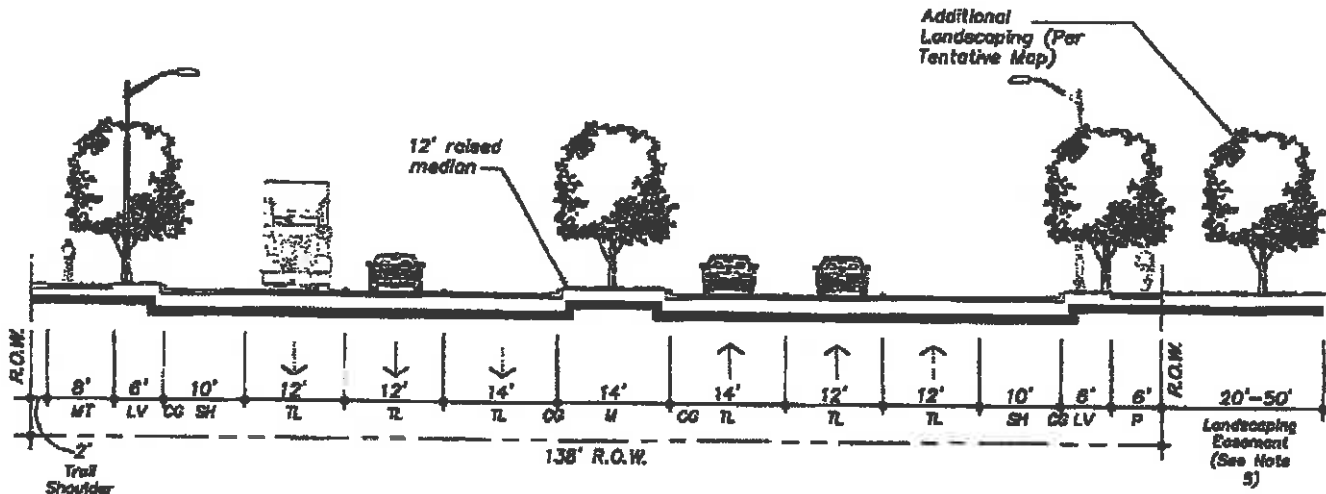
1. Combined thickness of base and surface to be determined by soil test.
2. Minimum paving thickness per Riverside County specifications.
3. Refer to Figure V-7 of the Specific Plan for the location of the Class I trail.
4. Design will match existing improvements where possible adjacent to Riverside National Cemetery.
5. In order to conform to the I-215/Van Buren interchange, two additional eastbound through lanes are added east improvements of Meridian Parkway.

FIGURE V-5A

**VAN BUREN BLVD (MERIDIAN PARKWAY STREET-Z TO I-215, FACING EAST)
MOD. ARTERIAL (URBAN) HIGHWAY, 7 LANES (WITH CLASS I MULTI-USE TRAIL)**

MARCH BUSINESS CENTER - SPECIFIC PLAN AMENDMENT

NOT TO SCALE



Legend

- TL = Travel Lane
- M = Median
- LV = Landscape Verge
- P = Pedestrian Way
- CG = Curb/Gutter
- SH = Shoulder
- MT = Class I Multi-Use Trail

Notes:

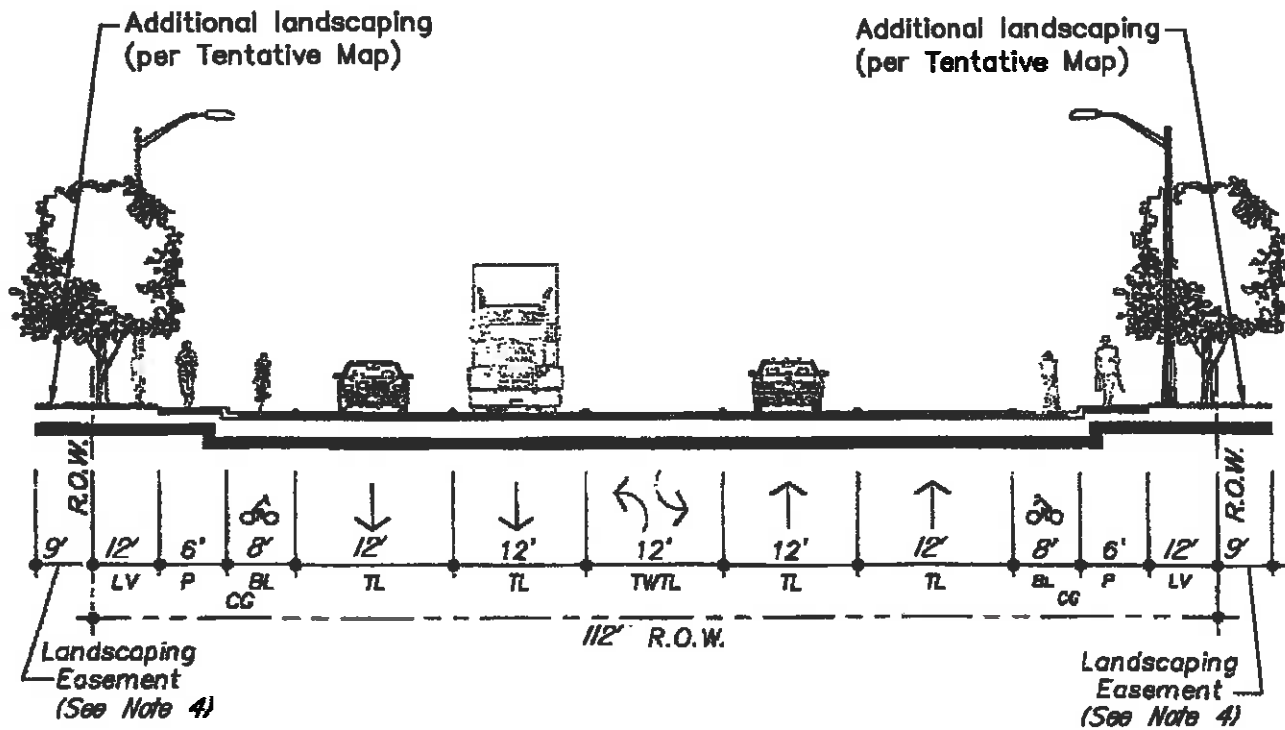
1. Combined thickness of base and surface to be determined by soil test.
2. Minimum paving thickness per Riverside County specifications.
3. Refer to Figure V-7 of the Specific Plan for the location of the Class I trail.
4. North side of Van Buren Boulevard to be constructed as proposed between Orange Crest Parkway and Street Z only.
5. -50' landscape easement on Van Buren Boulevard from South edge of Lot 39 to the east edge of South Campus Lot 22.
-20' landscape easement on Van Buren Boulevard from West edge of North Campus Lot 23 to East edge of South Campus Lot 26.
6. Design will match existing improvements where possible adjacent to Orangecrest.
7. Landscaping will not be provided adjacent to wildlife habitat.
8. Multi-use trail will transition from north side of Van Buren Blvd. to the south side at Orange Terrace Parkway.

FIGURE V-5B

VAN BUREN BLVD (WEST OF MERIDIAN PARKWAY STREET Z, FACING EAST)
MOD. ARTERIAL (URBAN) HIGHWAY (WITH CLASS I MULTI-USE TRAIL)

MARCH BUSINESS CENTER - SPECIFIC PLAN AMENDMENT

NOT TO SCALE



Legend

- TL = Travel Lane
- LV = Landscape Verge
- P = Pedestrian Way
- CG = Curb/Gutter
- BL = Class II Bike Lane
- TWTL = Two-Way Left Turn Lane

Notes:

1. Combined thickness of base and surface to be determined by soil test.
2. Minimum paving thickness per Riverside County specifications.
3. Refer to Figure V-7 of the Specific Plan for the location of the Class II bike lanes.
4. Provide 9' landscaping easement on Krameria Avenue and Street Z.
5. For Krameria Avenue and Street Z, the parkway slope from the back of sidewalk will be 6:1.
6. For Barton Street, provide 30' landscaping easement on east side only.
7. Assessment district will maintain all landscaping in public right-of-way.

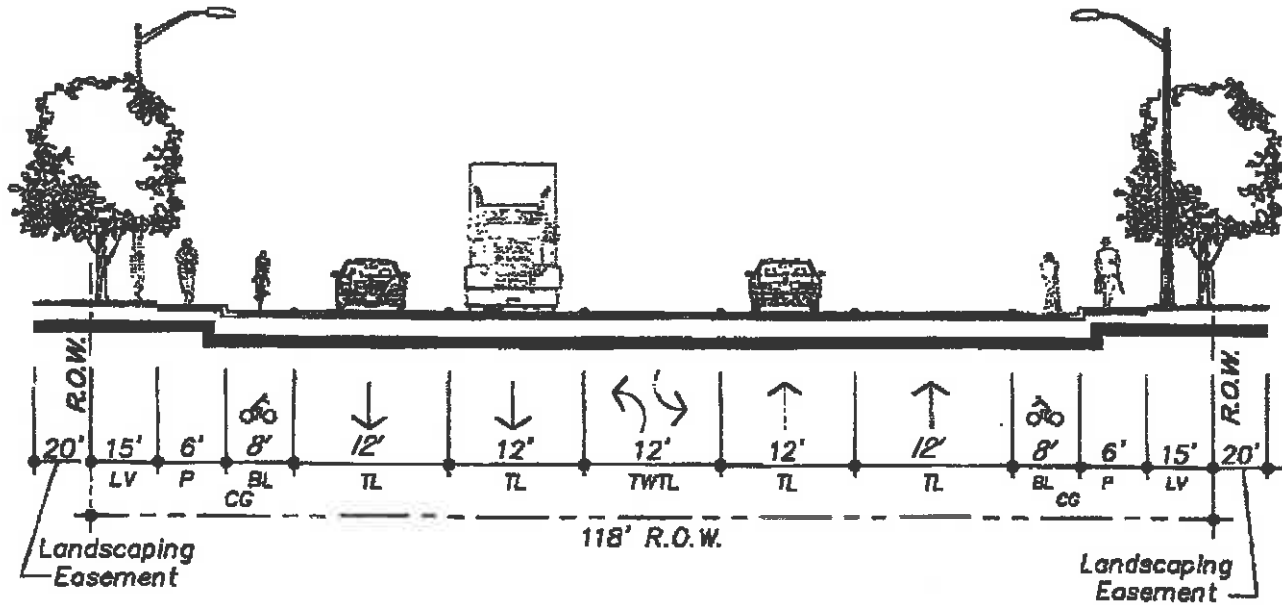
FIGURE V-5C

**BARTON STREET (EAST HALF ONLY), STREET Z, MERIDIAN PARKWAY
KRAMERIA AVENUE, AND VILLAGE WEST DR
MOD. SECONDARY HIGHWAY (WITH TWO WAY LEFT TURN LANE AND CLASS II BIKE LANES)**

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MARCH BUSINESS CENTER - SPECIFIC PLAN AMENDMENT

NOT TO SCALE



Legend

- TL = Travel Lane
- LV = Landscape Verge
- P = Pedestrian Way
- CG = Curb/Gutter
- BL = Class II Bike Lane
- TWTL = Two-Way Left Turn Lane

Notes:

1. Combined thickness of base and surface to be determined by soil test.
2. Minimum paving thickness per Riverside County specifications.
3. Refer to Figure V-7 of the Specific Plan for the location of the Class II bike lanes.
4. Assessment district will maintain all landscaping in public right-of-way.
5. Build Out of Future Development Area will necessitate widening of Cactus Avenue to Arterial (Urban) Highway Standards (110' pavement/152' right-of-way).

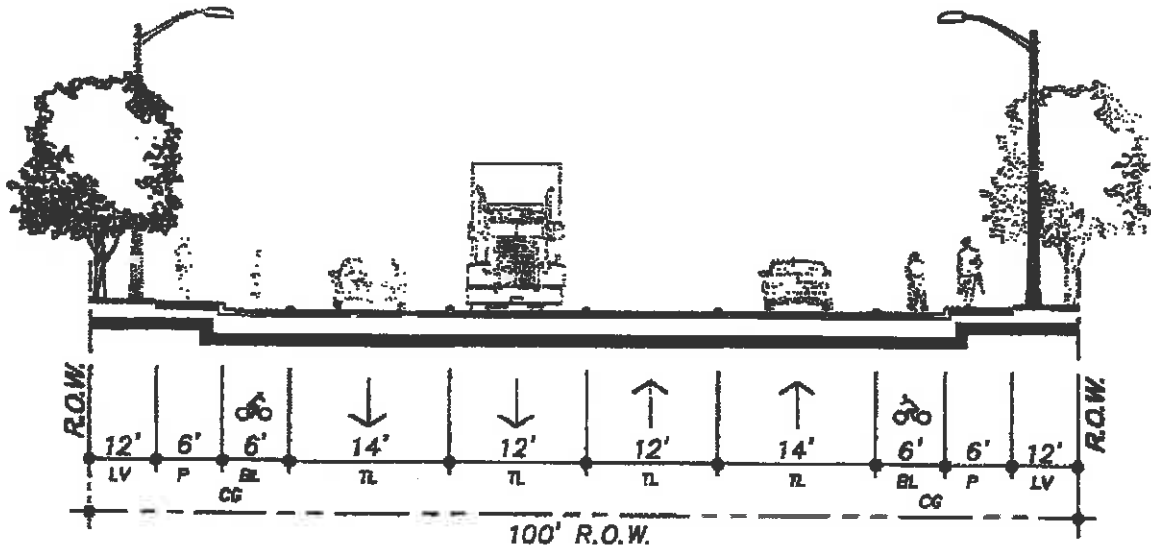
FIGURE V-5C1

CACTUS AVENUE (MERIDIAN PARKWAY STREET-2 TO EASTERN EDGE OF SPECIFIC PLAN)
MOD. SECONDARY HIGHWAY(WITH TWO WAY LEFT TURN LANE AND CLASS II BIKE LANES)

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MARCH BUSINESS CENTER - SPECIFIC PLAN AMENDMENT

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Legend

- TL = Travel Lane
- LV = Landscape Verge
- P = Pedestrian Way
- CG = Curb/Gutter
- BL = Class II Bike Lane

Notes:

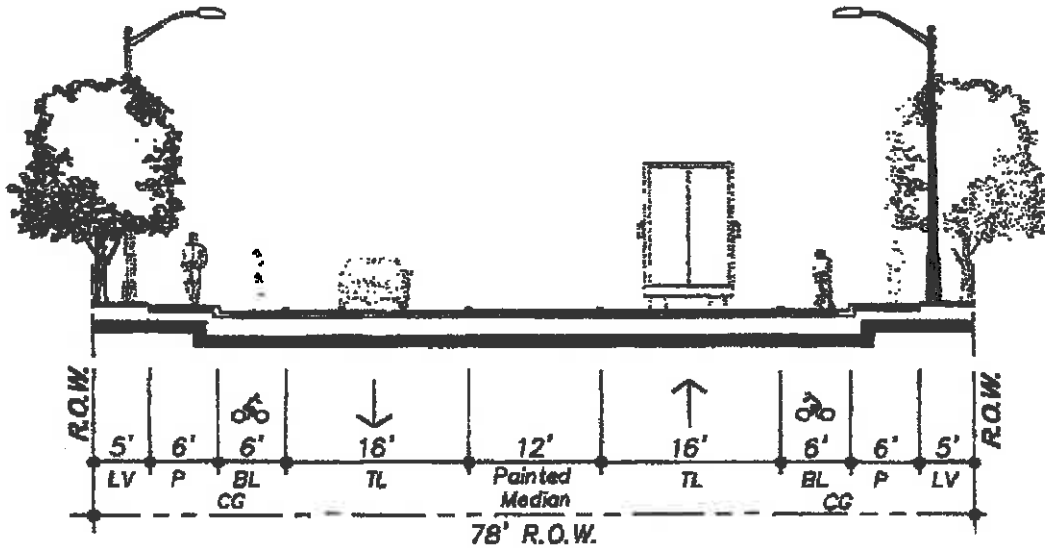
1. Combined thickness of base and surface to be determined by soil test.
2. Minimum paving thickness per Riverside County specifications.
3. Refer to Figure V-7 of the Specific Plan for the location of the Class II bike lanes.

FIGURE V-5D

**COYOTE BUSH ROAD
SECONDARY HIGHWAY (WITH CLASS II BIKE LANE)**

MARCH BUSINESS CENTER - SPECIFIC PLAN AMENDMENT

NOT TO SCALE



Legend

- TL = Travel Lane
- M = Median
- LV = Landscape Verge
- P = Pedestrian Way
- CG = Curb/Gutter
- BL = Class II Bike Lane

Notes:

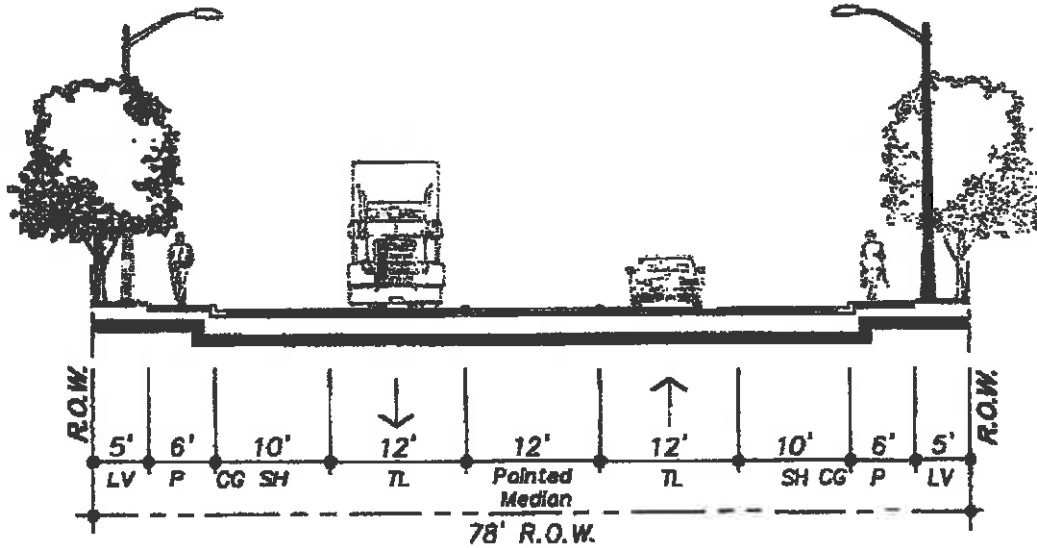
1. Combined thickness of base and surface to be determined by soil test.
2. Minimum paving thickness per Riverside County specifications.
3. All curb and gutter to be type "A-6" unless otherwise specified.
4. Refer to Figure V-7 of the Specific Plan for the location of Class II bike lanes.

FIGURE V-5E

OPPORTUNITY WAY (EAST OF MERIDIAN PARKWAY) ECONOMIC WAY (EAST OF MERIDIAN PARKWAY), STREET G, STREET E (EAST OF STREET Z), STREET F (EAST OF STREET Z), STREET K, STREET W, LURIN AVENUE, VILLAGE WEST DRIVE (SOUTH OF KRAMERIA AVE.) AND STREET Y, AND CAROLINE WAY INDUSTRIAL COLLECTOR STREET (WITH CLASS II BIKE LANE)

MARCH BUSINESS CENTER - SPECIFIC PLAN AMENDMENT

NOT TO SCALE



Legend

- TL = Travel Lane
- M = Median
- LV = Landscape Verge
- P = Pedestrian Way
- CG = Curb/Gutter
- SH = Shoulder

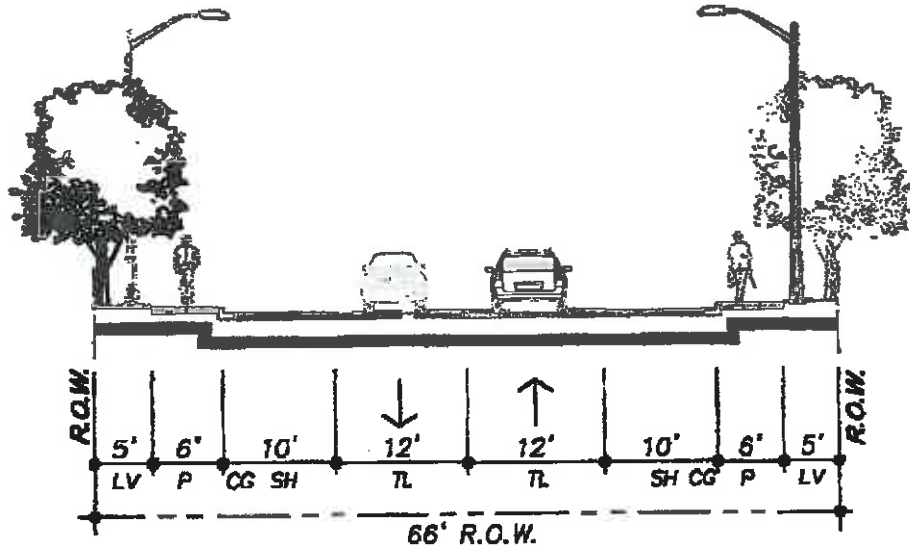
Notes:

1. Combined thickness of base and surface to be determined by soil test.
2. Minimum paving thickness per Riverside County specifications.
3. All curb and gutter to be type "A-6" unless otherwise specified.
4. Refer to Figure V-7 of the Specific Plan for the location of Class II bike lanes.
5. Parking allowed on shoulders.

FIGURE V-5F

INNOVATION DRIVE, SYSCO WAY, STREET A, STREET D, STREET E (FROM STREET Z TO STREET D)
 STREET M, STREET P, AND STREET Q
 INDUSTRIAL COLLECTOR STREET (NO CLASS II BIKE LANE)

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Legend

- TL = Travel Lane
- LV = Landscape Verge
- P = Pedestrian Way
- CG = Curb/Gutter
- SH = Shoulder

Notes:

1. Combined thickness of base and surface to be determined by soil test.
2. Minimum paving thickness per Riverside County specifications.
3. See Standard No. 212 for A.C. Dike Detail.
4. All curb and gutter to be type "A-6" unless otherwise specified.
5. This standard may also serve as a cul-de-sac in industrial or commercial use areas but shall not exceed 660 feet.

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FIGURE V-5G
 GLESS RANCH ROAD - STREET E (NORTH OF STREET D),
 STREET F (WEST OF STREET Z), STREET G, STREET H, STREET R, STREET S, STREET T AND STREET U,
 INDUSTRIAL COLLECTOR (MODIFIED) GUL-DE-SAG

4. "Reverse Commute"

As discussed in Section III, The March Business Center Specific Plan will serve as a major employment center in a portion of the County that is characterized by primarily residential land uses. This will improve the balance of population and employment in the project vicinity, providing an opportunity for residents to work locally, rather than commute to Los Angeles or Orange Counties. Jobs/housing balance will provide a transportation capacity benefit, reducing the concentration of work trips in the peak hour/peak direction of travel.

The benefits of matching jobs and housing can be illustrated in the context of typical peak hour commuting patterns. For example, people living in Perris or other locations to the south of the project area may work in Orange County. In the morning peak some of these motorists may choose to exit I-215 at Van Buren Boulevard and proceed westbound on route SR-91. The March Business Center would "capture" some of the trips travelling along this path. This would provide the following benefits:

1. The magnitude of the future volume increase on Van Buren Boulevard west of the project, and at the Van Buren Boulevard/SR-91 interchange, would not be as high as compared to a no-project or residential type of development
2. The distance and duration of certain additional work trips will be substantially reduced,¹⁰ providing fuel consumption and air quality benefits

The congestion, air quality, and fuel consumption benefits associated with the "capture" of trips along the route described above would also apply to other commuting patterns, both in the immediate project vicinity and the surrounding region.

