



# AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY AGENDA

Riverside County Administration Center  
4080 Lemon St., 1st Floor Hearing Room  
Riverside, California

Thursday 9:00 a.m., April 11, 2013

## CHAIR

Simon Housman  
Rancho Mirage

## VICE CHAIRMAN

Rod Ballance  
Riverside

## COMMISSIONERS

Arthur Butler  
Riverside

Glen Holmes  
Hemet

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Cathedral City

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Moreno Valley

NOTE: If you wish to speak, please complete a "SPEAKER IDENTIFICATION FORM" and give it to the Secretary. The purpose of the public hearing is to allow interested parties to express their concerns. Comments shall be limited to 5 minutes and to matters relevant to the item under consideration. Please do not repeat information already given. If you have no additional information, but wish to be on record, simply give your name and address and state that you agree with the previous speaker(s). Also please be aware that the indicated staff recommendation shown below may differ from that presented to the Commission during the public hearing.

Non-exempt materials related to an item on this agenda submitted to the Airport Land Use Commission or its staff after distribution of the agenda packet are available for public inspection in the Airport Land Use Commission's office located at 4080 Lemon Street, 14<sup>th</sup> Floor, Riverside, CA 92501 during normal business hours.

In compliance with the Americans with Disabilities Act, if any accommodations are needed, please contact Barbara Santos at (951) 955-5132 or E-mail at [basantos@rctlma.org](mailto:basantos@rctlma.org). Request should be made at least 48 hours or as soon as possible prior to the scheduled meeting.

## STAFF

Director  
Ed Cooper

John Guerin  
Russell Brady  
Barbara Santos

County Administrative Center  
4080 Lemon St, 14<sup>th</sup> Floor  
Riverside, CA 92501  
(951) 955-5132

[www.rcaluc.org](http://www.rcaluc.org)

### 1.0 INTRODUCTIONS

#### 1.1 CALL TO ORDER

#### 1.2 SALUTE TO FLAG

#### 1.3 ROLL CALL

### 2.0 PUBLIC HEARING: CONTINUED CASE

#### PALM SPRINGS INTERNATIONAL AIRPORT

- 2.1 ZAP1011PS13 – Desert Community College District (Representative: John Criste, Terra Nova Planning & Research, Inc.). A proposal to establish a West Valley campus of College of the Desert (COD) on 115-119 acres located westerly of Indian Canyon Drive, northerly of Tramview Road, and southerly of the Whitewater River in the City of Palm Springs. The campus would ultimately include multiple buildings with a total gross floor area up to 654,000 square feet, and a solar electric generation facility westerly of the building area. The site is located partially within Compatibility Zones D and E of the Palm Springs International Airport Influence Area and partially outside the Airport Influence Area; however, all of the buildings would be located within Compatibility Zone D. ALUC Staff Planner: John Guerin at (951) 955-0982, or e-mail at [jguerin@rctlma.org](mailto:jguerin@rctlma.org).

Staff Recommendation: CONTINUE TO MAY 9, 2013

**RECONSIDERATION IN PART (Originally heard on February 14, 2013)****MARCH AIR RESERVE BASE**

- 2.2 ZAP1086MA12 – Stater Bros. Markets and CVS Drugs (Representative: Dan Schultz, Mile Stone Management) – City of Riverside Case Nos.: P12-0444 (Design Review) and P12-0761 (Conditional Use Permit). Reconsideration of height limit for drug store (request 36 foot height). These cases relate to the expansion and redevelopment of an existing 5.43-acre Stater Bros. commercial center located at the southeast corner of Mary Street and Lincoln Avenue. P12-0444 proposes to expand the center to 6.3 acres, with a new 44,636 square foot Stater Bros. grocery store, an 11,465 square foot CVS drug store with pharmacy, and an 8,760 square foot pad for future commercial development. P12-0761 is a request for a Conditional Use Permit to allow (1) the operation of a drive-thru pharmacy and (2) alcohol sales for off-site consumption. (Area III of the March Air Reserve Base Airport Influence Area). ALUC Staff Planner: John Guerin at (951) 955-0982, or e-mail at [jguerin@rctlma.org](mailto:jguerin@rctlma.org).

Staff Recommendation: CONDITIONALLY CONSISTENT

**3.0 PUBLIC HEARING: NEW CASE****RIVERSIDE MUNICIPAL AIRPORT**

- 3.1 ZAP1057RI13 – Exclusive Towing, Inc. (Representative: Michael Carpenter) - City Case Nos. P11-0767 (Rezoning), P11-0768 (Conditional Use Permit), P11-0769 (Design Review), and P12-0138 (General Plan Amendment). P12-0138 is a proposal to amend the City of Riverside General Plan land use designation on a 2.07-acre property with a site address of 9920 Arlington Avenue (located at the southeasterly corner of Arlington Avenue and Crest Avenue) from Business/Office Park (B/OP) to Industrial (I). P11-0767 is a proposal to rezone that same property from Business and Manufacturing Park (BMP) to General Industrial (I), or General Industrial-Airport Protection Overlay Zone D (I-AP-D). P11-0768 and P11-0769 propose to establish a towing and impoundment business on the site (Zone D of Riverside Municipal Airport Influence Area). ALUC Staff Planner: John Guerin at (951) 955-0982, or e-mail at [jguerin@rctlma.org](mailto:jguerin@rctlma.org).

Staff Recommendation: GPA CONTINUE TO 5-9-13; CONSISTENT FOR THE REZONING AND CUP/DESIGN REVIEW

**4.0 ADMINISTRATIVE ITEMS****4.1 Director's Approvals**

- 4.2 Resolution No. 2013-01 Establishing a Fee for Review of Non-Impact Legislative Proposals

**5.0 APPROVAL OF MINUTES**

March 14, 2013

**6.0 ORAL COMMUNICATION ON ANY MATTER NOT ON THE AGENDA****7.0 COMMISSIONER'S COMMENTS**

COUNTY OF RIVERSIDE  
AIRPORT LAND USE COMMISSION  
STAFF REPORT

**AGENDA ITEM:** 2.1 2-4

**HEARING DATE:** April 11, 2013 (continued from March 14, 2013)

**CASE NUMBER:** ZAP1011PS13 – Desert Community College District (Representative: John Criste, Terra Nova Planning & Research, Inc.)

**APPROVING JURISDICTION:** Desert Community College District (earlier action by City of Palm Springs)

**JURISDICTION CASE NO.:** Not Applicable.

**MAJOR ISSUES:** *Compliance with Airport Compatibility Zone D non-residential intensity limitations and need for Federal Aviation Administration obstruction evaluation review of structures. Additional features proposed for the site include an energy facility with an evaporative cooling tower and a small wind turbine, extensive use of solar photovoltaic panels atop buildings and parking areas, and a retention basin. Visible plumes would constitute a hazard to flight, as would thermal plumes resulting in moderate or high levels of turbulence. This case was continued from the March 14 calendar at the request of the project representative.*

**RECOMMENDATION:** Staff recommends an *additional CONTINUANCE to ALUC's May 9, 2013 agenda to facilitate additional discussions regarding project design and layout, if this is acceptable to the project team. Otherwise, staff must recommend a finding of INCONSISTENCY for this project, based on exceedance of the single-acre intensity criteria for Airport Compatibility Zone D, unless the Commission is willing to make a special circumstances finding pursuant to Section 3.3.6 of the Countywide Policies, based on the proximity of the site to the Whitewater River floodplain.*

~~, in which case, staff would recommend CONTINUANCE to ALUC's April 11, 2013 public hearing agenda, pending electronic filing of Form 7460-1 for each of the proposed Phase I buildings with the Federal Aviation Administration's Obstruction Evaluation Service.~~



**PROJECT DESCRIPTION:**

The Desert Community College District proposes to establish a West Valley campus of College of the Desert (COD) on 115-119 acres in the City of Palm Springs. The campus would ultimately include multiple buildings with a total gross floor area up to 654,000 square feet, and a solar electric generation facility westerly of the building area. The site is located partially within Compatibility Zones D and E of the Palm Springs International Airport Influence Area and partially outside the Airport Influence Area; however, all of the buildings would be located within Compatibility Zone D.

**PROJECT LOCATION:**

The campus would be located westerly of Indian Canyon Drive, northerly of Tramview Road, and southerly of the Whitewater River in the City of Palm Springs, approximately 12,600 feet northwesterly of the northwesterly terminus of Runway 13R-31L at Palm Springs International Airport.

**LAND USE PLAN: 2005 Palm Springs International Airport Land Use Compatibility Plan**

- a. Airport Influence Area: Palm Springs International Airport
- b. Land Use Policy: Compatibility Zones D and E, and outside the AIA
- c. Noise Levels: Aircraft noise levels are below 60 CNEL.

**SPECIFIC PLAN:**

Land Use/Intensity: The site is located in Airport Compatibility Zones D and E, but also includes land beyond the Airport Influence Area boundary. However, all of the proposed buildings are located within Compatibility Zone D, as this is the most accessible area of the site. Airport Compatibility Zone D allows an average of 100 persons per acre. For sites at least three acres in size, the most intense single acre may accommodate up to 300 persons, so long as the average intensity does not exceed 100 persons per acre.

Given the overall acreage of the parcel, the average intensity would comply as long as the total number of persons on the property does not exceed 11,500. However, single-acre intensities are a concern. Floor plans and elevations are only available for the proposed buildings in Phase I, but the remaining buildings would be three to four stories in height and could potentially exceed the maximum allowable population intensity of 300 persons in any given acre.

As to the Phase I buildings, both the North Building and the South Building, as well as portions of the Central Plant and the building labeled "DEEC", are located within a single 210-foot-by-210-foot area (square acre). Leaving aside at this time the fractional portions of the Central Plant and the "DEEC" building, staff has determined that the first



floor land uses of the two buildings would accommodate at least 190 persons, the second floor land uses at least 126 persons, and the third floor land uses (where the classrooms are located) at least 138 persons, for a total of at least 454 persons. This calculation assumes that “lab” rooms are occupied at an intensity of one person per 50 square feet rather than the one person per 20 square feet assumed for regular classrooms and does not include any occupancy for the commercial kitchen and building service rooms.

Staff has advised the project representative of this concern and has requested additional information from the architect as to the planned occupancy load of several of the rooms and has suggested consideration of a redesign. A large portion of the site is located in Zone E or outside the Airport Influence Area entirely, so it appears that there is sufficient acreage to construct the buildings beyond the boundaries of the Airport Influence Area, where intensity would not be an issue in relation to safety. Unfortunately, the portion that is most accessible – the area along Indian Canyon Drive – is within Zone D. A second possible alternative would be to increase the separation between buildings such that they are located not less than 210 feet from each other. While this would reduce the single-acre intensity, it may not be sufficient to reduce the single-acre intensity to 300, as the intensity of the North Building alone would remain over 300. A third possible alternative would be to reduce the square footage of each of the buildings, which could involve a reduction in the number of stories.

This project was considered by the City of Palm Springs as the College Park Specific Plan, General Plan Amendment, and Change of Zone. At the time of Draft EIR preparation, both the City of Palm Springs and the project representative were advised of the need for ALUC review of these applications, but no filing was made until this year. In July, 2012, ALUC staff advised in a subsequent letter to the District that Zone D areas are subject to an average intensity limit of 100 persons per acre and a single-acre intensity maximum of 300 persons.

Prohibited and Discouraged Uses: ~~The applicant does not propose any uses that are prohibited (highly noise sensitive outdoor nonresidential uses such as amphitheaters and hazards to flight) or discouraged (children’s schools, hospitals, and nursing homes) in Airport Compatibility Zone D.~~

**Upon further review, staff notes that Exhibit 6 in the project initial study (excerpts of which are attached, including this exhibit) depicts locations of an evaporative cooling tower, retention basin, and small wind turbine. Additionally, the campus proposes to make extensive use of solar energy, with photovoltaic panels atop most buildings and canopies atop parking areas. Additional information is needed in order to ensure that neither visual nor thermal plumes will be created and that the retention basin will not become a wildlife attractant.**

Open Area: Projects 10 acres or larger in size within Compatibility Zone D are subject to the requirement that at least 10 percent of its land area be maintained as ALUC-qualified open area. **No open area exhibit has been provided to demonstrate that the project will meet the 10 percent open area requirement.** Staff has not focused on this criterion

due to the concern regarding the single-acre intensity, but it should be noted that the area directly north of the property is permanent open space – the Whitewater River floodplain.

Part 77: Developments within 20,000 feet of any point on the runway of a public use airport must be reviewed to determine whether notice to the Federal Aviation Administration Obstruction Evaluation Service is required. The site is located a minimum of 12,600 feet from the northwesterly terminus of Runway 13R-31L, which is located at an elevation of 474 feet above mean sea level. At a distance of 12,600 feet from the runway, objects at or above an elevation of 600 feet above mean sea level must be evaluated. Existing site ground elevations range from 616 to 692 feet above mean sea level (AMSL), according to the Riverside County Land Information System, although the ground elevations of buildings on-site will be below 660 feet. However, FAA notice will still be required. **The project representative has submitted a written Form 7460-1 to the FAA Obstruction Evaluation Service office in Fort Worth as part of a larger packet, including the master site plan, elevations, and vicinity maps.**

Other Special Conditions: Section 3.3.6 of the Countywide Policies of the Riverside County Airport Land Use Compatibility Plan states that the Commission may find “a normally incompatible use” to be “considered compatible because of terrain, specific location, or other extraordinary factors or circumstances related to the site.” Sub-section (b) states as follows: “In reaching such a decision, the Commission shall make specific findings as to why the exception is being made and that the land use will not create a safety hazard to people on the ground or aircraft in flight nor result in excessive noise exposure for the proposed use. Findings also shall be made as to the nature of the extraordinary circumstances that warrant the policy exception.”

The property directly to the north is federally-owned, while property to the north of the federal property is owned by the Coachella Valley Water District. **This land is not available for urban development. These facts may facilitate discussion regarding special circumstances.**

\*\*\*\*\*

**Note: The following conditions are proposed to mitigate the potential effects of the proposed project on the operations of Palm Springs International Airport and aircraft in the vicinity. They do not constitute mitigation for the proposed building intensities and do not render the proposed project consistent with the 2005 Palm Springs Airport Land Use Compatibility Plan.**

**CONDITIONS:**

1. Any outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Lights must be downward facing.
2. The following uses shall be prohibited:

- a. Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
  - b. Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
  - c. Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area, including landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, incinerators, fly ash disposal, and wastewater management facilities.
  - d. Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
3. The attached notice shall be provided to all potential purchasers and tenants, and shall be recorded as a deed notice.
  4. Prior to issuance of building permits for any new structure, the permittee or his/her designee or representative shall file Form 7460-1 with the Federal Aviation Administration, and no building permit shall be issued until a "Determination of No Hazard to Air Navigation" is received from the Federal Aviation Administration and filed with the City of Palm Springs Planning Department, the City of Palm Springs Building and Safety Department, and the Riverside County Airport Land Use Commission.
  5. **Any retention basin shall be designed so as to provide a maximum 48-hour detention period for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the retention basin that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.**
  6. **In the event that any incidence of glint, glare, or flash affecting the safety of air navigation occurs as a result of project operation, upon notification to the airport operator of an incidence, the airport operator shall notify the project operator in writing. Within 30 days of written notice, the project operator**



**shall be required to promptly take all measures necessary to eliminate such glint, glare, or flash. An "incidence" includes any situation that results in an accident, incident, "near-miss," or specific safety complaint regarding an in-flight experience to the airport operator or to federal, state, or county authorities responsible for the safety of air navigation. The project operator shall work with the airport operator to prevent recurrence of the incidence. Suggested measures may include, but are not limited to, reprogramming the alignment of the panels or covering them at the time of day when incidences of glare occur to diminish or eliminate the source of the glint, glare, or flash. For each such incidence made known to the project operator, the necessary remediation shall only be considered to have been fulfilled when the airport operator states in writing that the situation has been remediated to the airport operator's satisfaction.**

## **LETTER IN SUPPORT**

Dr. Mark S. Shirilau, P.E.  
8539 Barnwood Lane  
Riverside, California 92508-7126  
(951) 776-0251 ✖ Fax (951) 789-0783  
PlusMark@aol.com

2.4  
Extra Attachment

March 5, 2013

John Guerin  
Principal Planner  
Riverside County Airport Land Use Commission  
4080 Lemon Street, 14<sup>th</sup> Floor  
Riverside, CA 92501

**RE: College of the Desert West Valley Campus**

Dear John:

As the owner of a house less than one mile from the proposed campus, I fully and unequivocally support the construction of the West Valley Campus of the College of the Desert and its associated solar photovoltaic plant at Tramview and Indian Canyon in Palm Springs. Thus I hope that the ALUC will approve the application submitted by the Desert Community College District.

The neighborhood is seriously run-down, and the construction of the college campus will bring significant economic improvement to the local area. Furthermore, the campus will greatly increase the accessibility of higher education to persons in the Palm Springs and Desert Hot Springs areas, eliminating their present need to commute to Palm Desert for classes. Among other things, this will reduce overall pollution and greenhouse gas emissions by reducing commuting mileage for a large number of people.

I also fully support the construction of the solar power plant. My doctorate is in electric power systems and, among other things, I am also a licensed solar (C-46) contractor. My company is developing a similar, though slightly smaller, power plant in Desert Hot Springs approximately 4 miles north of the proposed COD campus. While I would hope that no one opposes the college proposal, I am also aware that erroneous information sometimes causes some members of the public to have irrational fears. I have in general conversation (and not about this specific site) heard people be concerned about solar plants causing distractions to airplanes. This could occasionally occur from solar-thermal power plants which use highly reflective mirrors to concentrate sunlight. However, the opposite is true of photovoltaic panels such as those that will be used at the College of the Desert location.

Photovoltaic panels are designed to absorb as much sunlight as possible directly into the panel, since this is how they produce electricity. They are black in appearance, and from the air they show as dark areas compared to the more-reflective desert sand. This fact is easily visible



from the top of the Aerial Tramway, which I happened to ride last week, where one can look down and see similar solar plants just north of the I-10 Freeway.

There is no reason not to approve the plans for the West Valley Campus and its solar plant. They will bring economic benefit to the area. They will help the environment. They will have virtually no effect on the Palm Springs Airport or its commercial or private flights.

As a property owner, knowledgeable professional, and concerned citizen, I encourage the Airport Land Use Commission to approve the community college district's plans.

Sincerely yours,

A handwritten signature in cursive script that reads "Mark S. Shirilau".

Mark S. Shirilau, Ph.D., P.E.

cc: John Guerin via email [jguerin@rctlma.org](mailto:jguerin@rctlma.org)  
Russell Brady via email [rbrady@rctlma.org](mailto:rbrady@rctlma.org)

John Criste  
Terra Nova Planning and Research, Inc.  
42635 Melanie Place #101  
Palm Desert, CA 92211-9113

Janice Ballinger  
550 West Sunview Avenue  
Palm Springs, CA 92262-1464



March 25, 2013

Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
2601 Meacham Blvd  
Fort Worth, TX 76193

**COPY**

**RE: Notice of Proposed Construction Form 7460-1 Submittal Packet for College of the Desert - West Valley Campus Facilities Master Plan and Phase I Project, Palm Springs, California**

Dear Sir/Madam:


On behalf of the Desert Community College District/College of the Desert (COD), enclosed please find FAA Form 7460-1 and associated materials for the FAA's review with regard to potential obstructions to navigation that may be associated with the development of the COD West Valley Campus (WVC). The land use plan for the subject WVC Facilities Master Plan and Phase I Project was approved as a part of the College Park Specific Plan by the City of Palm Springs in 2011. Prior to that approval, the City of Palm Springs approved a use permit for the construction of a 60-acre photovoltaic array on the westerly half of the subject property. The City owns and operates the Palm Springs International Airport.

The College has recently completed the WVC Facilities Master Plan further detailing the development plan for the subject campus. The master plan also incorporates the approved photovoltaic array, and was sited and designed, as was the campus, in response to comments from adjoining residents. Alternative locations for solar and academic facilities were considered as a part of the master planning process.

An Environmental Impact Report (EIR) has been prepared in conformance with the California Environmental Quality Act and is out for comment. A copy of the EIR has been sent to Mr. Victor Globa at FAA offices in Lawndale, California. The subject master plan is also currently being reviewed by the Riverside County Airport Land Use Commission.

We hope the enclosed packet provides all the information needed to make a timely determination. If you have any questions regarding the enclosed or would like additional information, please do not hesitate to contact me.

Sincerely,

  
John D. Criste, AICP  
COD Consulting Planner

Airport Land Use Commission  
Received

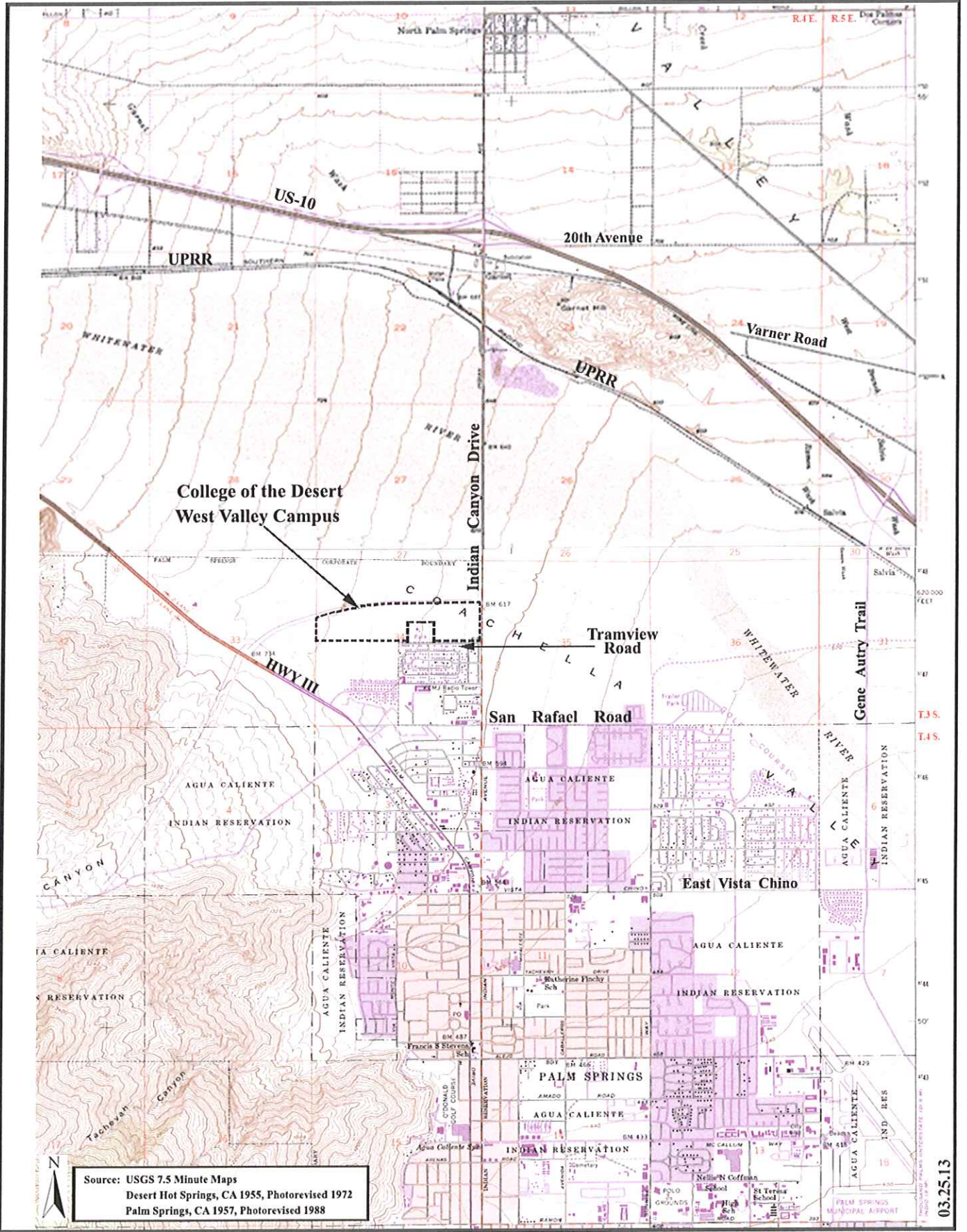
MAR 26 2013

cc:

Steve Renew, COD  
Mac McGinnis, COD  
John Guerin, RivCo ALUC  
David Ready, City of Palm Springs

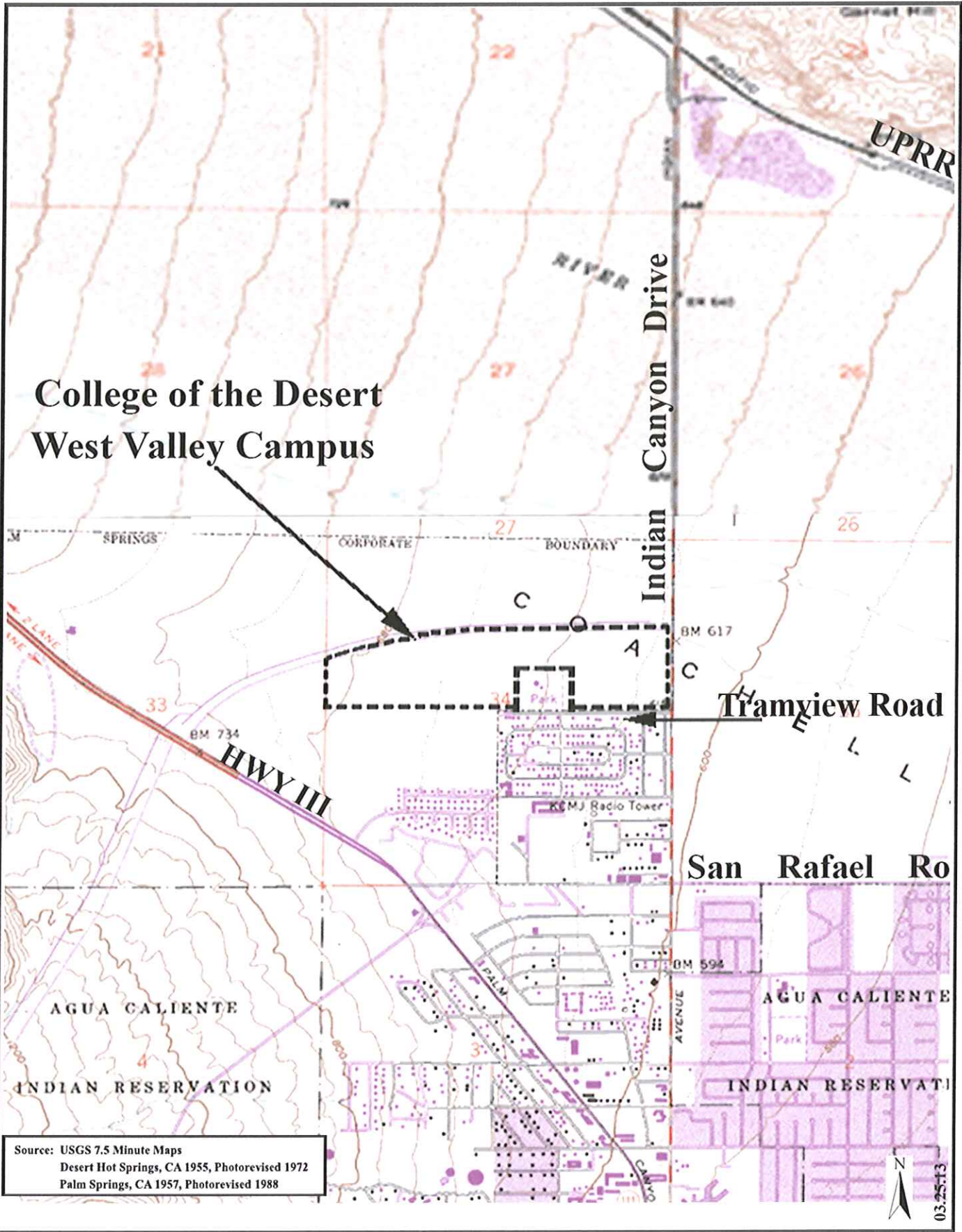






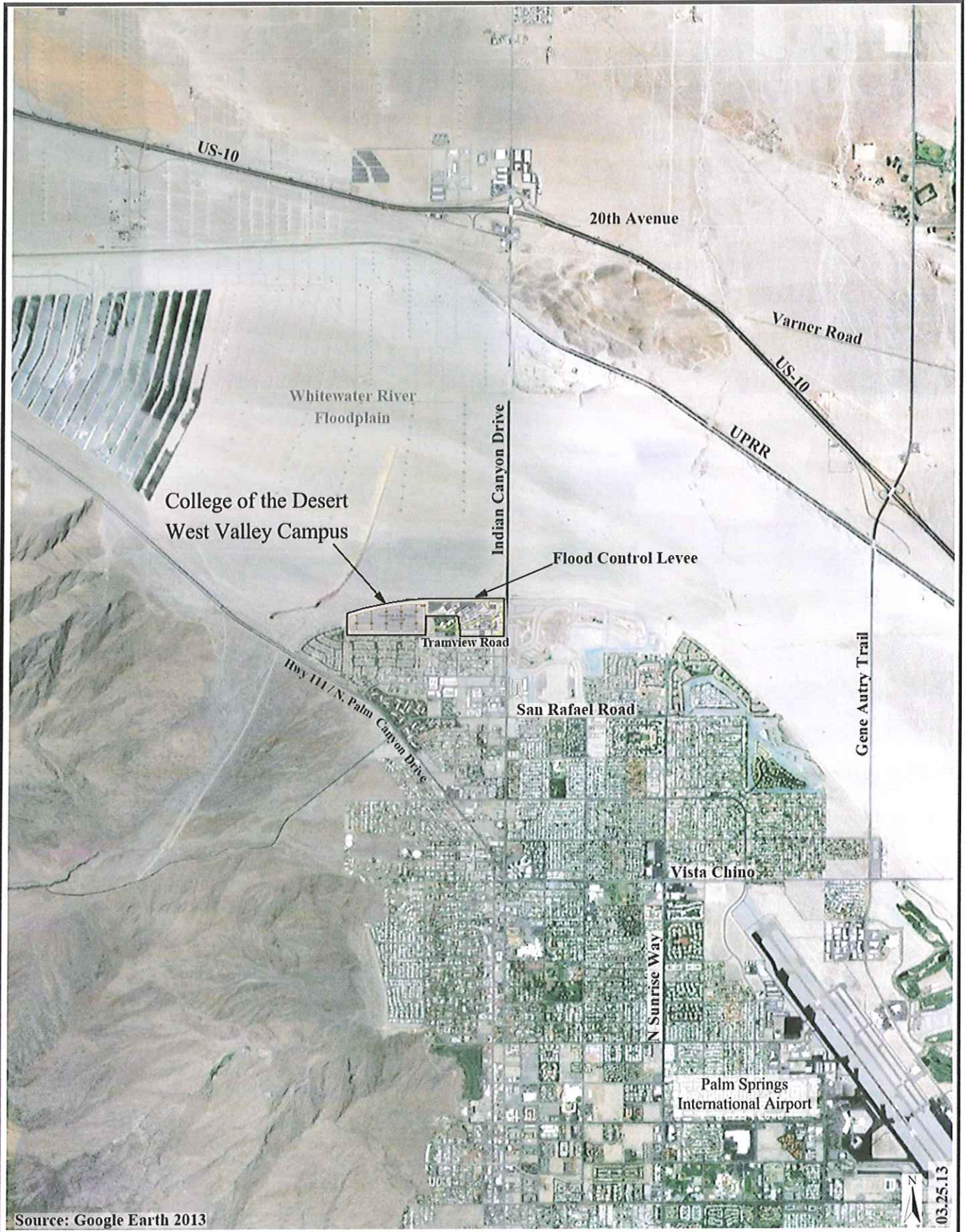
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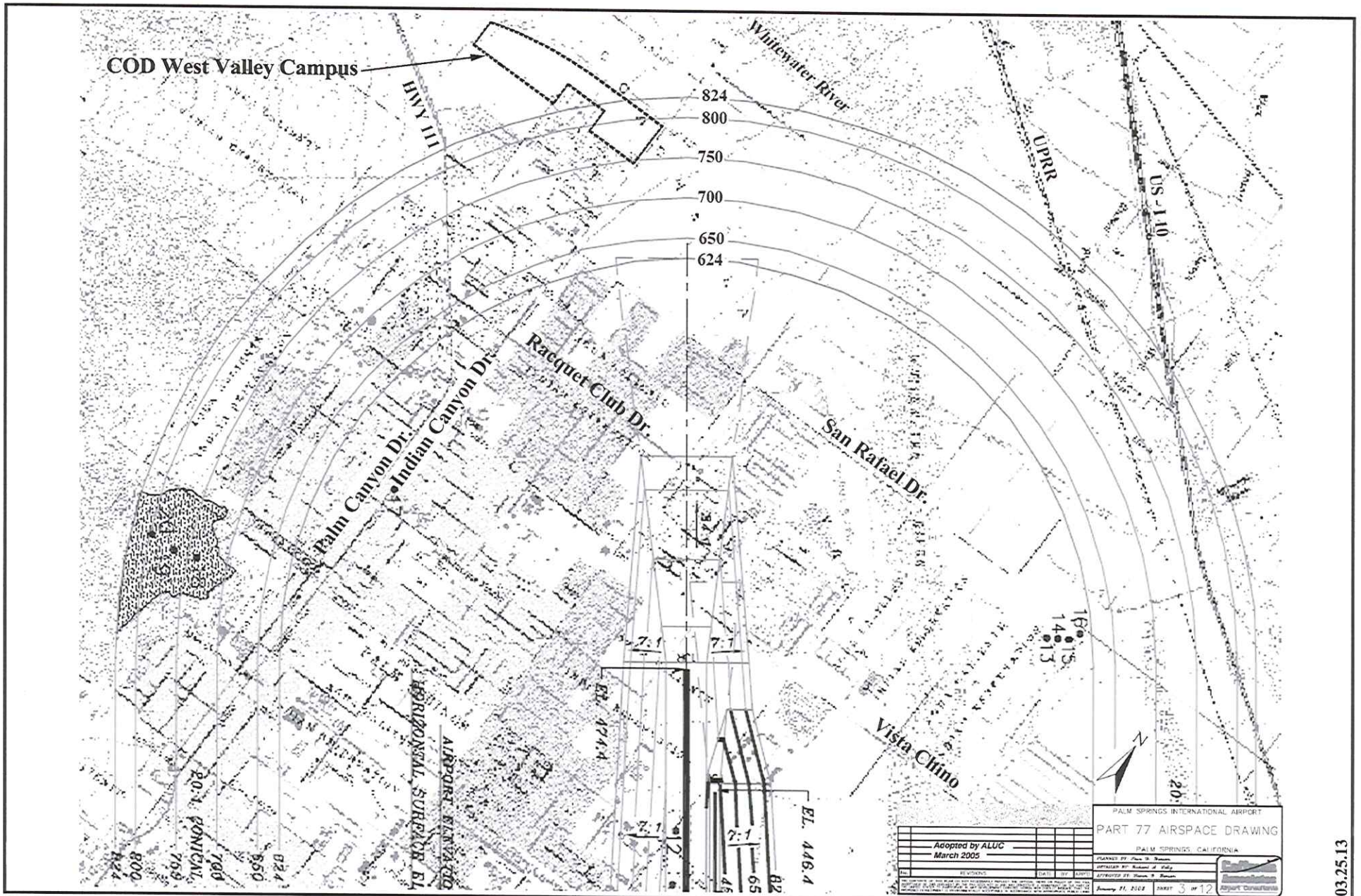


Source: USGS 7.5 Minute Maps  
 Desert Hot Springs, CA 1955, Photorevised 1972  
 Palm Springs, CA 1957, Photorevised 1988



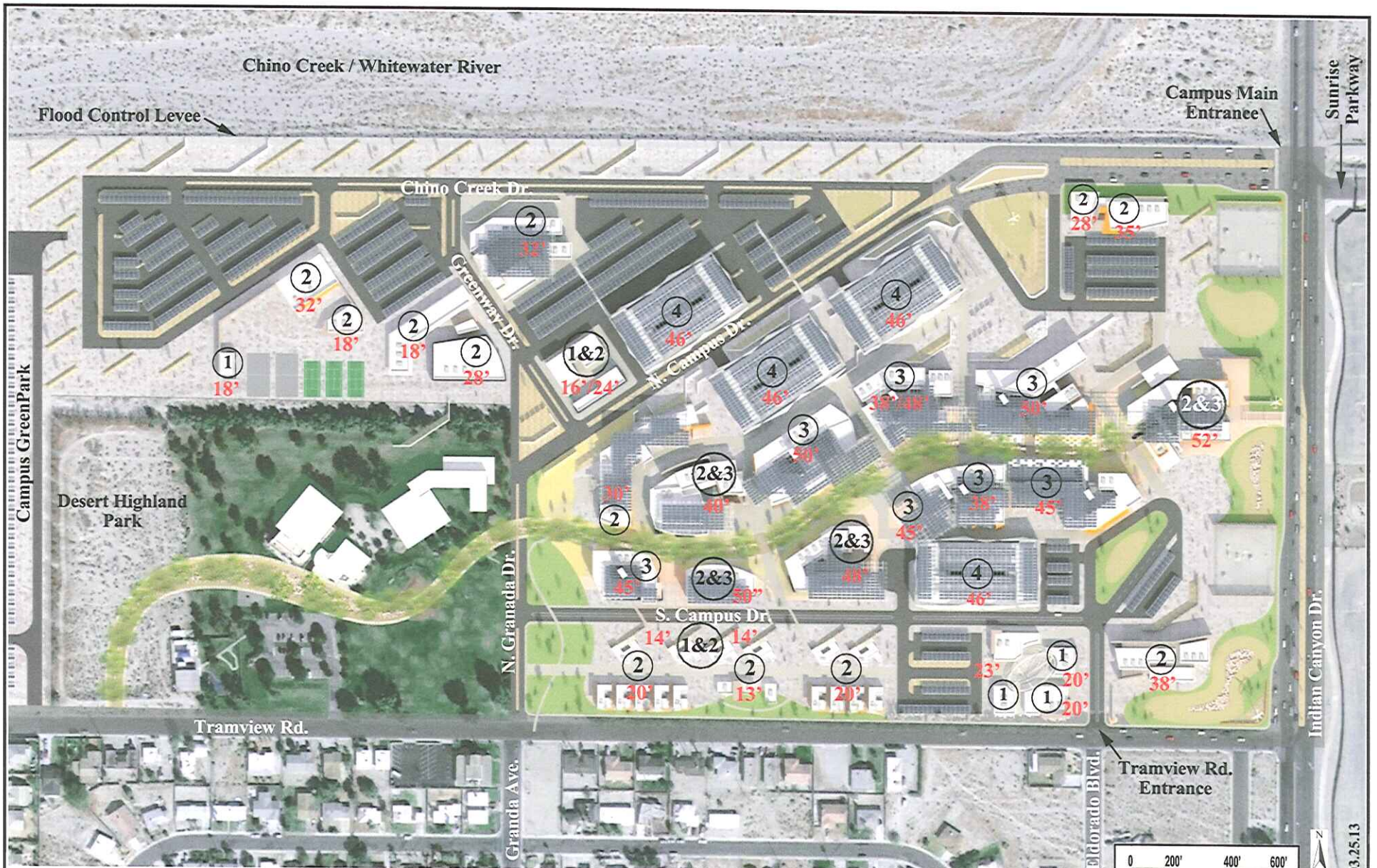






College of the Desert West Valley Campus  
 FAA Part 77 Airspace Drawing  
 Palm Springs, California





Source: HGA Architects & Engineers 2012



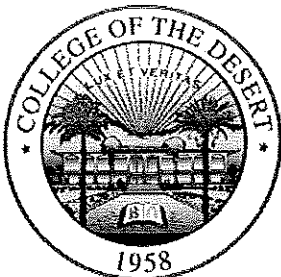












**College of the Desert  
West Valley Campus**

43500 Monterey Avenue  
Palm Desert, California 92260  
(760) 773-2511  
Fax (760) 773-2570

**CEQA Environmental Initial Study**

**Project Title:** College of the Desert / Desert Community College District  
West Valley Campus Facilities Master Plan and Phase I Project

**Case No:** COD WVC 003

**Lead Agency  
Name and Address:** College of the Desert  
43500 Monterey Avenue  
Palm Desert, California 92260  
(760) 773-2511

**Applicant:** College of the Desert/Desert Community College District

**Representative:** Terra Nova Planning & Research, Inc  
42635 Melanie Place, Suite 101  
Palm Desert, CA 92211  
760-341-4800

**Architect/Planner:** HGA Architects, et al  
1918 Main Street, Third Floor  
Santa Monica, CA 90405  
310-557-7600

**Contact Person:** John D. Criste, AICP  
**and Phone Number:** 760-341-4800

**I. PROJECT INFORMATION**

**Description of the Project:** The College of the Desert/Desert Community College District is proposing to construct a new West Valley Campus on approximately 119 acres owned by the District and located in the northern portion of the City of Palm Springs in the Coachella Valley area of Riverside County. In addition to the master planning for the West Valley Campus, the subject CEQA IS also evaluates the proposed Phase I project that will initiate development of the campus.

The campus master plan calls for a total of approximately 650,000 square feet to be constructed across five phases and will include core campus, academic partnership space and campus-related buildings. The core COD Campus will be comprised of 420,000± square feet of classroom, lecture, lab, administrative, and other support facilities to accommodate an enrollment of approximately 10,000 full-time-equivalent students. The Core Campus is intended to implement programs associated with the four pillars program described above. A variety of facilities are planned to support these curricula while achieving the College's goal of a sustainable campus. Several key campus buildings will be constructed to house classrooms and facilities associated with each of the four program pillars, and will also incorporate services to the campus community at large. On-campus housing is also planned.

In addition, site improvements will include areas for parking, open space, trails and fields. The following table was presented in Section I and is shown here to summarize the proposed land uses.



<b>COD West Valley Campus<sup>1</sup></b>	<b>Vacant AC</b>	<b>Developed AC</b>	<b>Total AC</b>	<b>Existing SF</b>	<b>Unbuilt SF</b>	<b>Total SF</b>
Core COD Campus/ Educational Partnership Uses			50±	0	420,000	420,000
Alternative Energy Park			10±	0	230,000	230,000
			60±	0		
<b>COD WVC Total</b>			<b>119.35</b>	<b>0</b>	<b>650,000</b>	<b>650,000</b>
<sup>1</sup> Campus acreage includes gross acreage. Approximately 3.26 acres will be used for right-of-way for future roadways. Gross acres include 1.21 acres of DWA well sites.						
<sup>2</sup> Based on core campus facilities of approximately 42 sf per student and a maximum buildout full-time equivalent (FTE) student population of 10,000.						
Source: Preliminary project description and conceptual development prepared by COD, May 2010.						

Other buildings and facilities will be constructed to house additional classroom and laboratory space, administrative offices, campus and grounds maintenance shops, and storage. Other uses will include support retail facilities, such as a bookstore, food court, copy center, convenience goods and services. The Core Campus will integrate renewable energy facilities, primarily photovoltaic (PV), on campus buildings and parking structures. Along with PV facilities envisioned elsewhere on the campus (see Campus-Related program, below), these PV installations are expected to generate a substantial portion, if not to exceed, the campus' electrical energy needs. The campus will be developed in five phases that will include academic facilities and those associated with public/private ventures planned for the campus and focused on the College's four academic pillars selected specifically to guide the development of this campus.

### Phase I Project

The Phase I COD WVC project is planned for development on the east side of the campus site and will take access from Tramview Road with secondary/emergency access from Sunrise Parkway (extended). The Phase I project will provide a total of 50,000± square feet. A total of 159 parking spaces will also be provided for the Phase I project. The space allocation planned for the Phase I project is as follows:

<b>Campus Use</b>	<b>Assigned Space (Sq. Ft.)</b>
Culinary Arts	6,669
Green Technology	6,143
Interdisciplinary Labs	2,866
Interdisciplinary Classrooms	3,640
Educational Labs	3,700
Library	1,100
Offices	1,237
Student Services	1,909
Food/Bookstore/PE	3,403

The Phase I project may also include the development of additional photovoltaic arrays north of the James O. Jessie Unity Community Center and Desert Highland Park. The array would also occupy lands in the eastern portion of the site and north of the "arroyo" design element, which roughly divides the eastern portion of the site north/south.

#### Campus GreenPark

The campus GreenPark is an approved 10 MW photovoltaic power facility being built by Southern California Edison (SCE) to be located on the western portion of the COD West Valley Campus site. The GreenPark may encompass approximately 60 acres. In addition to providing utility-scale solar electric production, the GreenPark may also provide facilities to support academic and technical training programs in renewable energy technologies demonstration, including test facilities for solar and wind technologies, and research and development (R&D) facilities. This project was approved by the City of Palm Springs and the compatibility of the proposed campus plan with the approved photovoltaic facility will be evaluated in the WVC EIR.

#### **Sustainable Design**

The COD West Valley Campus and Phase I project will implement a variety of sustainable design elements that are based on a "Five Zeros" principle; that is, net-zero off-site energy generation, net-zero carbon emissions, net-zero water consumption, net-zero waste generation and net zero pollutant use/emission. Specific design approaches will include ventilated wall systems (thermal mass) to manage heat gain and loss. Daylighting will be used to illuminate indoor space as much as possible, solar electric and thermal systems will be widely used, and rigorous application of water-conserving technologies and landscape design are intended to significantly reduce water demand.

Building features are to include low-flow fixtures to reduce water consumption; motion sensor lighting systems and optimized natural light for occupied space to reduce energy; air handling units with Demand Ventilation Controls to provide fresh air; and, green housekeeping products and procedures to complement the building's design. Construction waste management will be implemented during building construction. Post-construction, provision will be made to facilitate the collection of recyclable materials generated by students, faculty and administrative staff. Campus development will be designed to meet or be equivalent to LEED® Gold standards.

#### **Landscape Concept**

The landscape design and palette for the COD WVC and Phase I project is responsive to the City's climate and natural vegetation. While relative lush planting may be a part of courtyard and other enclosed landscape areas, the substantial majority of the site will be planed in native and non-invasive non-native desert and other drought tolerant vegetation. The landscape design also follows the College of the Desert Campus Standards Handbook guidelines for supporting landscape learning, including art in the landscape, creating outdoor spaces that respond to and provide shelter in the local climate, and which promote sustainable design and cost considerations.

The landscape of the site provides three layers of drought tolerant planting. The first layer will consist of decomposed granite, boulders, and low desert succulents to provide visibility throughout the site. The second layer will include a tree canopy that will provide shade for parking and pedestrian circulation. The third layer of the landscape design provides skyline planting of palm trees that will be visible from a distance, identifying the subject property as a unique place. The landscape scheme also provides clear and safe pedestrian access points on the north and south sides of the campus.

**Type of Project:**                      **Site Specific** X;              **Countywide** 0;              **Community** 0;              **Policy** 0  
Community College Campus Master Facilities Plan and Phase I Project

**Total Project Area:** 119± acres

**Assessor's Parcel No(s):** 669-330-029

**Street & Other References:** The project is bound by Indian Canyon Drive on the east, Tramview Road on the south, Riverside County Flood Control levee on the north and residential development on the west

**Section, Township, & Range Description or referenced/attach a Legal Description:** Subject property is located in a portion of Section 34, Township 3 South, Range 4 East, SBB&M.

**Brief description of the existing environmental setting of the project site and its surroundings:** The proposed COD West Valley Campus and Phase I project is located in the northern edge of the urbanized portion of the City of Palm Springs, on the valley floor in the western portion of the Coachella Valley. The foothills of the San Jacinto Mountains are located approximately one mile to the west. The Chino Creek Wash and Whitewater River flood plain are located to the immediate north and include lands in conservation under the Coachella Valley Multiple Species Habitat Conservation Plan. The site is located immediately south of the extensive windfarms development in the San Gorgonio Pass and extending east to Indian Canyon Drive. Two Desert Water Agency production wells are also located east of the project site and adjacent to Indian Canyon Drive. The James O. Jessie Desert Highland Community Center and Desert Highland Park are contiguous to the campus site, which will be developed to complement and integrate with the park

Surrounding land uses include:

**North:** Chino Creek/Whitewater River flood control levee and vacant lands within the MSHCP Whitewater Floodplain Conservation Area.

**East:** James O. Jessie Unity Center and Desert Highland Park and Indian Canyon Drive, beyond which is the partially developed Avalon planned residential community, with the Palermo residential community farther south and extending to San Rafael Drive.

**South:** Mountain Gate and Desert Highland residential neighborhoods comprised of single-family homes; Tramview Road

**West:** Mountain Gate residential neighborhood drainage facilities; vacant lands designated Open Space; farther west, North Palm Canyon Drive/State Highway 111, beyond which Chino Canyon alluvial cone and foothills of San Jacinto Mountains.

## II. APPLICABLE GENERAL PLAN LAND USE POLICIES AND ZONING

**Existing General Plan:** College Park Specific Plan and "School"

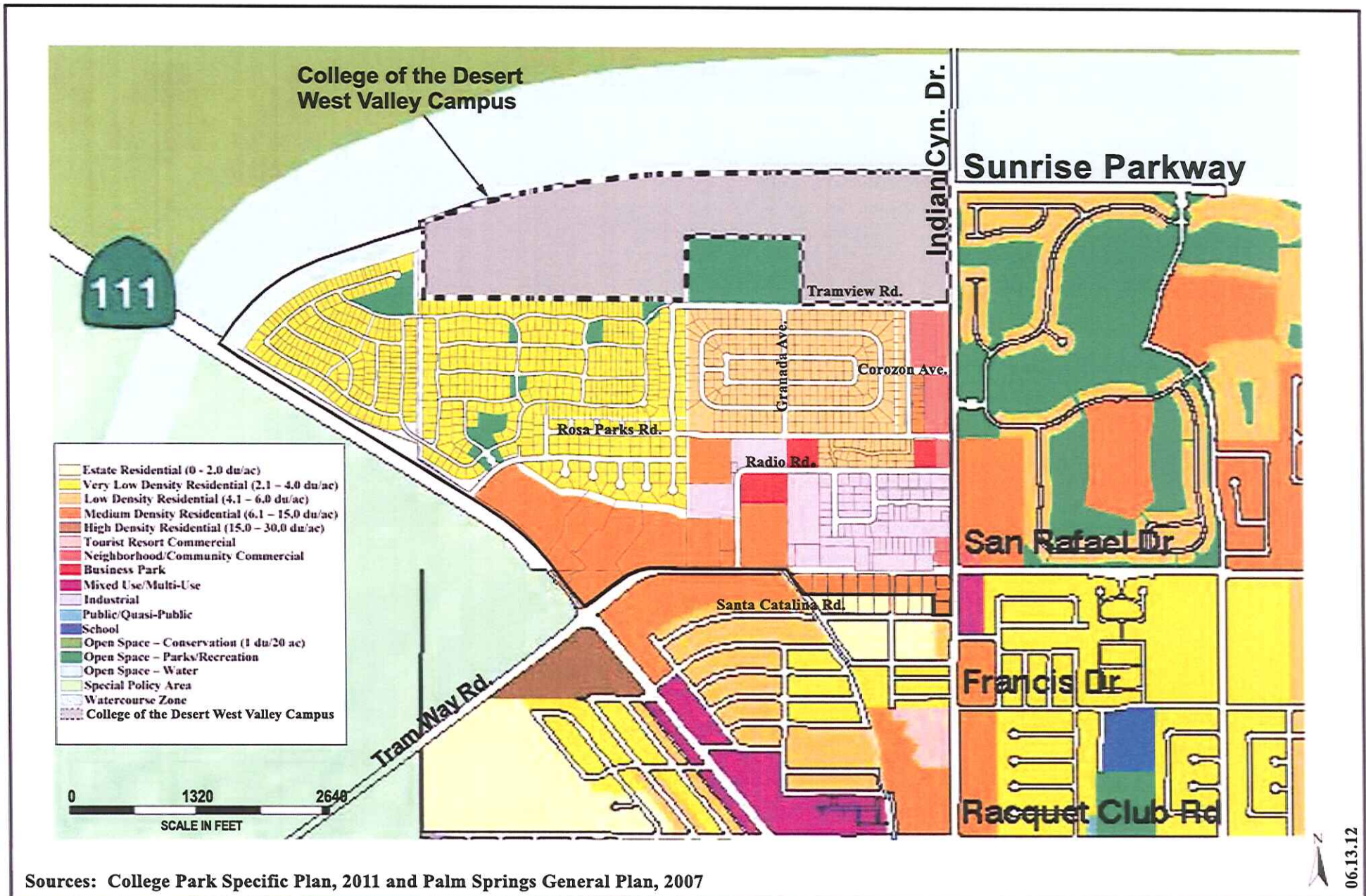
**Proposed General Plan:** No changes proposed

**Adjacent and Surrounding General Plan:** See College Park Specific Plan. To SW and S: R-1-C-SP, R-1-D-SP  
To East: R-1-C

**Existing Zoning:** C-SP (College Specific Plan)

**Proposed Zoning:** No change proposed.

**Adjacent and Surrounding Zoning:** North of the COD WVC site the land use and zoning designations are W: Water Course representing the Chino Wash/Whitewater River floodplain. Lands to the west include a narrow strip of vacant lands that extend to Highway 111 and are also designated W: Water Course. Lands to the west and south also include the built out Mountain Gate Community, which is designated R-1-C-SP: Residential Single Family Specific Plan (10,000 sf min.). Lands south of the eastern half of the campus site are comprised of the partially built out Desert Highland neighborhood with a zoning designation of R-1-D-Specific Plan (7,500 sf min.), which allows single family development. Lands to the east and across Indian Canyon Drive include the partially developed Avalon project, which is designated R-1-C: Single Family Residential (10,000 sf min.).