5. Transportation Demand Management

While the March Business Center will provide a regional transportation benefit, much of the traffic accessing the site will be concentrated in peak commuting hours. This can cause regional and local problems, such as peak hour traffic congestion, increased air pollution, and extended periods of time spent commuting. Transportation Demand Management (TDM) strategies will be implemented to shift trips outside the standard commuting hours and/or to non-"drive alone" modes of travel. This is accomplished through various employer-initiated measures, such as flexible working hours, encouragement of carpooling, and facilitating access for non-motorized (i.e., bicycling or walking) modes of travel. The following TDM measures are recommended:

1. The March JPA shall coordinate with the RCTC as the project Transportation Management Agency (TMA). The purpose of the TMA will be to:
 - Provide information on employee matching for carpools and van pools
 - Identify park and ride lot locations
 - Provide information on and encourage transit use

¹⁰ Transportation modeling done for the MEIR indicated the commuting trips out of Riverside County would be reduced by 32,000 vehicles per day with the implementation of the General Plan. Typical commuting distances would be reduced from 50 miles to 16 miles, reducing vehicle miles of travel by 350,000 per day (MEIR, page 3-69).

2. Each employer with more than 250 full-time employees shall submit a TDM plan to the JPA. The TDM plan shall address the following:
 - Designate a TDM coordinator
 - Provide a space (e.g., kiosk, bulletin board, etc.) for rideshare information
 - Provide preferential parking for carpools
 - Identify bus routes and bicycle facilities in the vicinity
 - Provide flexible working hours and/or a telecommuting program (to the extent feasible)
 - Bicycle storage facilities
 - Showers and locker rooms (optional)

C. Non-Automobile Circulation

1. Transportation Center

The existing General Plan Transportation Element identified a potential commuter rail/bus transfer facility near the I-215/Van Buren Boulevard interchange. This was actually constructed on Unit 2 Lot 4, whereas the construction was completed in December of 2015. The Transit Center, constructed by RCTC, has provided a commuter rail station and a transfer facility for express bus, local bus, and area shuttle service.

2. Local Transit Service

As established in the March Business Center Design Guidelines, site design and building orientation will facilitate pedestrian access and transit service. Where appropriate, this Specific Plan requires the installation of bus improvements, such as bus turnouts, bus stops, and terminals as part of the conditions of development for land uses that have a large number of employees. This Specific Plan also requires dedication of appropriate transit routes, stations, and stops as part of new development.

3. Truck Traffic

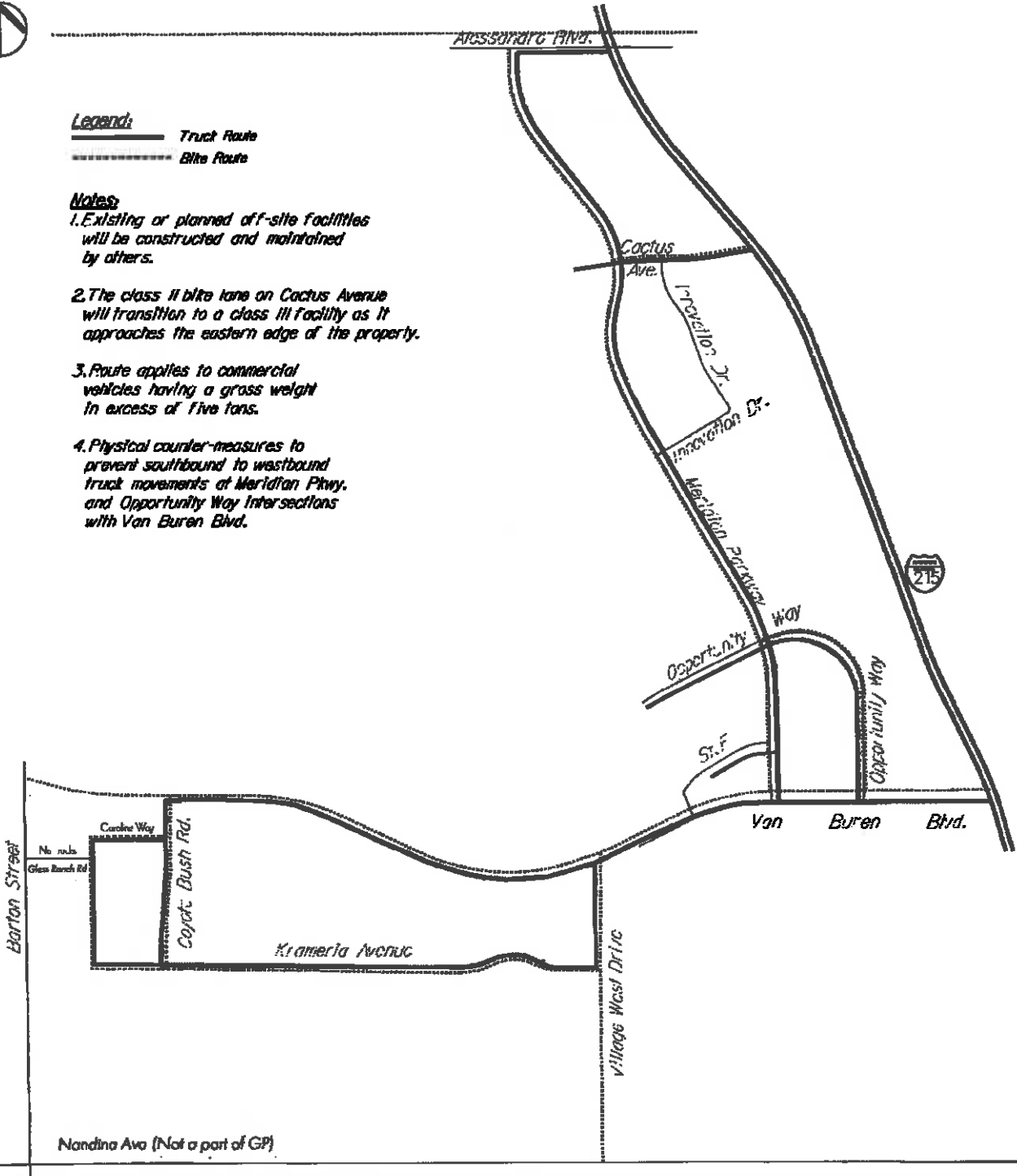
The industrial, business park, warehousing, and related uses typically generate a higher volume of truck traffic than other types of uses. The large size and acceleration/deceleration characteristics of trucks have a disproportionate impact on transportation capacity, as compared to passenger vehicles. In order to reduce the impacts of trucks on roadway and intersection capacity, trucks that are not on local delivery routes will be routed through the Specific Plan roadways in accordance with the circulation routes depicted on Figure V-6. Also, the project will cooperate with the City of Riverside to support measures to restrict the use of residential collector streets and secondary highways by trucks. All project through streets within the Specific Plan are classified as Industrial Collectors (or higher classifications) in order to accommodate trucks. Design of pavement sections will provide a structural depth sufficient for anticipated truck traffic. Key access intersections shall be designed to accommodate truck turns.

MARCH BUSINESS CENTER - SPECIFIC PLAN AMENDMENT



Legend:
 ——— Truck Route
 - - - - - Bike Route

- Notes:**
1. Existing or planned off-site facilities will be constructed and maintained by others.
 2. The class II bike lane on Cactus Avenue will transition to a class III facility as it approaches the eastern edge of the property.
 3. Route applies to commercial vehicles having a gross weight in excess of five tons.
 4. Physical counter-measures to prevent southbound to westbound truck movements at Meridian Pkwy. and Opportunity Way Intersections with Van Buren Blvd.



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FIGURE V-6

TRANSPORTATION SYSTEMS PLAN

4. Bicycle/Pedestrian Access

Bicycle and pedestrian linkages will help implement the trip reduction strategies outlined in the Transportation Demand Management section. The proposed network will consist of Multi-Use Bicycle paths (i.e., Class I facilities physically separated from vehicular traffic), Bike Lanes (Class II facilities), which are striped lanes on the shoulders of roadways, and Bike Routes (Class III facilities), which are designated by signs and traverse the shoulder of the roadway. Class III routes are not striped. The linkages were identified based on the following criteria:

1. The network was defined based on likely routes between March Business Center and existing and future residential development in the project area.
2. The proposed routes will provide linkages to bicycle and pedestrian facilities identified by adjacent jurisdictions. According to the City of Moreno Valley's Bikeway Plan, Cactus Avenue will accommodate a Class I facility east of I-215. According to the City of Riverside's General Plan, Alessandro Boulevard accommodates a Class II facility.
3. Bicyclists and pedestrians on Van Buren Boulevard should be physically separated from vehicular traffic due to the high-speed design and heavy volumes anticipated on this roadway.
4. Class II facilities should be provided on internal streets to facilitate access to project land uses and the Transit Center

Figure V-6 depicts the Class I, Class II, and Class III facilities on project streets and Van Buren Boulevard.

VI. INFRASTRUCTURE AND GRADING

A. Existing Infrastructure Issues

The March Business Center Specific Plan area, particularly the South Campus, consists primarily of vacant land, with minimal municipal utilities or services. Public facilities, services and infrastructure will be provided concurrently with the appropriate phase of project development. Figure I-3 on page I-4 of this Specific Plan shows the boundaries of each project development phase.

B. Sewer Service and Facilities

Existing sewer service in the March Business Center Specific Plan area is provided to the DRMO facility and the abandoned Arnold Heights residential development. Figure VI-1 shows sewer facilities in the project vicinity. Sewage is currently conveyed to an existing secondary treatment plant located west of I-215 and north of Nandina Avenue. The on-site sewer system has been transferred to the Western Municipal Water District (WMWD). The existing treatment plant has a capacity of 0.75 million gallons per day (mgd), of which 0.60 mgd is used by existing development. Substantial additional conveyance facilities and treatment capacity must be provided in order to accommodate the sewage to be generated by Specific Plan land uses.

WMWD is constructing a replacement tertiary reclamation plant that will expand and upgrade the treatment capacity of the existing plant to 1.0 mgd. Future expansions will increase the treatment capacity to 5.0 mgd. The costs associated with these expansions will be borne by future developments in the March Business Center and other areas served by the sewer system. WMWD will own and maintain all planned future sewer facilities, including lift stations.

C. Potable Water Service

When March Air Force Base was an active duty military installation, it consumed 2.14 million gallons of water a day for both domestic and irrigation uses. Potable water delivered to the General Plan area is supplied by the WMWD via a 54-inch distribution main operated by Eastern Municipal Water District. (Note: WMWD has taken over the share of this pipe's capacity that was formerly controlled by MAFB.) A 20-inch pipeline transports water from Lake Mathews to the Lt. Gen. Archie Old Golf Course and to Riverside National Cemetery.

The March Business Center will substantially increase water demand as compared to previous uses. Because of the location and capacity of existing facilities, an entirely new system must be built to accommodate the project. Figure VI-2 illustrates project water supply facilities. All potable water facilities, including water mains, zone transitions, pressure pumps and reducers, storage facilities, will be operated and maintained by WMWD.

MARCH BUSINESS CENTER - SPECIFIC PLAN AMENDMENT



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LEGEND

- SEWER SYSTEM
- XX" PROPOSED BUILDOUT SIZE
- - - EXISTING SEWER

NOTE:

LOCATION AND CAPACITY OF FACILITIES ARE PRELIMINARY, SUBJECT TO CHANGE BASED ON MORE DETAILED STUDIES AND PROJECT DEVELOPMENT PHASING.

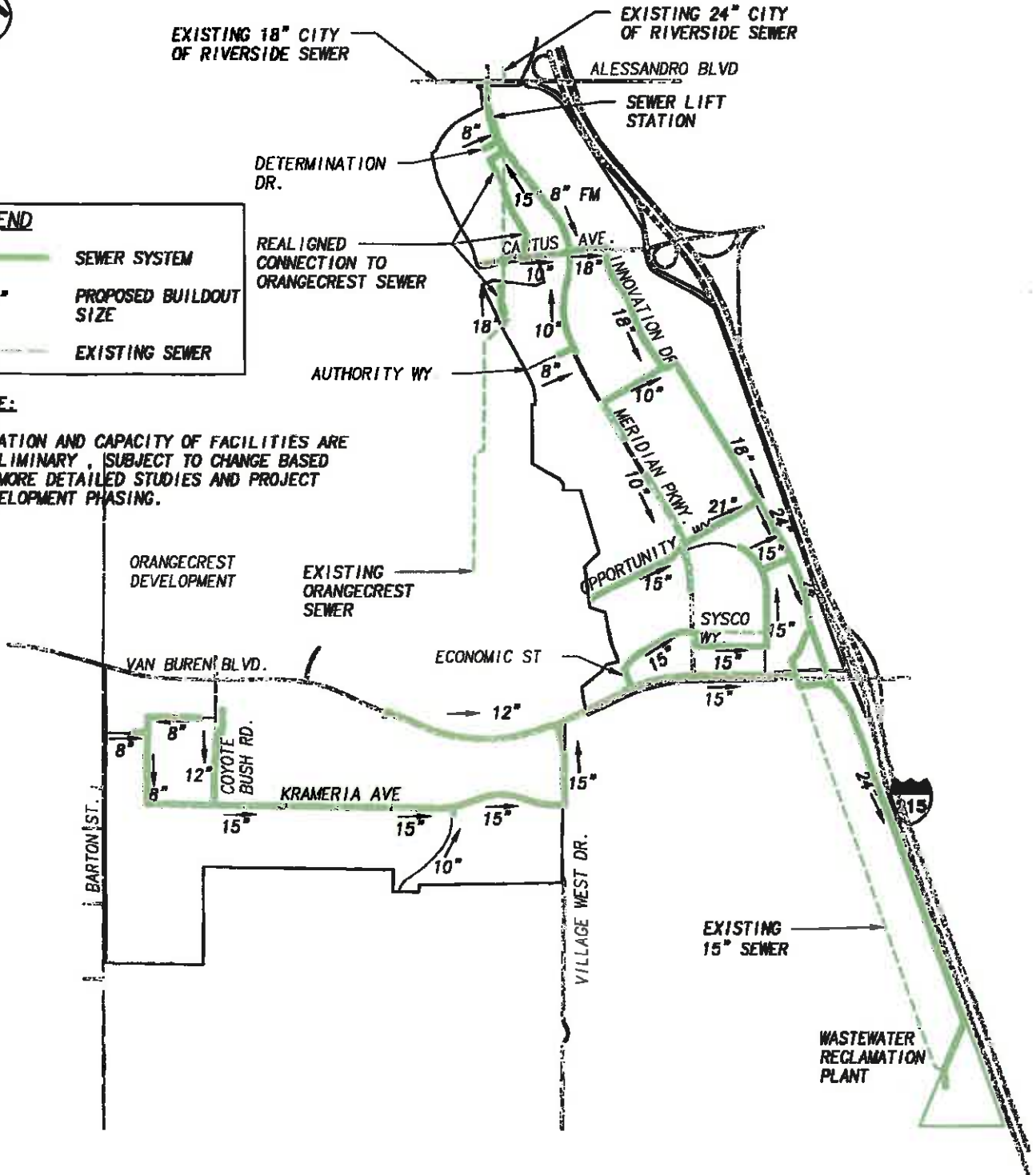


FIGURE VI-1

SEWER SYSTEM

MARCH BUSINESS CENTER - SPECIFIC PLAN AMENDMENT

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LEGEND

- WATER PIPELINE SYSTEM
 - CONNECTION TO WESTERN MUNICIPAL WATER DISTRICT SYSTEM
 - PRESSURE REDUCING VALVE (PRV)
 - EXISTING WATER LINE
 - PROPOSED PRESSURE ZONE BOUNDARY
- 1900** PROPOSED PRESSURE ZONE

NOTE:

LOCATION AND CAPACITY OF FACILITIES ARE PRELIMINARY, SUBJECT TO CHANGE BASED ON MORE DETAILED STUDIES AND PROJECT DEVELOPMENT PHASING.

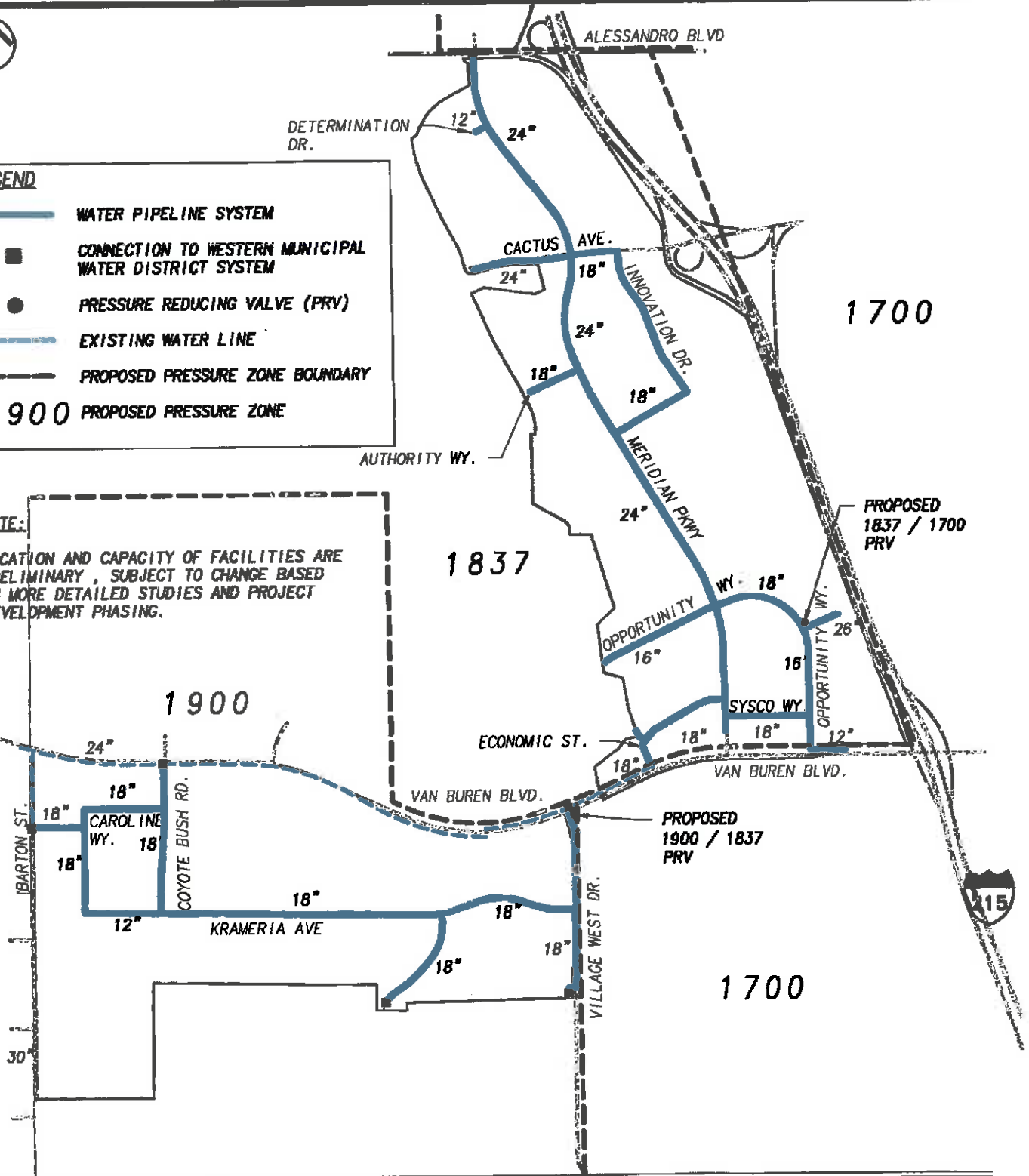


FIGURE VI-2

WATER SYSTEM

D. Reclaimed Water

The March Business Center project will use reclaimed water for landscape irrigation throughout the project area. As the project develops, reclaimed water pipelines will be constructed within the rights-of-way of new roadways. Currently, no Title 22 reclaimed water is available to the project area. This may require that the reclaimed water system be temporarily connected to the potable system. The existing wastewater treatment plant along I-215 at Nandina Avenue provides secondary effluent through a 12-inch force main to a holding reservoir at the golf course (0.33 million gallons (mg)). This effluent is used to irrigate the cemetery and the golf course. The effluent does not meet state or federal treatment requirements for use as irrigation water, so it will not be used by the March Business Center project. The improvements to the existing treatment plant described above will provide a treatment capability that meets the requirements (i.e., Title 22) of the Regional Water Quality Control Board. The expanded plant will treat wastewater generated by the March Business Center project and supply it to the project's reclaimed water system. Figure VI-3 illustrates the reclaimed water system. The proposed reclaimed water system will supply reclaimed water for all landscaped areas, streetscape, and for other open space areas that require irrigation. The reclaimed water distribution system, including pumps and storage facilities, will be maintained by WMMD.

E. Storm Water Management

The existing watersheds that drain the March Business Center project flow into either the Santa Ana River Basin or the San Jacinto River Basin. Both the northern portion of Phase 1 (which flows to the north) and the western edge of Phase 3 (which flows to the west) drain into the Santa Ana River Basin. The remainder of the project flows to the south into the San Jacinto River Basin. Soil types within the Specific Plan area consist primarily of Monserate sandy loam and Fallbrook sandy loam. A Drainage Plan has been prepared to identify and size drainage facilities required to accommodate the runoff resulting from the additional impervious area created by project development. This Plan is a project-specific supplement to the *March Air Force Base Reuse Drainage Plan* prepared for the Riverside County Flood Control and Water Conservation District (RCFC & WCD) for the entire General Plan area.

Figure VI-4 depicts proposed on-site drainage facilities. As shown in this figure, a detention basin will be provided in the eastern portion of the North Campus, north of Van Buren Boulevard. This area is designated as a Clear Zone (CZ) to accommodate aircraft operations at March ARB. Drainage facilities will intercept storm water runoff, reducing peak hour flows substantially below existing levels. The drainage system shall employ Best Management Practices for drainage, water quality, using basins, erosion control, and urban pollution removal prior to the discharge of runoff into natural watersheds or wetlands. Storm drain facilities shall be designed to the standards of the RCFC & WCD. Public storm drains 39" and larger will be maintained by the RCFC & WCD and private facilities shall be maintained by the property owner. The detention basins will be maintained by RCFC & WCD or an assessment district. Storm water detention basins would also be provided south of Van Buren Boulevard and east of Orange Terrace to detain storm water related to the Van Buren Boulevard widening improvements. Storm water would be collected by parkway drains leading to rip rap energy dissipators that spill into the basins. Water would percolate into the soil and then collected by a 6-inch perforated storm drain line located under each basin, then routed to an 18-inch high-density polyethylene (HDPE) storm drain line that leads to the existing storm drain under Van Buren Boulevard.

MARCH BUSINESS CENTER - SPECIFIC PLAN AMENDMENT



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LEGEND

- PROPOSED MBG RECLAIMED WATER SYSTEM
- EXISTING PIPELINE
- FUTURE EXTENSIONS
- CONNECTION TO MMWD NON-POTABLE SYSTEM

NOTE:

LOCATION AND CAPACITY OF FACILITIES ARE PRELIMINARY, SUBJECT TO CHANGE BASED ON MORE DETAILED STUDIES AND PROJECT DEVELOPMENT PHASING.