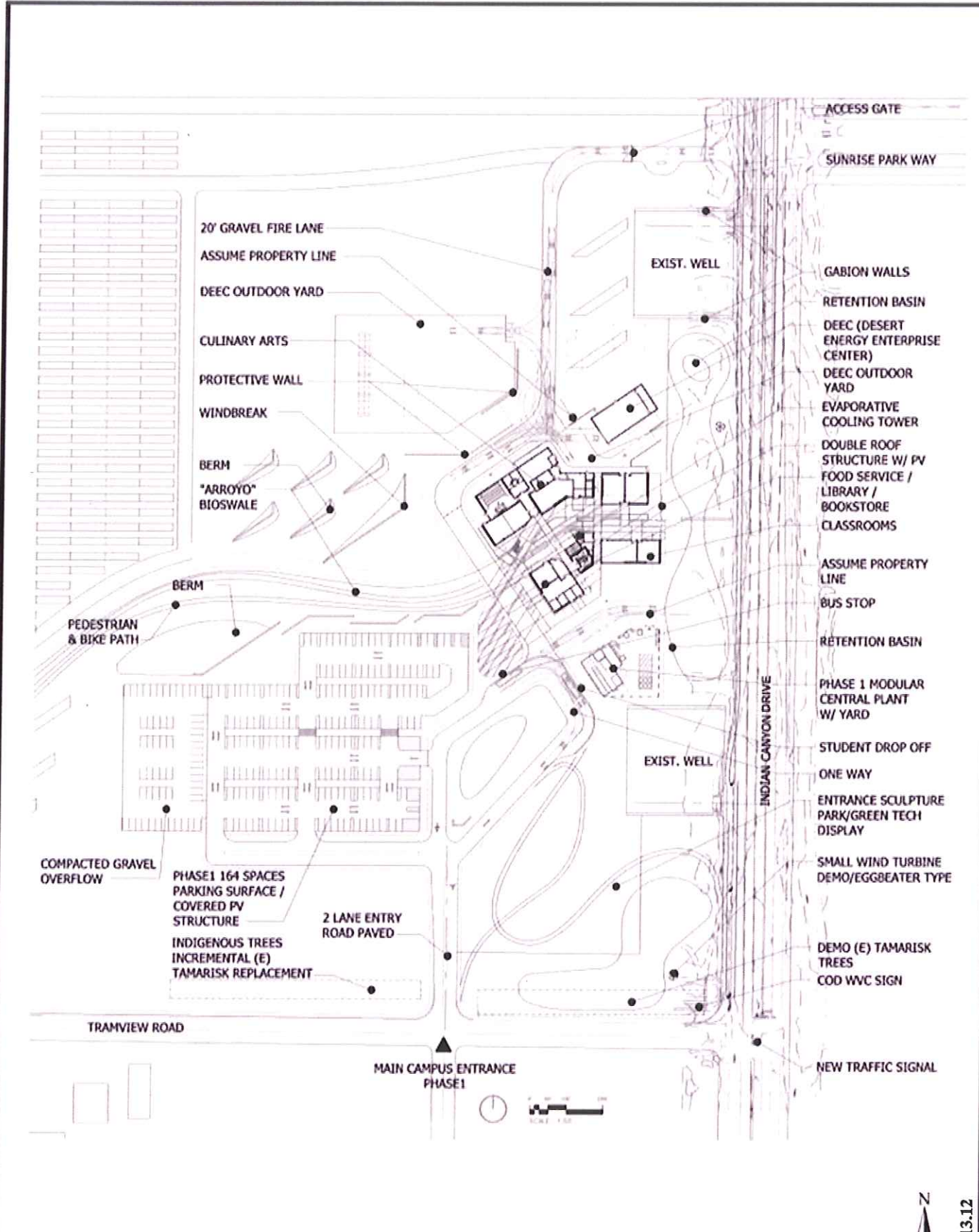






Source: Google, 2012





Source: HGA Architects, et. al. (2012)

06.13.12



**College of the Desert West Valley Campus  
Phase I Project Site Plan  
Palm Springs, California**

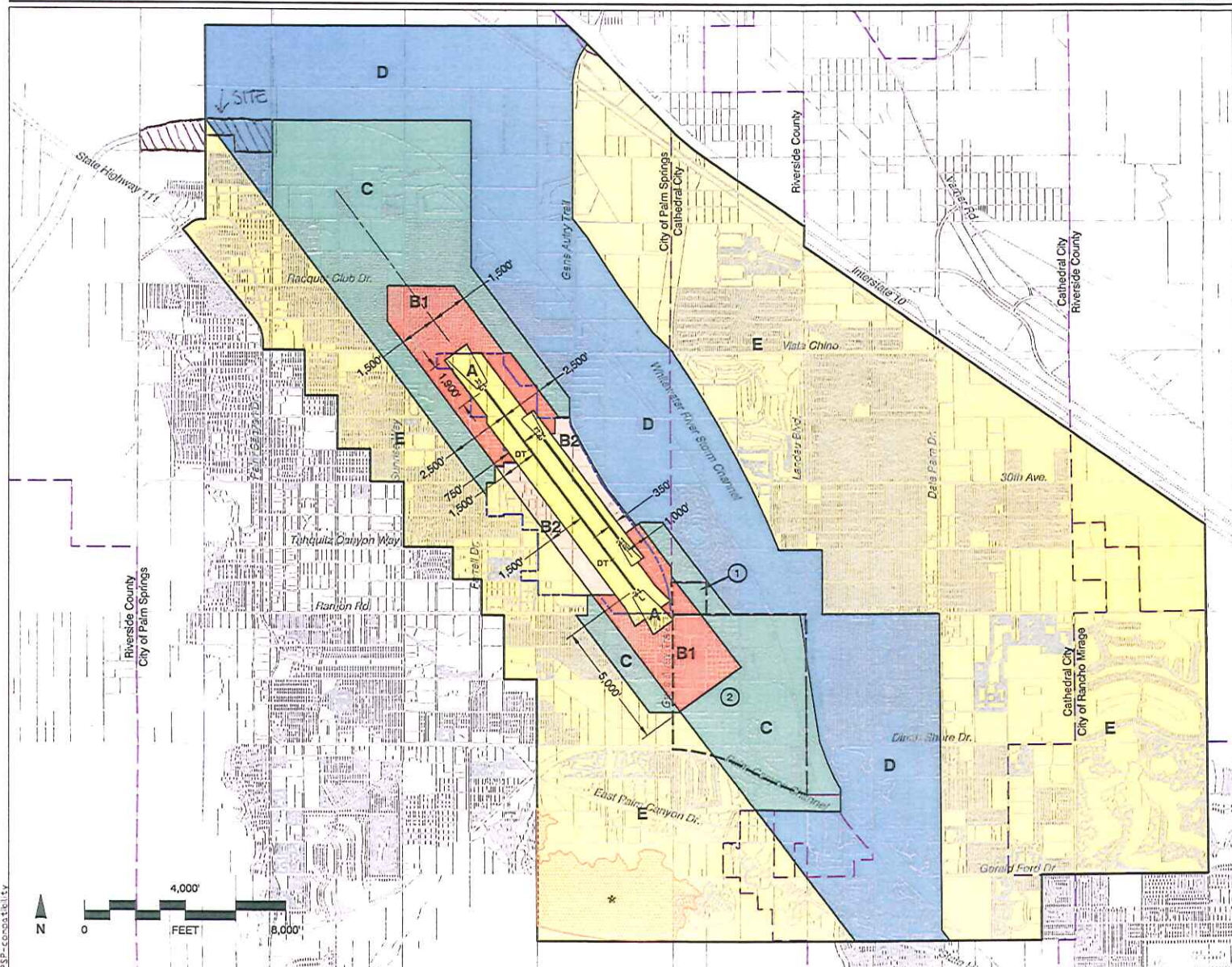


**Exhibit**  
**6**



# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



**Legend**

- Compatibility Zones**
- Airport Influence Area Boundary
  - Zone A
  - Zone B1
  - Zone B2
  - Zone C
  - Zone D
  - Zone E
  - ✪
 Height Review Overlay Zone

- Boundary Lines**
- Airport Property Line
  - City Limits

- Notes**
- All dimensions measured from runway ends and centerlines.
  - DT = Displaced Threshold
  - See Chapter 2, Table 2A for compatibility criteria associated with this map.
  - ✪ See Policy PS.2.1.

**Riverside County**  
**Airport Land Use Commission**  
**Riverside County**  
**Airport Land Use Compatibility Plan**  
**Policy Document**  
 (Adopted March 2005)

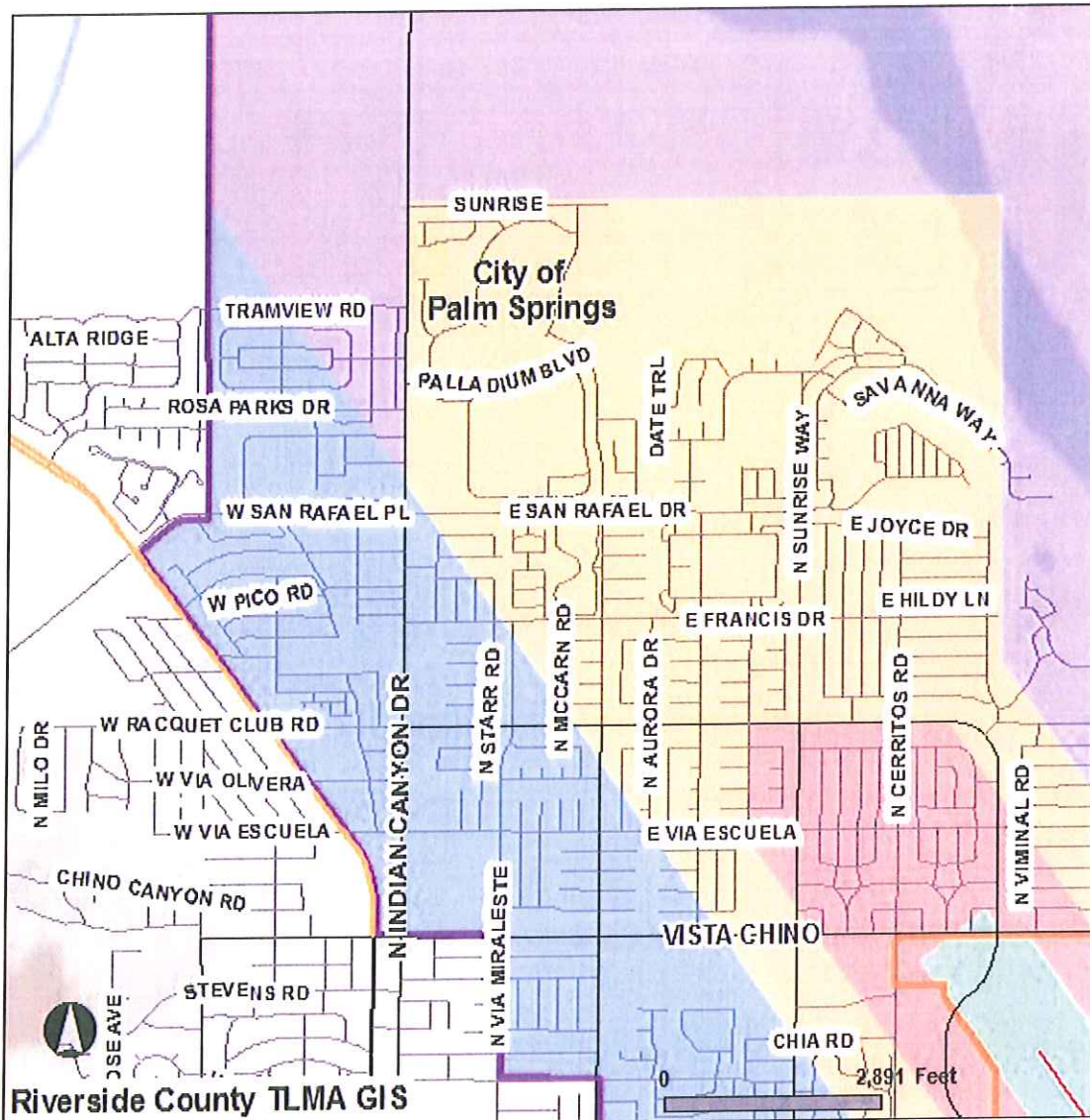
Map PS-1

**Compatibility Map**  
**Palm Springs International Airport**





RIVERSIDE COUNTY GIS



Selected parcel(s):  
669-330-047

**AIRPORTS**

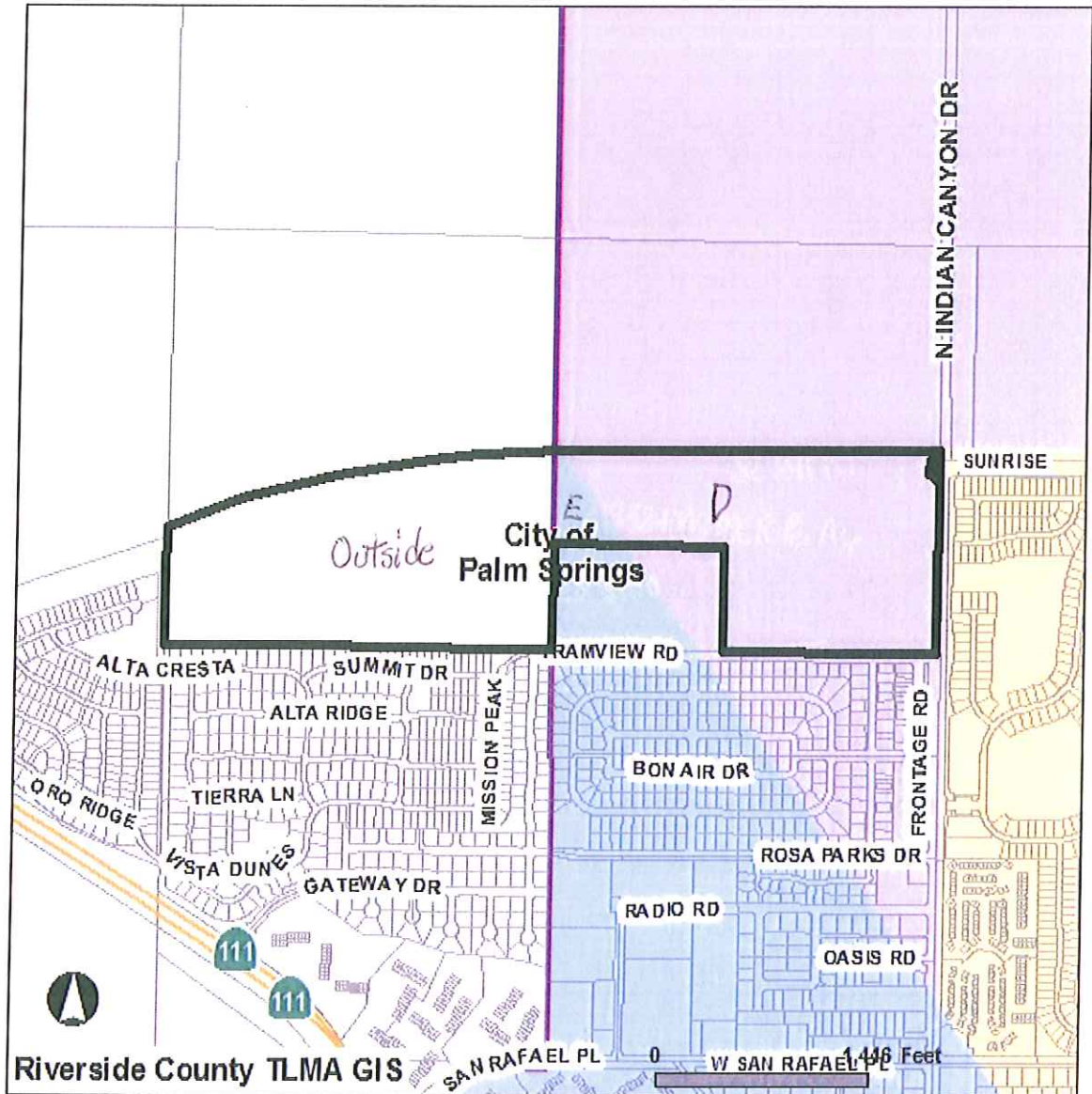
- |                         |                      |                      |                       |
|-------------------------|----------------------|----------------------|-----------------------|
| INTERSTATES             | HIGHWAYS             | STREETS              | AIRPORT RUNWAYS       |
| AIRPORT INFLUENCE AREAS | AIRPORT BOUNDARIES   | COMPATIBILITY ZONE A | COMPATIBILITY ZONE B1 |
| COMPATIBILITY ZONE C    | COMPATIBILITY ZONE D | COMPATIBILITY ZONE E |                       |

**\*IMPORTANT\***  
Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

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Version 121101

RIVERSIDE COUNTY GIS



Selected parcel(s):  
669-330-047

**AIRPORTS**

- SELECTED PARCEL
- N INTERSTATES
- N HIGHWAYS
- PARCELS
- AIRPORT INFLUENCE AREAS
- COMPATIBILITY ZONE C
- COMPATIBILITY ZONE D
- COMPATIBILITY ZONE E

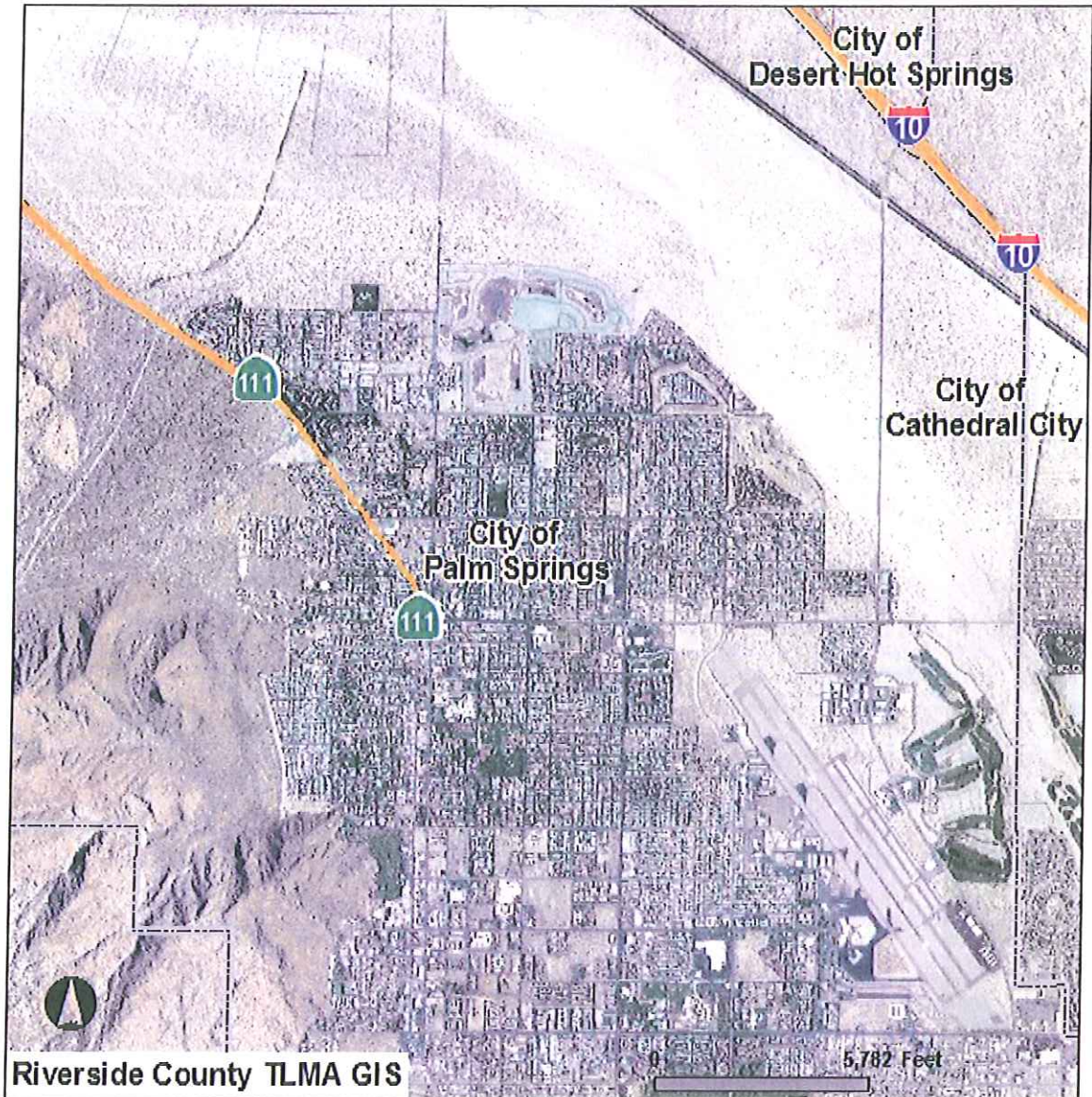
**\*IMPORTANT\***  
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Version 121101



RIVERSIDE COUNTY GIS



Selected parcel(s):  
669-330-047

**ELEVATION-CONTOUR**

INTERSTATES

HIGHWAYS

STREETS

**\*IMPORTANT\***

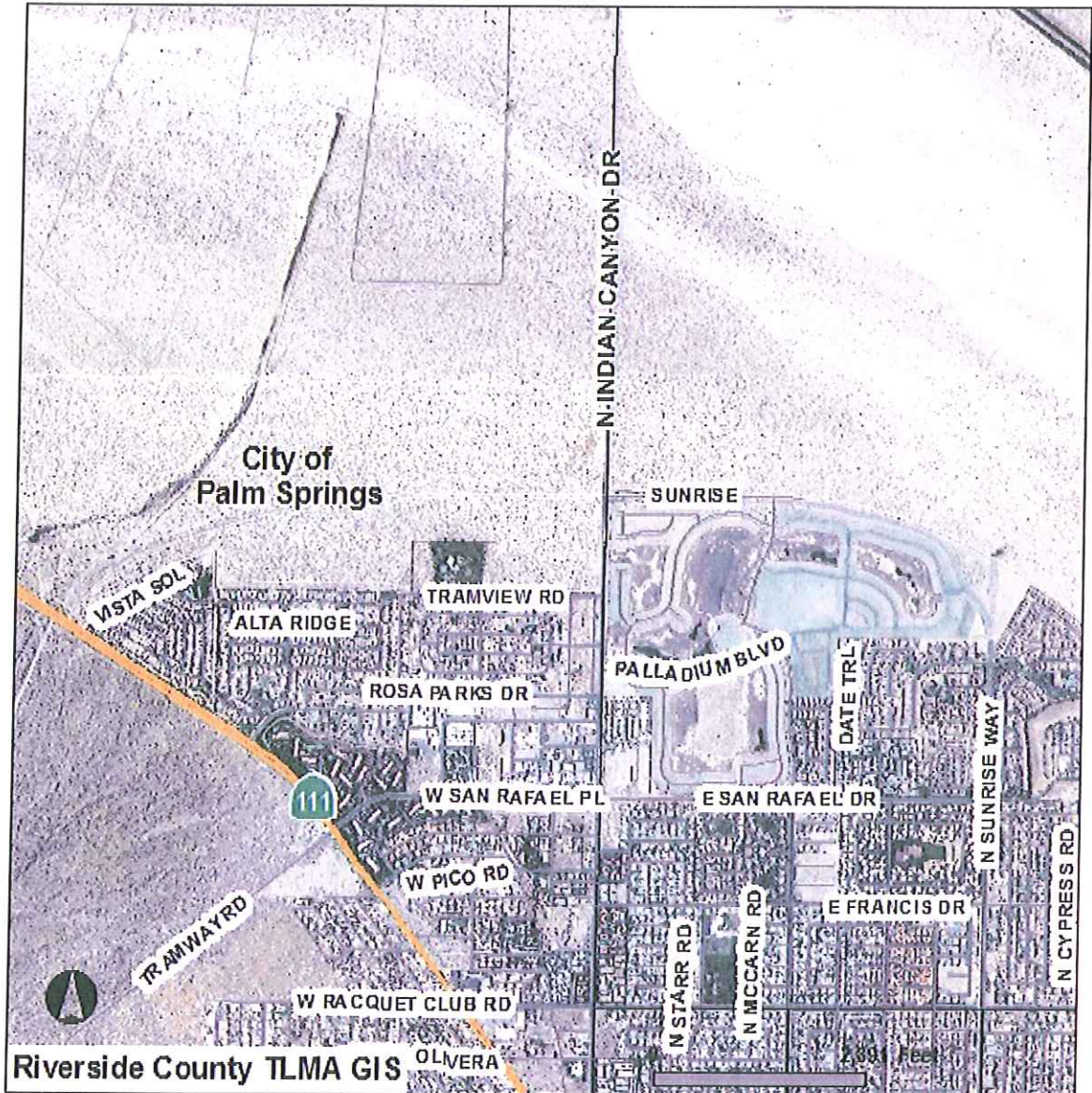
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Version 121101