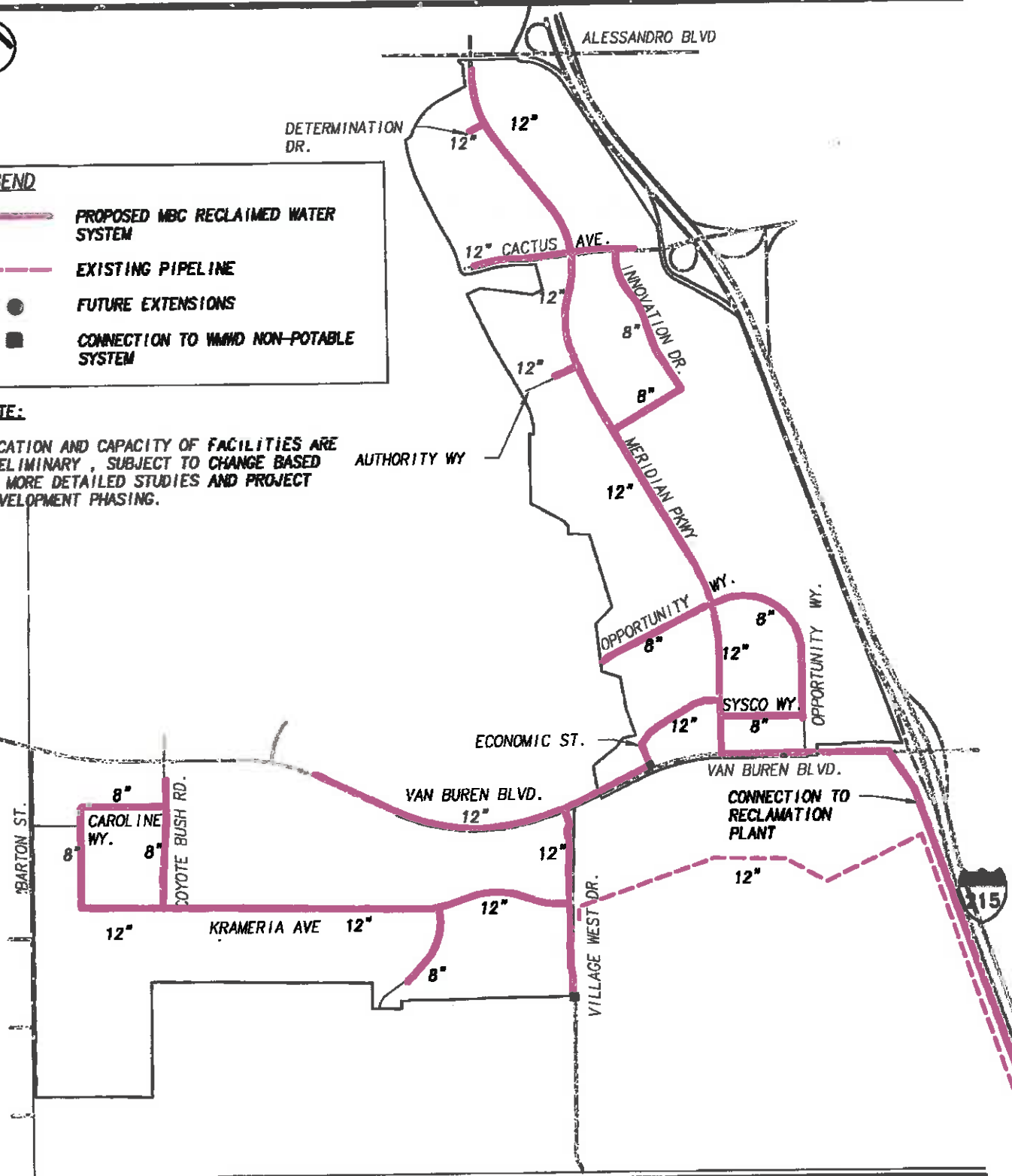


FIGURE VI-3

RECLAIMED WATER SYSTEM

MARCH BUSINESS CENTER - SPECIFIC PLAN AMENDMENT

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LEGEND

- STORM DRAIN SYSTEM
- DETENTION BASIN
- OPEN CHANNEL
- DRAINAGE DIVIDE

NOTE:

LOCATION AND CAPACITY OF FACILITIES ARE PRELIMINARY, SUBJECT TO CHANGE BASED ON MORE DETAILED STUDIES AND PROJECT DEVELOPMENT PHASING.

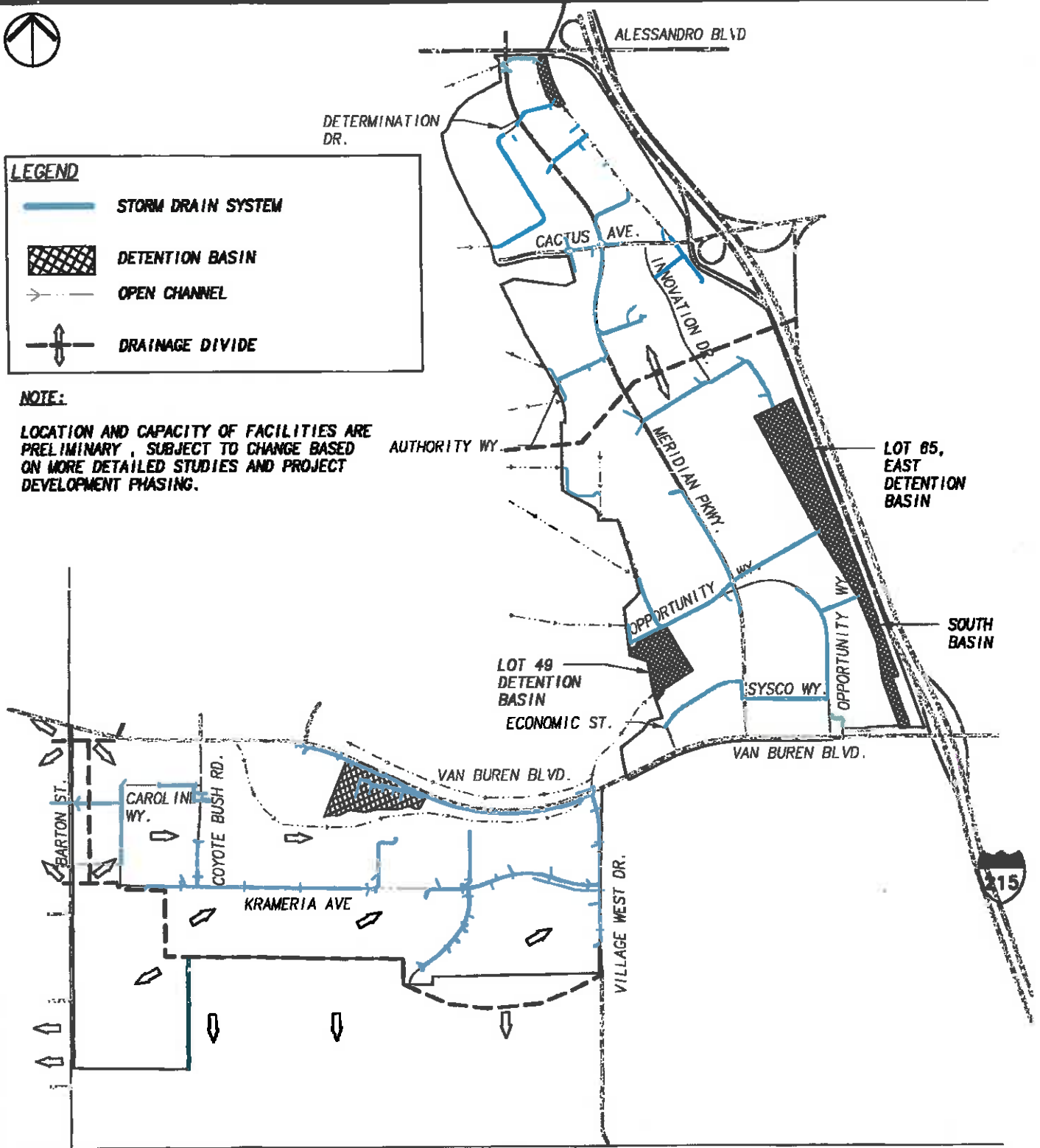


FIGURE VI-4

STORM DRAIN SYSTEM

F. Gas and Electrical Services

The March Business Center planning area will have its own electrical distribution system. A "Municipal Utilities District" has been formed to provide electrical power to the site. This District's members include the Cities of Riverside, Moreno Valley and Perris. (Riverside County is prohibited by state law from participating). This district is working to procure electric power directly from the City of Riverside, which has distribution facilities adjacent to March Business Center. Southern California Gas Company provides natural gas. A 10-inch transmission main located west of I-215 traverses the planning area, and the Master Developer will install distribution from this line to individual building lots at the time that other land improvements and utilities are constructed.

G. Solid Waste

When MAFB was an active duty military base, it generated 13.1 tons per day of solid waste. When fully built out, the entire General Plan will generate about 49.1 tons per day. Solid waste in western Riverside County is disposed of at the El Sobrante, Lambs Canyon, and Badiands landfills. In order to reduce the amount of material generated by the Specific Plan, the March Business Center will comply with the requirements of the County of Riverside's Source Reduction and Recycling Element (SRRE).

H. Grading

A conceptual grading design will be required for each Tentative Map application consistent with the Development Code. Grading designs will implement the goals and policies of the March JPA General Plan.

Grading Plan Development Standards

- All grading activities shall be in substantial conformance with the approved tentative map or development permit and shall implement any grading-related mitigation measures outlined in the accompanying EIR for the March Business Center.
- Prior to any development within any area of the Specific Plan, an overall grading plan for the portion in process shall be submitted for approval by the JPA. The grading plan for each area shall be used as a guideline for subsequent grading plans for individual stages of development.
- All streets shall have a gradient not exceeding use minimums and maximums established by the County of Riverside or as approved by the March JPA
- A precise grading plan shall be prepared prior to any on-site grading for individual projects.
- The project developer/applicant shall be responsible for installation and maintenance of all planting and irrigation systems on manufactured slopes until those responsibilities are assumed by the Landscape Maintenance District or other parties.
- To the extent that it is feasible, the overall shape, height, and gradient of any cut and fill slope shall be designed to be consistent with the existing natural contours and scale of the natural terrain.
- Potential brow ditches, terrace drains, or other minor swales, determined necessary at future stages of project review, shall be concealed, as feasible and possible, with landscape plantings, earth berms, and similar features.

- Graded but undeveloped pads shall be maintained weed-free, appropriate erosion control measures within ninety (90) days of completion of grading, unless building permits are obtained from the JPA. Appropriate desiltation basins are required for graded areas.
- Cut and fill slopes shall be constructed at inclinations of no steeper than two horizontal feet to one vertical foot, unless otherwise approved by the March JPA. Variable slope ratios will be used to avoid abrupt changes from the pads to the slopes.
- All newly created slopes exceeding 10 feet in vertical height shall be landscaped with a permanent irrigation system approved by the JPA prior to final acceptance. Landscaping shall be consistent with the Landscape Concept Plan and the March Business Center Design Guidelines.
- Grading shall not be permitted to commence prior to approval of grading permits for any proposed development. Mass grading will only occur for those areas undergoing development, or for those areas specifically identified as borrow or disposal sites.
- Grading operations within the confines of the Specific Plan Area shall conform to all applicable March JPA Development Code standards.
- Project grading design shall make reasonable efforts to balance cut and fill on site to avoid the need for excessive importing or exporting of soil.

Manufactured slopes greater than 10 feet in vertical height, together with landscaping and irrigation systems, will be maintained by a maintenance assessment district or owner's association. These slope areas will be entirely within a separate lot or easement. Irrigation systems maintained by the assessment district or owner's association will be separate from private systems. All slopes less than 10 feet in vertical height will be maintained by each project consistent with the March JPA Development Code.

VII. IMPLEMENTATION

The procedures for filing applications for permits, variances, appeals, amendments, and approvals shall be in accordance with the Development Code unless otherwise defined in this Specific Plan.

A. Phasing Plan

The project will be constructed in planning phases, as illustrated in Figure I-3 on page I-4 of this Specific Plan. As shown in this figure, the first development phase encompasses the North Campus area between Alessandro Boulevard and the to-be-demolished military family housing development, known as Arnold Heights. Phase 2 includes the area occupied by the to-be-demolished Arnold Heights development. Phase 3 includes the South Campus. Each planning area may be implemented in smaller "sub-phases," with one or more Final Maps.

B. Financing Strategies

(a) Financing of Public Facilities

(1) Purpose and Intent

The public health, safety, and welfare require that employees in newly developing areas be adequately served with access, parks, open space, fire and police protection, and other public facilities concurrent with the need.

(2) Financial Responsibility

All necessary public facilities shall be assured by the project developer, either directly or by other means such as a charge against the area within the Specific Plan that benefits from the public facility.

(3) Financial Programs for Municipal Facilities

The Municipal Facilities required for the March Business Center may be financed through any of these programs, either individually or in combination:

(A) Facilities Benefit Assessment or Development Impact Fee.

- (i) For facilities that already exist outside of the March Business Center Specific Plan, but which require additions or expansions to existing facilities to meet the requirements of the Specific Plan: i.e., police and public works facilities.**

- (ii) For new facilities necessitated by the March Business Center: i.e., fire station and transportation facilities.
- (iii) For facilities which extend beyond the limit of March Business Center, whose service area is also greater than the March Business Center and the need for which is not solely created by the March Business Center Specific Plan: costs for improvements so constructed may be partially offset by reimbursements from development in those service areas greater than the March Business Center.
- (iv) For facilities within or without the community which are intended for the use of residents such as: street scene improvements (landscaping of the medians and right-of-way along major streets), traffic signals at the intersection of major streets, and other transportation facilities.

(B) Improvement District

An Improvement District under the provisions of State Law or local procedural ordinance may be created to create assessments against the land to generate funds to finance facilities which are related to each individual planned district area by amount of benefit received. The facilities to be provided by this improvement district may include, but not be limited to; major perimeter streets; transit improvements, both municipal and other public utilities and drainage facilities contained therein. The boundary of each improvement district will be the centerline of the bordering perimeter streets, or other applicable limit, of each individual development plan area within the March Business Center as the JPA shall determine.

(C) On-Site Municipal Improvements

The on-site municipal facilities, those within the individual project and not provided by (i) or (ii) above, such as streets, storm drains, and sewer, water, gas, power, and telephone utilities, will be provided by the subdivider under the conventional bonded subdivision agreement.

(D) Off-Site Municipal Improvements

The off-site municipal improvements are those outside of an individual development plan area at the time of its development and not provided under the conventional subdivision process for off-site improvements. The off-site improvements so constructed may be subject to a reimbursement agreement between the persons who constructed the improvements and the March JPA. Reimbursement pursuant to that agreement will be generated by the subdivider(s) of the subsequent development plan areas, where adjacent, and will be paid to the appropriate subdivider(s) as and when such funds are generated within the subareas covered by the reimbursement agreement.

(E) Any other programs approved by the March JPA Board.

(F) Implementation

No final subdivision map for the development of the property shall be approved by the March JPA Executive Director unless and until the following have been accomplished or are in formation:

- (i) A Capital Improvement Program (CIP) for all public facilities needed to support the project, as required by the March JPA, has been adopted. The CIP may be adopted in phases.
- (ii) There has been established either a Facilities Benefit Assessment or a Development Impact Fee applicable to the property covered by the development plan, or a greater area, and the March JPA has by resolution, set the amount of such Facilities Benefit Assessment or Development Impact Fee. Streets, storm drains, and sewer, water, gas, power, and telephone utilities, will be provided by the subdivider under the conventional bonded subdivision agreement.

(b) Tax Increment Financing

In January 1996, the March JPA established the March Joint Powers Redevelopment Agency. In June 1996, the March Joint Powers Redevelopment Agency prepared a Redevelopment Plan, which established a process and framework for implementation of the redevelopment of the former MAFB. With the adoption of the Redevelopment Plan, the Redevelopment Agency was authorized to finance Redevelopment using various sources. The most important source from a redevelopment perspective is tax increment financing.

Tax increment financing is a redevelopment tool authorized by State statute and used by cities and development authorities, such as the March Joint Powers Redevelopment Agency, to finance certain public redevelopment costs. Projects financed with tax increment financing must serve a public purpose such as redeveloping blighted areas, constructing low- and moderate-income housing, providing employment opportunities and improving the tax base. When a tax increment financing district is established, the tax capacity of the properties located within the district are "frozen." For the district's duration, which varies depending on the type of district, the property taxes resulting from any increase in the tax capacity above the frozen level are available to the Redevelopment Agency to finance public project costs. This financing approach will be a valuable tool for financing public facilities that will serve the March Business Center. Because the properties comprising the Specific Plan have not as yet been subjected to taxation, the tax capacity will be frozen at zero.

After pass-through of property tax receipts that are required to provide adequate levels of police and fire protection and reserves for the maintenance of public roadways, a significant amount of funds will be allocated back to the March Redevelopment Agency. The Agency will be in position to use this ever-increasing annual influx of funding to issue bonds that would provide the funding to build any number of designated projects within the Redevelopment area. These types of bonds (tax-increment secured) are of minimal risk to both the issuers and holders as the annual payment of property taxes by owners/users is the source of bond debt service. Given that properties within the March Redevelopment Area have never previously been on tax rolls, a relatively high percentage of collected taxes will go directly to the Agency.

(c) Grants

The March JPA will apply for any number of federal, state and/or local grants that are available to it. The project area qualifies in many categories to be eligible for grants such as those administered by the federal Economic Development Administration (EDA), the state Infrastructure Bank, California State Transportation Improvement Program (STIP), Community Development Block Grants (CDBG) and many others. The March JPA has retained the services of firms specializing in securing grants.

C. Project Review and Processing

1. March JPA staff and their consultants shall endeavor to review all development applications in an expeditious manner.
2. All proposals for new development shall proceed toward approval through the following process:
 - A. **Pre-application:** A pre-application meeting shall be scheduled with March JPA staff to assure that the use is permitted and that the development requirements are accurately conveyed to the applicant. The applicant shall provide a conceptual site plan at the time of the pre-application meeting.
 - B. **March Business Center Implementation Committee:** Prior to submittal of the project construction plans, the project developer shall prepare a detailed site plan, conceptual landscape plan, and colored building elevations for the review and approval of the March Business Center Implementation Committee. The Committee shall review new development applications for consistency with the March Business Center Design Guidelines. Any project denial may be appealed to the March JPA Commission.
 - C. **Construction Plan Submittal:** Upon approval of the project by the March Business Center Implementation Committee, the project developer shall submit completed construction plans, including a detailed site plan, landscape plan, irrigation plans, grading plans, foundation plans, building elevations, fire suppression plans, electrical plans, plumbing plans, structural plans, civil plans, and other plans, as required by the March JPA.

D. Roles of the JPA Staff and March Business Center Implementation Committee

(a) General Provisions

- (1) The March JPA Planning Manager shall administer the March Business Center Specific Plan. The March JPA Planning Manager shall ensure compliance with the regulations and procedures of this section. The March Business Center Specific Plan as presently adopted or as amended from time to time, shall be used in reviewing any development permit applied for under these regulations. Building permits shall be required as identified in the Uniform Building Code.
- (2) Where not otherwise specified in this Specific Plan, the provisions of the March JPA Development Code apply.

Where there is a conflict between the Development Code and this Specific Plan, this Specific Plan applies.
- (3) The following projects may be approved or denied by the March JPA Planning Manager:

- (A) The proposed use is consistent with the land use designation and text of the March JPA General Plan.
 - (B) The proposal is in compliance with the March Business Center Specific Plan, particularly with respect to the financing of public facilities, permitted uses, and property development regulations.
 - (C) The project does not require any action that requires approval by the March Joint Powers Commission or March Business Center Implementation Committee.
- (4) All other projects shall be approved or denied by the March JPA Planning Commission or the March Joint Powers Commission in accordance with the Specific Plan or Development Code:
- (b) **March Business Center Implementation Committee**
- (1) **March Business Center Implementation Committee**
- (A) It is hereby created a March Business Center Implementation Committee (“the Committee”) which shall be composed of three members. The members shall consist of three at-large members appointed by the March JPA Commission. One of the three at-large members shall serve as the Committee Chair.
 - (B) The at large member shall be specifically qualified by reason of interest, training or experience in land development, landscape, architecture, planning, urban design or other relevant business or profession upon the property values, and development of surrounding areas.
 - (C) The Committee may adopt rules of procedure to supplement those contained within this Specific Plan. Two voting members shall constitute a quorum for the transaction of business and a majority vote; and not less than two affirmative votes shall be necessary to make any Committee decision.
 - (D) The March JPA Planning Manager or his designated representative shall serve as Secretary of the Committee and maintain records of all official actions of the Committee.
 - (E) All Commission Members of the March JPA shall endeavor to cooperate with the Committee and render reasonable assistance to it.
 - (F) The Committee shall render a report annually on March 31, or on request, to the March JPA Executive Director.
- (2) **Powers and Duties**

It shall be the duty of the Committee to review Development Review Applications and comment on development plans, the Capital Improvement Plan implementation and on major public improvements. The Committee shall approve or deny the design of the site plan, landscape plan and building elevations. Appeals of committee denials shall be placed on the next available March JPA Commission agenda for final determination. The Committee shall submit its recommendations or comments on other items to the March JPA Executive Director. The Committee shall also recommend to the March JPA Executive Director any changes to the regulations, provided such changes are necessary for the proper execution of the adopted plan. The Committee may also consider items of broader scope that may affect the March Business Center Specific Plan and, when

appropriate, offer its recommendations on these matters to the March JPA Executive Director.

E. Maintenance

The March JPA will create a landscape maintenance district to accommodate the maintenance of portions of the open space and landscaped areas within the March Business Center. The district would be formed per the Landscape and Lighting Act of 1972 which allows for properties to be assessed via property tax billing for allocated maintenance items. The Master Developer, with assistance from the March JPA staff, will be responsible for the formation and initial funding of the district, which will be approved and adopted by resolution by Riverside County. The district will be used exclusively for the ongoing maintenance of the improvements described below. The district will not be for the capital expenditures and/or construction of the improvements. The improvements will be constructed by the Master Developer or future developers as outlined in a particular subdivision's conditions of approval or otherwise mandated by approving jurisdiction(s). Subdivision conditions of approval will require that all projects within the Specific Plan will be subject to the district, as well as all subsequent subdivisions/phases of the March Business Center.

The district's maintenance responsibilities and budgeted items will include, but not be limited to, the following:

- Street lighting electricity, maintenance and replacement;
- Landscape maintenance of the parkways, medians within all public streets, and landscape easements shall be maintained by the landscape and lighting district.
- Irrigation water for the above;
- Tree trimming and/or color replacement;
- Maintenance of drainage structures not maintained by RCFCD or Riverside County;
- Project entry monumentation, maintenance and repairs;
- Any insurance requirements;
- Slope maintenance (over 10 feet);
- Graffiti control;
- Traffic signal maintenance;
- District management fee.

The district's engineer will prepare the annual budget for the district as well as determine budget amounts for new properties entering the district. All other terms of the district will follow those dictated by the Act, including the methodology of interfacing with the Riverside County Tax Collector/auditor and other agencies involved in the collection and disbursement of the assessments.

The maintenance of major regional infrastructure components will be as follows:

- Water, sewer, reclaimed water: Western Municipal Water District
- Drainage facilities: Riverside County Flood Control & Water Conservation District
- Public Streets: County of Riverside

Other facilities will be maintained as described in the table below.