RIVERSIDE COUNTY GIS



Selected parcel(s):  
669-330-047

ELEVATION-CONTOUR

 INTERSTATES

 HIGHWAYS

 STREETS

**\*IMPORTANT\***

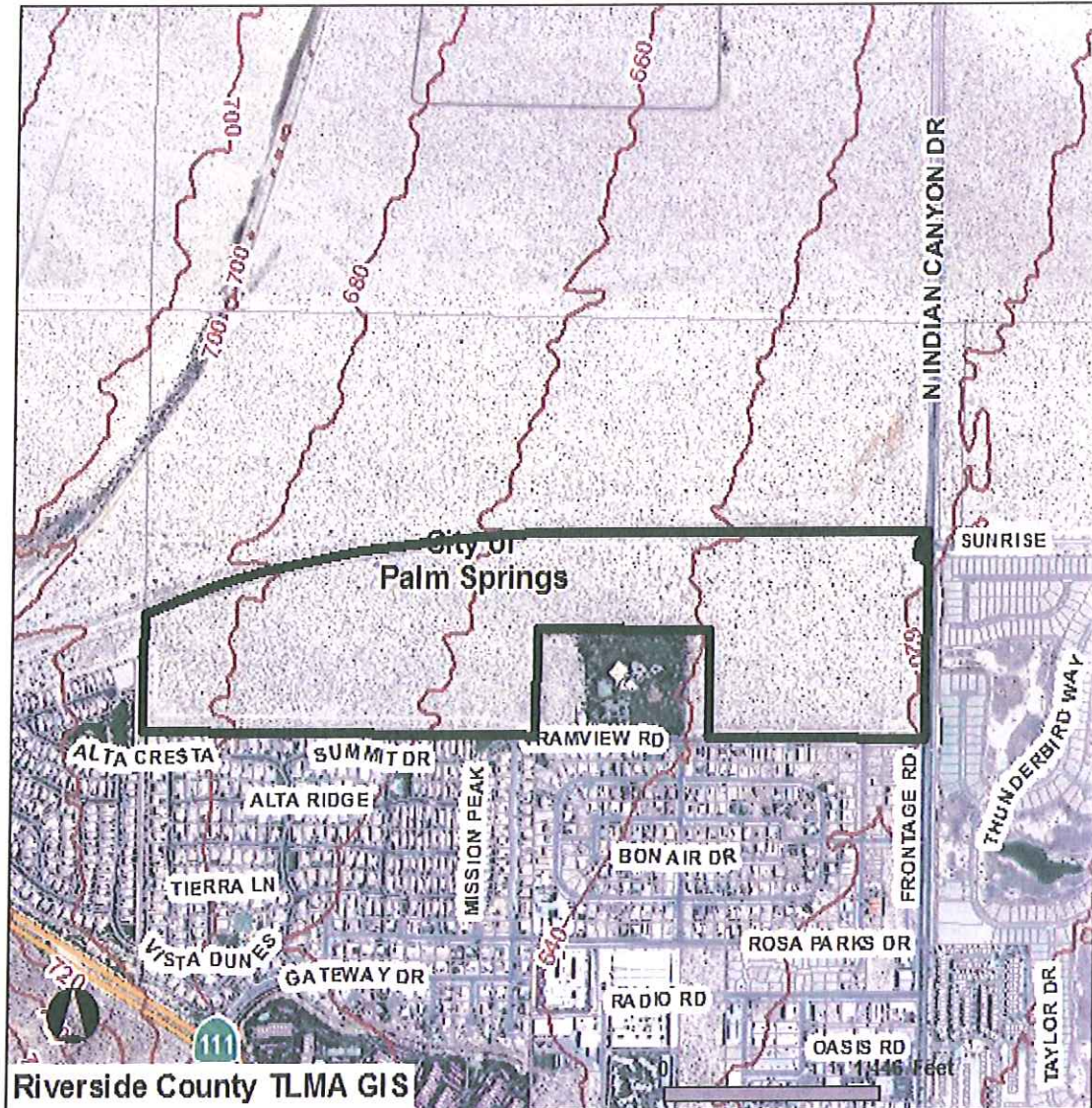
Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

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Version 121101




RIVERSIDE COUNTY GIS




Selected parcel(s):  
669-330-047

ELEVATION-CONTOUR

-  SELECTED PARCEL
-  PARCELS

 CONTOUR LINES

 INTERSTATES

 HIGHWAYS

**\*IMPORTANT\***

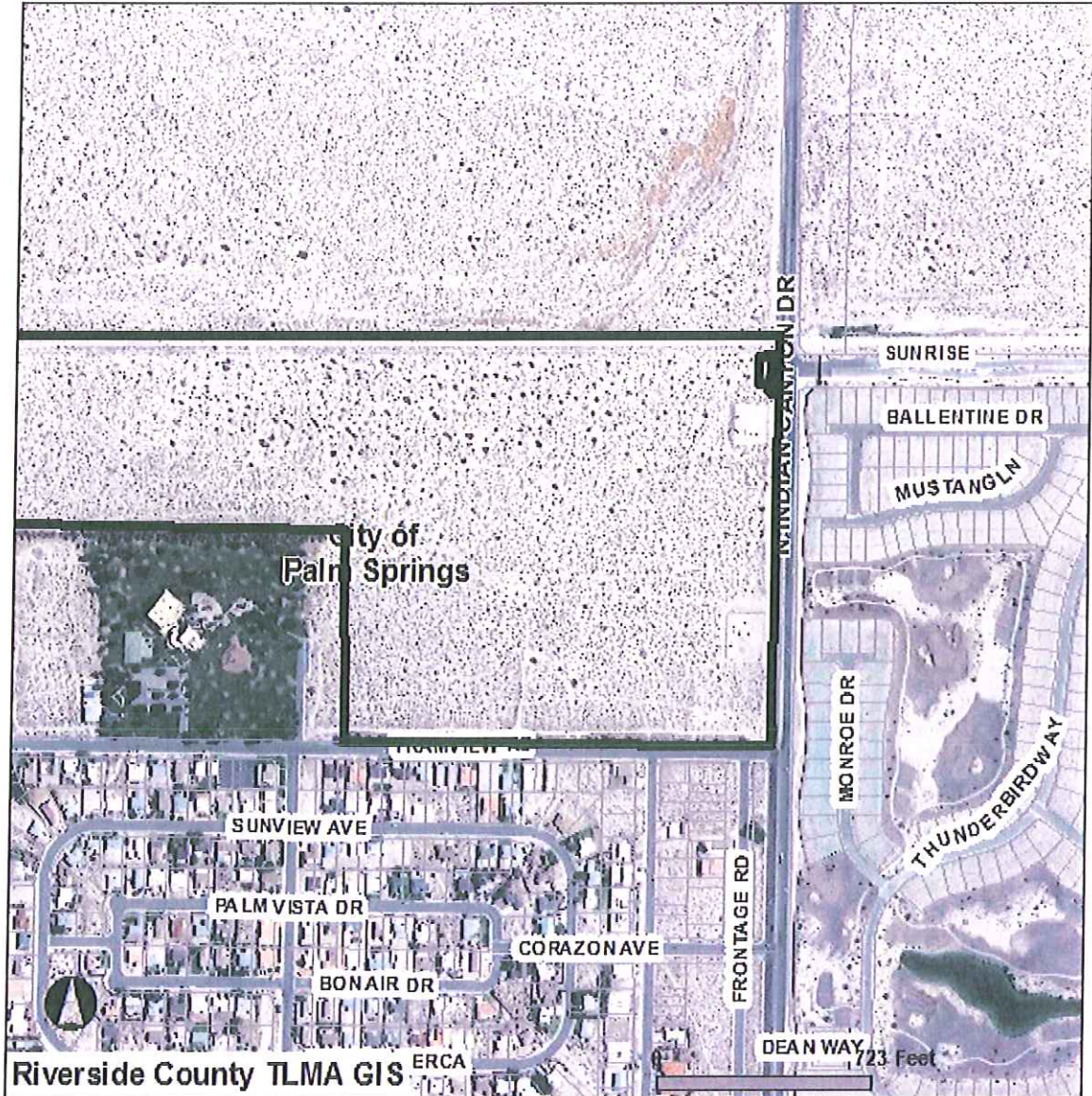
Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

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Version 121101



RIVERSIDE COUNTY GIS



Selected parcel(s):  
669-330-047

LEGEND

- SELECTED PARCEL
- N INTERSTATES
- H HIGHWAYS
- PARCELS
- CITY

**\*IMPORTANT\***  
 Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

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Version 121101





















COLLEGE OF THE DESERT  
WEST VALLEY CAMPUS  
4201 N. INDIAN CANYON DR.  
PALM SPRINGS, CA 92262

PHASE 1 BUILDING COMPLEX



HERRING CONSULTANTS, INC.  
1000 N. GARDEN ST., SUITE 100  
PALM SPRINGS, CA 92262  
TEL: 951.850.1000 FAX: 951.850.1001

Local/Architect	700,000,000
Structural	800,000,000
Mechanical/Plumbing/Electrical	800,000,000
Cost	700,000,000
Architect	700,000,000
Interior Design	700,000,000
Acoustic	700,000,000
Aviation/Travel/Transportation/Security	700,000,000
Code/Plan Preparation	700,000,000
Construction Management	700,000,000
Interior Design/Consulting	700,000,000
Structural/Structural Consulting	700,000,000
Transportation Consulting	700,000,000

REGISTRATION STAMP



DATE: 11/19/2012  
PROJECT: COLLEGE OF THE DESERT WEST VALLEY CAMPUS  
REGISTRATION NUMBER: 111100012

AGENCY APPROVAL

DIVISION OF THE STATE ARCHITECT	APPL #
DATE	DATE
IDENTIFICATION STAMP	

ISSUE

MARK	DATE	DESCRIPTION
HGA	COM. NO. 3108	
SCALE	As indicated	
DATE	11/19/2012	
DRAWN BY	R.E.	

KEY PLAN

MARK DATE DESCRIPTION  
HGA COM. NO. 3108  
SCALE As indicated  
DATE 11/19/2012  
DRAWN BY R.E.

SHEET TITLE  
OVERALL BUILDINGS - SECOND LEVEL FLOOR PLAN

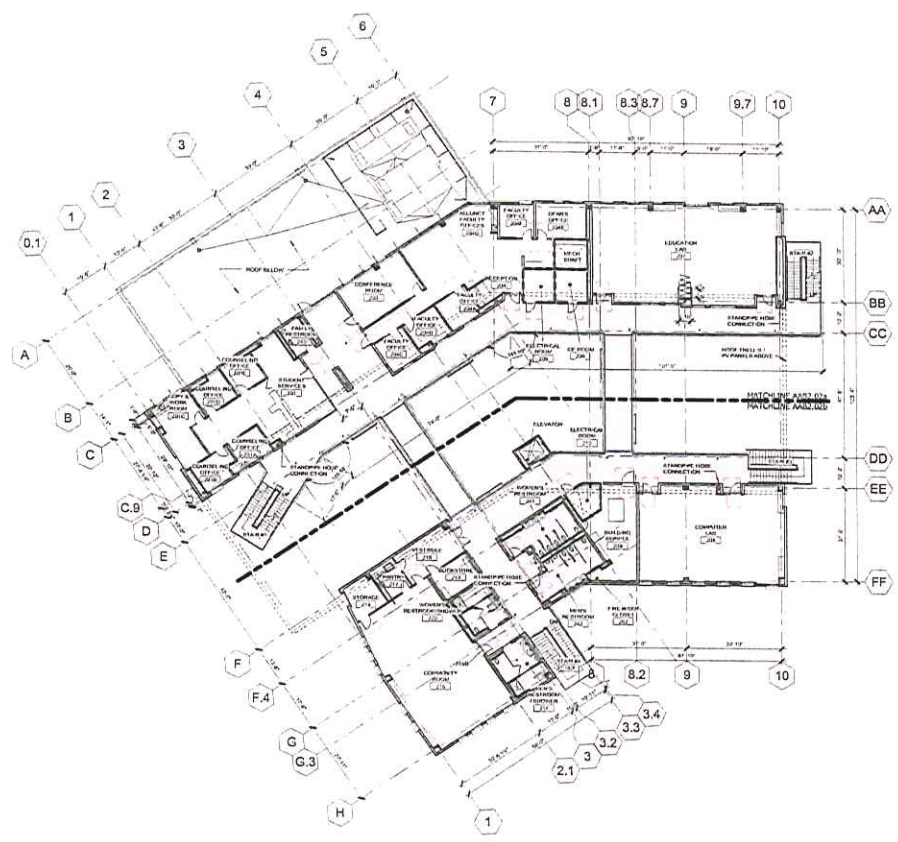
SHEET NUMBER  
AAB2.02

FLOOR PLAN LEGEND

- WALL NOTING
- 1/4" RATED CONSTRUCTION
  - RATING
- NOTES AND SYMBOLS
- FLOOR GRAIN PER PLUMBING DRAWINGS
  - EXTERIOR WALL TYPE, SEE SHEET A.A. FOR DETAILS
  - INTERIOR WALL TYPE, SEE SHEET A.A. FOR DETAILS
  - DOOR NUMBER, SEE A.A. FOR DOOR SCHEDULE
  - TOPPING SLAB

FLOOR PLAN KEY NOTES

- 1 -



FLOOR PLAN GENERAL NOTES

1. ALL WORK SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE CALIFORNIA BUILDING CODE AND ALL APPLICABLE LOCAL ORDINANCES.
2. ALL WORK SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE CALIFORNIA ELECTRICAL CODE AND ALL APPLICABLE LOCAL ORDINANCES.
3. ALL WORK SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE CALIFORNIA MECHANICAL CODE AND ALL APPLICABLE LOCAL ORDINANCES.
4. ALL WORK SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE CALIFORNIA PLUMBING CODE AND ALL APPLICABLE LOCAL ORDINANCES.
5. ALL WORK SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE CALIFORNIA FIRE AND LIFE SAFETY CODE AND ALL APPLICABLE LOCAL ORDINANCES.
6. ALL WORK SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE CALIFORNIA ACCESSIBILITY STANDARDS AND ALL APPLICABLE LOCAL ORDINANCES.
7. ALL WORK SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE CALIFORNIA ENVIRONMENTAL QUALITY ACT AND ALL APPLICABLE LOCAL ORDINANCES.
8. ALL WORK SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE CALIFORNIA HISTORIC PRESERVATION ACT AND ALL APPLICABLE LOCAL ORDINANCES.
9. ALL WORK SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE CALIFORNIA LAND USE ACT AND ALL APPLICABLE LOCAL ORDINANCES.
10. ALL WORK SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE CALIFORNIA NATURAL RESOURCES AND GENERAL DEVELOPMENT ACT AND ALL APPLICABLE LOCAL ORDINANCES.
11. ALL WORK SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE CALIFORNIA PUBLIC RESOURCES ACT AND ALL APPLICABLE LOCAL ORDINANCES.
12. ALL WORK SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE CALIFORNIA WATER RESOURCES ACT AND ALL APPLICABLE LOCAL ORDINANCES.
13. ALL WORK SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE CALIFORNIA WILDFIRE ACT AND ALL APPLICABLE LOCAL ORDINANCES.
14. ALL WORK SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE CALIFORNIA AIR RESOURCES ACT AND ALL APPLICABLE LOCAL ORDINANCES.
15. ALL WORK SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE CALIFORNIA CLIMATE CHANGE ACT AND ALL APPLICABLE LOCAL ORDINANCES.
16. ALL WORK SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE CALIFORNIA ENERGY EFFICIENCY ACT AND ALL APPLICABLE LOCAL ORDINANCES.
17. ALL WORK SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE CALIFORNIA GREEN BUILDING ACT AND ALL APPLICABLE LOCAL ORDINANCES.
18. ALL WORK SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE CALIFORNIA INFRASTRUCTURE ACT AND ALL APPLICABLE LOCAL ORDINANCES.
19. ALL WORK SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE CALIFORNIA TRANSPORTATION ACT AND ALL APPLICABLE LOCAL ORDINANCES.
20. ALL WORK SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE CALIFORNIA UTILITIES ACT AND ALL APPLICABLE LOCAL ORDINANCES.





Zone	Locations	Maximum Densities / Intensities				Additional Criteria		
		Residential (d.u./ac) <sup>1</sup>	Other Uses (people/ac) <sup>2</sup>			Req'd Open Land <sup>3</sup>	Prohibited Uses <sup>4</sup>	Other Development Conditions <sup>5</sup>
		Aver-	Single	with				
		age <sup>6</sup>	Acre <sup>7</sup>	Bonus <sup>8</sup>				
<b>A</b>	Runway Protection Zone and within Building Restriction Line	0	0	0	0	All Remaining	<ul style="list-style-type: none"> <li>› All structures except ones with location set by aeronautical function</li> <li>› Assemblages of people</li> <li>› Objects exceeding FAR Part 77 height limits</li> <li>› Storage of hazardous materials</li> <li>› Hazards to flight <sup>9</sup></li> </ul>	› Avigation easement dedication
<b>B1</b>	Inner Approach/Departure Zone	0.05 (average parcel size ≥20.0 ac.)	25	50	65	30%	<ul style="list-style-type: none"> <li>› Children's schools, day care centers, libraries</li> <li>› Hospitals, nursing homes</li> <li>› Places of worship</li> <li>› Bldgs with &gt;2 aboveground habitable floors</li> <li>› Highly noise-sensitive outdoor nonresidential uses <sup>10</sup></li> <li>› Aboveground bulk storage of hazardous materials <sup>11</sup></li> <li>› Critical community infrastructure facilities <sup>12</sup></li> <li>› Hazards to flight <sup>9</sup></li> </ul>	<ul style="list-style-type: none"> <li>› Locate structures maximum distance from extended runway centerline</li> <li>› Minimum NLR of 25 dB in residences (including mobile homes) and office buildings <sup>13</sup></li> <li>› Airspace review required for objects &gt;35 feet tall <sup>14</sup></li> <li>› Avigation easement dedication</li> </ul>
<b>B2</b>	Adjacent to Runway	0.1 (average parcel size ≥10.0 ac.)	100	200	260	No Req't	Same as Zone B1	<ul style="list-style-type: none"> <li>› Locate structures maximum distance from runway</li> <li>› Minimum NLR of 25 dB in residences (including mobile homes) and office buildings <sup>13</sup></li> <li>› Airspace review required for objects &gt;35 feet tall <sup>14</sup></li> <li>› Avigation easement dedication</li> </ul>
<b>C</b>	Extended Approach/Departure Zone	0.2 (average parcel size ≥5.0 ac.)	75	150	195	20%	<ul style="list-style-type: none"> <li>› Children's schools, day care centers, libraries</li> <li>› Hospitals, nursing homes</li> <li>› Bldgs with &gt;3 aboveground habitable floors</li> <li>› Highly noise-sensitive outdoor nonresidential uses <sup>10</sup></li> <li>› Hazards to flight <sup>9</sup></li> </ul>	<ul style="list-style-type: none"> <li>› Minimum NLR of 20 dB in residences (including mobile homes) and office buildings <sup>13</sup></li> <li>› Airspace review required for objects &gt;70 feet tall <sup>15</sup></li> <li>› Deed notice required</li> </ul>
<b>D</b>	Primary Traffic Patterns and Runway Buffer Area	(1) ≤0.2 (average parcel size ≥5.0 ac.) or <sup>16</sup> (2) ≥5.0 (average parcel size ≤0.2 ac.)	100	300	390	10%	<ul style="list-style-type: none"> <li>› Highly noise-sensitive outdoor nonresidential uses <sup>10</sup></li> <li>› Hazards to flight <sup>9</sup></li> </ul>	<ul style="list-style-type: none"> <li>› Airspace review required for objects &gt;70 feet tall <sup>15</sup></li> <li>› Children's schools, hospitals, nursing homes discouraged <sup>17</sup></li> <li>› Deed notice required</li> </ul>
<b>E</b>	Other Airport Environs	No Limit	No Limit <sup>18</sup>		No Req't	› Hazards to flight <sup>9</sup>	<ul style="list-style-type: none"> <li>› Airspace review required for objects &gt;100 feet tall <sup>15</sup></li> <li>› Major spectator-oriented sports stadiums, amphitheaters, concert halls discouraged beneath principal flight tracks <sup>18</sup></li> </ul>	
<b>*</b>	Height Review Overlay	Same as Underlying Compatibility Zone			Not Applicable	Same as Underlying Compatibility Zone	<ul style="list-style-type: none"> <li>› Airspace review required for objects &gt;35 feet tall <sup>14</sup></li> <li>› Avigation easement dedication</li> </ul>	

See Chapter 3 for airport-specific additions or exceptions to these policies

Table 2A

## Basic Compatibility Criteria

- 3.3.3. *Reconstruction:* An existing nonconforming development that has been fully or partially destroyed as the result of a calamity may be rebuilt only under the following conditions:
- (a) Nonconforming residential uses may be rebuilt provided that the expansion does not result in more dwelling units than existed on the parcel at the time of the damage.
  - (b) A nonconforming nonresidential development may be rebuilt provided that it has been only partially destroyed and that the reconstruction does not increase the floor area of the previous structure or result in an increased intensity of use (i.e., more people per acre). Partial destruction shall be considered to mean damage that can be repaired at a cost of no more than 75% of the assessor's full cash value of the structure at the time of the damage.
  - (c) Any nonresidential use that has been more than 75% destroyed must comply with all applicable standards herein when reconstructed.
  - (d) Reconstruction under Paragraphs (1) or (2) above must begin within 24 months of the date the damage occurred.
  - (e) The above exceptions do not apply within *Zone A* or where such reconstruction would be in conflict with a county or city general plan or zoning ordinance.
  - (f) Nothing in the above policies is intended to preclude work required for normal maintenance and repair.
- 3.3.4. *Development by Right:* Nothing in these policies prohibits:
- (a) Construction of a single-family home, including a second unit as defined by state law, on a legal lot of record if such use is permitted by local land use regulations.
  - (b) Construction of other types of uses if local government approvals qualify the development as effectively existing (see Policy 1.2.10 for definition).
  - (c) Lot line adjustments provided that new developable parcels would not be created and the resulting gross density or intensity of the affected property would not exceed the applicable criteria indicated in the Compatibility Criteria matrix, Table 2A.
- 3.3.5. *Parcels Lying within Two or More Compatibility Zones:* For the purposes of evaluating consistency with the compatibility criteria set forth herein, any parcel that is split by compatibility zone boundaries shall be considered as if it were multiple parcels divided at the compatibility zone boundary line. However, the density or intensity of development allowed within the more restricted portion of the parcel can (and is encouraged to) be transferred to the less restricted portion. This transfer of development is permitted even if the resulting density or intensity in the less restricted area would then exceed the limits which would otherwise apply within that compatibility zone.
- 3.3.6. *Other Special Conditions:* The compatibility criteria set forth in this *Plan* are intended to be applicable to all locations within each airport's influence area. However, it is recognized that there may be specific situations where a normally incompatible use can be considered compatible because of terrain, specific location, or other extraordinary factors or circumstances related to the site.



- (a) After due consideration of all the factors involved in such situations, the Commission may find a normally incompatible use to be acceptable.
- (b) In reaching such a decision, the Commission shall make specific findings as to why the exception is being made and that the land use will not create a safety hazard to people on the ground or aircraft in flight nor result in excessive noise exposure for the proposed use. Findings also shall be made as to the nature of the extraordinary circumstances that warrant the policy exception.
- (c) The burden for demonstrating that special conditions apply to a particular development proposal rests with the project proponent and/or the referring agency, not with the ALUC.
- (d) The granting of a special conditions exception shall be considered site specific and shall not be generalized to include other sites.
- (e) Special conditions that warrant general application in all or part of the influence area of one airport, but not at other airports, are set forth in Chapter 3 of this *Compatibility Plan*.

## 4. SUPPORTING COMPATIBILITY CRITERIA

### 4.1. Noise

- 4.1.1. *Policy Objective:* The purpose of noise compatibility policies is to avoid establishment of noise-sensitive land uses in the portions of airport environs that are exposed to significant levels of aircraft noise.
- 4.1.2. *Noise Contours:* The evaluation of airport/land use noise compatibility shall consider both the current and future Community Noise Equivalent Level (CNEL) contours of each airport as depicted in Chapter 3 of this *Plan*.
  - (a) At most airports in the county, anticipated growth in aircraft operations results in projected future noise contours being larger than current ones. However, in some instances, factors such as introduction of a quieter aircraft fleet mix, planned changes to the configuration of airport runways, or expected modifications to flight procedures can result in current contours being larger than the future contours in some or all of the airport environs. In these cases, a composite of the contours for the two time frames shall be considered in compatibility analyses.
  - (b) For airport at which aircraft activity has substantial seasonal or weekly characteristics, noise contours associated with the peak operating season or days of the week shall be taken into account in assessing land use compatibility.
  - (c) Projected noise contours included in Chapter 3 are calculated based upon forecasted aircraft activity as indicated in an airport master plan or that is considered by the Riverside County Airport Land Use Commission to be plausible (refer to activity data in the Background Data volumes). The Airport Land Use Commission or the entities that operate airports in Riverside County should periodically review these projected noise level contours and update them if appropriate.

# NOTICE OF PUBLIC HEARING

## RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the application described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. The proposed project application may be viewed at the Riverside County Administrative Center, 4080 Lemon Street, 14<sup>th</sup> Floor, Riverside, California 92501, Monday through Thursday from 8:00 a.m. to 5:00 p.m.

PLACE OF HEARING: Riverside County Administration Center  
4080 Lemon St., 1<sup>st</sup> Floor Hearing Room  
Riverside, California

DATE OF HEARING: March 14, 2013

TIME OF HEARING: 9:00 A.M.

### CASE DESCRIPTION:

ZAP1011PS13 – Desert Community College District (Representative: John Criste, Terra Nova Planning & Research, Inc.). A proposal to establish a West Valley campus of College of the Desert (COD) on 115-119 acres located westerly of Indian Canyon Drive, northerly of Tramview Road, and southerly of the Whitewater River in the City of Palm Springs. The campus would ultimately include multiple buildings with a total gross floor area up to 654,000 square feet, and a solar electric generation facility westerly of the building area. The site is located partially within Compatibility Zones D and E of the Palm Springs International Airport Influence Area and partially outside the Airport Influence Area; however, all of the buildings would be located within Compatibility Zone D.

FURTHER INFORMATION: Contact John Guerin at (951) 955-0982 or Russell Brady at (951) 955-0549. The ALUC holds hearings for local discretionary permits within the Airport Influence Areas, reviewing for aeronautical safety, noise and obstructions. All other concerns should be addressed to the Desert Community College District at (760) 773-2511.



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Us Dept of Interior  
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669 381 009  
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222 W Avenida Cerca  
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555 N Commercial Rd #10  
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669 382 008  
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669 401 003  
Paul Henderson  
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Castaic, CA 91384

669 401 004  
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345 W Tramview Rd  
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669 401 005  
New Bethel Church of God In Christ  
303 W Tramview Rd  
Palm Springs, CA 92262

669 401 006  
New Bethel Church of God In Christ  
303 W Tramview Rd  
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669 401 007  
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669 401 008  
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669 401 009  
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669 401 010  
Redev Agcy of Palm Spgs-Community  
NO STREET NAME or NUMBER  
Palm Springs, CA 90044

669 401 014  
Owner Record  
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669 401 021  
Lillian Mae Young  
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669 401 022  
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669 401 023  
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669 401 027  
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669 401 033  
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669 401 040  
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669 580 054  
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669 640 013  
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669 640 047  
C Malcolm Riggle  
1087 Vista Sol  
Palm Springs, CA 92262

669 640 048  
Jacquelyn Rae Hill  
1075 Vista Sol  
Palm Springs, CA 92262

669 640 050  
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669 740 018  
Lv Palm Springs Village LLC  
172 Ballentine Dr  
Palm Springs, CA 92262

669 740 019  
Lv Palm Springs Village LLC  
154 Ballentine Dr  
Palm Springs, CA 92262

669 740 020  
Lv Palm Springs Village LLC  
136 Ballentine Dr  
Palm Springs, CA 92262

669 740 021  
Lv Palm Springs Village LLC  
118 Ballentine Dr  
Palm Springs, CA 92262

669 740 022  
Lv Palm Springs Village LLC  
100 Ballentine Dr  
Palm Springs, CA 92262

669 740 023  
Lv Palm Springs Village LLC  
4469 Rambler Ln  
Palm Springs, CA 92262

669 740 024  
Lv Palm Springs Village LLC  
4443 Rambler Ln  
Palm Springs, CA 92262

669 740 025  
Lv Palm Springs Village LLC  
4467 Rambler Ln  
Palm Springs, CA 92262

669 740 026  
Lv Palm Springs Village LLC  
4391 Rambler Ln  
Palm Springs, CA 92262

669 740 027  
Lv Palm Springs Village LLC  
4365 Rambler Ln  
Palm Springs, CA 92262

669 740 028  
Lv Palm Springs Village LLC  
167 Ballentine Dr  
Palm Springs, CA 92262

669 740 029  
Lv Palm Springs Village LLC  
183 Ballentine Dr  
Palm Springs, CA 92262

669 740 048  
Lv Palm Springs Village LLC  
184 Mustang Ln  
Palm Springs, CA 92262

669 740 049  
Lv Palm Springs Village LLC  
168 Mustang Ln  
Palm Springs, CA 92262

669 740 050  
Lv Palm Springs Village LLC  
103 Mustang Ln  
Palm Springs, CA 92262

669 740 051  
Lv Palm Springs Village LLC  
121 Mustang Ln  
Palm Springs, CA 92262

669 740 052  
Lv Palm Springs Village LLC  
139 Mustang Ln  
Palm Springs, CA 92262

669 740 053  
Lv Palm Springs Village LLC  
157 Mustang Ln  
Palm Springs, CA 92262

669 740 054  
Lv Palm Springs Village LLC  
175 Mustang Ln  
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669 750 013  
Lv Palm Springs Village LLC  
4010 Monroe Dr  
Palm Springs, CA 92262

669 750 014  
Lv Palm Springs Village LLC  
4036 Monroe Dr  
Palm Springs, CA 92262

669 750 015  
Lv Palm Springs Village LLC  
4062 Monroe Dr  
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669 750 016  
Lv Palm Springs Village LLC  
4088 Monroe Dr  
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669 750 017  
Lv Palm Springs Village LLC  
4114 Monroe Dr  
Palm Springs, CA 92262

669 750 018  
Lv Palm Springs Village LLC  
4140 Monroe Dr  
Palm Springs, CA 92262

669 750 019  
Lv Palm Springs Village LLC  
4166 Monroe Dr  
Palm Springs, CA 92262

669 750 020  
Lv Palm Springs Village LLC  
4192 Monroe Dr  
Palm Springs, CA 92262

669 750 021  
Lv Palm Springs Village LLC  
4219 Monroe Dr  
Palm Springs, CA 92262

669 750 022  
Lv Palm Springs Village LLC  
176 Starfire Pl  
Palm Springs, CA 92262

669 750 023  
Lv Palm Springs Village LLC  
158 Starfire Pl  
Palm Springs, CA 92262

669 750 024  
Lv Palm Springs Village LLC  
140 Starfire Pl  
Palm Springs, CA 92262

669 750 025  
Lv Palm Springs Village LLC  
120 Starfire Pl  
Palm Springs, CA 92262

669 750 026  
Lv Palm Springs Village LLC  
102 Starfire Pl  
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669 750 027  
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4235 Monroe Dr  
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669 750 029  
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4183 Monroe Dr  
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669 750 030  
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4157 Monroe Dr  
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669 750 031  
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669 750 032  
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Palm Springs, CA 92262

669 750 033  
Lv Palm Springs Village LLC  
4079 Monroe Dr  
Palm Springs, CA 92262

669 750 034  
Lv Palm Springs Village LLC  
4053 Monroe Dr  
Palm Springs, CA 92262

669 750 035  
Lv Palm Springs Village LLC  
4027 Monroe Dr  
Palm Springs, CA 92262

669 750 036  
Lv Palm Springs Village LLC  
4001 Monroe Dr  
Palm Springs, CA 92262



Mr. Mac McGinnis  
Director of Bond Services  
College of the Desert  
43-500 Monterey Avenue  
Palm Desert, CA 92260

Steve Renew  
Director of Facilities Services  
College of the Desert  
43-500 Monterey Avenue  
Palm Desert, CA 92260

Pamela Pence  
Associate Program Manager  
College of the Desert  
Bond Management Office  
43-500 Monterey Avenue  
Palm Desert, CA 92260

John D. Criste, AICP  
Terra Nova Planning & Research, Inc.  
42635 Melanie Place  
Suite 101  
Palm Desert, CA 92211

**APPLICATION FOR MAJOR LAND USE ACTION REVIEW**  
**RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION**

ALUC Identification No.

ZAP 1011 PS13

**PROJECT PROPONENT (TO BE COMPLETED BY APPLICANT)**

Date of Application January 25, 2013  
 Property Owner Desert Community College District Phone Number 760-773-2511  
 Mailing Address 43-500 Monterey Avenue  
Palm Desert, CA 92260

Agent (if any) John D. Criste, AICP Phone Number 760-341-4800  
 Mailing Address Terra Nova Planning & Research, Inc.  
42635 Melanie Place, Suite 101  
Palm Desert, CA 92211

**PROJECT LOCATION (TO BE COMPLETED BY APPLICANT)**

*Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways*

Street Address NW corner of Indian Canyon Dr. & Tramview Road in the city of Palm Springs  
 Assessor's Parcel No. 669-330-047 Parcel Size 119 ± acres  
 Subdivision Name College of the Desert West Valley Campus Zoning  
 Lot Number \_\_\_\_\_ Classification School

**PROJECT DESCRIPTION (TO BE COMPLETED BY APPLICANT)**

*If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed*

Existing Land Use (describe) vacant, two public agency well sites

Proposed Land Use (describe) Community college campus with a total of up to 654,000 ± square feet.

For Residential Uses Number of Parcels or Units on Site (exclude secondary units) N/A

For Other Land Uses Hours of Use 7 AM - 10 PM

(See Appendix C) Number of People on Site Maximum Number (33/ac avg.)  
 Method of Calculation Parking Space Requirements

Height Data Height above Ground or Tallest Object (including antennas and trees) 18 - 60 feet ft.  
 Highest Elevation (above sea level) of Any Object or Terrain on Site 663 ± ft.

Flight Hazards Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?  Yes  
 No

If yes, describe Solar panels use non-glare coatings; no broadcast facilities planned. Tallest buildings 60 ± or less.

P5

**REFERRING AGENCY (TO BE COMPLETED BY AGENCY STAFF)**

Date Received	<u>2-11-13</u>	Type of Project	<input type="checkbox"/> General Plan Amendment
Agency Name	<u>Desert Community College District</u>	<input type="checkbox"/> Zoning Amendment or Variance	<input type="checkbox"/> Subdivision Approval
Staff Contact	<u>John Criste ?</u>	<input type="checkbox"/> Use Permit	<input type="checkbox"/> Public Facility
Phone Number	<u>(760) 311-4800</u>	<input type="checkbox"/> Other	
Agency's Project No.	<u>Specific Plan Review</u>		

**ALUC REVIEW (TO BE COMPLETED BY ALUC EXECUTIVE DIRECTOR)**

Application Receipt	Date Received _____	By _____
	Is Application Complete? <input type="checkbox"/> Yes <input type="checkbox"/> No	
	If No, cite reasons _____	

Airport(s) Nearby	_____								
Primary Criteria Review	Compatibility Zone(s)	<input type="checkbox"/> A	<input type="checkbox"/> B1	<input type="checkbox"/> B2	<input type="checkbox"/> C	<input type="checkbox"/> D	<input type="checkbox"/> E	<input type="checkbox"/> Ht.	
	Allowable (not prohibited) Use?	<input type="checkbox"/> Yes	<input type="checkbox"/> No	_____					
	Density/Intensity Acceptable?	<input type="checkbox"/> Yes	<input type="checkbox"/> No	_____					
	Open Land Requirement Met?	<input type="checkbox"/> Yes	<input type="checkbox"/> No	_____					
	Height Acceptable?	<input type="checkbox"/> Yes	<input type="checkbox"/> No	_____					
	Easement/Deed Notice Provided?	<input type="checkbox"/> Yes	<input type="checkbox"/> No	_____					

Special Conditions	Describe: _____
	_____

Supplemental Criteria Review	Noise	_____
	Safety	_____
	Airspace Protection	_____
	Overflight	_____

**ACTIONS TAKEN (TO BE COMPLETED BY ALUC EXECUTIVE DIRECTOR)**

ALUC Executive Director's Action	<input type="checkbox"/> Approve	Date _____
	<input type="checkbox"/> Refer to ALUC	
ALUC Action	<input type="checkbox"/> Consistent	Date _____
	<input type="checkbox"/> Consistent with Conditions (list conditions/attach additional pages if needed)	_____
	<input type="checkbox"/> Inconsistent (list reasons/attach additional pages if needed)	_____





# TRANSMITTAL


## TERRA NOVA PLANNING & RESEARCH, INC.<sup>®</sup>

42635 Melanie Place, Suite 101  
Palm Desert, CA. 92211  
Phone: (760) 341-4800  
FAX#: (760) 341-4455  
E-mail: [kcuza@terranovaplanning.com](mailto:kcuza@terranovaplanning.com)

**DATE:** February 1, 2013

**Ontrac Overnight**

**TO:** John Guerin  
Airport Land Use Commission  
4080 Lemon, 14<sup>th</sup> Floor  
Riverside, CA 92501

**FROM:** Kimberly Cuza   
Administrative Assistant  
John D. Criste, AICP

**RE:** College of the Desert West Valley Campus - Master Plan Review

Per the email dated January 29, 2012, from John Criste to you, enclosed please find:

- APLUC Application for Major Land Use Action Review form (previously electronically submitted by email on 1/31/13)
- Check in the amount of \$2,911 for the Specific Plan Review
- Site Plan Overview (Sheet A0.19) 11x17 and 8 ½ x 11
- Overall Buildings - First Level Floor Plan (Sheet AAB2.01) 11x17 and 8 ½ x 11
- Overall Buildings - Second Level Floor Plan (Sheet AAB2.02) 11x17 and 8 ½ x 11
- Overall Buildings - Third Level Floor Plan (Sheet AAB2.03) 11x17 and 8 ½ x 11
- North and South Buildings - Roof Plan (Sheet AAB2.04) 11x17 and 8 ½ x 11
- North Building - Exterior Elevations (Sheet AAB3.01) 11x17 and 8 ½ x 11
- South Building - Exterior Elevations (Sheet AAB3.03) 11x17 and 8 ½ x 11
- Facilities Master Plan & Phase 1 Project (Academic Area) 8 ½ x 11
- 4 sets of address labels for the owner and representative (COD and TN)
- 4 sets of address labels of the referring agency (same as above sets)

Documents outstanding - to follow under separate cover:

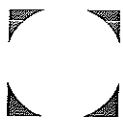
- 300' Radius packet - ordered on January 30, 2013 - due Monday, February 4<sup>th</sup> in Palm Desert (we will overnight this packet and labels as soon as we receive it)

Please let me know if you are in need of anything additional, or if I have inadvertently left something out. Thank you for all your help in facilitating this submission.

Enclosures:  Yes     No  
                   E-Mail

Documents to follow:  ONTRAC/UPS  
                                   Mail

Confidentiality Notice: This transmittal is intended only for the use of the individual or entity to which it is addressed and may contain information that is privileged, confidential, and exempt from disclosure under applicable law. If the reader of this message is not the intended recipient, or the employee or agent responsible for delivering the message to the intended recipient, you are hereby notified that any dissemination, distribution, or copying of this communication is strictly prohibited. If you have received this communication in error, please notify us immediately by telephone and return the original message to us at the above address via the U.S Postal Service. Thank You.



# TRANSMITTAL

**TERRA NOVA PLANNING & RESEARCH, INC.®**

42635 Melanie Place, Suite 101  
Palm Desert, CA. 92211  
Phone: (760) 341-4800  
FAX#: (760) 341-4455  
E-mail: [kcuza@terranovaplanning.com](mailto:kcuza@terranovaplanning.com)

**DATE:** February 19, 2013

**Ontrac Overnight**

**TO:** John Guerin  
Airport Land Use Commission  
4080 Lemon, 14<sup>th</sup> Floor  
Riverside, CA 92501

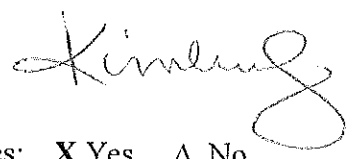
**FROM:** Kimberly Cuza  
Administrative Assistant  
John D. Criste, AICP

**RE:** College of the Desert West Valley Campus - Master Plan Review

Per your conversation with John Criste, enclosed please find:

- Site Plan Overview (Sheet A0.19) 24x36
- Overall Buildings - First Level Floor Plan (Sheet AAB2.01) 24x36
- Overall Buildings - Second Level Floor Plan (Sheet AAB2.02) 24x36
- Overall Buildings - Third Level Floor Plan (Sheet AAB2.03) 24x36
- North and South Buildings - Roof Plan (Sheet AAB2.04) 24x36
- North Building - Exterior Elevations (Sheet AAB3.01) 24x36
- South Building - Exterior Elevations (Sheet AAB3.03) 24x36
- Facilities Master Plan & Phase 1 Project (Academic Area) 24x36
- CD containing all of the above

Please let me know if you are in need of anything further.

cc: file 

Enclosures:  Yes    No  
                   E-Mail

Documents to follow:  ONTRAC/UPS  
                                   Mail

Confidentiality Notice: This transmittal is intended only for the use of the individual or entity to which it is addressed and may contain information that is privileged, confidential, and exempt from disclosure under applicable law. If the reader of this message is not the intended recipient, or the employee or agent responsible for delivering the message to the intended recipient, you are hereby notified that any dissemination, distribution, or copying of this communication is strictly prohibited. If you have received this communication in error, please notify us immediately by telephone and return the original message to us at the above address via the U.S Postal Service. Thank You.

**COUNTY OF RIVERSIDE  
AIRPORT LAND USE COMMISSION**

**STAFF REPORT**

**AGENDA ITEM:** 2.2 ~~2.1~~

**HEARING DATE:** April 11, 2013: RECONSIDERATION IN PART  
(Originally heard on February 14, 2013)

**CASE NUMBER:** ZAP1086MA12 – Stater Bros. Markets and CVS Drugs  
(Representative for Reconsideration: Dan Schultz, Mile  
Stone Management) ~~Scott Limbacher~~)

**APPROVING JURISDICTION:** City of Riverside

**JURISDICTION CASE NO:** **PROJECTS PROPOSED FOR RECONSIDERATION:**  
~~P12-0442 (General Plan Amendment), P12-0443 (Rezoning),  
P12-0444 (Design Review), and P12-0761 (Conditional Use  
Permit)~~

**MAJOR ISSUES:** ~~The proposed buildings~~ *CVS drug store building will require a revised obstruction evaluation review by the Federal Aviation Administration relative to Riverside Municipal Airport due to the increased proposed height (36 feet).*

**RECOMMENDATION:** Staff recommends that the proposed ~~General Plan Amendment and Rezone Conditional Use Permit and Design Review~~ be found **CONDITIONALLY CONSISTENT** with the 1984 Riverside County Airport Land Use Plan, as applied to the March Air Reserve Base Airport Influence Area, ~~At this time, staff recommends that ALUC consideration of the Design Review and Conditional Use Permit be CONTINUED to the Commission's March 14 hearing; however, in the event that Form 7460-1 is submitted to the Federal Aviation Administration (FAA) and is recognized as a "Work in Progress" prior to February 14, staff would then recommend a finding of CONDITIONAL CONSISTENCY for those cases, subject to the conditions included herein and such additional or modified conditions as may be necessary to comply with FAA requirements. (The associated General Plan Amendment and Rezone were previously found consistent at ALUC's February 14, 2013 public hearing, and do not require reconsideration.)~~

**PROJECT DESCRIPTION:** The cases cited above relate to the expansion and redevelopment of an existing 5.43-acre commercial center in the City of Riverside currently developed with a grocery store, drug store, and vacant retail shops. P12-0444 is a proposal to expand the center to 6.3 acres, with a new 44,636 square foot Stater Brothers grocery store, an 11,465 square foot drugstore, and an 8,760 square foot pad for future commercial development. ~~P12-0442 is a proposal to amend the City of Riverside General Plan land use designation of the expansion area (0.86 acres) from P-Park to C-Commercial, and to amend the land use designation of a separate area of up to 1.34 acres from LDR-Low Density Residential to P-Park. P12-0443 proposes to rezone the expansion area (0.86 acres)~~



~~from R-1-13,000 (Single Family Residential) to CR-NC (Commercial Retail Neighborhood Center).~~ P12-0761 is a request for a Conditional Use Permit to allow (1) the operation of a drive-thru pharmacy and (2) alcohol sales for off-site consumption.

**PROJECT LOCATION:** The affected areas are located along the east side of Mary Street, southerly of Lincoln Avenue and northerly of Victoria Avenue, in the City of Riverside, approximately 38,500 feet northwesterly of Runway 14-32 at March Air Reserve Base and 14,400 feet southeasterly of Runway 9-27 at Riverside Municipal Airport. Specifically, the existing commercial center is located at the southeast corner of Mary Street and Lincoln Avenue. The 0.86-acre expansion area (which is being conveyed from the City to Stater Brothers) is located southerly of the existing center, adjacent to an existing park. ~~Stater Brothers is, in turn, conveying to the City a 0.54-acre parcel located at the northeast corner of Mary Street and Victoria Avenue and arranging for an additional area of up to 0.80 acres to the east (presently owned by a church) to also be conveyed to the City. The City proposes to re-designate both of those areas so as to enable their inclusion in the park. (Those areas would retain their existing R-1-13,000 zoning.)~~

**LAND USE PLAN:** 1984 Riverside County Airport Land Use Plan, as applied to March Air Reserve Base (The site is closer to Riverside Municipal Airport, but lies just outside its influence area.)

- a. Airport Influence Area: March Air Reserve Base
- b. Land Use Policy: Area III
- c. Noise Levels: Below 60 CNEL from aircraft, according to the 2005 Air Installation Compatible Use Zones Report and the March Air Reserve Base Joint Land Use Study (Exhibit 2-9)

**ANALYSIS:**

Non-Residential Land Use Intensity: The site is located in Area III of the March Air Reserve Base Airport Influence Area. Non-residential land use intensity is not limited within Area III, based on the 1984 Riverside County Airport Land Use Plan. The site is not located within or near an Accident Potential Zone as established by the U.S. Air Force.

Pursuant to the draft Joint Land Use Study for the March Air Reserve Base/Inland Port Airport, the site would be located within Compatibility Zone E, Other Airport Environs. No draft land use intensity criteria are included for Compatibility Zone E.

The applicant does not propose any uses prohibited or discouraged in Area III or draft Compatibility Zone E within the project.

Noise: The site is located outside the 60 CNEL contour as depicted on the maps included in the

2005 Air Installation Compatible Use Zone Study and the March Air Reserve Base/Inland Port Airport Joint Land Use Study. It is also outside the 65 CNEL contour, as depicted on Figure 4.1-2 of the F-15 Aircraft Conversion Environmental Impact Study prepared for the 144<sup>th</sup> Fighter Wing of the California Air National Guard, which studies the impact of the replacement of F-16 fighters with F-15 fighters. Therefore, no special measures to mitigate aircraft-generated noise are required.

Part 77: The elevation of Runway 14-32 at its northerly terminus (the nearest point to the project site) is 1,535 feet above mean sea level (AMSL). According to the Riverside County Land Information System, the elevation of this site ranges from 940 to 956 feet AMSL. The applicant has indicated that the maximum elevation at the top of any proposed building will not exceed 990 feet above mean sea level. Therefore, the elevation at top point will be below the elevation of the runway at March. However, the closest airport is Riverside Municipal Airport. The elevation of Runway 9-27 at its closest point is 816 feet above mean sea level. At a distance of 14,400 feet from the southeasterly terminus of the runway to the corner of Mary Street and Lincoln Avenue, structures with an elevation at top point above 960 feet above mean sea level require notice to the Federal Aviation Administration.

**The Federal Aviation Administration reviewed both the market and drug store buildings and issued Determinations of No Hazard to Air Navigation in February, 2013. However, the drug store building was reviewed at a height of 25 feet. The proposed drug store is actually 36 feet in height. Therefore, the applicant team was required to file a new Form 7460-1 with the Federal Aviation Administration Obstruction Evaluation Service. The applicant team requested ALUC reconsideration at the April hearing in order to amend project conditions of approval so as to allow a maximum building height of 36 feet for the drug store.**

Avigation Easement: Pursuant to Policy 3 of the 1984 Riverside County Airport Land Use Plan, an avigation easement is required for all land uses located within the airport influence areas developed pursuant to that Plan.

Open Area: Area III of the 1984 Riverside County Airport Land Use Plan and draft Compatibility Zone E of the Joint Land Use Study for the March Air Reserve Base/Inland Port Airport do not have any requirements for provision of open space.

#### **CONDITIONS:**

1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses shall be prohibited:
  - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight

final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.

- (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
  - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
  - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
3. Prior to issuance of any building permits, the landowner shall convey and have recorded an avigation easement to the March Inland Port Airport Authority. Contact March Joint Powers Authority at (951) 656-7000 for additional information.
  4. Prior to issuance of any building permits, the applicant shall submit a Notice of Proposed Construction or Alteration (Form 7460-1) to the Federal Aviation Administration (FAA) for each building and shall have received a determination of "Not a Hazard to Air Navigation" from the FAA. Copies of the FAA determination shall be provided to the City of Riverside Planning Department and the Riverside County Airport Land Use Commission. **[Upon issuance of a determination for the drug store building pursuant to Aeronautical Study No. 2013-AWP-1858-OE, this condition shall be considered to have been MET with regard to the grocery store and drug store, but shall remain in effect and applicable relative to future building(s) on the commercial pad.]**
  5. The attached notice shall be provided to all prospective purchasers and/or tenants of the property.

**Additional conditions will be added pending receipt of the FAA Obstruction Evaluation Service letter for Aeronautical Study No. 2013-AWP-1858-OE. It is anticipated that they will be similar to Conditions 6 through 11 included in the letter from ALUC to the City of Riverside dated February 26, 2013, except that Condition No. 8 would reference a maximum height of 36 feet, rather than 25 feet, for the drug store, and a maximum elevation at top point of 974 feet above mean sea level, rather than 963 feet.**





Federal Aviation Administration

The OE/AAA application will have scheduled hardware maintenance on March 27, 2013 from 7AM to 8AM EDT. Due to this hardware maintenance you may experience intermittent connection behavior to OE/AAA. The system will be going offline at 7 pm US/Eastern on Thursday, March 28, 2013 for upgrades. We apologize for any inconvenience.