Table VII-1 summarizes the funding source for the initial capital expenditure and the responsibility for ongoing operations and maintenance for services and infrastructure to be provided for the March Business Center.

| TABLE VII-1 INFRASTRUCTURE/SERVICE FUNDING RESPONSIBILITY | | |
|--|------------------------------------|---|
| Service /Infrastructure | Capital Expense ¹¹ | Operations and Maintenance |
| Street Lighting | Master Developer | Landscape and Lighting Maintenance District |
| Fire Protection | Impact Fee collected in permitting | Property tax |
| Reclaimed Water Delivery System | Master Developer | <u>Distribution system:</u> Western Municipal Water District <u>Irrigation system:</u> Landscape and Lighting Maintenance District |
| Police | Impact Fee collected in permitting | Contract with County Sheriff by JPA |
| Storm Drains | Master Developer | <u>Public lines:</u> Riverside County Flood Control District (36"+); Riverside County – Public Lines ≤ 36" <u>Private line:</u> Landscape and Lighting District or Owners of Individual Lots |
| Detention Basins | Master Developer | <u>Large basin near I-215/Van Buren:</u> Riverside County Flood Control District <u>Other Basins:</u> Landscape and Lighting Maintenance District |
| Traffic Signals/Streets, sidewalk, curb and gutter | Master Developer | March JPA/County ¹² |
| Bus Stops | Master Developer | RTA |
| Bus Shelters | Riverside Transit Authority | RTA |
| Landscaping | Master Developer | <u>Public R/W "letter lots" and landscaping easements:</u> Landscape and Lighting Maintenance District <u>Other Landscaping:</u> Owner of Individual Lots |

F. Specific Plan Amendments

Specific Plan amendments will be subject to the Major Project Development Review Process, as identified in the March JPA Development Code.

¹¹ Capital commitments as noted does not preclude funding from other sources, such as Transportation Impact Fees, grants, or state and federal transportation funding programs.

¹² Transportation facilities will be conveyed to Riverside County. The County will be responsible for O&M after they have accepted the transportation facilities.

VIII. Consistency with the General Plan

A. Overview

The March Business Center Specific Plan is based upon the goals and policies set forth in the March JPA General Plan. This section addresses the conformance of the March Business Center Specific Plan to the General Plan on a general or conceptual basis.

B. General Plan Elements

1. Land Use

Goal: *Land Use Plan provides for a balanced mix of land uses that contribute to the regional setting, can capitalize on the assets of the Planning Area, while insuring compatibility throughout the Planning Area and with regional plans.*

Consistency: Development of March Business Center will occur in a logical pattern of growth, compatible with adjacent land uses and regional plans. The project will provide a large employment center in a portion of the County that is largely residential. This will improve the balance of population and employment in the project vicinity, providing an opportunity for residents to work locally, rather than commute to surrounding areas throughout the region.

Goal: *Locate land uses to minimize land use conflict or creating competing land uses, and achieve maximum land use compatibility while improving or maintaining the desired integrity of the Planning Area and subregion.*

Consistency: The land use summary in Table III-1 provides a mixture of compatible land uses that may be developed in the Specific Plan area. Incompatible or competing land uses will not be allowed in the Specific Plan area.

Goal: *Manage growth and development to avoid adverse environmental and fiscal effects.*

Consistency: Development of the project will be phased to the assurance of required infrastructure and services. The Specific Plan accommodates a number of financing strategies to fund public improvements.

Goal: *Develop an identity and foster quality development within the Planning Area.*

Consistency: The March Business Center Design Guidelines establishes architectural, signage, parking, and landscaping standards that will achieve the goals of both project identity and quality development.

Goal: *Maximize and enhance the tax base and generation of jobs through new, reuse and joint use opportunities.*

Consistency: The proposed land uses will be a major employment center. As such, it will provide a substantial enhancement to the tax base.

Goal: *Support the continued Military Mission of March Air Reserve Base, and preservation of the airfield from incompatible land use encroachment.*

Consistency: The project is designed to incorporate appropriate uses within the development-limited areas as defined in the Air Installation Compatible Use Zone (AICUZ) Study done in 1998.

Goal: *Maximize the development potential as a regional transportation facility to support passenger service.*

Consistency: The March Business Center will accommodate a transportation center for inter-city passenger rail service.

Goal: *Preserve the natural beauty, minimize degradation of the March JPA Planning Area, and provide enhancement of environmental resources and scenic vistas.*

Consistency: The project proposes the establishment of open space along the riparian corridor within the South Campus.

Goal: *Preserve the integrity of the historic and cultural resources of the Planning Area and provide for their enhancement.*

Consistency: The project area does not impact significant historic or cultural resources.

Goal: *Avoid undue burdening of infrastructure, public facilities, and services by requiring new development to contribute to the improvement and development of the March JPA Planning Area.*

Consistency: This Specific Plan identifies a number of financing strategies, including tax increment financing, to pay for needed public facilities.

Goal: *Plan for the location of convenient and adequate public services to serve the existing and future development of March JPA Planning Area.*

Consistency: All public facility connections are located adjacent to the site, and adequate capacity has been deemed available by the responsive agencies. Service facility letters were obtained from these agencies and their comments/recommendation have been incorporated into the project accordingly.

Goal: *Ensure, plan, and provide adequate infrastructure for all facility reuse and new development, including but not limited to, integrated infrastructure planning, financing and implementation.*

Consistency: Development of the project will be phased to the assurance of required infrastructure and services. This Specific Plan identifies a number of financing strategies, including tax increment financing, to pay for needed public facilities.

Goal: *Secure adequate water supply system capable of meeting normal and emergency demands for existing and future land uses.*

Consistency: As described in Section VI, the water supply system will have sufficient capacity to accommodate projected normal and emergency needs.

Goal: *Establish, extend, maintain and finance a safe and efficient wastewater collection, treatment and disposal system, which maximizes treatment and water recharges, minimizes water use, and prevents groundwater contamination.*

Consistency: As described in Section VI, the March Business Center will provide the necessary conveyance and treatment facilities to achieve this goal.

Goal: *In compliance with state law, ensure solid waste collection, siting and construction of transfer and/or disposal facilities, operation of waste reduction and recycling programs, and household hazardous waste disposal programs and education are consistent with the County Solid Waste Management Plan.*

Consistency: the March Business Center will comply with the requirements of the County of Riverside's Source Reduction and Recycling Element (SRRE).

Goal: *Adequate supplies of natural gas and electricity from utility purveyors and the availability of communications services shall be provided within the March JPA Planning Area.*

Consistency: All public facility connections are located adjacent to the site, and adequate capacity has been deemed available by the responsive agencies. Service facility letters were obtained from these agencies and their comments/recommendation have been incorporated into the project accordingly.

Goal: *Adequate flood control facilities shall be provided prior to, and concurrent with, development in order to protect the lives and property within the March JPA Planning Area.*

Consistency: As discussed in Section VI, the Specific Plan will provide drainage facilities to achieve this goal.

2. Transportation

Goal: *Establish and provide for a comprehensive transportation system that captures the assets and opportunities of the planning area, existing transportation facilities, and planned transportation facilities for the future growth and development of the planning area and sub-region.*

Consistency: Where feasible, existing transportation facilities, such as the Burlington Northern Santa Fe (BNSF) rail line, are incorporated into the Specific Plan transportation network.

Goal: *Build and maintain a transportation system which capitalizes on the multi-faceted elements of transportation planning and systems, designed to meet the needs of the planning area, while minimizing negative effects on air quality, the environment and adjacent land uses and jurisdictions.*

Consistency: The March Business Center will accommodate a transportation center, local transit service, bicycle lanes, and pedestrian facilities. A Transportation Demand Management (TDM) plan will be implemented to limit peak hour traffic impacts.

Goal: *Develop a transportation system that is safe, convenient, efficient and provides adequate capacity to meet local and regional demands.*

Consistency: The March Business Center will construct an internal street network and provide transportation capacity improvements to existing facilities off-site based on future demand. Transportation improvements will be constructed in phases based on March Business Center development and projected background traffic growth.

Goal: *Provide a balanced transportation system that ensures the safe and efficient movement of people and goods throughout the planning area, while minimizing the use of land for transportation facilities.*

Consistency: Project internal streets are sized to accommodate projected future traffic in an efficient manner.

Goal: *Plan and encourage land use patterns and designs, which enhance opportunities for non-vehicular circulation and improve trip reduction strategies.*

Consistency: Site plans for individual buildings shall be reviewed to ensure that pedestrian, bicycle and transit access is facilitated. A bicycle and pedestrian circulation network is provided.

Goal: *Establish vehicular access control policies in order to maintain and insure the effectiveness and capacity of arterial roadways.*

Consistency: Project internal roadways will be designed in accordance with the "County Road Improvement Standards and Specifications," published by the County of Riverside, and take into account additional landscaping requirements established in the Riverside County Integrated Plan County standards limit intersection intervals on arterial roadways.

Goal: *Facilitate and develop transportation demand management and transportation systems management programs, and use of alternate transportation modes.*

Consistency: Transportation Demand Management (TDM) strategies will be implemented to shift trips outside the standard commuting hours and/or to non-"drive alone" modes of travel. This is accomplished through various employer-initiated measures, such as flexible working hours, encouragement of carpooling, and facilitating access for non-motorized (i.e., bicycling or walking) modes of travel.

Goal: *Adequate, affordable, equitably distributed and energy efficient public and mass transit services which promote the mobility to, from, and within the planning area shall be provided.*

Consistency: The project will be designed to accommodate both local transit service and inter-city passenger rail service. The local transit system of bus stops and bus shelters will be approved by the Riverside Transit Agency (RTA).

Goal: *Develop measures which will reduce the number of vehicle-miles traveled during peak travel periods.*

Consistency: The March Business Center improve jobs/housing balance in western Riverside County by providing a large employment center in an area that is largely residential. This will provide an opportunity for residents to work locally, rather than commute to Los Angeles or Orange Counties. Jobs/housing balance will help reduce vehicle miles of travel.

Goal: *Regulate the travel of trucks on March JPA Planning Area streets.*

Consistency: The project is designed to accommodate truck traffic. In addition, trucks will be required to travel on designated routes as they traverse the March Business Center internal streets.

Goal: *Adequate off-street parking for all land uses shall be provided which requires adequate on-site parking to prevent spill over on the adjacent street system.*

Consistency: This Specific Plan provides parking ratios that will limit the potential for parking spillover.

Goal: *Plan for and seek to establish an area-wide system of bicycling trails, with linkages within the planning area and with adjacent jurisdictions, and in compliance with sub-regional plans.*

Consistency: The project will include bicycle and pedestrian linkages as defined in Section V of this Specific Plan. The network will consist of Multi-Use Bicycle paths and Bike Lanes.

Goal: *Goods movement through the San Jacinto Rail Branchline shall be capitalized.*

Consistency: The project includes the existing BNSF railway line is incorporated into the project design.

Goal: *In accordance with state and federal law, promote and provide mobility for the disabled.*

Consistency: Development plans and public improvement plans shall take into account the accessibility requirements of the Americans with Disability Act (ADA).

3. Noise/Air Quality

Goal: *Ensure that land uses are protected from excessive and unwanted noise.*

Consistency: Project development shall be consistent with the land use limitations established in the AICUZ study and the Riverside County Airport Land Use Plan.

Goal: *Minimize incompatible noise level exposures throughout the Planning Area, and where possible, mitigate the effect of noise incompatibilities to provide a safe and healthy environment.*

Consistency: (see above)

Goal: *Work toward the reduction of noise impacts from vehicular traffic, and aviation and rail operations.*

Consistency: The project shall implement the noise related mitigation established in MEIR.

Goal: *Promote alternative modes of travel.*

Consistency: The March Business Center will accommodate a transportation center, local transit service, bicycle lanes, and pedestrian facilities. A Transportation Demand Management (TDM) plan will be implemented to limit peak hour traffic impacts.

Goal: *Reduce emissions associated with vehicle miles traveled by enhancing the jobs/housing balance of the subregion of western Riverside County.*

Consistency: The March Business Center improve jobs/housing balance in western Riverside County by providing a large employment center in an area that is largely residential. This will provide an opportunity for residents to work locally, rather than commute to Los Angeles or Orange Counties. Jobs/housing balance will help reduce vehicle miles of travel, resulting in reduced emissions.

Goal: *Reduce air pollution through proper land use, transportation and energy use planning.*

Consistency: The March Business Center Specific Plan will provide access using a variety of transportation modes, including highways, local bus service, inter-city rail, bicycles, and pedestrians. The project's Transportation Demand Management (TDM) strategy will accommodate the shift of some trips from "drive-alone" to transit or non-motorized modes of travel.

Goal: *Pursue reduced emissions for stationary and mobile sources through the use and implementation of new and advancing technologies.*

Consistency: Where feasible and appropriate, development of March Business Center shall accommodate the use of advancing technologies, such as alternate fueled vehicles and other innovations that would provide air quality benefits.

Goal: *Maximize the effectiveness of air quality control programs through coordination with other governmental entities.*

Consistency: Development in March Business Center will comply with the policies outlined in Air Quality Goal 5 of the March JPA General Plan.

Goal: *Reduce emissions associated with vehicle/engine use.*

Consistency: The March Business Center improve jobs/housing balance in western Riverside County by providing a large employment center in an area that is largely residential. This will provide an opportunity for residents to work locally, rather than commute to Los Angeles or Orange Counties. Jobs/housing balance will help reduce vehicle miles of travel.

Goal: *Reduce emissions associated with energy consumption.*

Consistency: Development in March Business Center will comply with the policies outlined in Air Quality Goal 7.

Goal: *Reduce air pollution emissions and impacts through siting and building design.*

Consistency: Development in March Business Center will comply with the policies outlined in Air Quality Goal 8.

Goal: *Reduce fugitive dust and particulate matter emissions.*

Consistency: Development in March Business Center will comply with the policies outlined in Air Quality Goal 9.

4. Housing

The General Plan does not allow for housing opportunities within the March JPA Planning Area due to incompatible uses with the airfield, the need to focus on the reestablishment of the numerous jobs lost due to base realignment, and the housing rich environment of Western Riverside County. The March Business Center Specific Plan maintains consistency with the General Plan's absence of a residential land use designation within the Planning Area.

3. Resource Management

Goal: *Conserve and protect surface water, groundwater, and imported water resources.*

Consistency: The project will be constructed to minimize impacts to the existing drainage channels. The landscape plan includes drought tolerant plant materials. Irrigation will be moisture sensitive to limit irrigation during times of heavy rains.

Goal: *Control flooding to reduce major losses of life and property.*

Consistency: The March Business Center Specific Plan will provide a number of drainage facilities, including culverts, open channels, and retention basins, to control flooding.

Goal: *Conserve and protect significant land forms, important watershed areas, mineral resources and soil conditions.*

Consistency: The MEIR and the Focused EIR have been prepared to assess and, if appropriate, mitigate project impacts on geology, soils, and hydrology.

Goal: *Conserve energy resources through use of available energy technology and conservation practices.*

Consistency: As appropriate, the March Business Center shall comply with applicable regulations relating to energy conservation.

Goal: *Conserve and protect significant stands of mature trees, native vegetation, and habitat within the planning area.*

Consistency: The project will protect and preserve areas of riparian habitat. This preservation area will include associated drainage channels and wetlands.

Goal: *Provide an effective and efficient waste management system for solid and hazardous wastes that is financially and environmentally responsible.*

Consistency: The March Business Center shall comply with appropriate and applicable regulations and standards with respect to the management of solid and hazardous wastes.

Goal: *Promote cultural awareness through preservation of the planning area's historic, archaeological and paleontological resources.*

Consistency: The project area does not impact significant historic, archaeological or paleontological resources.

Goal: *Develop and maintain recreational facilities as economically feasible, and that meet the needs of the community for recreational activities, relaxation and social interaction.*

Consistency: The project will include a 52-acre passive park facility to be utilized by employees of the business center.

Goal: *Create a network of open space areas and linkages throughout the Planning Area that serves to preserve natural resources, protect health and safety, contributes to the character of the community, provide active and passive recreational use, as well as visual and physical relief from urban development.*

Consistency: The project abuts the Stephens' kangaroo rat (SKR) management area, which seeks to protect the endangered species. This area encompasses 1,178 acres and is located between residential uses and the project. Within this open space area, a variety of natural resources will be protected, including riparian communities.

Goal: *Establish standards for scenic corridors, trails and vistas that contribute to the quality of the planning area.*

Consistency: The March Business Center will provide landscaped lots adjacent to major arterial roadways and will provide additional landscaping within easements along internal streets adjacent to large industrial lots. A Class I multi-use pedestrian/bicycle trail will be provided along Van Buren Boulevard between Barton Street and I-215.

4. Safety/Risk Management

Goal: *Minimize injury and loss of life, property damage, and other impacts caused by seismic shaking, fault rupture, ground failure, and landslides.*

Consistency: A geological reconnaissance has been conducted for the property. That study revealed that there are no active or inactive faults crossing the property and that the property is suitable for development.

Goal: *Minimize grading and otherwise changing the natural topography, while protecting the public safety and property from geologic hazards.*

Consistency: Grading within the Specific Plan area is designed to minimize impacts to the existing topography. The project will incorporate grading development standards and recommendations, which will minimize any potential geotechnical and site development constraints that occur on-site.

Goal: *Minimize injury, loss of life, property damage, and economic and social disruption caused by flood hazards.*

Consistency: The March Business Center Specific Plan will provide a number of drainage facilities, including culverts, open channels, and retention basins, to control flooding.

Goal: *Reduce threats to public safety and protect property from wildland and urban fire hazards.*

Consistency: As appropriate, the March Business Center shall comply with applicable regulations and guidelines relating to brush management and fire protection services.

Goal: *Reduce the potential for hazardous material exposure or contamination in the Planning Area.*

Consistency: To the extent that it is appropriate, the March Business Center shall comply with regulations and guidelines relating to hazardous material exposure/contamination.

Goal: *Ensure to the fullest extent practical that, in the event of a major disaster, critical structures and facilities remain safe and functional.*

Consistency: To the extent that it is appropriate, the March Business Center shall comply with regulations and guidelines relating to the functionality of critical structures in the event of a major disaster.

Goal: *Reduce the possible risk of upset, injury and loss of life property damage, and other impacts associated with an aviation facility.*

Consistency: The project is designed to incorporate appropriate uses within the development-limited areas as defined in the Air Installation Compatible Use Zone (AICUZ) Study done in 1998. The project will also comply with the Airport Land Use Plan.

Goal: *Plan for emergency response and recovery from natural and urban disasters.*

Consistency: The project shall comply with appropriate and applicable regulations and guidelines relating to emergency response and recovery from natural and urban disasters.

Appendix A Land Use Definitions

The following definitions are intended to provide a general description of each use category. Under each category, example uses are provided. These examples are not all-inclusive, but are intended to provide a sample of uses that would fit in a particular category. Uses not addressed in the Land Use Table (i.e., Table III-1) are prohibited. However, the March JPA Planning Manager has the discretion to make land use interpretations based upon the description of the proposed use and similarities with the listed uses.

INDUSTRIAL

Hazardous Waste Treatment Facility: Activities include the treatment, transfer, storage, resource recovery, disposal, or recycling of hazardous waste.

Bio-Medical Waste Treatment Facility: Activities include the treatment, transfer, storage, disposal or recycling of wastes generated during the diagnosis, treatment or immunization of patients.

Manufacturing - Custom: Activities typically include: manufacturing, processing, assembling, packaging, treatment, or fabrication of custom made products. These types of business establishments do not utilize raw materials for their finished products, but rather, may utilize semi-finished type of manufactured materials for their custom made-to-order products. The finished products from these business establishments are ready for use or consumption and may include incidental on-site display, wholesale and retail sale of the goods produced, not to exceed 25% of the building. Such uses may include: jewelry, household furniture, art objects, apparel products, small instruments (musical, electronic and photographic), stationary, signs, advertising displays, stained glass products, and leather products. The uses do not produce odors, noise, and vibration or particulate that would adversely affect uses in the same structure or on the same site.

Manufacturing - Light: Activities typically include: labor-intensive manufacturing, assembly, fabrication or repair processes which do not involve frequent large container truck traffic or the transport of large scale bulky products. The new products may be finished in the sense that it is ready for use or consumption or it may be semi-finished to become a component for further assembly and packaging. These types of business establishments are customarily directed to the wholesale market, inter-plant transfer rather than the direct sale to the consumer, however, may include incidental on-site display, wholesale and retail sale of the goods produced, not to exceed 25% of the building. Such uses may include: electronic microchip assembly, printing, publishing, candy, confectionery products, canned/bottled soft drinks, bottles water, apparel, paper board containers, boxes, drugs, small fabricated metal products, such as hand tools, general hardware, architectural and ornamental metal; and, toys amusement, sports and athletic goods. The activities do not produce odors, noise vibration, hazardous materials or particulate, which would adversely affect other uses in the structure on the same site.

Manufacturing - Medium: Activities typically include: manufacturing, compounding of materials, processing, assembly, packaging, treatment or fabrication of materials and products which require frequent large container truck traffic or rail traffic, or the transport of heavy, bulky items. The new products are semi-finished to be a component for further manufacturing, fabrication and assembly. These types of business establishments are customarily directed to inter-plant transfer, or to order from industrial uses, rather than for direct sale to the domestic consumer. However, may include incidental on-site display, wholesale and retail sale of the goods products not to exceed 25% of the building. Such uses

may include, but not limited to: canned food, textile products; furniture and fixtures converted paper and paper board product; plastic products made from purchased rubber, plastic or resin; fabricated metal products made from sheet metals; electrical and electronic machinery, equipment and supplies; office, commuting and accounting machines. Activities may produce noise, odors, vibrations and illumination or particulate that affects the persons residing in or conducting business in the vicinity.

Manufacturing - Heavy: Activities typically include: manufacturing, compounding of material, processing, assembly, packaging, treatment or fabrication. Activities in this area may have frequent rail or truck traffic and the transportation of heavy large-scale products. Characteristics of use activities permitted within this area may include massive structures outside of buildings such as cranes, conveyor systems, cooling towers or open-air storage of large quantities of products including, but not limited to forge shops, metal fabricating facilities, open welding shop, lumber woodworking facilities, heavy machine shops, chemical storage and distribution, plastic, plants, light or vacuum casting facilities, vehicular assembly plants, concrete products manufacturing activities, batch plants, air melting foundries and aggregate or asphalt yards.

Mining and Extractive Industries: Activities typically include: prospecting for or exploration of minerals for commercial purposes, surface mining, and excavations or grading.

Newspaper Publishing Plants: Activities typically include the production and distribution of newspapers and related publications.

Parcel Delivery Terminals: Activities typically include sorting, processing, and distribution of parcels to the consumer or to other inter-transfer facilities.

Research and Development: Activities typically include: research, design, analysis and development, and/or testing of a product. Uses typically include testing laboratories, acoustical chambers, wind tunnels, and computer services. Such uses do not promote odors, noise, vibration or particulate that would adversely affect uses in the same structure or on the same site.

Trucking/Transportation Terminals: Activities typically include the temporary storage and transfer of trailers.

Wrecking and Dismantling of Motor Vehicles: Activities typically include: temporary storage and wrecking/dismantling of passenger cars and other vehicles and sales and/or distribution of salvaged parts and other materials.

WHOLESALE, STORAGE AND DISTRIBUTION

Public Storage/Mini-Warehouses: Activities include mini-warehouse or recreational vehicle storage facilities for the rental or lease of small scale enclosed storage units or parking spaces primarily to individuals rather than firms or organizations.

Business Enterprise: Activities typically include: wholesale, storage, and warehousing services and storage and wholesale to retailers from the premises of finished goods and food products. Activities under this classification are typically conducted in enclosed buildings and occupy 50,000 200,000 square

feet or less of divisible building space within the South Campus. May include incidental display and retail sales from the premises, not to exceed 25% of the building.