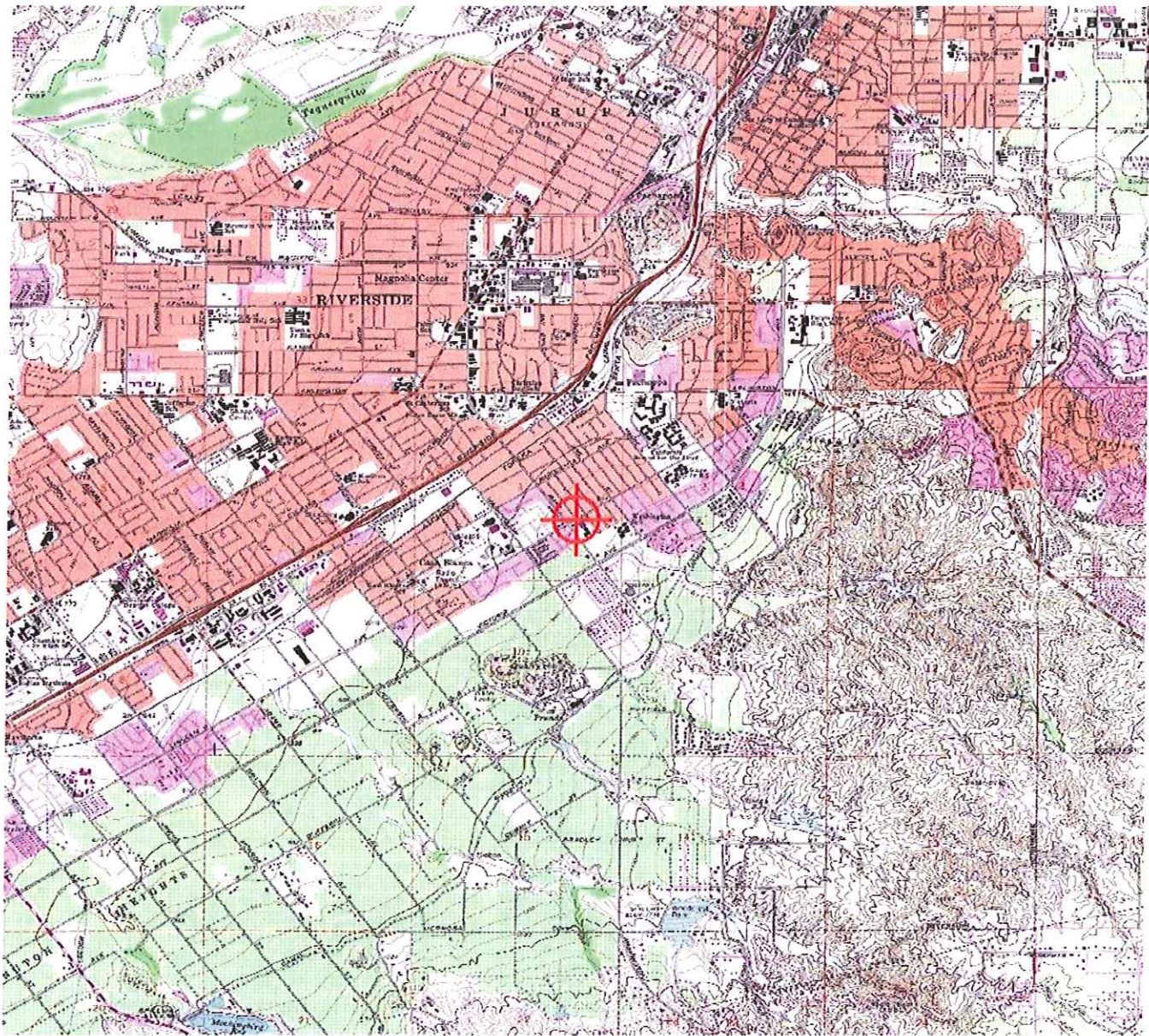
<< OE/AAA

Form 7460-1 for ASN: 2013-AWP-1858-OE

**For information only.**  
 This proposal has not yet been studied. Study outcomes will be posted at a later date. Public comments are not requested, and will not be considered at this time.

Overview											
<b>Study (ASN):</b> 2013-AWP-1858-OE	<b>Received Date:</b> 03/26/2013										
<b>Prior Study:</b> 2013-AWP-852-OE	<b>Entered Date:</b> 03/26/2013										
<b>Status:</b> Work In Progress	<b>Map:</b> View Map										
Construction Info											
<b>Notice Of:</b> CONSTR	<b>Structure Summary</b>										
<b>Duration:</b> PERM (Months: 0 Days: 0)	<b>Structure Type:</b> Building										
<b>Work Schedule:</b> 05/15/2014 to 05/15/2015	<b>Structure Name:</b> CVS Pharmacy										
	<b>FCC Number:</b>										
Structure Details											
<b>Latitude (NAD 83):</b> 33° 56' 08.15" N	<b>Height and Elevation</b>										
<b>Longitude (NAD 83):</b> 117° 23' 14.41" W											
<b>Datum:</b> NAD 83	<b>Site Elevation:</b> 938										
<b>City:</b> Riverside	<b>Structure Height:</b> 36										
<b>State:</b> CA	<b>Total Height (AMSL):</b> 974										
	<b>Frequencies</b>										
	<table border="1"> <thead> <tr> <th>Low Freq</th> <th>High Freq</th> <th>Unit</th> <th>ERP</th> <th>Unit</th> </tr> </thead> <tbody> <tr> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> </tbody> </table>	Low Freq	High Freq	Unit	ERP	Unit					
Low Freq	High Freq	Unit	ERP	Unit							

Check Edit



**LETTER FROM ALUC TO CITY OF RIVERSIDE DATED FEBRUARY 26, 2013**





# AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY

CHAIR February 26, 2013

Simon Housman  
Rancho Mirage

Ms. Yvette M. Sennewald, Senior Planner

VICE CHAIRMAN  
Rod Ballance  
Riverside

City of Riverside Community Development Department, Planning Division  
3900 Main Street, Third Floor  
Riverside CA 92522

COMMISSIONERS

Arthur Butler  
Riverside

John Lyon  
Riverside

Glen Holmes  
Hemet

Greg Pettis  
Cathedral City

Richard Stewart  
Moreno Valley

STAFF

Director  
Ed Cooper

John Guerin  
Russell Brady  
Barbara Santos

County Administrative Center  
4080 Lemon St., 14<sup>th</sup> Floor.  
Riverside, CA 92501  
(951) 955-5132

[www.rcaluc.org](http://www.rcaluc.org)

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW**

File No.: ZAP1086MA12

Related File No.: P12-0442 (General Plan Amendment), P12-0443 (Rezone),  
P12-0444 (Design Review), P12-0761 (Conditional Use Permit)

APN: 235-110-014 through 235-110-018; 235-110-001; 235-110-004;  
235-110-011

Dear Ms. Sennewald:

On February 14, 2013, the Riverside County Airport Land Use Commission (ALUC) found City of Riverside Case No. P12-0442, a proposal to amend the City General Plan land use designation of 0.86 acres located easterly of Mary Street and southerly of Lincoln Avenue from P-Park to C-Commercial and to amend the land use designation of up to 1.34 acres located northerly of Victoria Avenue and easterly of Mary Street from LDR-Low Density Residential to P-Park **CONSISTENT** with the 1984 Riverside County Airport Land Use Plan, as applied to the Airport Influence Area of March Air Force Base (now March Air Reserve Base/March Inland Port).

On February 14, 2013, the Riverside County Airport Land Use Commission (ALUC) found City of Riverside Case No. P12-0443, a proposal to rezone 0.86 acres located easterly of Mary Street and southerly of Lincoln Avenue from R-1-13,000 (Single Family Residential) to CR-NC (Commercial Retail – Neighborhood Commercial Overlay) **CONSISTENT** with the 1984 Riverside County Airport Land Use Plan, as applied to the Airport Influence Area of March Air Force Base (now March Air Reserve Base/March Inland Port).

On February 14, 2013, the Riverside County Airport Land Use Commission (ALUC) found City of Riverside Case No. P12-0444 (Design Review), a proposal to expand the existing commercial center to 6.3 acres, construct a new 44,636 square foot Stater Brothers grocery store, construct a new 11,465 square foot CVS drug store, and establish an 8,760 square foot pad for future commercial development, and City of Riverside Case No. P12-0761, a request for a Conditional Use Permit to allow the operation of a drive-through pharmacy and the sales of alcohol for off-site consumption **CONDITIONALLY CONSISTENT** with the 1984 Riverside County Airport Land Use Plan, as applied to the Airport Influence Area of March Air Force Base (now March Air Reserve Base/March Inland Port), pending Federal Aviation Administration (FAA) review, which has now been completed, subject to the following conditions (as modified to incorporate the provisions of the FAA's Determination of No Hazard to Air Navigation letters issued on February 15, 2013 and February 20, 2013, respectively):

**CONDITIONS:**

1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses shall be prohibited:
  - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
  - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
  - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
  - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
3. Prior to issuance of any building permits, the landowner shall convey and have recorded an aviation easement to the MARB/MIP Airport. Contact March Joint Powers Authority at (951) 656-7000 for additional information.
4. Prior to issuance of any building permits, the applicant shall submit a Notice of Proposed Construction or Alteration (Form 7460-1) to the Federal Aviation Administration (FAA) for each building and shall have received a determination of "Not a Hazard to Air Navigation" from the FAA. Copies of the FAA determination shall be provided to the City of Riverside Planning Department and the Riverside County Airport Land Use Commission. **[This condition shall be considered to have been MET with regard to the grocery store and drug store, but shall remain in effect and applicable relative to future building(s) on the commercial pad.]**
5. The attached notice shall be provided to all prospective purchasers and/or tenants of the property.

**The following conditions have been added subsequent to the ALUC hearing pursuant to the terms of the FAA Obstruction Evaluation Service letters issued on February 15, 2013 for Aeronautical Study No. 2013-AWP-779-OE and on February 20, 2013 for Aeronautical Study No. 2013-AWP-852-OE:**

6. **The Federal Aviation Administration has conducted aeronautical studies of the proposed grocery store building (Aeronautical Study No. 2013-AWP-779-OE) and the proposed drug store building (Aeronautical Study No. 2013-AWP-852-OE) and**

has determined that neither marking nor lighting of these structures is necessary for aviation safety. However, if marking and/or lighting for aviation safety are accomplished on a voluntary basis, such marking and/or lighting (if any) shall be installed in accordance with FAA Advisory Circular 70/7460-1 K Change 2 and shall be maintained in accordance therewith for the life of the project.

7. The maximum height of the grocery store structure, including all roof-mounted appurtenances (if any), shall not exceed 45 feet above ground level, and the maximum elevation at the top of the structure shall not exceed 990 feet above mean sea level.
8. The maximum height of the drug store structure, including all roof-mounted appurtenances (if any), shall not exceed 25 feet above ground level, and the maximum elevation at the top of the structure shall not exceed 963 feet above mean sea level.
9. The specific coordinates, height, and top point elevation of the grocery store and drug store structures shall not be amended without further review by the Airport Land Use Commission and the Federal Aviation Administration; provided, however, that reduction in building height or elevation shall not require further review by the Airport Land Use Commission.
10. Temporary construction equipment used during actual construction of the structures shall not exceed the height of the applicable structure, unless separate notice is provided to the Federal Aviation Administration through the Form 7460-1 process.
11. Within five (5) days after construction of each building reaches its greatest height, FAA Form 7460-2 (Part II), Notice of Actual Construction or Alteration, shall be completed by the project proponent or his/her designee and e-filed with the Federal Aviation Administration. (Go to <https://oeaaa.faa.gov> for instructions.) This requirement is also applicable in the event the project is abandoned.

If you have any questions, please contact John Guerin, ALUC Principal Planner, at (951) 955-0982.

Sincerely,  
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

  
Edward C. Cooper, Director

JG:bks

Attachments: Notice of Airport in Vicinity  
FAA Aeronautical Study Nos. 2013-AWP-779-OE and 2013-AWP-852-OE.

cc: Scott Limbacher, Stater Brothers Markets  
Anthony Lopez, KZ Dev Co LP  
Gary Gosliga, Airport Manager, March Joint Powers Authority  
Mark Ripley, Manager, Riverside Municipal Airport  
ALUC Staff



# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



Mail Processing Center  
 Federal Aviation Administration  
 Southwest Regional Office  
 Obstruction Evaluation Group  
 2601 Meacham Boulevard  
 Fort Worth, TX 76137

Aeronautical Study No.  
 2013-AWP-779-OE

Issued Date: 02/15/2013

Scott Limbacher  
 Stater Bros. Markets  
 301 S. Tippecanoe Avenue  
 San Bernardino, CA 92408

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building Stater Bros. Market  
 Location: Riverside, CA  
 Latitude: 33-56-04.73N NAD 83  
 Longitude: 117-23-10.12W  
 Heights: 945 feet site elevation (SE)  
 45 feet above ground level (AGL)  
 990 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part I)
- Within 5 days after the construction reaches its greatest height (7460-2, Part II)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed and maintained in accordance with FAA Advisory circular 70/7460-1 K Change 2.

This determination expires on 08/15/2014 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS BEFORE TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates , heights, frequency(ies) and power . Any changes in coordinates , heights, and frequencies or use of greater power will void this determination. Any future construction or alteration , including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

If we can be of further assistance, please contact our office at (817) 321-7760. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2013-AWP-779-OE.

**Signature Control No: 182709500-183454140**

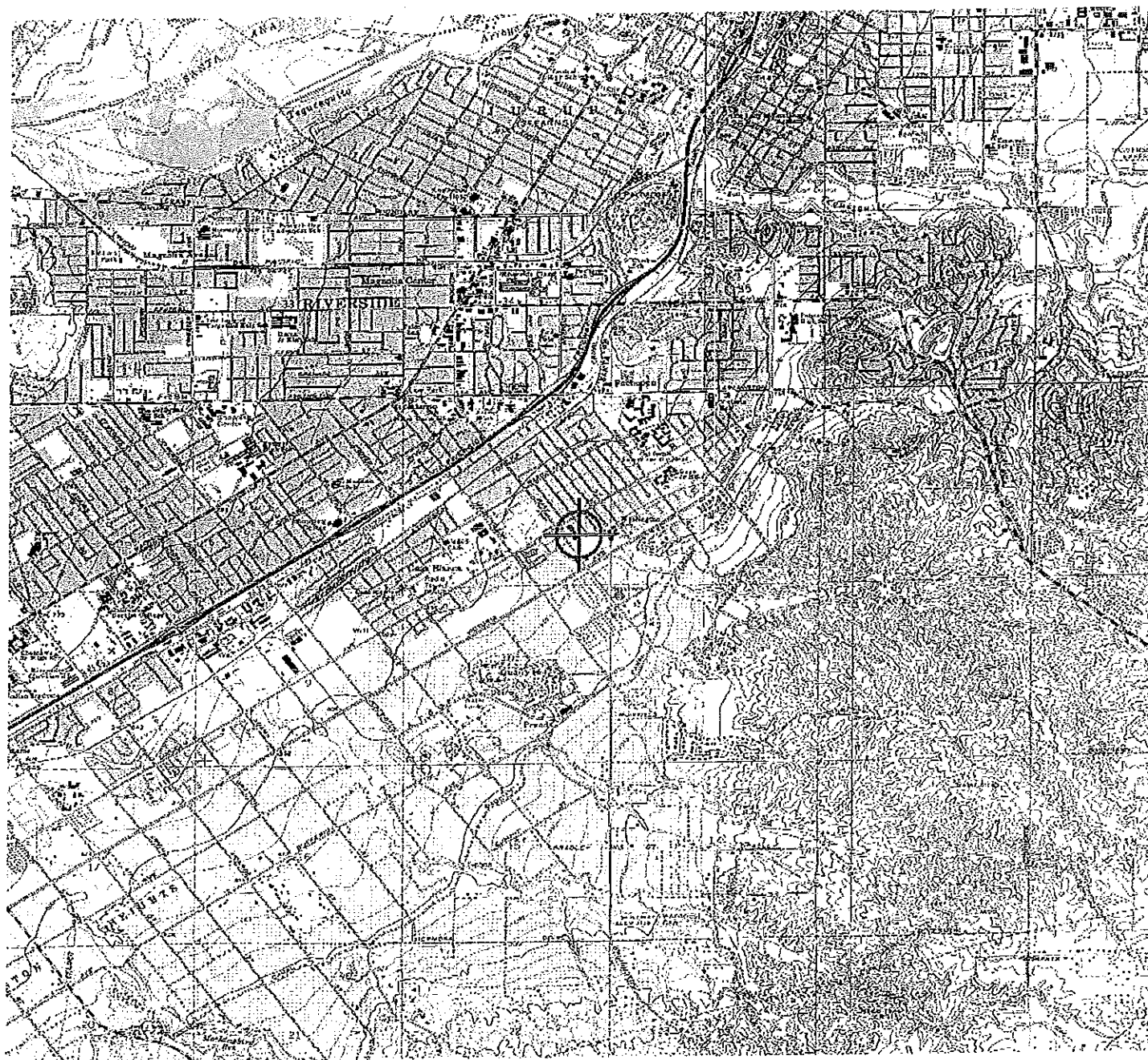
( DNE )

Joan Tengowski  
Technician

Attachment(s)  
Map(s)



TOPO Map for ASN 2013-AWP-779-OE





Mail Processing Center  
 Federal Aviation Administration  
 Southwest Regional Office  
 Obstruction Evaluation Group  
 2601 Meacham Boulevard  
 Fort Worth, TX 76137

Aeronautical Study No.  
 2013-AWP-852-OE

Issued Date: 02/20/2013

Anthony S. Lopez  
 KZ DevCo, LP  
 18818 Teller Avenue  
 Suite 100  
 Irvine, CA 92612

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building CVS Pharmacy  
 Location: Riverside, CA  
 Latitude: 33-56-08.15N NAD 83  
 Longitude: 117-23-14.41W  
 Heights: 938 feet site elevation (SE)  
 25 feet above ground level (AGL)  
 963 feet-above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part I)
- Within 5 days after the construction reaches its greatest height (7460-2, Part II)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed and maintained in accordance with FAA Advisory circular 70/7460-1 K Change 2.

This determination expires on 08/20/2014 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates , heights, frequency(ies) and power . Any changes in coordinates , heights, and frequencies or use of greater power will void this determination. Any future construction or alteration , including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

If we can be of further assistance, please contact our office at (817) 321-7760. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2013-AWP-852-OB.

**Signature Control No: 182786522-183687224**

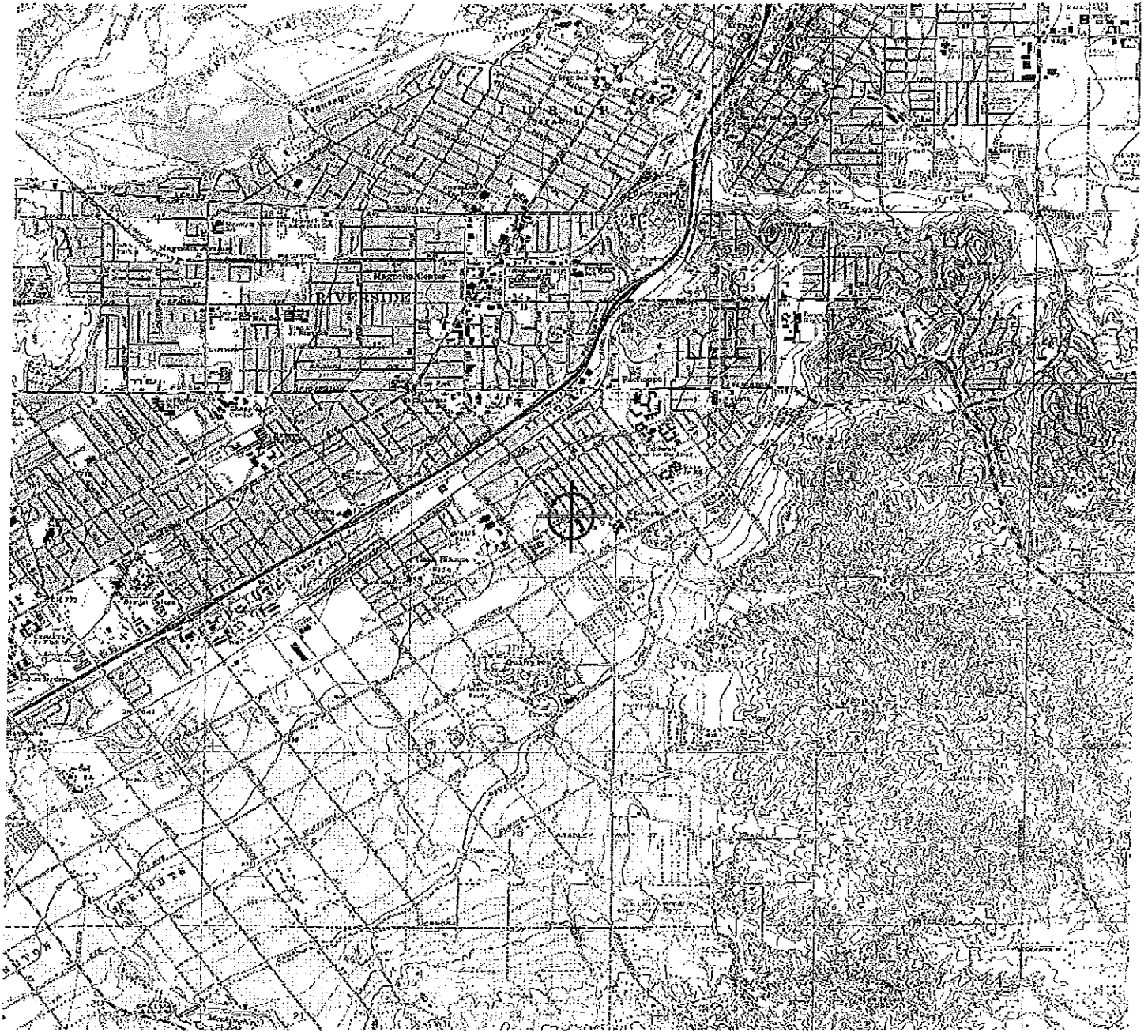
( DNE )

Joan Tengowski  
Technician

Attachment(s)  
Map(s)



TOPO Map for ASN 2013-AWP-852-OE



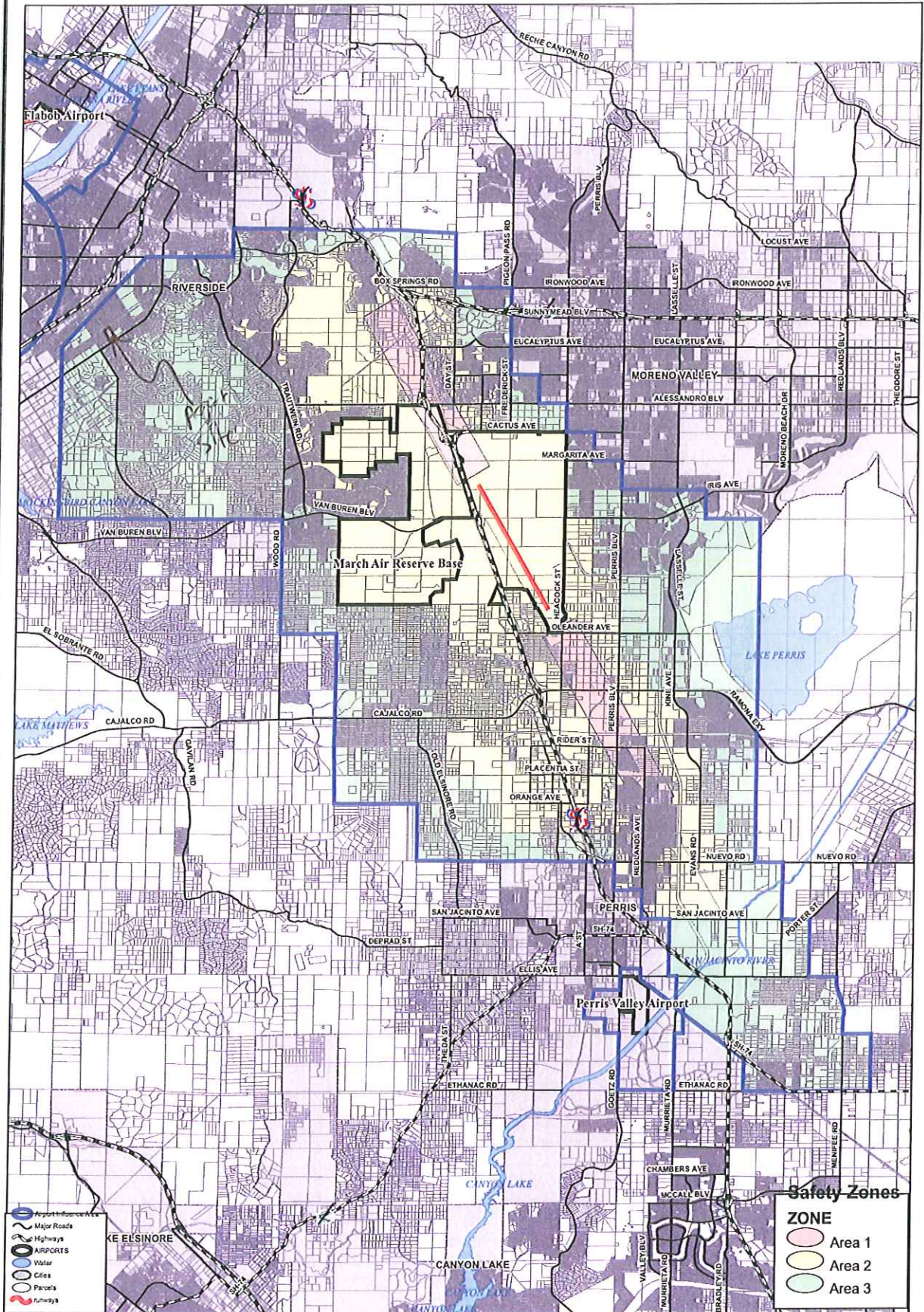
# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



# Riverside County Airports

## March Air Reserve Base



**Legend**

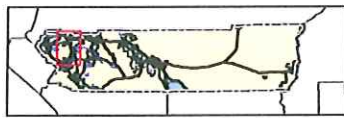
- Airports
- Major Roads
- Highways
- ARPORTS
- Water
- Cities
- Parcels
- Runways

**Safety Zones**

**ZONE**

- Area 1
- Area 2
- Area 3

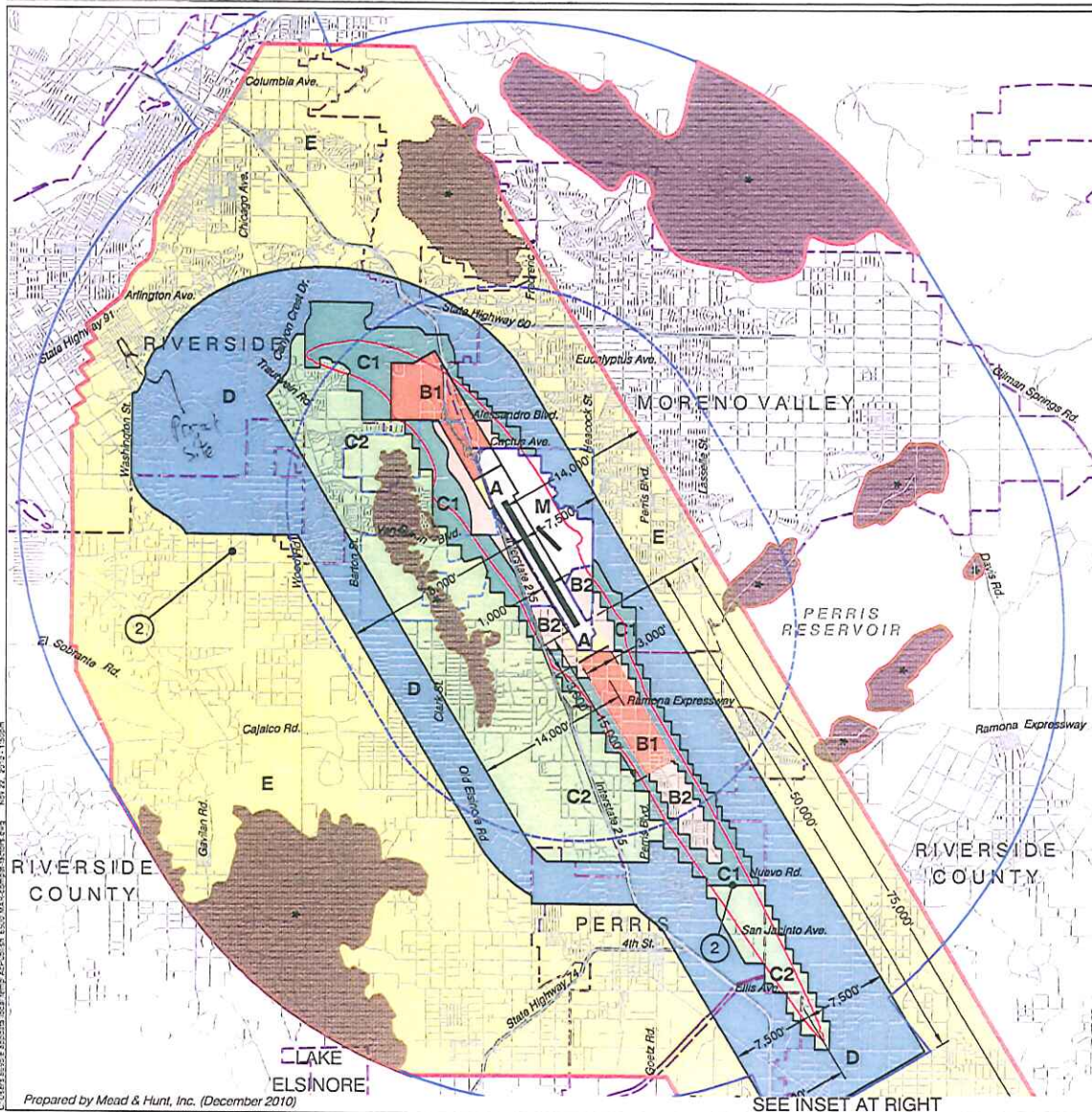
Date: 12-29-2004  
Source: c:\workspace\documents\RPV-11x17-Portals.mxd



**E** The County of Riverside assumes no warranty or legal responsibility for the information contained on this map. Data and information presented on this map is subject to updates, modifications and may not be complete or appropriate for all purposes. County GIS and other sources should be consulted for the most current information. Do not copy or reuse this map.