Wholesale, Storage and Distribution - Medium: Activities typically include: wholesale, storage and warehousing services, including cold storage, moving and storage services, storage and wholesaling to retailers from the premises of finished goods and food products, and distribution facilities for large scale retail firms. Activities under this classification are typically conducted in enclosed buildings and occupy greater than 50,000 square feet of building space.

Wholesale, Storage and Distribution - Heavy: Activities typically include: warehousing, storage, freight handling, shipping, trucking services and terminals; storage and wholesaling from the premises of unfinished, raw or semi-refined products requiring further processing fabrication or manufacturing. Typical uses include, but are not limited to, trucking firms, cold storage, automotive storage or impound yards, and the wholesaling of metals, minerals and agricultural products.

OFFICE

Financial Institutions: Banks, savings and loan associations and similar establishments.

Government Offices: Offices to accommodate administrative and/or operational functions of local, county, state and federal agencies.

Medical Clinics: Activities include medical clinics, family planning, in-patient and out-patient health care, inclusive of hospitals and convalescent homes.

Offices, Business and Professional: Offices or firms or organizations providing professional, executive, management, or administrative services, such as architectural, engineering, real estate, insurance, investment, legal, and medical/dental offices. This classification includes medical/dental laboratories incidental to an office use, but excludes banks and savings and loan associations.

Regional and Corporate Headquarters: Office buildings solely occupied by a single business or entity for the purpose of managing or organizing other, affiliated units.

COMMERCIAL

Agricultural Equipment Repair Shops: Activities typically include: temporary storage and repair and maintenance of agricultural equipment, such as tractors, harvesters, irrigation equipment, etc.

Agricultural/Nursery Supplies and Services: Activities typically include: retail sale from the premises of feed and grain, fertilizers, pesticides, herbicides, and similar goods, feed and grain stores, well drilling, tree services and plant materials and nursery/landscape services.

Alcoholic Beverage Outlets: Activities typically include: retail sale from the premises of beer, wine, and other alcoholic beverages.

Animal Care/Pet Hotels: Activities typically include: provision of animal care treatment, and boarding services of large and small animals, animal clinics, large and small animal hospitals, kennels and catteries.

Assembly and Entertainment: Indoor or outdoor facilities to accommodate concerts and/or civic events. This land use will be restricted in capacity and ancillary services in order to limit potential noise and parking impacts.

Automotive Parts and Accessory Sales: Activities typically include: retail sale from the premises of automobile components, lubricants, specialized tools, and related accessories.

Automotive Fleet Storage: Activities typically include: storage of vehicles used regularly in business operations and not available for sale on-site. Such uses typically include: overnight storage of service vehicles, mobile catering trucks and taxicabs, inclusive of dispatching services.

Automotive Service Stations: Activities typically include: the sale of goods and the provision of service normally required in the day-to-day operation of motor vehicles, including the principal sale of petroleum products, the incidental sale of tires, batteries and replacement items, and the performance of minor repairs, such as tune-up, tire changes, part replacement, oil change and brake work. Activities include incidental convenience, food and beverage sales.

Automotive/Truck Repair – Major: Activities typically include: heavy automobile and truck repair such as transmission and engine repair, the painting of automobile vehicles, automotive body work, and the installation of major accessories.

Automotive/Truck Repair – Minor: Activities typically include: automotive and light truck repair, the retail sale of goods and services for vehicles, and the cleaning and washing of automotive vehicles, brake, muffler and tire shops and automotive drive-through car washes. Heavier automotive repair such as transmission and engine repair are not included.

Building and Site Maintenance Services: Activities include maintenance and custodial services, window cleaning services, disinfecting and exterminating services, pool and landscape services.

Building Contractor's Storage Yards: Activities typically include: offices and storage of equipment materials, and vehicles for contractors who are in trades involving construction activities which include: plumbing, painting, electrical, roofing, carpentry, and other services.

Building Material and Equipment Sales: Activities typically include: retail sale or rental from the premises of goods and equipment, including paint, glass, hardware, fixtures, electrical supplies, rototillers, small trailers and lumber.

Business Supply/Equipment Sales/Rentals: Activities include retail sales, rental or repair from the premises of office equipment, office supplies and similar office goods primarily to firms and other organizations utilizing the goods rather than to individuals. The exclude the sale of materials used in construction industry.

Business Support Services: Activities include services that support the activity of other local businesses, such as clerical, employment, protective, personal services, or minor processing, including blueprint and copying services. Activities not included in this category are the printing of books.

Child Care Facilities: Any childcare facility licensed by the State of California; includes infant care centers, preschools and extended day care facilities. Excludes family day care homes.

Churches and Place of Religious Assembly: Structures and/or assembly areas to be used for worship, related meetings, ministerial residence, and/or religious education.

Communication Facilities, Antennas & Satellite Dishes: Activities typically include: broadcasting and other information relay services accomplished primarily through use of electronic and telephonic mechanisms, inclusive of television and radio studios, telegraph offices, and cable, cellular and telecommunication facilities. The use of antennas, satellite dishes and similar communication facilities shall be regulated pursuant to AICUZ and the Airport Land Use Plan.

Consumer Goods, Furniture, Appliances, Equipment Sales: Typically a freestanding retail store where merchandise is sold to an end user, typically in small quantities.

Convenience Sales: Activities typically include: retail sales from the premises of frequently needed small personal convenience items and professional services that are used frequently. Uses include drug stores, stores selling toiletries, tobacco, and magazines, shoe repair and apparel laundering and dry cleaning.

Energy Generation and Distribution Facilities: Activities typically include: conversion of other forms of energy, such as water power (i.e., hydroelectric), fossil fuels, nuclear power, and solar power, into electrical energy. These facilities typically produce electric energy and provide electricity to transmission systems or to electric power distribution systems.

Equestrian Show and Exhibition Facilities: Activities typically include: training and competition of horses and riders in cross-country equestrian events and the exhibition of participating horses.

Exhibit Halls and Convention Facilities: Temporary display of materials and products associated with a specific trade group, recreational organization, or other affiliation.

Fairgrounds: Large display of agricultural products, such as livestock and produce, to consumers and the general public. Fairgrounds are typically associated with county or state agricultural agencies and are held on an annual basis.

Food and Beverage Sales: Activities include retail sale from the premises of food and beverages for off-premises consumption. Including mini-markets, liquor stores and retail bakeries, catering businesses except chain type grocery stores.

Funeral and Mortuary Services: Activities include services involving the care, preparation, and disposition of human or pet dead, inclusive funeral homes, crematories and mausoleums, inclusive of above ground and in-ground interment.

Golf Courses, Driving Ranges, and Pitch and Putt Courses: Activities typically include: recreational golfing, driving range, short game practice, and tournament competition.

Grocery Store: Activities include retail sales of food products, produce, and household supplies, and may include prepackaged alcoholic beverages as an incidental commodity to the establishment.

Heavy Equipment Sales & Rentals: Activities typically include: the sale or rental from the premises of heavy construction equipment, farm equipment, trucks and aircraft together with maintenance, including aircraft, farm equipment, heavy truck, large boats and heavy construction equipment dealers.

Horticultural Nurseries and Greenhouses: Activities typically include the cultivation of various indoor and outdoor plants for sale to the public.

Hospitals, Intermediate Care Facilities and Nursing Facilities: A hospital is a freestanding institution where the sick or injured are given medical or surgical care. Emergency medical treatment is usually provided.

Hotel/Motel: Activities typically include: lodging services to transient guests on a less-than-monthly basis, other than in the case of uses classified as residential uses, including hotels, motels, boarding houses and resorts.

Instructional Studios: Instructional studios are establishments in which skills including dance, art, and martial arts are taught to individuals or groups. Instructional studios do not include educational facilities.

Interpretive Centers: Interpretive centers are structures or facilities designed to inform and educate the public about the surrounding environment.

Laundry Services: Activities typically include: institutional or commercial linen supply and laundry services, dry cleaning plants, rug cleaning and diaper service laundries.

Maintenance and Repair: Facilities to accomplish the repair and maintenance of non-automotive devices and other appliances.

Major Transmission, Relay or Communications Switching Stations: Telecommunications facilities accommodating fiber optics, Integrated Services Digital Network (ISDN) and digital switching infrastructure.

Museums: Activities typically include the display of items, materials, and media of historical and/or cultural significance.

Bars and Grill: A restaurant or pub where food is predominately sold.

Open Air Markets for the Sale of Agriculture-Related Products and Flowers: Typically informal outdoor facilities to accommodate the sale of agricultural materials to the general public and other buyers.

Outdoor Commercial: Activities typically include: those that produce or may produce a substantial impact upon the surrounding area. Including flea markets, outdoor auction sales or swap meet activities.

Outpatient Medical Clinics: Medical facilities providing limited treatment to patients not requiring an overnight stay.

Parking Facilities as Primary Use: Paved lots to accommodate the temporary storage of passenger cars and other vehicles.

Personal Services: Activities typically include: services of a personal nature, including photography studios and barber/beauty shops.

Petroleum Products Storage: Activities include bulk storage sale, and distribution of gasoline, liquefied petroleum gas, and other petroleum products.

Pets and Pet Supplies: Activities typically include: sale of mammals, fish, reptiles and birds as pets, sales of food, toys and other pet supplies, and related services, such as pet grooming.

Private Clubs, Lodges, and Fraternal Organizations: Private clubs, lodges, and fraternal organizations are associations of persons, whether incorporated or unincorporated, for the promotion of some common social, cultural, educational, religious, or recreational objective. This use does not include *churches* or any group whose primary objective is a business customarily carried on for a profit.

Radio and Television Studios: Activities typically include: production, taping, editing, distribution, and broadcasting of various programs and/or advertisements for radio, television and other media.

Recreational Facilities: Activities include sports performed either indoor or outdoors which require a facility for conducting the recreational activity, such as health clubs, exercise studios or classes, swimming centers, skating rinks, bowling alleys, tennis courts, sports fields, golf courses and amusement parks.

Recycling Facilities: Activities include: drop-off facilities, reverse vending machines, small and large collection facilities, green materials composting facilities, mixed organics composting facilities, and tire processing facilities

Repair Services: Activities include repair services involving articles such as upholstery, furniture and large electrical appliance repair services.

Restaurant (fast food): Activities typically include: the retail sale from the premises of unpackaged food or beverages generally prepared for immediate on-premises or off-site consumption, including restaurants and delicatessens, inclusive of drive-through facilities.

Restaurant (sit down): Activities typically include: the retail sale from the premises of unpackaged food or beverages generally prepared for immediate on-premises consumption, including restaurants and bars and delicatessens, exclusive of drive-through facilities.

Sidewalk Cafes: Eating and drinking establishments with outdoor dining facilities adjacent to public street sidewalks and other pedestrian-oriented areas.

Social Service Institutions: Activities typically include organizing and executing local, regional, and national service and charitable campaigns.

Sundries, Pharmaceutical and Convenience Sales: Freestanding establishment selling food items, beverages, and other items. Sales are typically in small quantities. This use may also provide up to four vehicle fueling spaces.

Swap Meets and Other Large Outdoor Retail Facilities: Activities typically include sales of a range of specialized products to the general public, usually from designated stalls.

Theaters: Activities typically include the performance of plays or music from a stage in an indoor or outdoor venue.

Trade Schools: Activities typically include: information, instruction and similar services, including computer training, driving schools, travel bureaus, photography studios, and vocational and trade schools.

Vehicle, Boat and Trailer Sales: Activities typically include: display, retail sale, leasing, rental of new and used vehicles, boats and trailers, with incidental minor repair, body work, and sale and installation of accessories. Vehicles include automobiles, motorcycles, boats, recreational vehicles and golf carts.

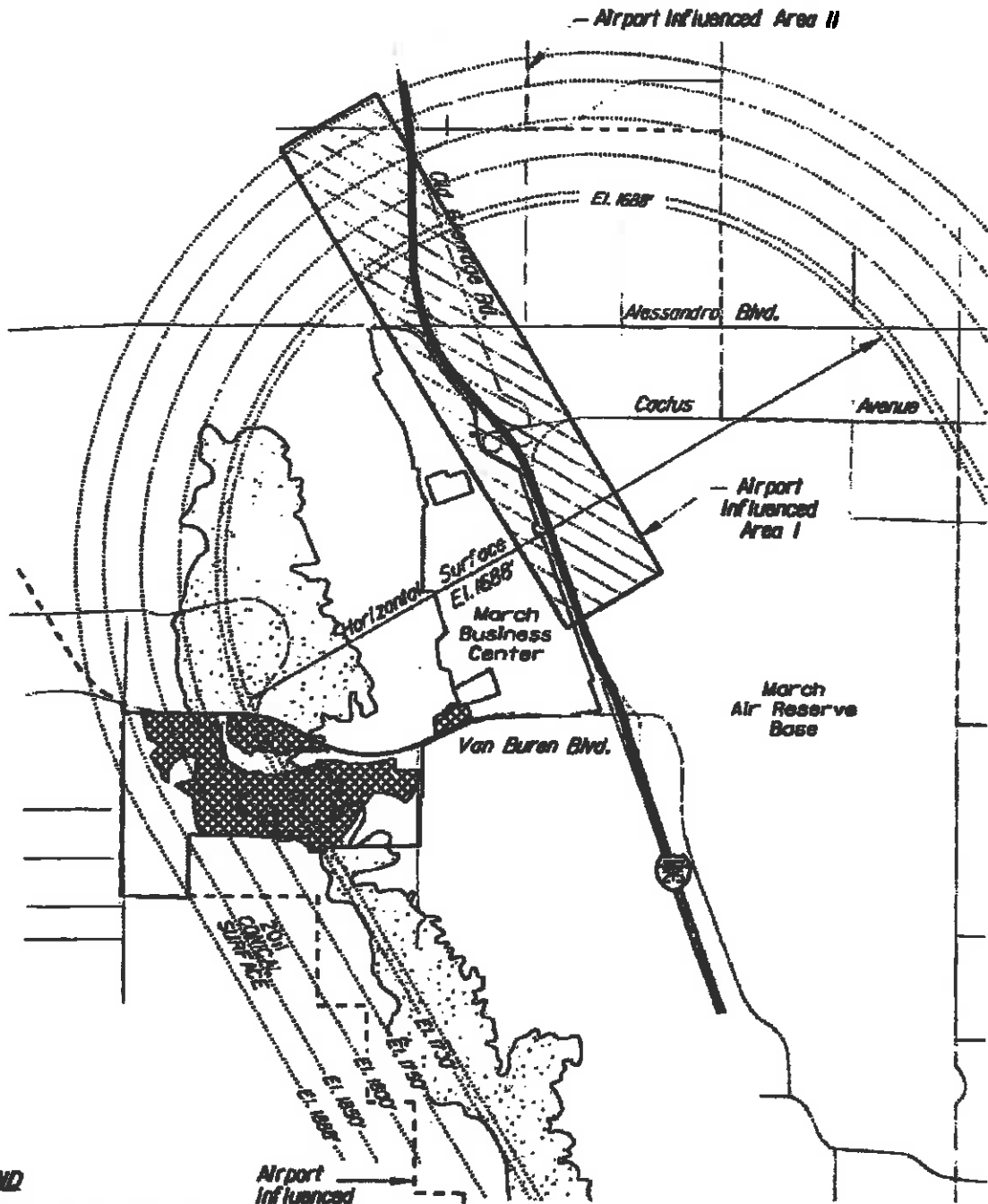
Vehicle Storage: Uses include the storage of operable and inoperative vehicles, including impound yards.

Veterinary Clinics and Animal Hospitals: Activities typically include: provision of routine and emergency medical attention to domestic pets and other animals.





Zoological Parks: Wilderness areas and freestanding facilities designed to house animals that are foreign to the surrounding area. Typical zoological parks also include aquariums, aviaries, and natural wildlife areas.

Appendix B Airport Land Use Plan Exhibits

March Business Center - Specific Plan



LEGEND

-  HEIGHT CAUTION ZONE
-  TERRAIN PENETRATION OF FAR PART 77 SURFACE
-  CONICAL SURFACE ELEVATIONS
-  CONICAL SURFACE CONTOURS

DATE: 1-18-2011 10:00:00 AM

Figure B-1

Land Use Compatibility and FAR Part 77 Surfaces

PAGE BREAK





AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY

November 16, 2020

Ms. Alyssa Berlino, Project Planner
City of Riverside Community and Economic Department – Planning Division
3900 Main Street, 3rd Floor
Riverside CA 92501

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Russell Betts
Desert Hot Springs

VICE CHAIR
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Palm Springs

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Riverside

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Director
Simon A. Houseman

Paul Paul
Barbara Santos

County Administrative Center
4000 Lanon St., 4th Floor
Riverside, CA 92501
(951) 955-5132

www.rcaluc.org

RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW – DIRECTOR’S DETERMINATION

File No.: ZAP1438MA20
Related File No.: P20-0372 (General Plan Amendment), P20-0373 (Rezone), P20-0374 (Conditional Use Permit), P20-0376 (Design Review)
APN: 266-020-022
Airport Zone: Compatibility Zone E

Dear Ms. Berlino:

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to ALUC Resolution No.15-01 (as adopted on August 13, 2015), staff reviewed City of Riverside Case Nos. P20-0372 (General Plan Amendment), P20-0373 (Rezone), a proposal to amend the site’s General Plan land use designation from Very Low Density Residential (VLDR) to Commercial (C), and change its zoning from R-1-1/2 Acre- SP – Single Family Residential and Specific Plan (Orangecrest) Overlay Zones to CR-SP-X-15-S-2 – Commercial Retail, Specific Plan (Orangecrest), Building Stories (Two-Story Maximum) and Building Setback (15 feet from Van Buren Boulevard) Overlay Zones, on 0.87 acres located southerly of Van Buren Boulevard, westerly of Little Court, northerly of Blue Sky Street, and easterly of Alta Cresta Avenue.

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to Policy 1.5.2(d) of the Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan, staff reviewed City of Riverside Case Nos. P20-0374 (Conditional Use Permit), and P20-0376 (Design Review), a proposal to construct a 6,412 square foot commercial building which includes 3,804 square feet of retail area, and 2,608 square foot drive-thru restaurant, on 0.87 acres located southerly of Van Buren Boulevard, westerly of Little Court, northerly of Blue Sky Street, and easterly of Alta Cresta Avenue.

The site is located within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, non-residential intensity is not restricted.

The elevation of Runway 14-32 at March Air Reserve Base/Inland Port Airport is approximately 1,535 feet above mean sea level (AMSL) at its northerly terminus. At a distance of 22,900 feet from the project to the nearest point on the runway, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with an elevation at top of roof exceeding 1,764 feet AMSL. The site’s finished floor elevation is 1,606 feet AMSL,

AIRPORT LAND USE COMMISSION

and the proposed building height is 28 feet, resulting in a top point elevation of 1,634 feet AMSL. Therefore, FAA OES review for height/elevation reasons was not required.

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, provided that the City of Riverside applies the following recommended conditions:

CONDITIONS:

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Hazards to flight.
4. The attached notice shall be provided to all prospective purchasers of the property and tenants of the building.
5. Any proposed detention basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the detention basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the detention basin(s) shall

AIRPORT LAND USE COMMISSION

not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the detention basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at RCALUC.ORG which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin.

If you have any questions, please contact Paul Rull, ALUC Principal Planner, at (951) 955-6893.

Sincerely,

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Simon A. Housman, ALUC Director

Attachments: Notice of Airport in Vicinity

cc: Mike Sadeghian dba VBLIC (applicant/property owner)
Hunt Public Relations, Tom Hunt (representative)
Gary Gosliga, Airport Manager, March Inland Port Airport Authority
Doug Waters, Deputy Base Civil Engineer, March Air Reserve Base
ALUC Case File

Y:\AIRPORT CASE FILES\March\ZAP1438MA20\ZAP1438MA20.LTR - final.doc

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

NOTICE

**THERE IS AN AIRPORT NEARBY.
THIS STORM WATER BASIN IS DESIGNED TO HOLD
STORM WATER FOR ONLY 48 HOURS AND
NOT TO ATTRACT BIRDS**

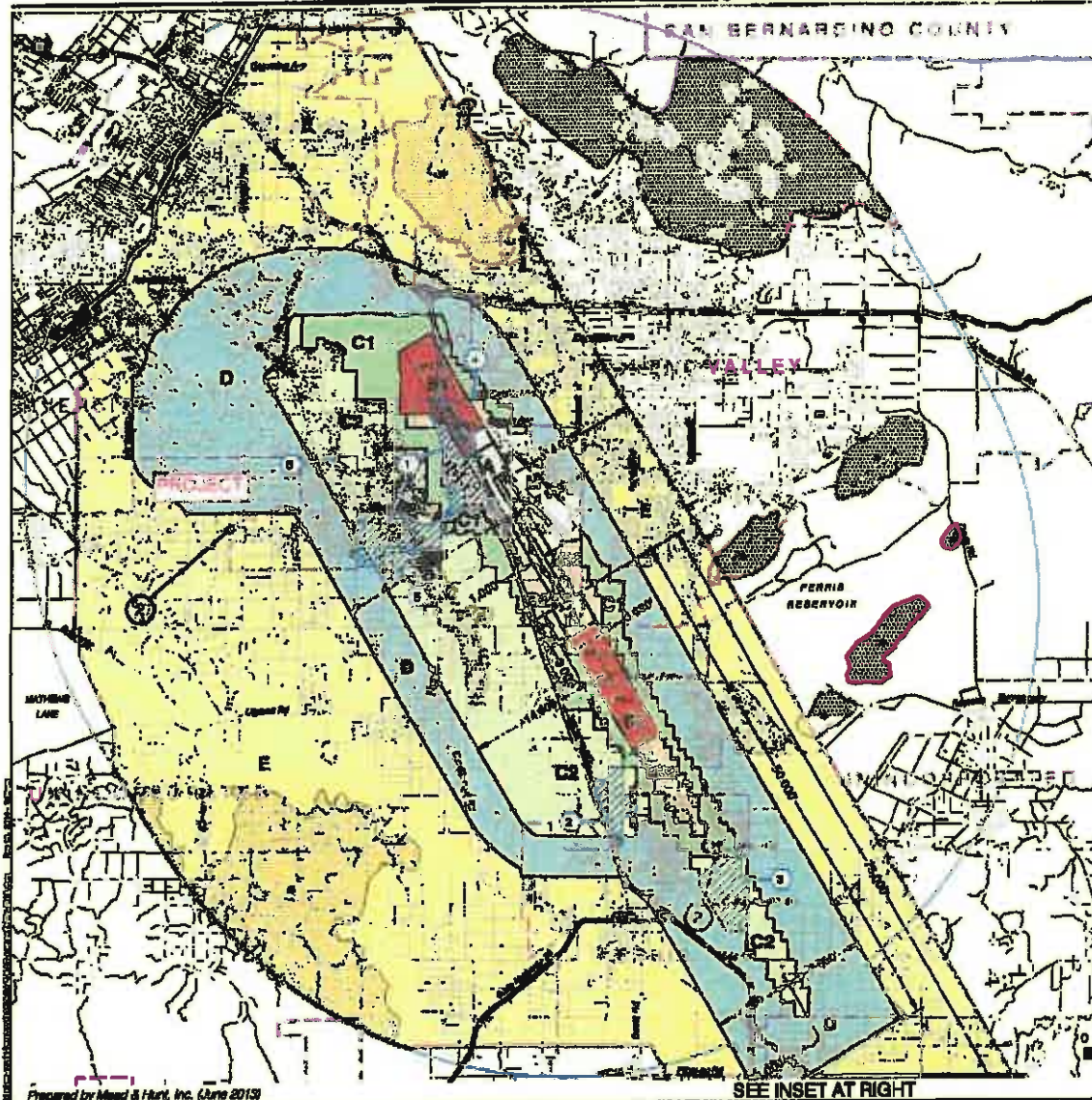
**PROPER MAINTENANCE IS NECESSARY TO AVOID
BIRD STRIKES**



IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:

Name: _____

Phone: _____



LEGEND

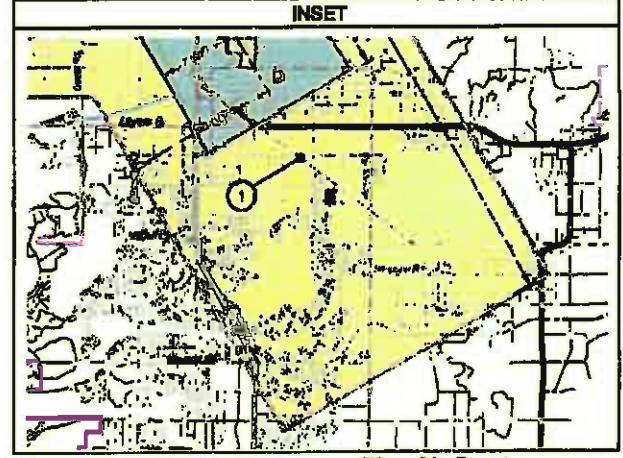
Compatibility Zones

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Obstacle Horizontal Surface Limits
- FAR Part 77 Notification Area

Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- Site-Specific Exceptions (existing local agency commitments to development projects)
- March JPA: March Business Center/Meridian
- Perris: Harvest Landing
- Perris: Park West
- Moreno Valley: Affordable Housing
- March JPA: Ben Clark Training Center
- Riverside: Ridge Crest Subdivision

- ① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway and. Airport Elevation is 1,896 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.



Riverside County
 Airport Land Use Commission
 March Air Reserve Base / Inland Port Airport
 Land Use Compatibility Plan
 (Adopted November 13, 2014)

Note:
 All dimensions are measured from
 runway ends and centerlines.



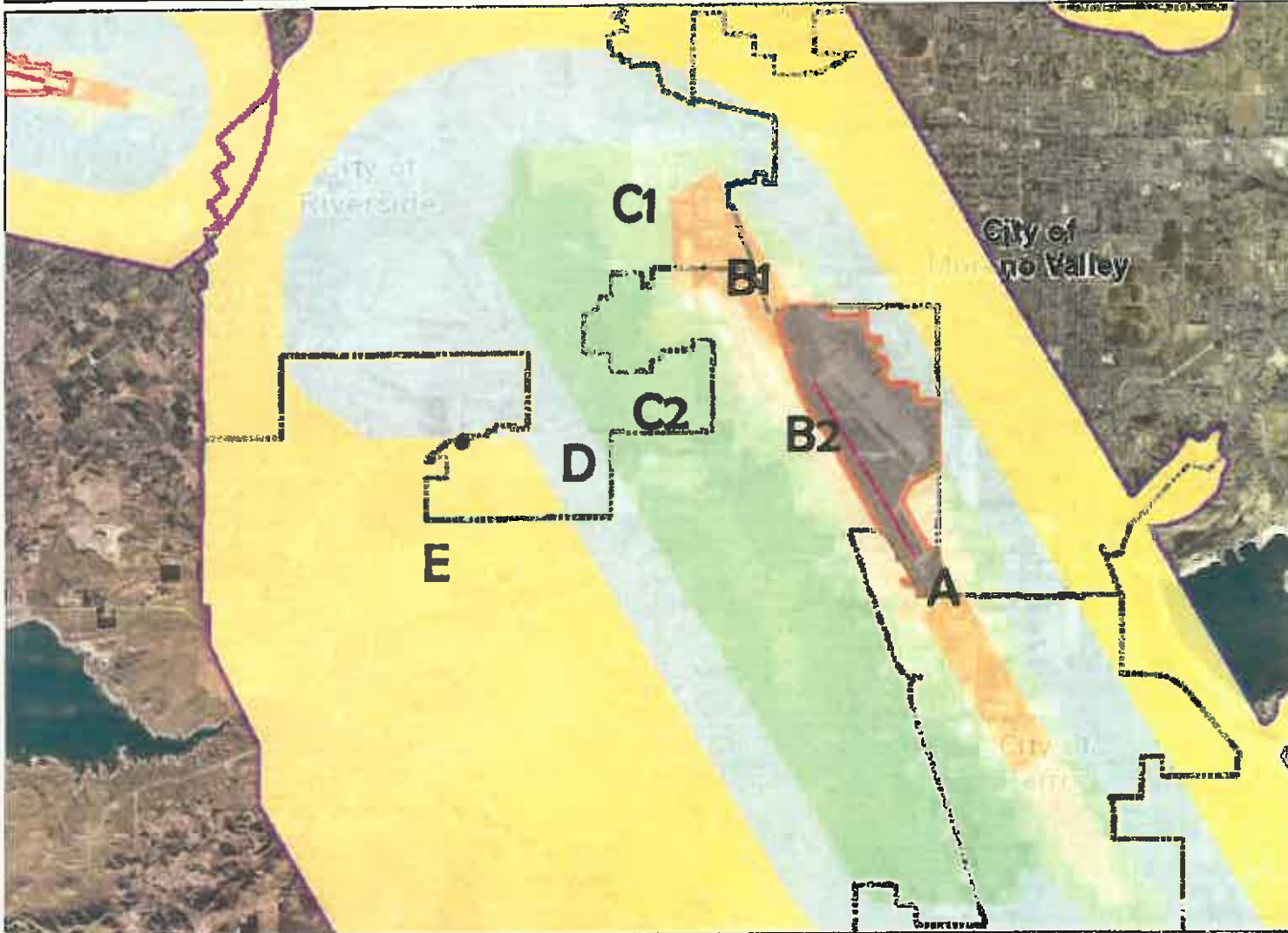
Prepared by Mead & Hunt, Inc. (June 2013)

SEE INSET AT RIGHT

Base map source: County of Riverside 2013

Map MA-1
Compatibility Map
 March Air Reserve Base / Inland Port Airport

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-AFZ I
- B1-AFZ I-EXC1
- B1-AFZ II
- B1-AFZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC8



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

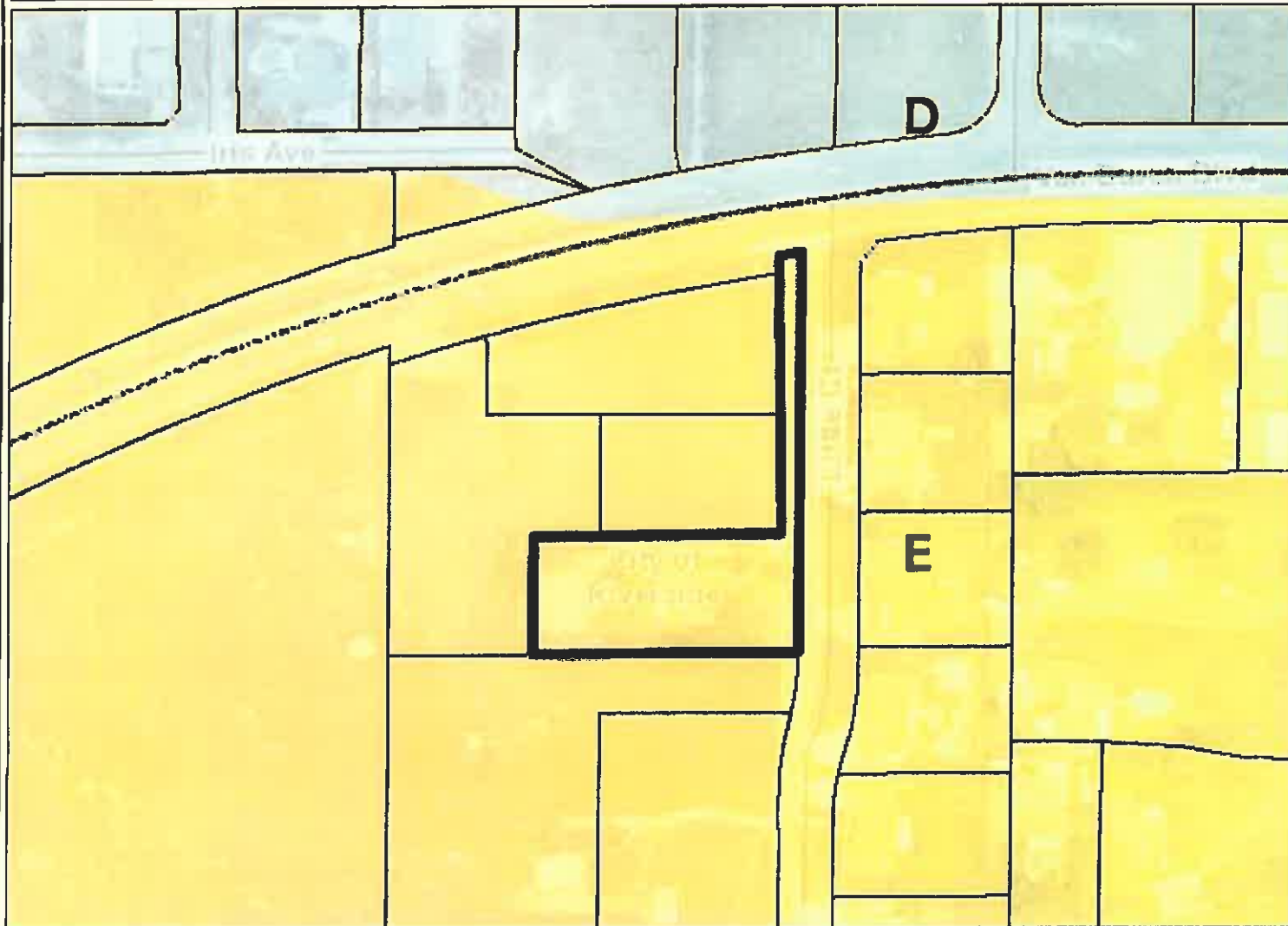
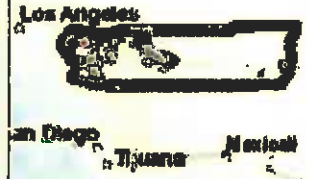
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Notes

Map My County Map



Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5



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Notes

Map My County Map



- ### Legend
- Parcels
 - County Centerline Names
 - County Centerlines
 - Blueline Streams
 - City Areas
 - World Street Map



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Notes

0 188 376 Feet

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Map My County Map



Legend

- Blue line Streams
- City Areas
- World Street Map



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Notes

0 3 6,019 Feet

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Map My County Map



Legend

- Blue line Streams
- City Areas
- World Street Map



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Notes

Map My County Map



Legend

- Parcels
- Blueline Streams
- City Areas
- World Street Map



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0 752 1,505 Feet

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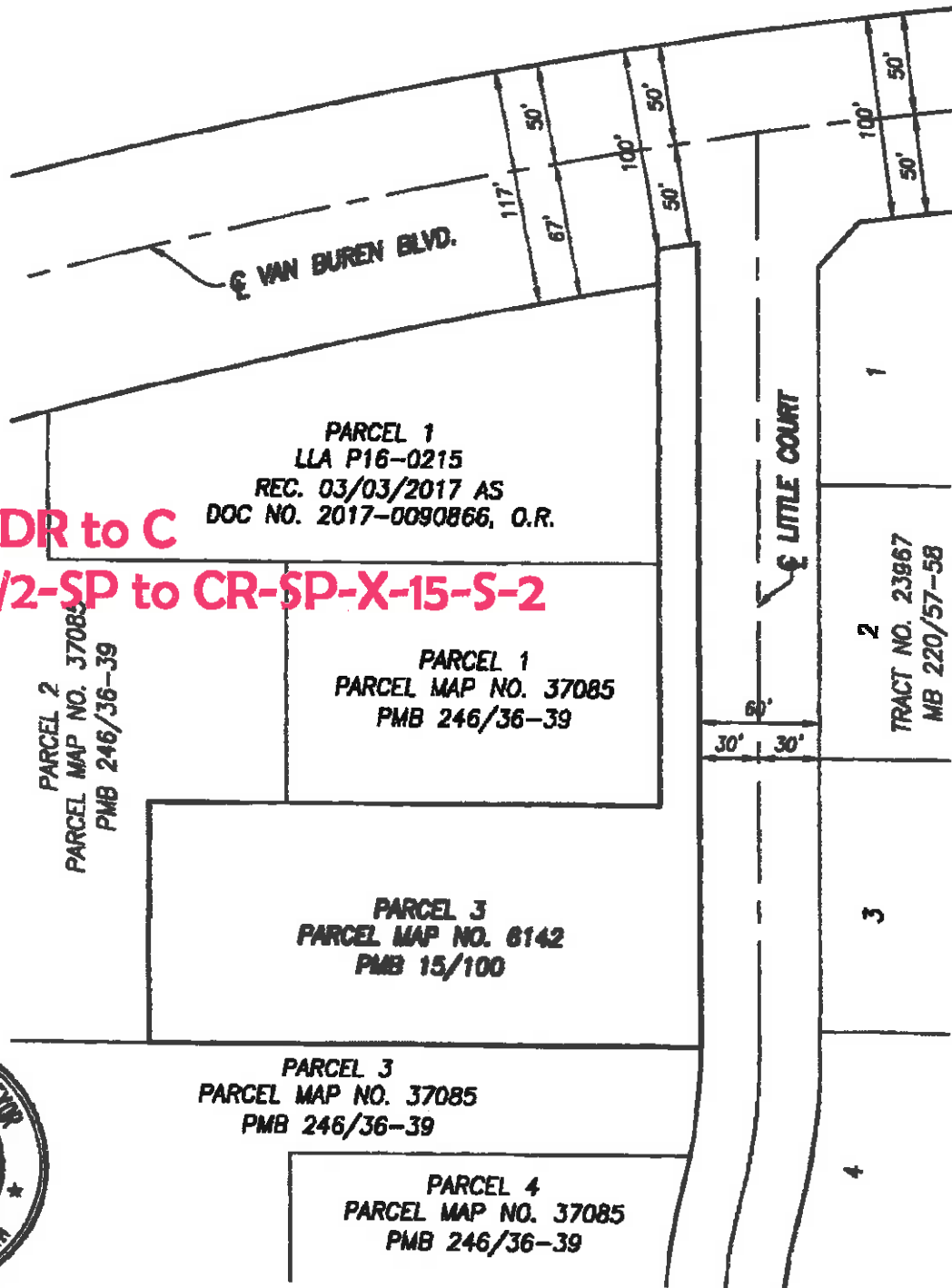
© Riverside County GIS

Notes

FROM: RA30; R-1-1/2 ACRE SINGLE FAMILY RESIDENTIAL ZONE
 TO: CR; COMMERCIAL RETAIL ZONE



1" = 80'



GPA - VLDR to C
ZC - R-1-1/2-SP to CR-SP-X-15-S-2



adkan ENGINEERS
 Civil Engineering • Surveying • Planning
 6820 Airport Drive, Riverside, CA 92504
 Tel: (951) 688-0241 • Fax: (951) 688-0599

10-13-2020 18233 VAN BUREN BLVD. - CHANGE OF ZONE

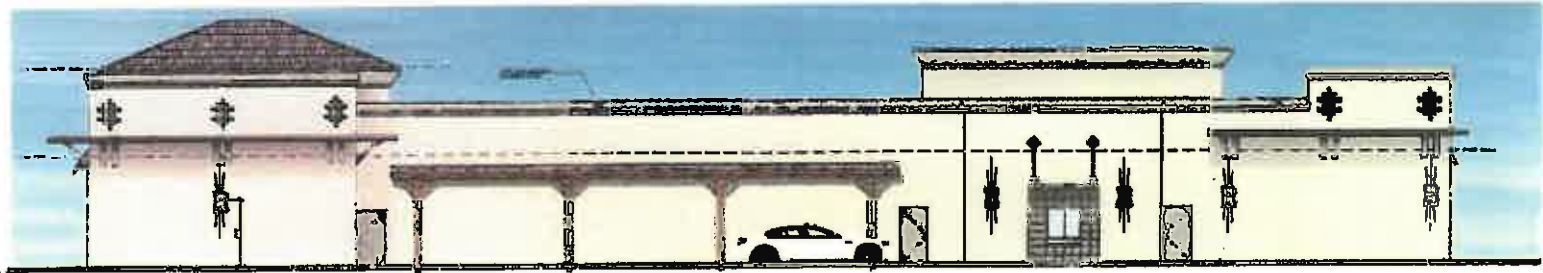
APPROVED BY:

 MITCHELL JAY ADKISON, PLS 8936

PARCEL 3
 PMB 15/100
 RIVERSIDE COUNTY, CA
 JN. 9999 SHEET 2 OF 2

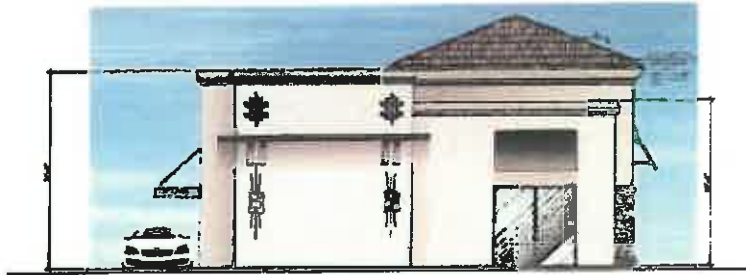


FRONT NORTH - EXTERIOR ELEVATION



REAR SOUTH - EXTERIOR ELEVATION

18233 VAN BUREN BLVD., RIVERSIDE CA 92506
PRELIMINARY DESIGN



LEFT SIDE - EXTERIOR ELEVATION



RIGHT SIDE - EXTERIOR ELEVATION

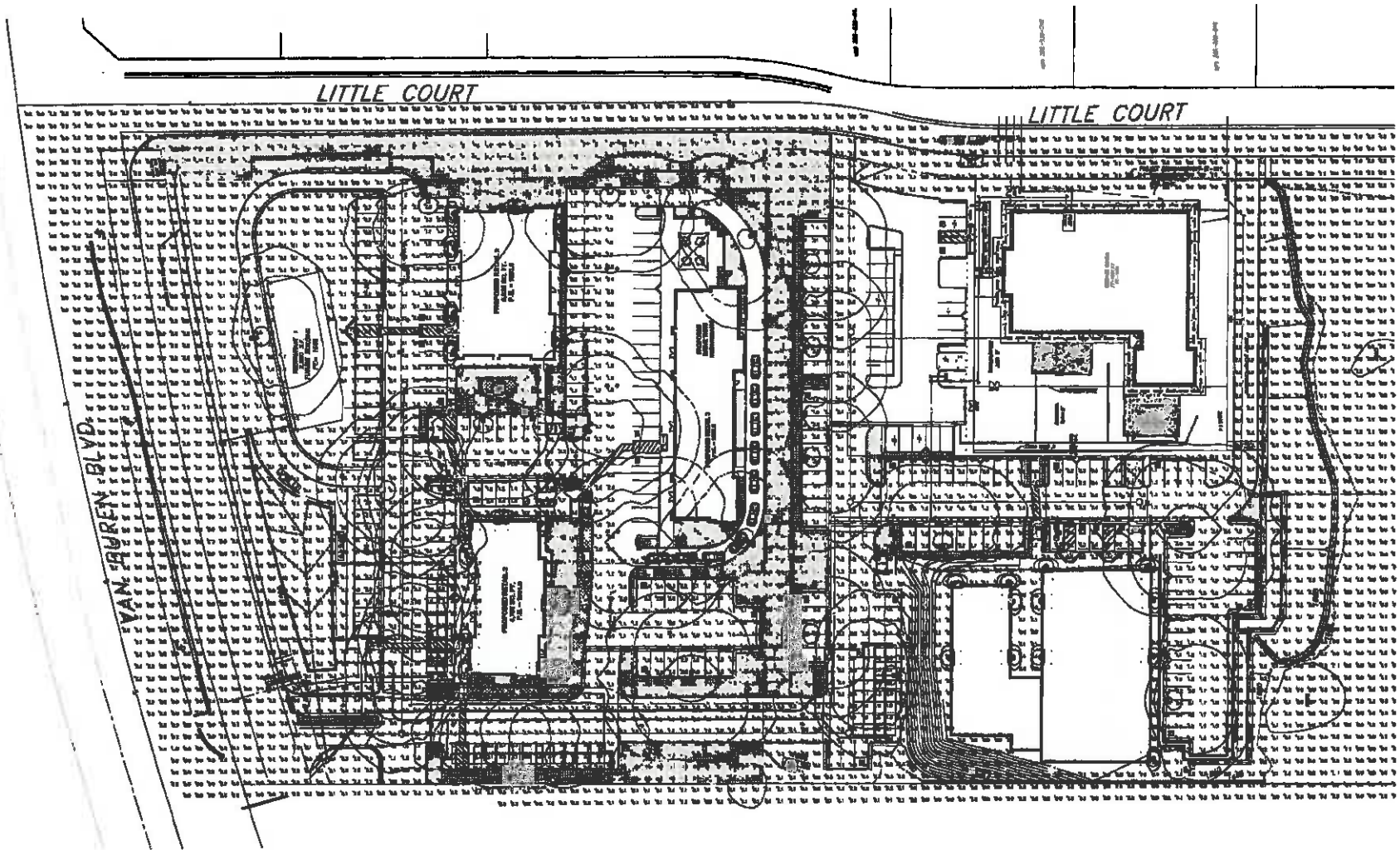


EXHIBIT "A"
LEGAL DESCRIPTION – CHANGE OF ZONE
FROM: RA30; R-1-1/2 ACRE SINGLE FAMILY RESIDENTIAL ZONE
TO: CR; COMMERCIAL RETAIL ZONE

APN: 266-020-022

IN THE CITY OF RIVERSIDE, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA:

**PARCEL 3 OF PARCEL MAP NO. 6142, PER MAP FILED IN BOOK 15, PAGE 100 OF PARCEL MAPS, RECORDS
OF RIVERSIDE COUNTY, STATE OF CALIFORNIA.**

PREPARED BY ME OR UNDER MY DIRECTION

A handwritten signature in blue ink, appearing to read "M. Adkison", followed by the date "0-19-20".

MITCHELL JAY ADKISON
PLS 8936

PAGE BREAK





AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY

December 10, 2020

Mr. Brian Norton, Project Planner
City of Riverside Community and Economic Department – Planning Division
3900 Main Street, 3rd Floor
Riverside CA 92501

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Russell Belts
Desert Hot Springs

VICE CHAIR
Staven Stewart
Palm Springs

COMMISSIONERS

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Director
Simon A. Housman

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County Administrative Center
4080 Lemon St., 14th Floor
Riverside, CA 92501
(951) 955-6132

www.aluc.org

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW –
DIRECTOR'S DETERMINATION**

File No.: ZAP1442MA20
Related File No.: P19-0926 (General Plan Amendment), P19-0943 (Rezone), P19-0958 (Design Review), P19-0959 (Variance)
APN: 219-270-006
Airport Zone: Compatibility Zone E

Dear Mr. Norton:

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to ALUC Resolution No.15-01 (as adopted on August 13, 2015), staff reviewed City of Riverside Case Nos. P19-0926 (General Plan Amendment), P19-0943 (Rezone), a proposal to amend 1.33 acres (of 1.40 acre) site's General Plan land use designation from LDR (Low Density Residential) to C (Commercial), and rezone the site's 1.40 acres from R-1-7000 (Single Family Residential) and CG (Commercial General) Zone to CG-CS (Commercial General and Commercial Storage Overlay Zones), located southerly of Ivy Street, easterly of State Route 91, and westerly of the AT & SF railroad.