© County of Riverside, 11/04-08/2014





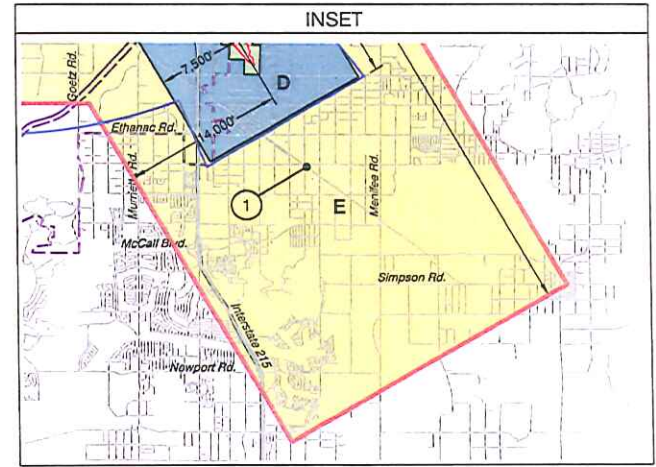
**LEGEND**

- Compatibility Zones**
- Airport Influence Area Boundary
  - Zone A
  - Zone B1
  - Zone B2
  - Zone C1
  - Zone C2
  - Zone D
  - Zone E
  - Zone M
  - High Terrain Zone
  - FAR Part 77 Military Outer Horizontal Surface Limits
  - FAR Part 77 Notification Area

- Boundary Lines**
- March Air Reserve Base / Inland Port Airport
  - March Joint Powers Authority Property Line
  - City Limits

- ① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.

**Note:**  
All dimensions are measured from runway ends and centerlines.



**March Air Reserve Base / Inland Port Airport Joint Land Use Study**  
(December 2010)

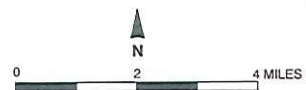


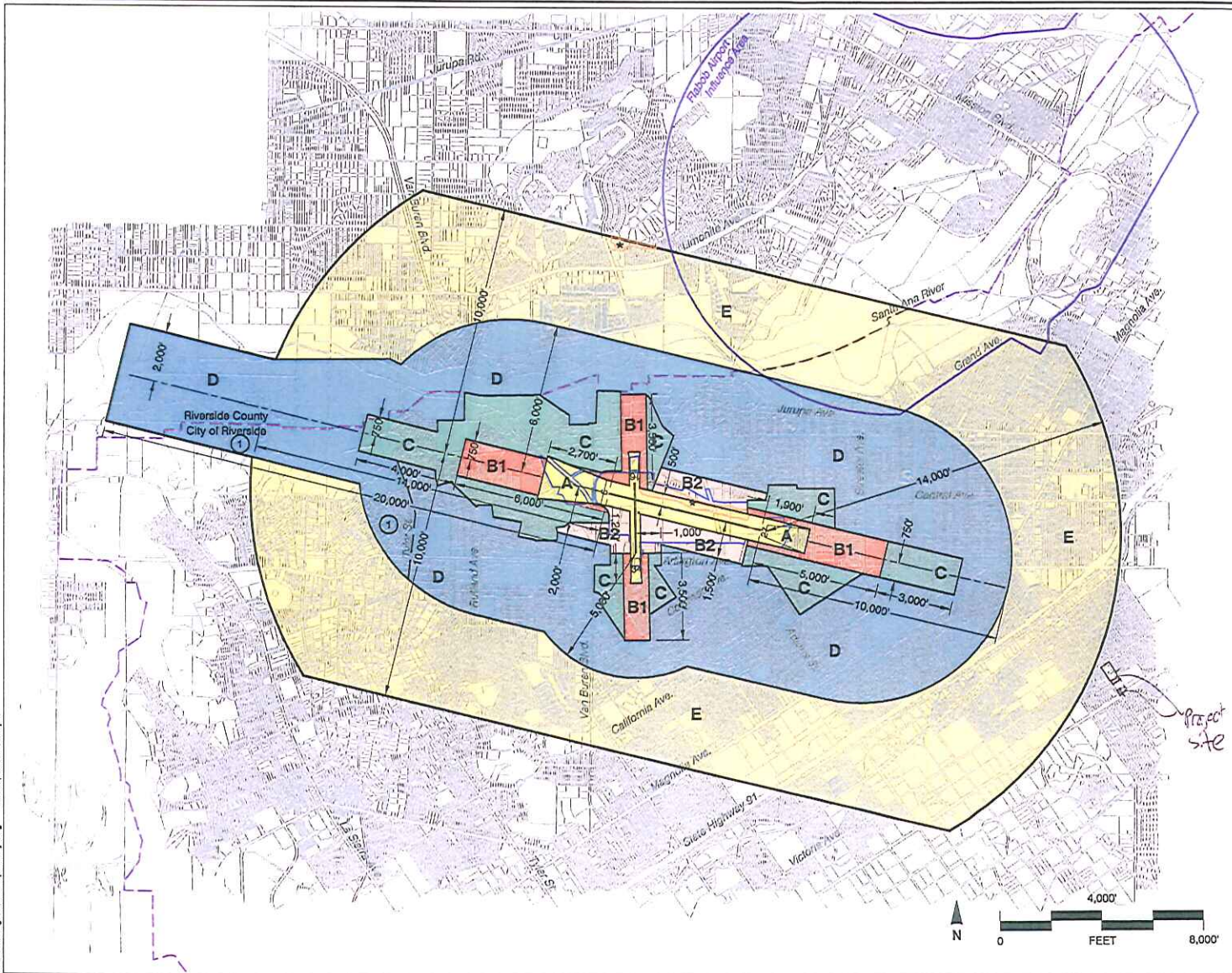
Exhibit 3-3

**Compatibility Map**  
March Air Reserve Base / Inland Port Airport

SEE INSET AT RIGHT

C:\Users\jblibb\appdata\local\temp\jblibb\14000\MAR\_COMPAT\_ACTION\_647 Nov 27, 2010 1:15:28m  
 Prepared by Mead & Hunt, Inc. (December 2010)





- Legend**
- Compatibility Zones**
- Airport Influence Area Boundary
  - Zone A
  - Zone B1
  - Zone B2
  - Zone C
  - Zone D
  - Zone E
  - ★ Height Review Overlay Zone

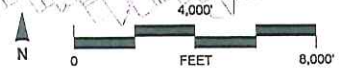
- Boundary Lines**
- Airport Property Line
  - City Limits

**Note**  
 Airport influence boundary measured from a point 200 feet beyond runway ends in accordance with FAA airspace protection criteria (FAR Part 77). All other dimensions measured from runway ends and centerlines.  
 See Chapter 2, Table 2A for compatibility criteria associated with this map. See Section RI.2 for special exceptions to the Table 2A criteria.

**Riverside County**  
**Airport Land Use Commission**  
**Riverside County**  
**Airport Land Use Compatibility Plan**  
**Policy Document**  
*(Adopted March 2005)*

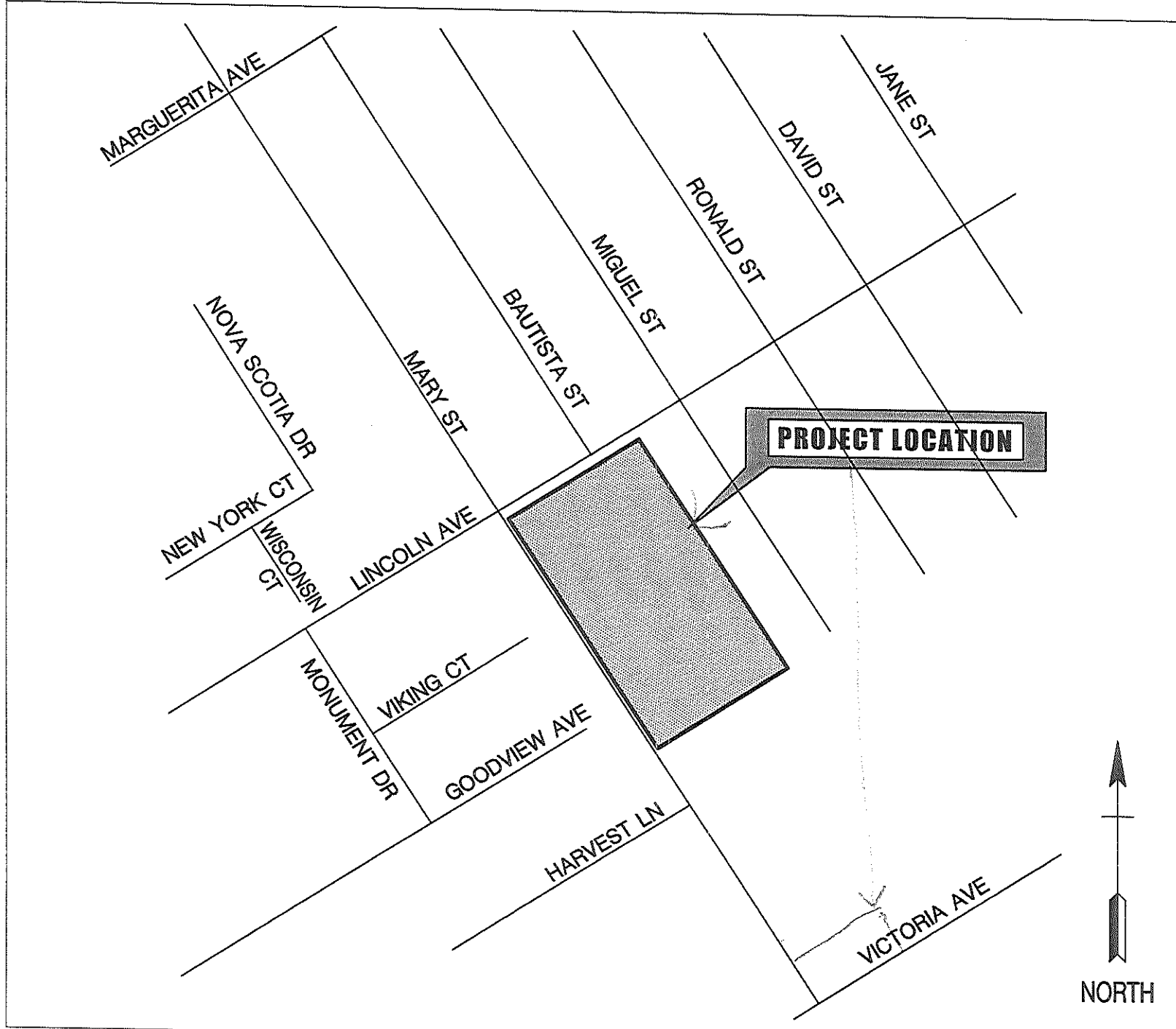
Map RI-1

**Compatibility Map**  
Riverside Municipal Airport



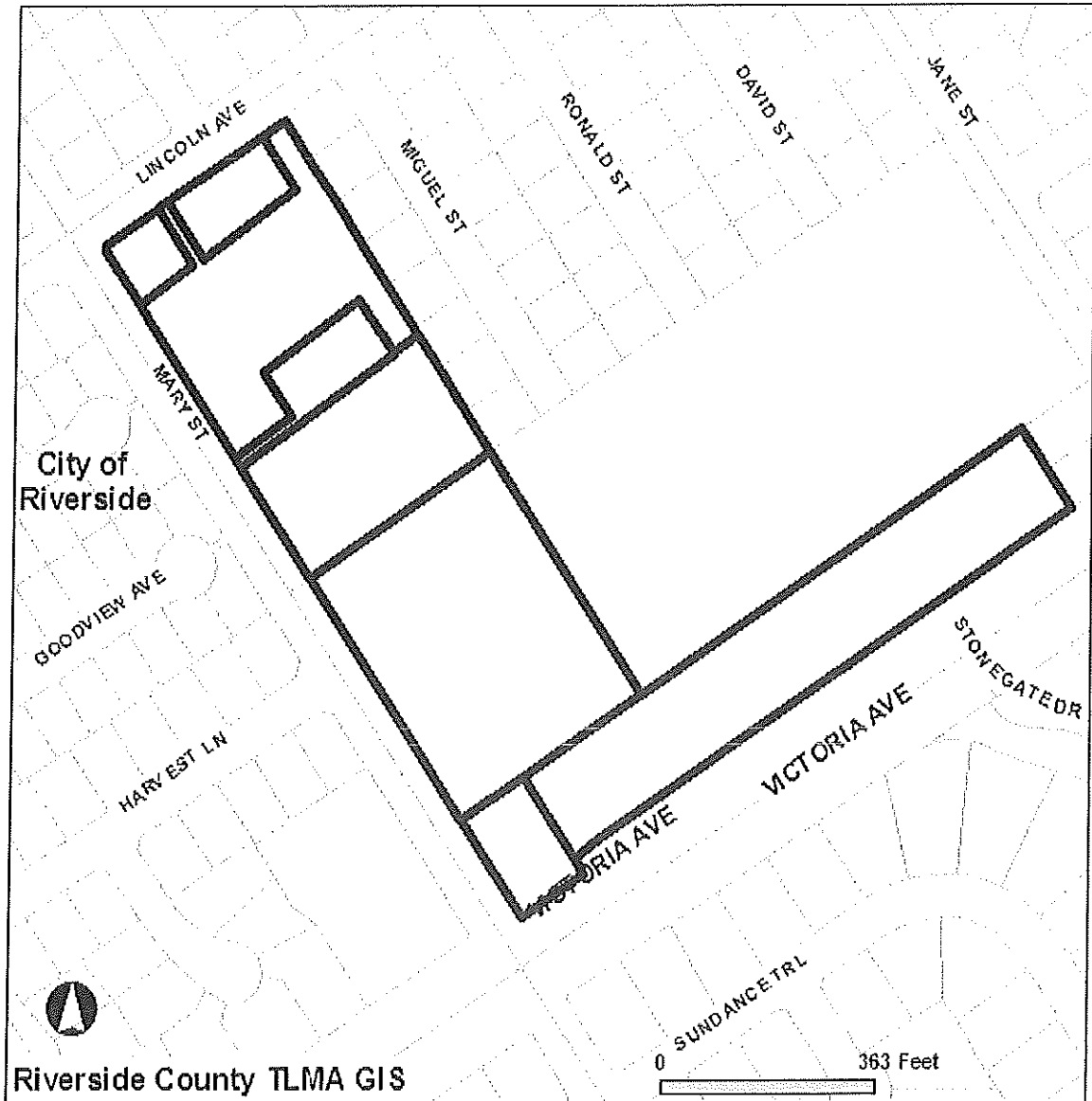
P:\FCO Drawings\BAL-compatibility.dwg Mar 22, 2005 - 12:47pm

# VICINITY MAP





RIVERSIDE COUNTY GIS



**Selected parcel(s):**

235-101-014 235-101-015 235-101-016 235-101-017 235-101-018 235-110-001 235-110-004  
 235-110-011

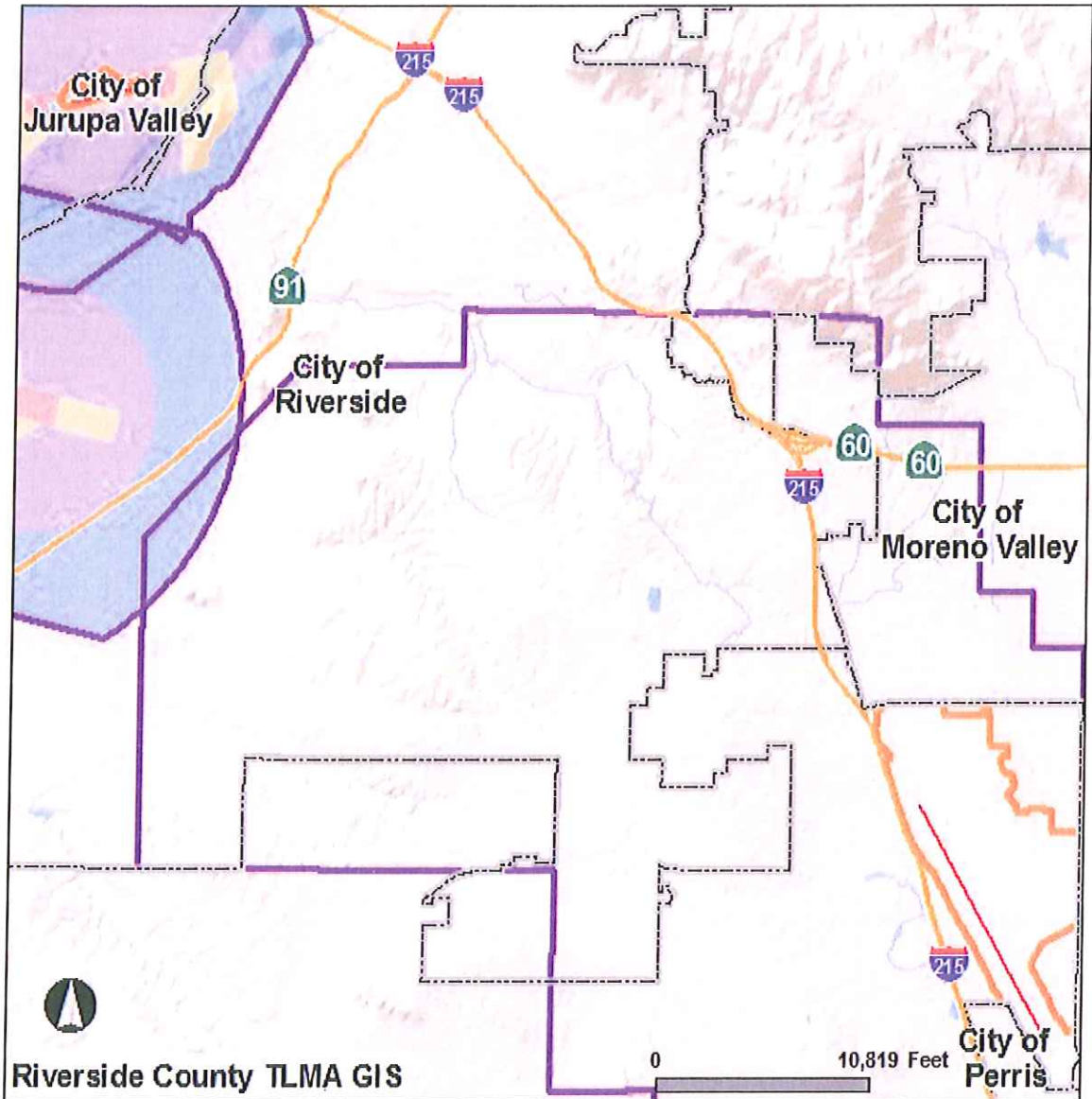
**\*IMPORTANT\***

Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON...Tue Jan 29 12:30:06 2013

Version 121101

RIVERSIDE COUNTY GIS



Selected parcel(s):

235-101-014 235-101-015 235-101-016 235-101-017 235-101-018 235-110-001 235-110-011

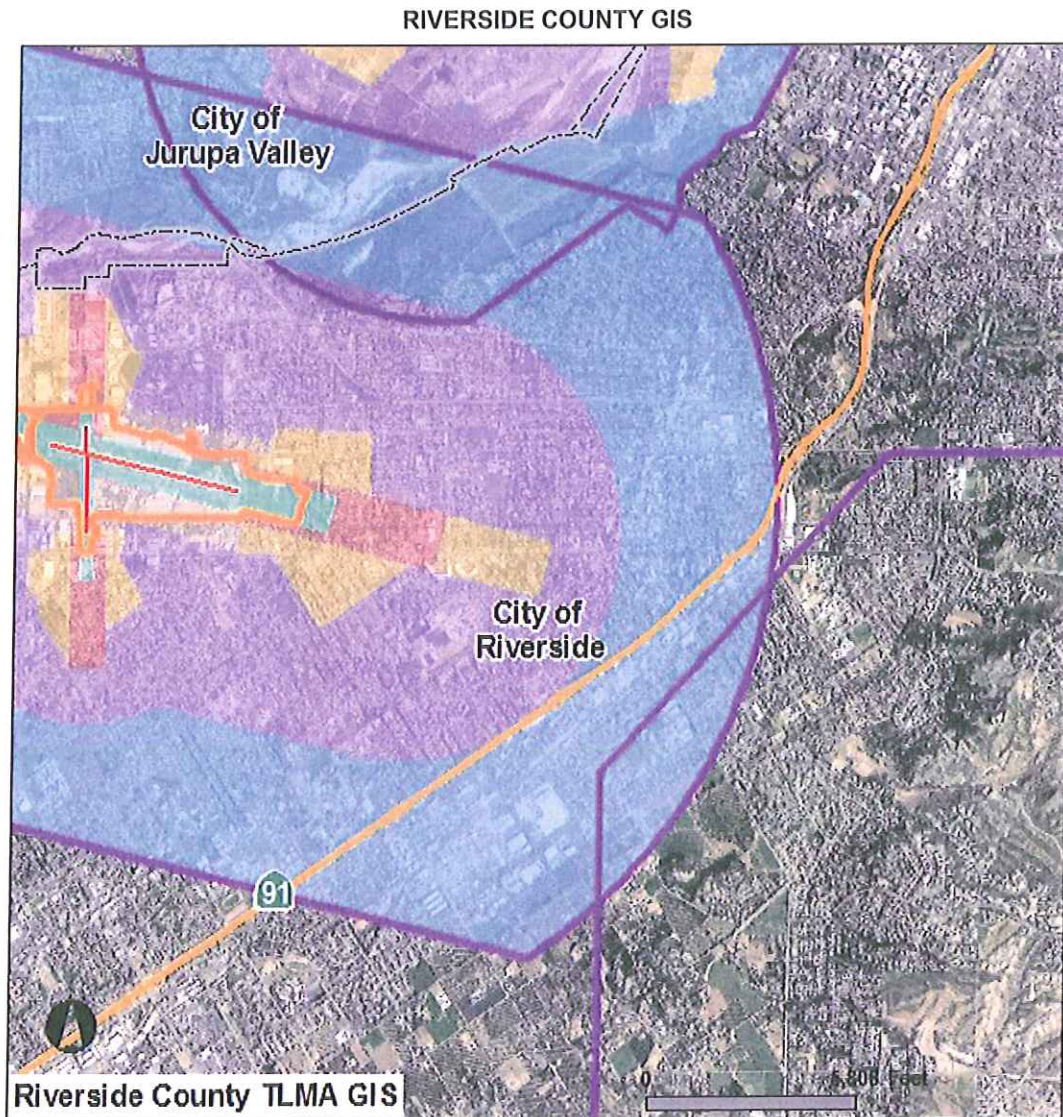
**\*IMPORTANT\***

Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON...Mon Jan 28 11:10:03 2013

Version 121101





**Selected parcel(s):**  
 235-101-014 235-101-015 235-101-016 235-101-017 235-101-018 235-110-001 235-110-004  
 235-110-011

**AIRPORTS**

- |                         |                     |                     |                      |
|-------------------------|---------------------|---------------------|----------------------|
| INTERSTATES             | HIGHWAYS            | STREETS             | AIRPORT RUNWAYS      |
| AIRPORT INFLUENCE AREAS | AIRPORT BOUNDARIES  | COMPATIBILTY ZONE A | COMPATIBILTY ZONE B1 |
| COMPATIBILTY ZONE B2    | COMPATIBILTY ZONE C | COMPATIBILTY ZONE D | COMPATIBILTY ZONE E  |
| WATER BODIES            |                     |                     |                      |