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to Policy 1.5.2(d) of the Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan, staff reviewed City of Riverside Case Nos. P19-0958 (Design Review), P19-0959 (Variance), a proposal to expand an existing self-storage facility on an adjacent 1.40 acre parcel, including the new construction of 13,400 square feet of self-storage area, located southerly of Ivy Street, easterly of State Route 91, and westerly of the AT & SF railroad. A variance is required to allow walls and fences to exceed municipal code standards.

The site is located within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, non-residential intensity is not restricted.

Although the project is located within the March Air Reserve Base/Inland Port Airport Influence Area, the nearest runway is actually Runway 9-27 at Riverside Municipal Airport. The elevation of Runway 9-27 at Riverside Municipal Airport is approximately 815.8 feet above mean sea level (AMSL) at its southeasterly terminus. At a distance of 16,980 feet from the project to the nearest point on the runway, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with an elevation at top of roof exceeding 985

AIRPORT LAND USE COMMISSION

feet AMSL. The site's finished floor elevation is 912 feet AMSL, and the proposed maximum building height is 12 feet, resulting in a top point elevation of 924 feet AMSL. Therefore, FAA OES review for height/elevation reasons was not required.

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, provided that the City of Riverside applies the following recommended conditions:

CONDITIONS:

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Hazards to flight.
4. The attached notice shall be provided to all prospective purchasers of the property and tenants of the building.
5. Any proposed detention basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the detention basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of

AIRPORT LAND USE COMMISSION

contiguous canopy, when mature. Landscaping in and around the detention basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the detention basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at RCALUC.ORG which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin.

If you have any questions, please contact Paul Rull, ALUC Principal Planner, at (951) 955-6893.

Sincerely,
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

Simon A. Housman

Simon A. Housman, ALUC Director

Attachments: Notice of Airport in Vicinity

cc: Port Real Estate, Inc., Blake Schoenberg (applicant)
Larry Vessely Architect (representative)
Tierra Corporation (property owner)
Gary Gosliga, Airport Manager, March Inland Port Airport Authority
Doug Waters, Deputy Base Civil Engineer, March Air Reserve Base
ALUC Case File

Y:\AIRPORT CASE FILES\March\ZAP1442MA20\ZAP1442MA20.LTR.doc

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b)

(13)(A)

NOTICE

**THERE IS AN AIRPORT NEARBY.
THIS STORM WATER BASIN IS DESIGNED TO HOLD
STORM WATER FOR ONLY 48 HOURS AND
NOT TO ATTRACT BIRDS**

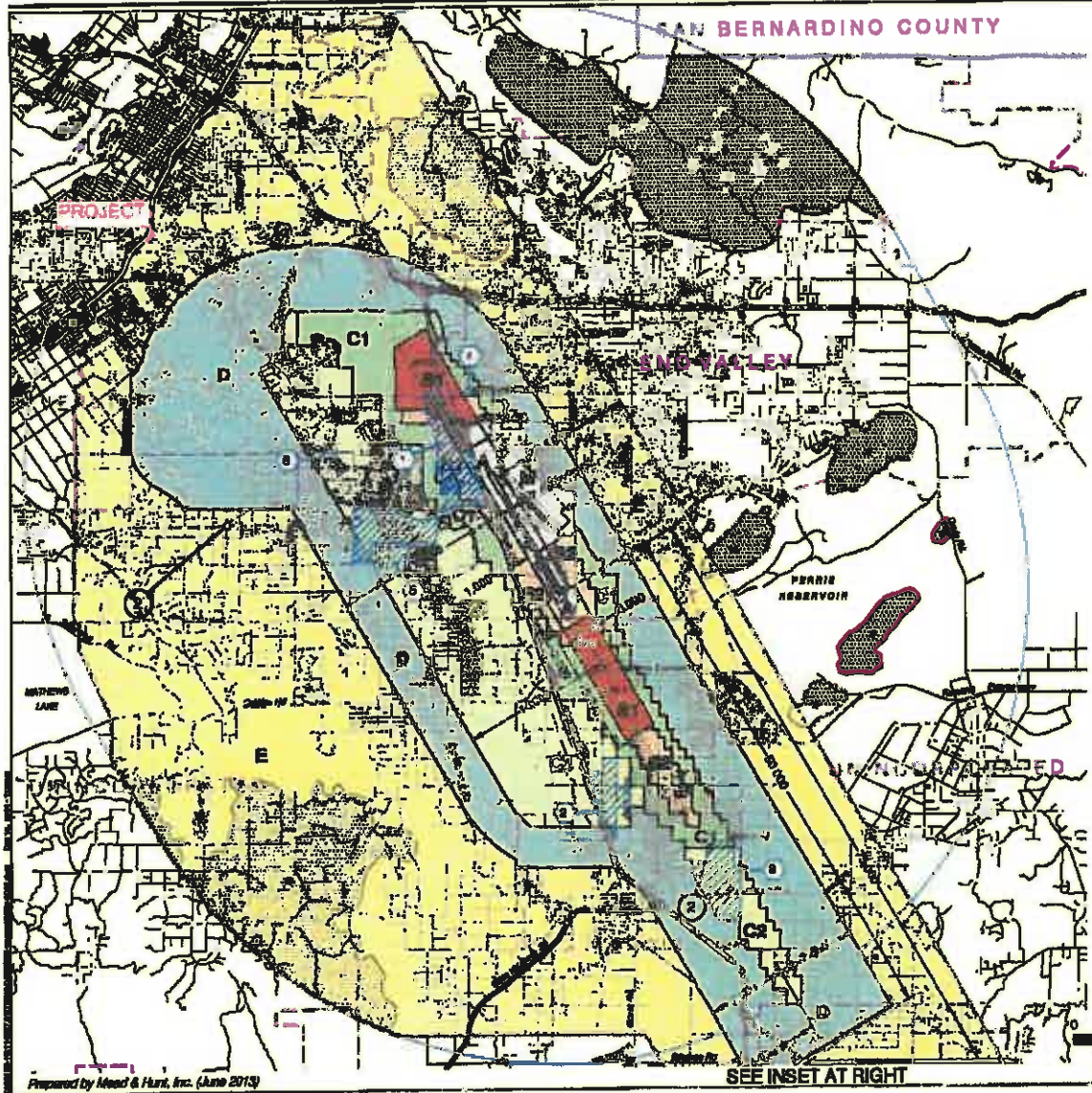
**PROPER MAINTENANCE IS NECESSARY TO AVOID
BIRD STRIKES**



IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:

Name: _____

Phone: _____



LEGEND

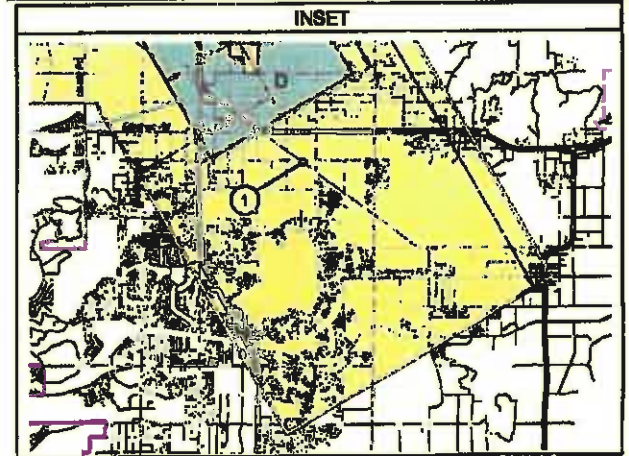
Compatibility Zones

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

- ① Point at which aircraft on Runway 38 ILS approach descend below 4,000 feet above runway end. Airport Elevation is 1,836 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.

Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- - - City Limits
- ▨ Site-Specific Exceptions (existing local agency commitments to development projects)
- ① March JPA: March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA: Ben Clark Training Center
- ⑥ Riverside: Ridge Crest Subdivision



Riverside County
Airport Land Use Commission

March Air Reserve Base / Inland Port Airport
Land Use Compatibility Plan

(Adopted November 13, 2014)

Map MA-1

Compatibility Map

March Air Reserve Base / Inland Port Airport

Note:
All dimensions are measured from
runway ends and centerlines.

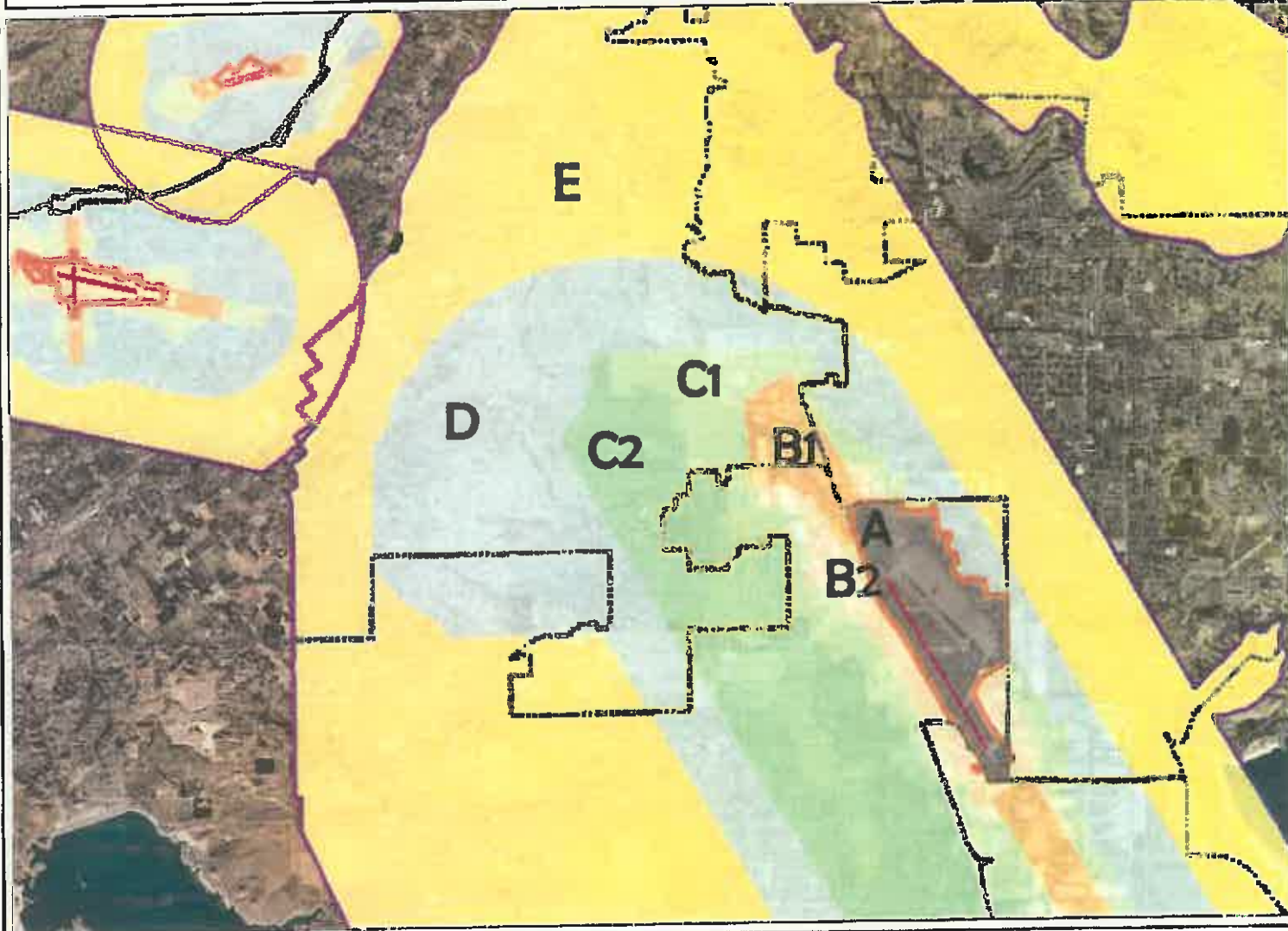
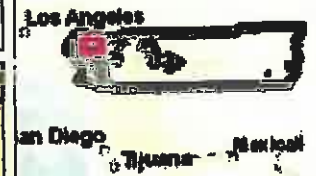


Base map source: County of Riverside 2013

Prepared by Mead & Hunt, Inc. (June 2013)

SEE INSET AT RIGHT

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6

Notes



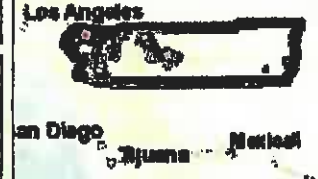
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0 12 24,629 Feet
314

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© Riverside County GIS

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC8

Notes



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Map My County Map



Legend
 County Centerline Names
 World Street Map



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Notes



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Map My County Map



Legend

World Street Map

Notes



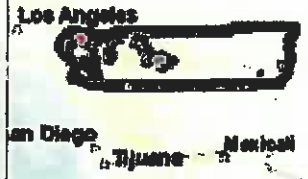
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0 12 24,629 Feet

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Map My County Map



Legend
World Street Map

Notes



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0 3. 6,157 Feet
079

REPORT PRINTED ON... 12/4/2020 8:17:23 AM

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Map My County Map



Legend

County Centerline Names
World Street Map

Notes



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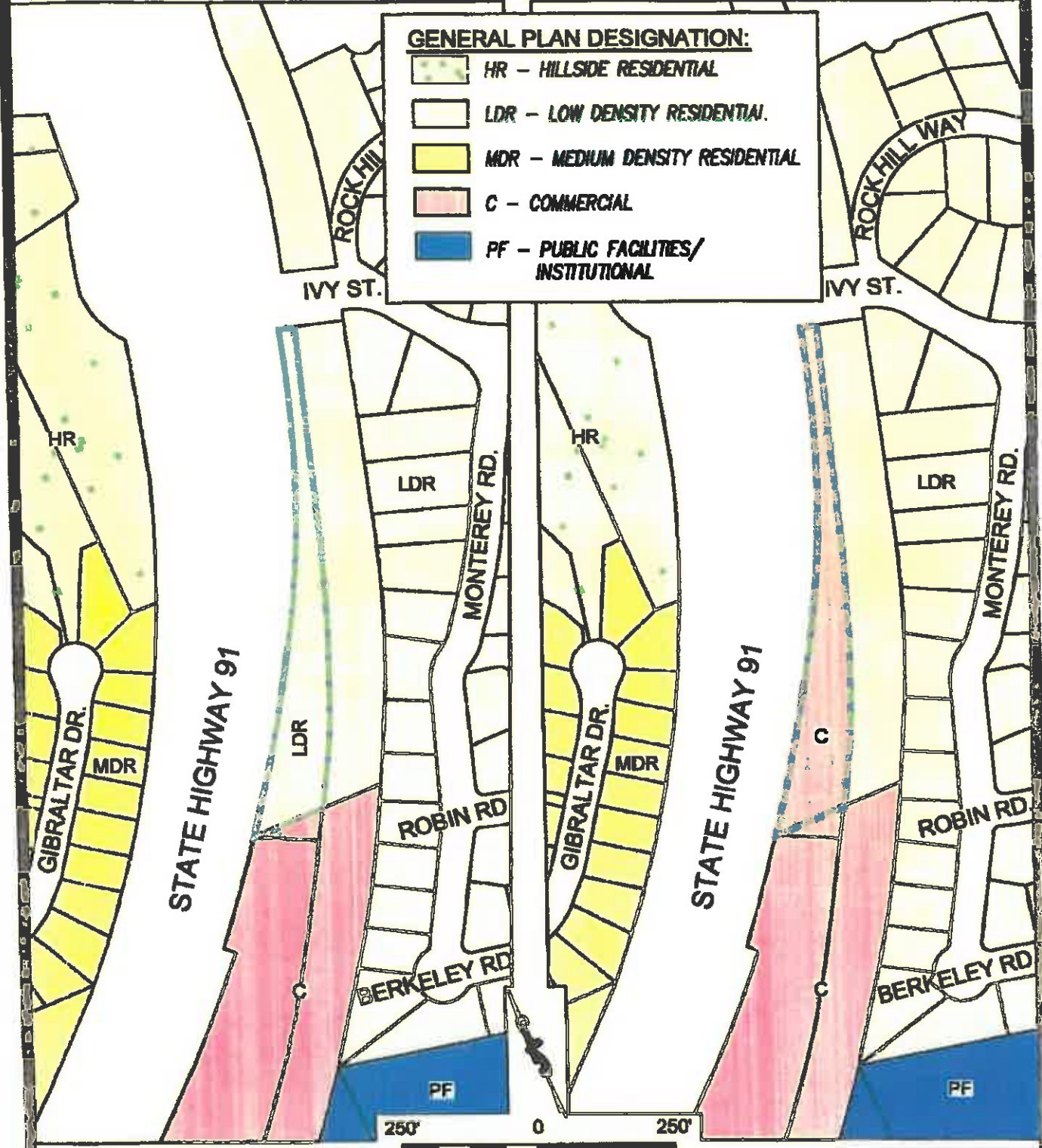
© Riverside County GIS

EXISTING GENERAL PLAN DESIGNATION

PROPOSED GENERAL PLAN DESIGNATION

GENERAL PLAN DESIGNATION:

-  HR - HILLSIDE RESIDENTIAL
-  LDR - LOW DENSITY RESIDENTIAL
-  MDR - MEDIUM DENSITY RESIDENTIAL
-  C - COMMERCIAL
-  PF - PUBLIC FACILITIES/ INSTITUTIONAL



SCALE: 1"=250'

JOB NO: 184524

DATE: 10-06-2020

PREPARED BY: JOSEPH E. BONADIMAN & ASSOCIATES, INC.

NOTE: JOSEPH E. BONADIMAN & ASSOC., INC. DOES NOT WARRANT THE ACCURACY OF THE INFORMATION PRESENTED IN THIS EXHIBIT. THIS EXHIBIT CONTAINS INFORMATION PROVIDED BY THE CITY OF RIVERSIDE, CA.

GENERAL PLAN AMENDMENT & REZONE
2998 IVY ST.
CITY OF RIVERSIDE, CA

SHT. NO.

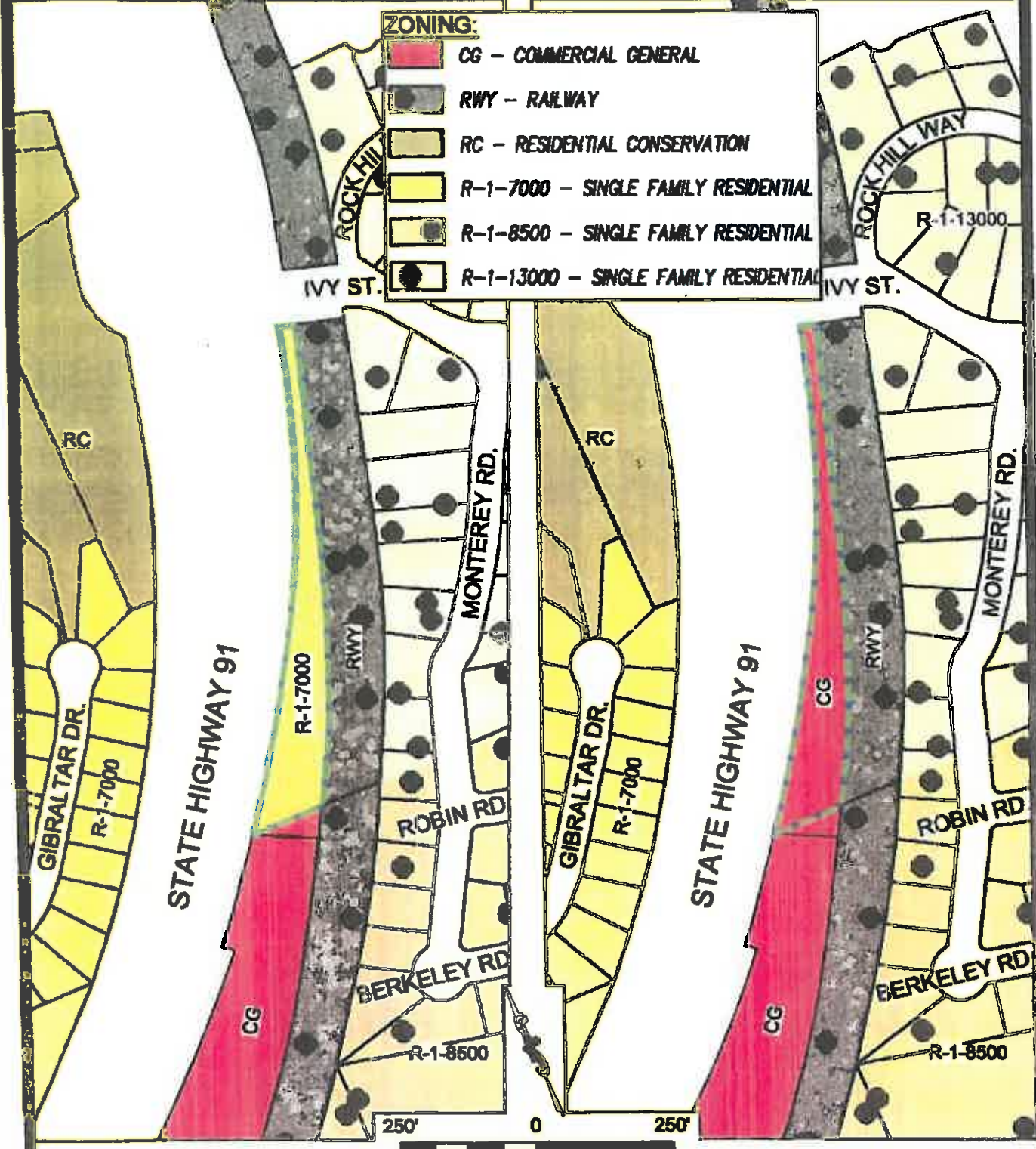
2
OF 2

EXISTING ZONING

PROPOSED ZONING

ZONING:

- CG - COMMERCIAL GENERAL
- RWY - RAILWAY
- RC - RESIDENTIAL CONSERVATION
- R-1-7000 - SINGLE FAMILY RESIDENTIAL
- R-1-8500 - SINGLE FAMILY RESIDENTIAL
- R-1-13000 - SINGLE FAMILY RESIDENTIAL



250' 0 250'
SCALE: 1"=250'

JOB NO: 184524

DATE: 10-08-2020

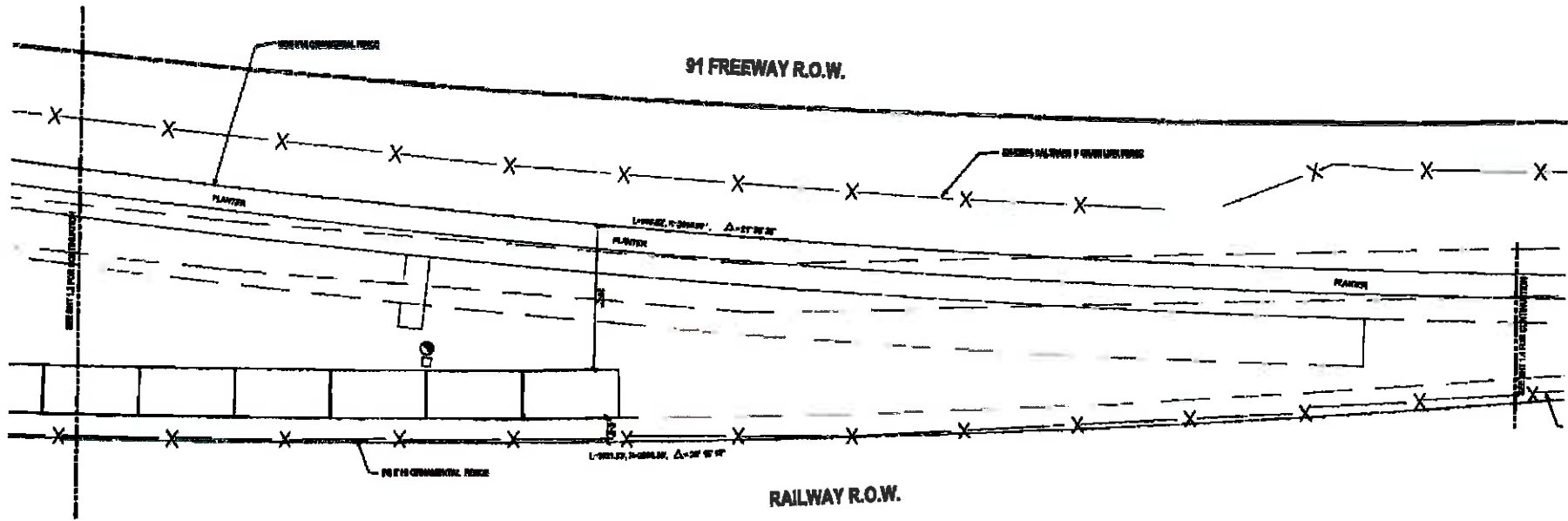
PREPARED BY: JOSEPH E. BONADIMAN & ASSOCIATES, INC.

NOTE: JOSEPH E. BONADIMAN & ASSOC., INC. DOES NOT WARRANTY THE ACCURACY OF THE INFORMATION PRESENTED IN THIS EXHIBIT. THIS EXHIBIT CONTAINS INFORMATION PROVIDED BY THE CITY OF RIVERSIDE, CA.

**GENERAL PLAN
 AMENDMENT & REZONE
 2998 IVY ST.
 CITY OF RIVERSIDE, CA**

SHT. NO.

1
 OF 2

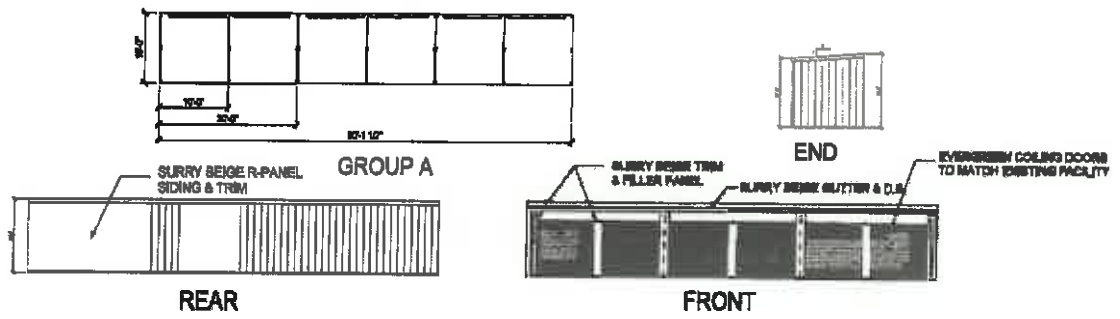


SELF STORAGE EXPANSION FOR
TERRA CORPORATION
 10000 W. 10TH AVENUE
 DENVER, CO 80202



DATE: 08/20/20
 DATE: 10/20/20
 SCALE: 1/4" = 1'-0"
 SHEET

1.3
 ENLARGED SITE PLAN



VERBLY ARCHITECT ARCHITECT PLANNING
 102 West Park, Suite 100
 Fort Worth, Texas 76102
 Phone: 817.335.1111
 Fax: 817.335.1112
 www.verbly.com

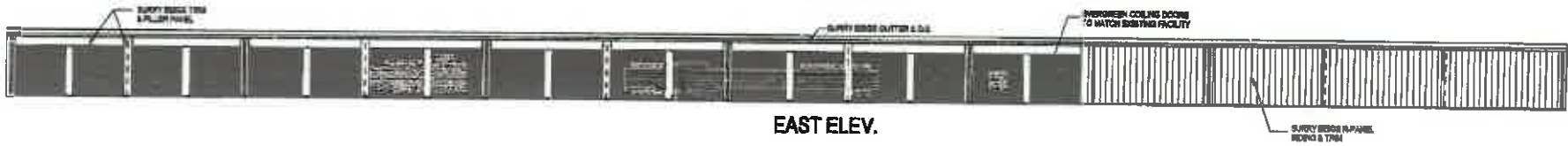
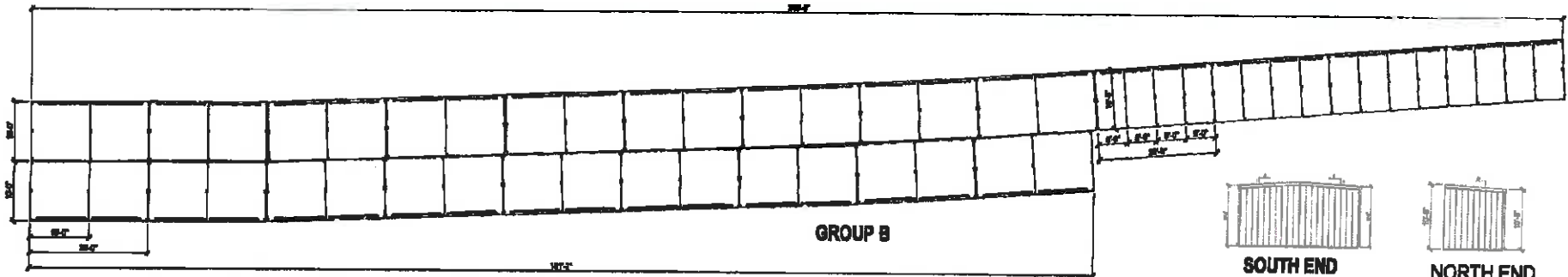


SELF-STORAGE EXPANSION FOR
TERRA CORPORATION
 PREPARED BY: VERBLY ARCHITECT



DATE PLOTTED
 DATE PLOTTED
 SCALE 1/8"=1'-0"
2

BUILDING GROUP A



ARCHITECT
PLANNING
ARCHITECTURE
 100 WEST 11TH STREET
 NEW YORK, NY 10011
 TEL: 212 677 1111
 FAX: 212 677 1112



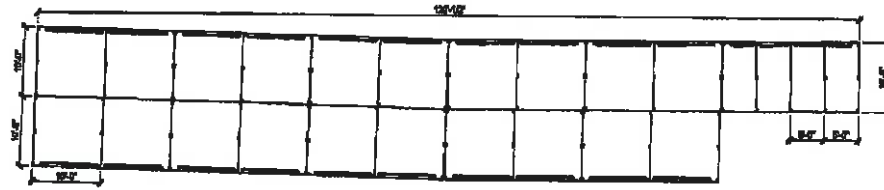
SELF STORAGE EXPANSION FOR
TERRA CORPORATION
 100 WEST 11TH STREET
 NEW YORK, NY 10011



DATE REVISED
 DATE TYPED
 DATE SET-UP
3

BUILDING
GROUP B

100



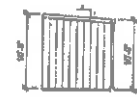
GROUP C



WEST ELEV



EAST ELEV.



NORTH END



SOUTH END



0148 01/10/2010 10:00 AM

YEBELLY ARCHITECTURE ARCHITECT PLANNING
 100 West 10th Street, Suite 100
 Denver, CO 80202
 303.733.1111

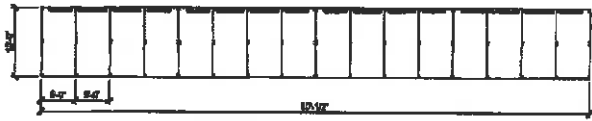


SELF-STORAGE EXPANSION FOR
TERRA CORPORATION
 2000 WEST 10TH STREET
 DENVER, CO 80202



PLAN: 01-1010
 DATE: 11/04/09
 SCALE: 1/8"=1'-0"
 4

BUILDING GROUP C



GROUP E



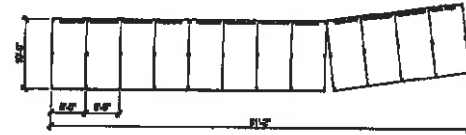
NORTH ELEV



SOUTH ELEV



END ELEV



GROUP D



NORTH ELEV



SOUTH ELEV



END ELEV



SELF STORAGE EXPANSION FOR
TERRA CORPORATION
RIVERSIDE, CALIFORNIA
3000 HWY 78 STREET



DATE: 11/20/20
DRAWN: 11/21/20
SCALE: 1/8" = 1'-0"
5

BUILDING GROUP D & E

PAGE BREAK



Rull, Paul

From: WATERS, DOUGLAS S GS-13 USAF AFRC 452 MSG/CE <douglas.waters.2@us.af.mil>
Sent: Saturday, November 14, 2020 3:53 PM
To: Rull, Paul; Pacino, Brian
Cc: MARTIN, WILLIAM R II Col USAF AFRC 452 AMW/CV; MCCRAINE, RODNEY E Col USAF AFRC 452 MSG/CC; MORRISON, LARA B Col USAF AFRC 452 OG/CC; SHAW, DAVID N Maj USAF AFRC 452 CE/CC; Housman, Simon
Subject: RE: ZAP1439MA20 (MJPA) D1 Gateway Aviation Center Project
Attachments: MARB Response to email from Mr. Jeff Smith 3 Sept 20 MC.pdf

Mr. Rull,

Thanks for your email. Our concerns are outlined in Brig Gen Coburn's letter of 3 September 2020. Brig General concluded attached letter with the comment, "It is our position that any decision to approve this development by the MJPA at this time, regardless of any conditions of approval (COA), would be premature. These approvals should be received prior to any action to approve the project or authorize the developer to begin any improvements."

In addition, the 162 ATKW Air National Guard unit a tenant on March ARB also expressed the following comment: The 163d Attack Wing has safety concerns regarding the proposed Gateway Aviation Center Project, as it could negatively impact the operation of our MQ-9 remotely piloted aircraft (RPA). Specifically, it could interfere with our Line-of-Sight C-band operations. Both the proposed buildings height and material it is constructed from could cause degradation, reflection, and loss of C-band signal while on base and final approach to runway 32, resulting in loss of command and control of the RPA. More information would be needed and studies conducted in order to validate the extent to which this proposal would negatively affect our operations.

Based on the above, it can be concluded that March ARB does not concur with this development at this time and various studies and approvals will be necessary prior to any concurrence from the Base.

Please enter these comments in the record. We expect that Col Martin, Col Morrison or Col McCraine will make remarks to the same at the ALUC Commission meeting on December 10, 2020.

Douglas Waters
Chief Engineering
951-655-4852

FY21 Year of March BCE

From: Rull, Paul <PRull@RIVCO.ORG>
Sent: Tuesday, November 3, 2020 9:30 AM
To: Pacino, Brian <Brian.Pacino@jacobs.com>; WATERS, DOUGLAS S GS-13 USAF AFRC 452 MSG/CE <douglas.waters.2@us.af.mil>
Subject: [Non-DoD Source] ZAP1439MA20 (MJPA) D1 Gateway Aviation Center Project

Good Morning,

Please note that ALUC has received the D1 Gateway Aviation Center Project (in the MJPA jurisdiction), a proposal for a 201,200 sq.ft. gateway air freight cargo/warehouse building, and a 69,620 sq.ft. maintenance building, located in ALUC Zone B2, westerly of Heacock Street, northerly of Oleander Avenue, on 56.03 acres.

Please note that it appears the project also intersects with the 2018 AICUZ for Runway 30 clear zones and APZ-I. The applicant's site plan shows the AICUZ zones overlaid.

If you can please provide any comments by November 19, 2020, it would be appreciated. The project is tentatively scheduled for the December 10, 2020 ALUC public hearing meetings.

If you have any questions, please feel free to contact me.

Paul Rull
ALUC Principal Planner



Riverside County Airport Land Use Commission
4080 Lemon Street, 14th Floor
Riverside, Ca 92501
(951) 955-6893
(951) 955-5177 (fax)
PRULL@RIVCO.ORG
www.rcaluc.org

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County of Riverside California



DEPARTMENT OF THE AIR FORCE
AIR FORCE RESERVE COMMAND

3 September 2020

Brig Gen Melissa A. Coburn
Commander
452d Air Mobility Wing
2145 Graeber Street
March Air Reserve Base CA 92518

Mr. Jeffrey Smith
Senior Planner
March Joint Powers Authority
1455 Meridian Parkway, Suite 140
Riverside, CA 92518

Dear Mr. Smith

This is in response to your Project Review Transmittal – Parcel D1 - Gateway “1. Parcel D1_Gateway AvCtr Project-Project Description. 19 Aug2020.pdf” (Project Description). The Developer name is not provided; however, we know from previous information and meetings that Lewis Group is the developer and is referred to herein as the Developer. We have several concerns regarding this project.

First, the Existing Conditions on page one of the Project Description states the project site is “...owned by March JPA.” The maps and written description clearly show that the project is partially on March Air Reserve Base (MARB) real property and various modifications are proposed to the MARB taxiway that would require a real property instrument. Prior to any real property approval by the Commander of MARB, the Air Force Reserve Command and the Air Force Civil Engineer Center must approve any real property instrument to allow any modifications to the MARB airfield. Also, if as proposed, the Developer were to construct any modification, alter any real property, or provide any non-real property items to the Air Force, approval of these gifts must be approved by the Secretary of the Air Force or her designee pursuant to Air Force regulations. The process to obtain these approvals could take a lengthy amount of time, perhaps up to two or more years depending on the level of environmental review required. Until this approval is in place, the developer will not be allowed to begin construction on Air Force real property.

Second, the project would have an impact on March ARB airfield operations. This would include an increase of 17 flights per day or 34 operations (arrivals and departures) per day on the MARB airfield (~10,600 operations per year). The Joint Use Agreement allows (after approval by the Air Force) up to 21,000 operations per year. Present operations are just

under 4,000 per year according to March JPA data, and the total operations with the proposed new facility would be approximately 14,600 operations. The projected increase over the holiday season of an additional 512 operations would make the total joint use of the airfield over 15,000 operations. Such an increase would undoubtedly cause additional impacts on the entire airfield in terms of maintenance, tower operations, military use of the airfield, and air quality.

Third, with increased operational activities and modifications of MARB real property, in accordance with the National Environmental Protection Act (NEPA) and 32 Code of Federal Regulations 989 (32 CFR 989) an Environmental Impact Analysis Process (EIAP) will be required. Under NEPA, the Commander of MARB must make a Record of Decision (ROD) resulting from an Environmental Impact Statement (EIS) or a Finding of No Significant Impact (FONSI) resulting from an Environmental Assessment (EA) of this project. With this in mind, we do not recommend that the MIPA consider any approval of this project until the necessary approvals are received from the Air Force.

In addition to the above overarching concerns, we have several specific concerns with regard to the project as a whole. These concerns are as follows:

1. **Airfield Operations:** MARB understands the Joint Use Agreement permits March JPA/March Inland Port Airport Authority (MIPAA) an annual maximum 21,000 aircraft movements (total arrivals and departures). However, even a proposed increase to 12 average daily flights (up to 25 daily flights during peak holiday periods) between 0700 – 2300 hours to support the Gateway project is cause for concern due to the following:
 - Potential workload and safety impacts on MARB Air Traffic Control staffing resources may not be commensurate with proposed air traffic increases. Air Traffic controllers are Air Force Civil Service employees and any additional manpower or overtime will come at a cost to the AF Reserve Command and MARB.
 - Increased aircraft operations could potentially negatively impact critical 452d AMW readiness, training, and alert mission requirements, given that MARB continues to function as a single-runway airfield.
 - Physical degradation of MARB runways, taxiways, and other critical airfield pavements would increase at an unknown rate and could negatively affect alert missions at MARB.
 - According to the drawings, there would be four aircraft parked on the north side of the proposed aviation center that would not have the ability to access taxiway Golf or Alpha due to MARB boundary constraints. Expansion/encroachment into March ARB property to access taxiway Alpha has many issues related to who would pay for the work, who would accomplish the work, who would maintain the construction, and finally from where would the maintenance funds come.
 - Noise complaints are likely to escalate from the surrounding community due to early morning and late evening air cargo flight activity. The Developer has stated they plan to work within the current “Quiet Hour” restriction at MARB, but the Project Description is vague with regards to actual hours planned. This could increase the workload for MARB in the long term to address noise complaints and increase

communication with the surrounding community to manage public perceptions and concerns. These adverse effects could impact any future proposed military exercises in that time frame.

- During the construction phase on Taxiway Alpha, the timing, route availability, and runway availability may drive our alert mission to be either be re-allocated or moved to a different geographic location.
 - Lewis Group verbally briefed MARB staff that Taxiway Alpha (part of the standard taxiway to Runway 32) will be completely closed on two separate occasions during construction, thereby restricting our access to the full length of the runway. Once we receive the complete plan from the Lewis Group, we will require time to coordinate the potential impacts to our alert missions with USNORTHCOM, USSTRATCOM, HQ AMC, and HQ AFRC, as well as other military base tenants.
 - While C-17 operations should not be affected during construction, post-construction civilian operations will render it difficult, if not impossible, to keep pilots proficient in assault landings and NVG operations. A significant increase in civilian operations will eventually render night C-17 NVG operations impossible as the lighting configuration for NVG approaches is not compatible with normal airfield lighting required for a traditional military and normal civilian traffic.
- We are concerned that if the base reaches a 50/50 civilian-military construct (by 2030 as outlined in their latest draft proposal) that C-17 military operations will be significantly degraded as operations grow post-construction.

2. Environmental and Utilities: Initial MARB concerns relate to proposed stormwater management based on the pre-application site plan, as well as potential Bird Air Strike Hazard (BASH) incidents. Summary comments are as follows:

- Drainage from the proposed site will flow into the recently improved Heacock channel, which also functions as the primary eastbound outflow for MARB. Any modifications made to this drainage will need to be coordinated, as it may have an adverse impact on the water quality of MARB outflow.
- Proposed on-site bio-retention facilities, as well as increased daily civilian air cargo flight activity, elevate the BASH concern and potential adverse cumulative impacts for MARB operations.
- Previously approved projects in the vicinity of MARB have not considered the potential cumulative impacts of additional distribution warehouses being constructed. As a part of the environmental impact analysis, this must be considered up front. This includes the increased pressure to open Heacock Street to truck traffic, which we consider to be inconsistent with airfield flight patterns and activity and could be dangerous to the public.
- There is significant concern over the plans to remediate contaminated soil and ground water within the project boundaries.

3. Safety, Security, and Traffic: The 452d Security Forces Squadron (SFS), Civil Engineer Squadron (CES), and Fire Chief (CEF) have concerns regarding potential encroachment resulting from the proposed Gateway Project as it concerns the following:

- Line of sight obstructions, particularly in the SFS's ability to see outside the fence in a southerly direction from MARB, is a concern for base force protection. The proposed Gateway Aviation Center facility would create further visual obstruction. While the developer has offered to replace the current fence to mitigate some of these impacts, there are concerns this may become a gift issue requiring longer lead times in planning and coordination. Ownership of the perimeter fence must remain with MARB.
- Portions of the proposed site are also located within the 2,000-foot explosive ordnance disposal (EOD) cordon, which presents a clear safety concern.
- The proposed Gateway Maintenance Building is an encroachment issue for the MARB Fire Training Area and comes within only a few hundred feet of critical live fire training facilities.
- Increased traffic along Heacock Street could cause potential adverse impacts on utilization of emergency gate 51.
- This project could result in increased pressure to build a paved extension of Heacock Street south to Harley Knox Boulevard.
- MARB needs further details regarding the proposed new traffic signal at Heacock Street and the relationship to aircraft load/offload traffic flows through the proposed site.

It is our position that any decision to approve this development by the MJPA at this time, regardless of any conditions of approval (COA), would be premature. These approvals should be received prior to any action to approve the project or authorize the developer to begin any improvements.

Should you have any questions or would like to discuss this further, please feel free to contact me directly or to contact Major David Shaw, Base Civil Engineer, at (951) 655-4851 or Mr. Douglas Waters, Chief Engineering & Environmental Flight, at (951) 655-2197.

Sincerely

COBURN.MELISSA
.ANN.1076529376

Digitally signed by
COBURN.MELISSA.ANN.107652
9376
Date: 2020.09.04 15:56:02 -0700

MELISSA A. COBURN
Brigadier General, USAF
Commander

cc:
452 AMW/CV
452 MSG/CC



**AIRPORT LAND USE COMMISSION MEETING
MINUTES
DECEMBER 10, 2020**

DRAFT

12-17-20

COMMISSIONERS PRESENT LIVE: Russell Betts, Arthur Butler, Richard Stewart

COMMISSIONERS PRESENT REMOTELY: Steve Manos, John Lyon, Steven Stewart, Gary Youmans
COMMISSIONERS ABSENT: None

2.0 PUBLIC HEARING: CONTINUED ITEMS

NONE

3.0 PUBLIC HEARING: NEW CASES

- 3.1 Staff report recommended: **CONSISTENT**
Staff recommended at hearing: **CONSISTENT**
ALUC Commission Action: **CONSISTENT (Vote 7-0)**
Motion: Steve Manos
Second: Richard Stewart
- ZAP1051TH20 – Thermal House, LLC (Representative: Fullerton Architects)** – Riverside County Building & Safety Case No. BRS2002282 (Building Permit). A proposal to construct a 624 square foot rooftop solar panel system on a proposed single family residence located at 86814 Newton Way within the Thermal Motorclub, located northerly of 62nd Avenue, westerly of Polk Street, easterly of Tyler Street, and southerly of Avenue 60 (Airport Compatibility Zone C of the Jacqueline Cochran Regional Airport Influence Area). Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rivco.org.
Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rivco.org

4.0 PUBLIC HEARING: MISCELLANEOUS ITEMS

- 4.1 **Approve Residential Solar Policy** – Presentation by ALUC Director Simon Housman or his designee. The ALUC by a unanimous vote of 7-0 approved the Residential Solar Policy.
Motion: Richard Stewart
Second: Art Butler

5.0 ADMINISTRATIVE ITEMS

5.1 Director's Approvals – Information Only

5.2 Update March Air Reserve Base Compatibility Use Study (CUS)

Simon Housman, ALUC Director informed the Commission that the March CUS is continuing, and is currently evaluating request for proposals for different consultants and working with local jurisdictions, and anticipate making an initial proposal to a consultant at the end of December.

VIDEO:

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A video recording of the entire proceedings is available on the ALUC website at www.rcaluc.org. If you have any questions please contact Barbara Santos, ALUC Commission Secretary, at (951) 955-5132 or E-mail at basantos@rivco.org

**AIRPORT LAND USE COMMISSION MEETING
MINUTES
DECEMBER 10, 2020**

6.0 APPROVAL OF MINUTES

Arthur Butler motioned to approve the November 12, 2020 minutes, seconded by Gary Youmans.
(Vote 7-0)

7.0 ORAL COMMUNICATION ON ANY MATTER NOT ON THE AGENDA

None

8.0 COMMISSIONER'S COMMENTS

Commissioner Lyon commented that Riverside County is very fortunate to have this Airport Land Use Commission (ALUC). In the County of Los Angeles duties of the ALUC have been delegated to the Department of Regional Planning and as a result of that LA City Council wants Whiteman Airport to close. Russell Betts, Chair announced the passing of Chuck Yeager at the age of 97. Commissioner Richard Stewart expressed that today starts Hanukkah and for those who celebrate Hanukkah, Happy Hanukkah! .

9.0 ADJOURNMENT

Russell Betts, Chair adjourned the meeting at 9:58 a.m.

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VIDEO:

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