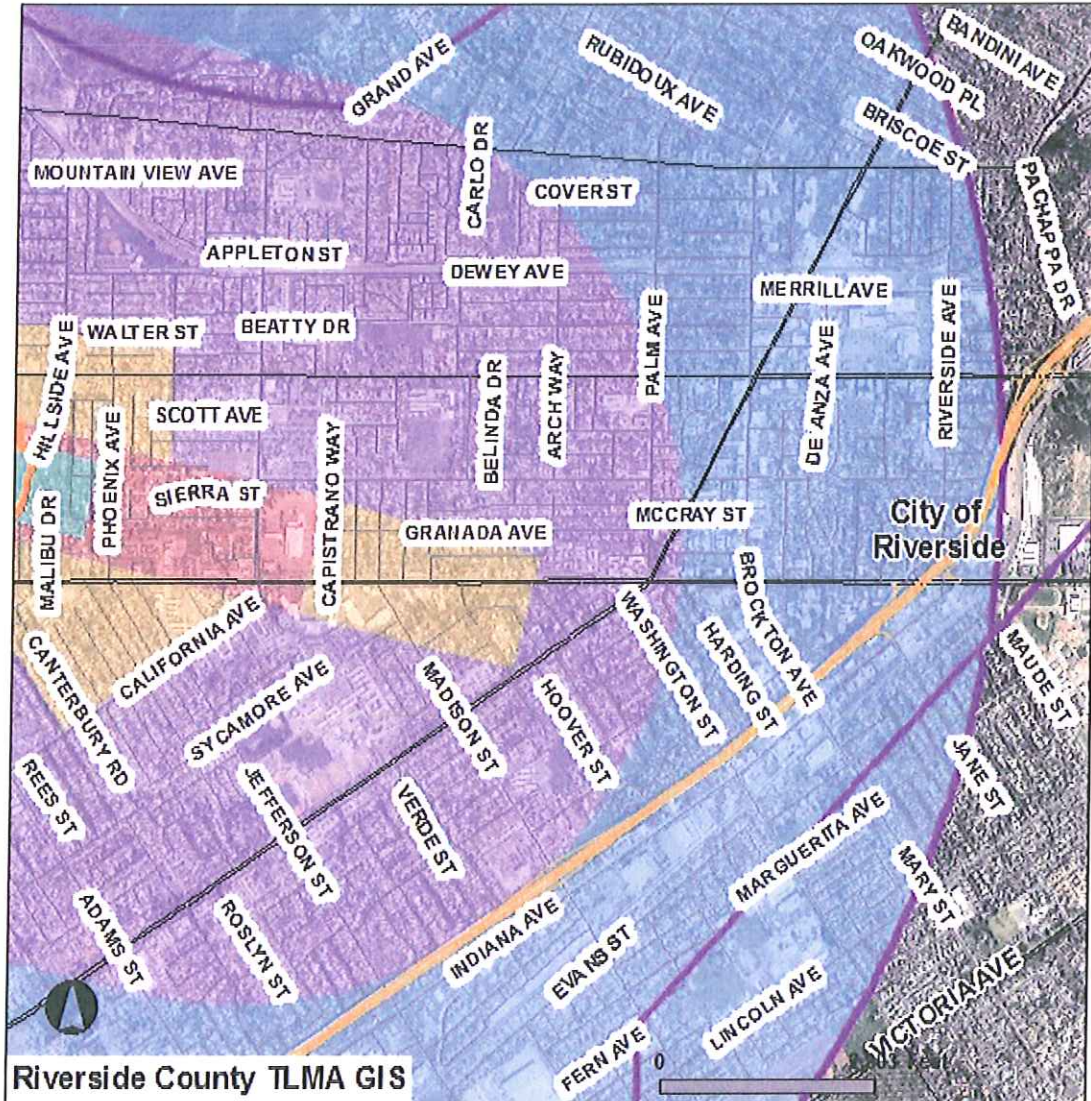
**\*IMPORTANT\***  
 Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON...Tue Jan 29 12:34:12 2013

Version 121101



RIVERSIDE COUNTY GIS



Selected parcel(s):

235-101-014 235-101-015 235-101-016 235-101-017 235-101-018 235-110-001 235-110-004  
235-110-011

AIRPORTS

- |                         |                      |                      |                       |
|-------------------------|----------------------|----------------------|-----------------------|
| INTERSTATES             | HIGHWAYS             | STREETS              | AIRPORT RUNWAYS       |
| AIRPORT INFLUENCE AREAS | AIRPORT BOUNDARIES   | COMPATIBILITY ZONE A | COMPATIBILITY ZONE B1 |
| COMPATIBILITY ZONE C    | COMPATIBILITY ZONE D | COMPATIBILITY ZONE E |                       |

**\*IMPORTANT\***

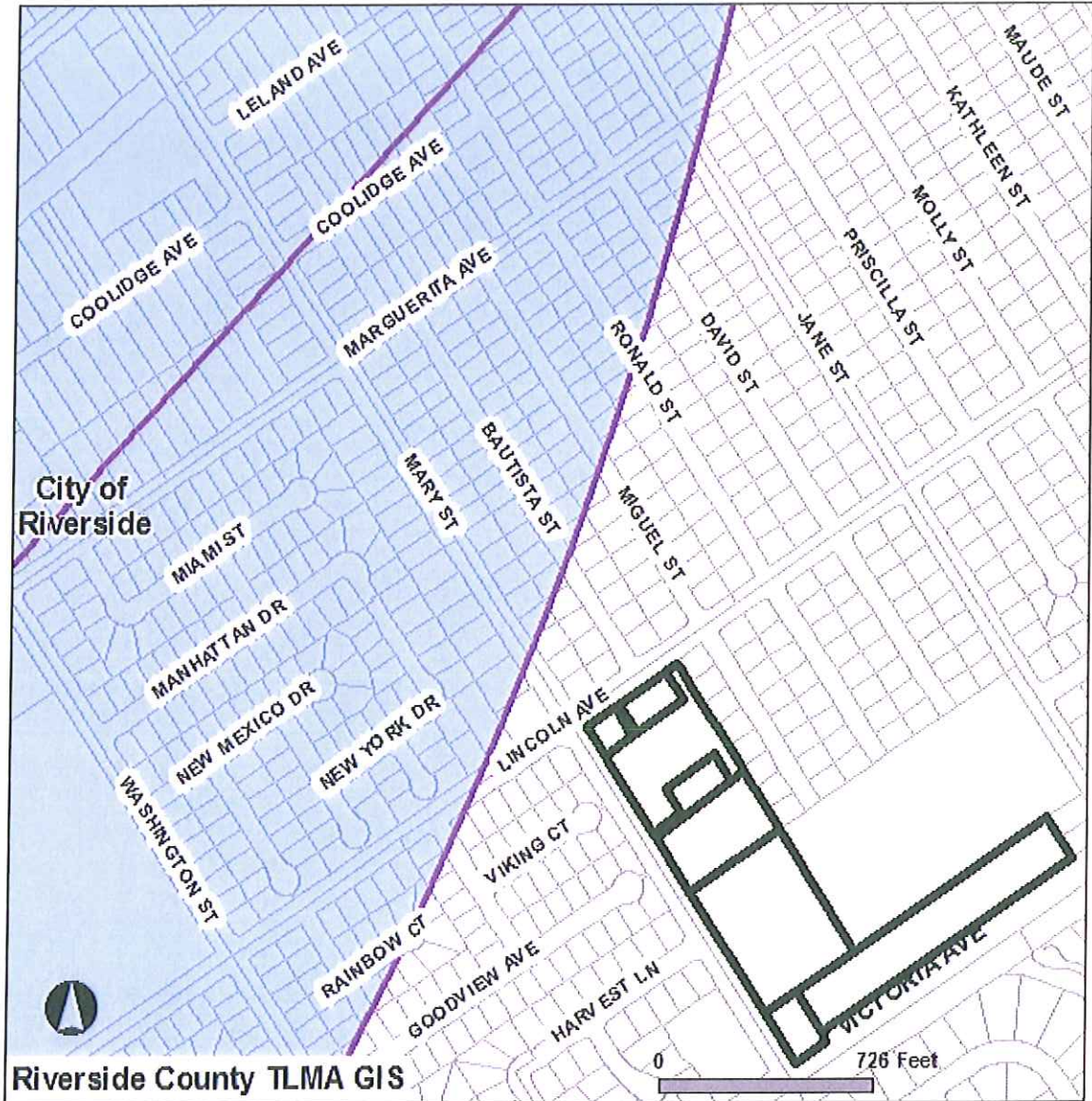
Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON...Tue Jan 29 12:33:37 2013

Version 121101



RIVERSIDE COUNTY GIS



Selected parcel(s):

235-101-014 235-101-015 235-101-016 235-101-017 235-101-018 235-110-001 235-110-004  
235-110-011

AIRPORTS

- SELECTED PARCEL
- AIRPORT INFLUENCE AREAS
- COMPATIBLTY ZONE E
- INTERSTATES
- HIGHWAYS
- PARCELS

**\*IMPORTANT\***

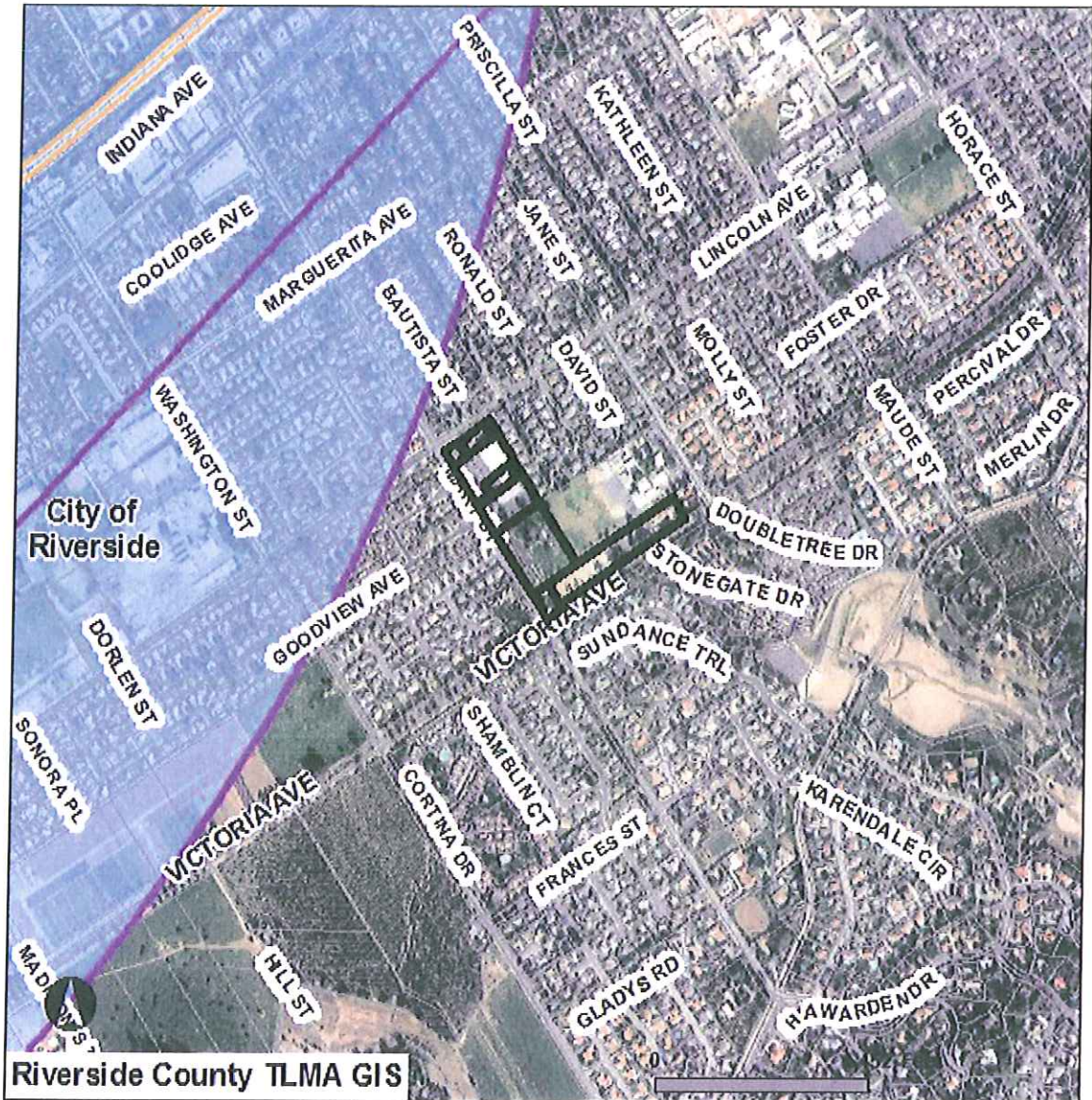
Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON...Tue Jan 29 12:31:47 2013

Version 121101



RIVERSIDE COUNTY GIS



Selected parcel(s):

235-101-014 235-101-015 235-101-016 235-101-017 235-101-018 235-110-001 235-110-004  
235-110-011

AIRPORTS

- SELECTED PARCEL
- AIRPORT INFLUENCE AREAS
- INTERSTATES
- HIGHWAYS
- COMPATIBILITY ZONE E
- PARCELS

**\*IMPORTANT\***

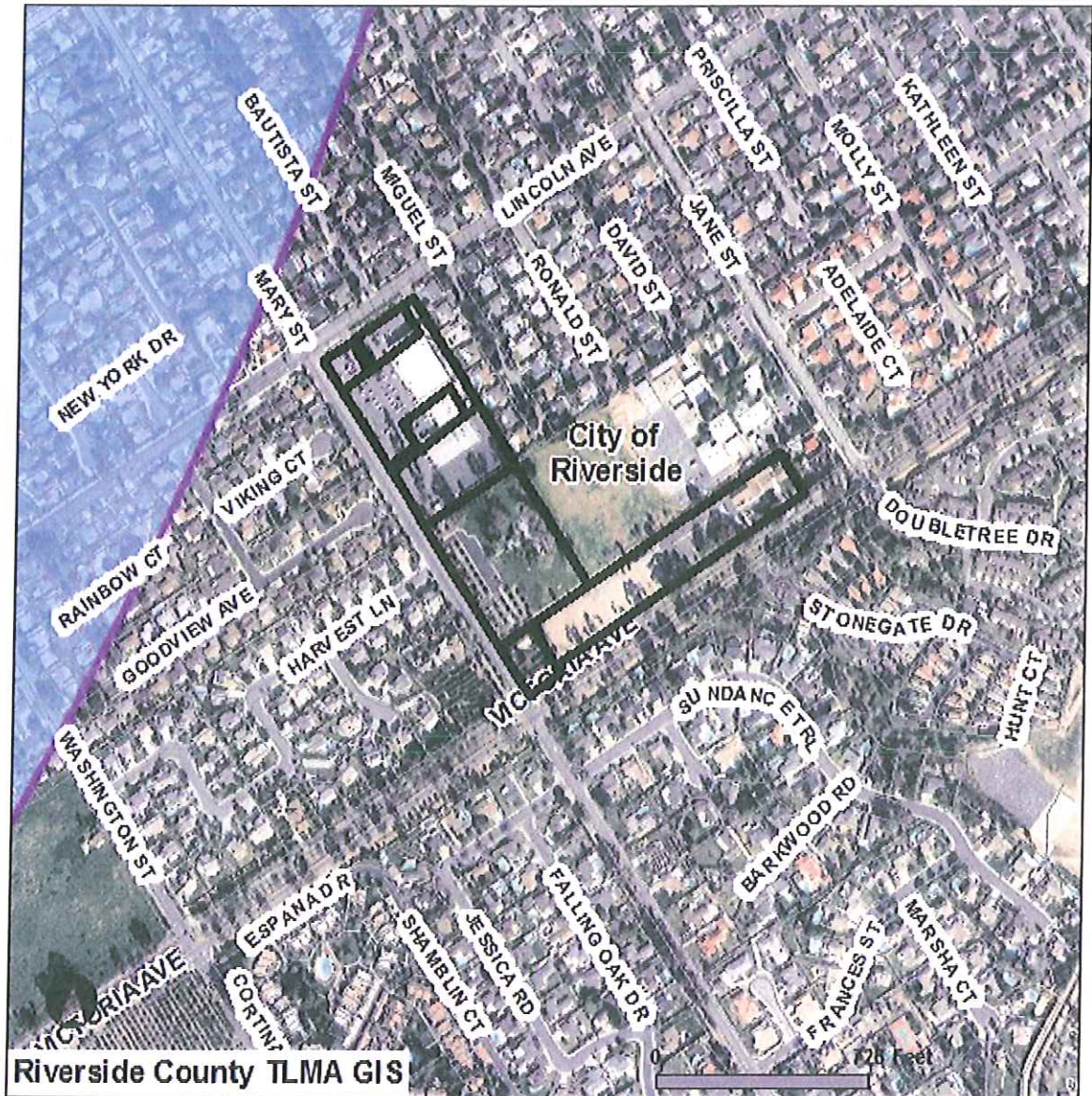
Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON...Tue Jan 29 12:38:01 2013

Version 121101



RIVERSIDE COUNTY GIS



Selected parcel(s):

235-101-014 235-101-015 235-101-016 235-101-017 235-101-018 235-110-001 235-110-004  
235-110-011

AIRPORTS

- SELECTED PARCEL
- AIRPORT INFLUENCE AREAS
- INTERSTATES
- HIGHWAYS
- COMPATIBILITY ZONE E
- PARCELS

**\*IMPORTANT\***

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REPORT PRINTED ON...Tue Jan 29 12:38:29 2013

Version 121101



RIVERSIDE COUNTY GIS



**Selected parcel(s):**

235-101-014 235-101-015 235-101-016 235-101-017 235-101-018 235-110-001 235-110-004  
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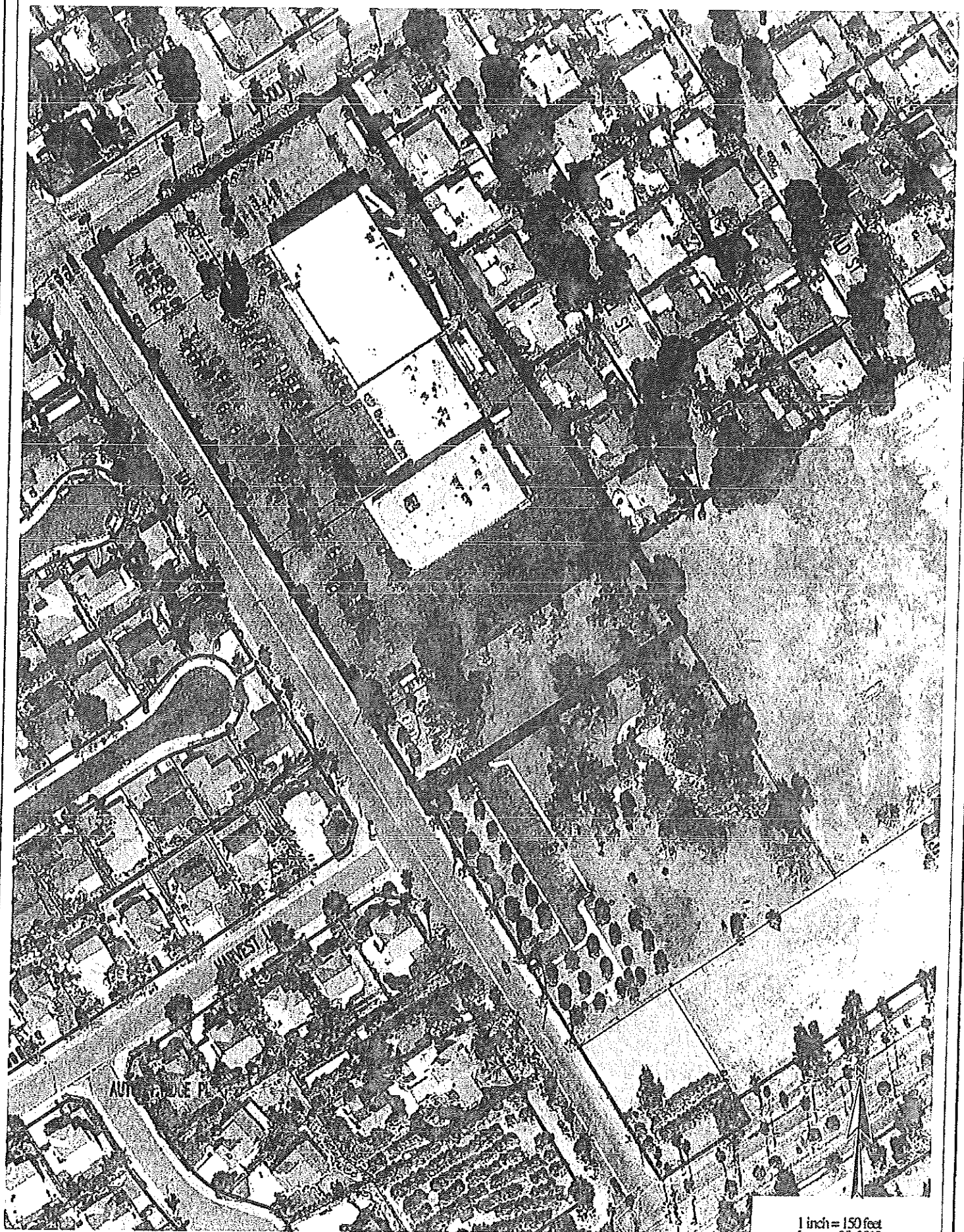
**\*IMPORTANT\***

Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON...Tue Jan 29 12:29:28 2013

Version 121101





# Stater Bros Mary Street Shopping Center - Redevelopment

1 inch = 150 feet  
January 08, 2013  
Aerial photograph taken February 2012. The City of  
Brockton makes no warranty on the accuracy or  
extent of the data provided hereon. The  
Brockton Office of Planning and Development  
has reviewed this map. The City of Brockton Official  
Hardy YSENEWALD

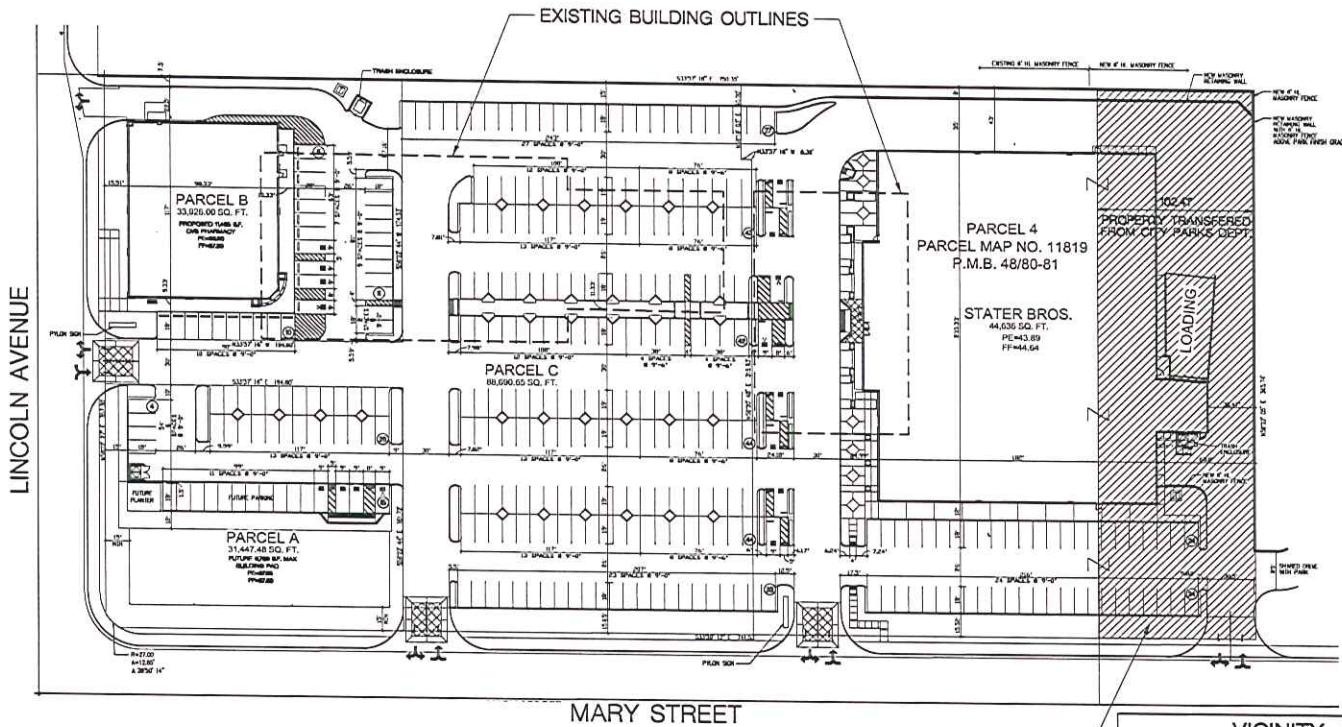




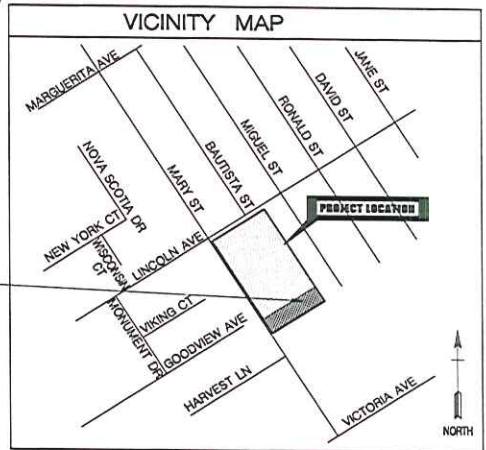
# Stater Bros Mary Street Shopping Center – Redevelopment

1 inch = 150 feet  
January 08, 2013  
Aerial photograph courtesy of the City of Riverside  
Riverside makes no warranty on the accuracy or  
extent of the data shown on this map. The  
City of Riverside (and/or its staff)  
RESERVE ALL RIGHTS. © 2013  
Riverside, CA  
Prepared by: YSENNEWALD





APPLICANT:  
 Stater Bros. Markets  
 301 S. Tippecanoe Avenue, San Bernardino, CA 92408  
 Phone: (909) 783-5005



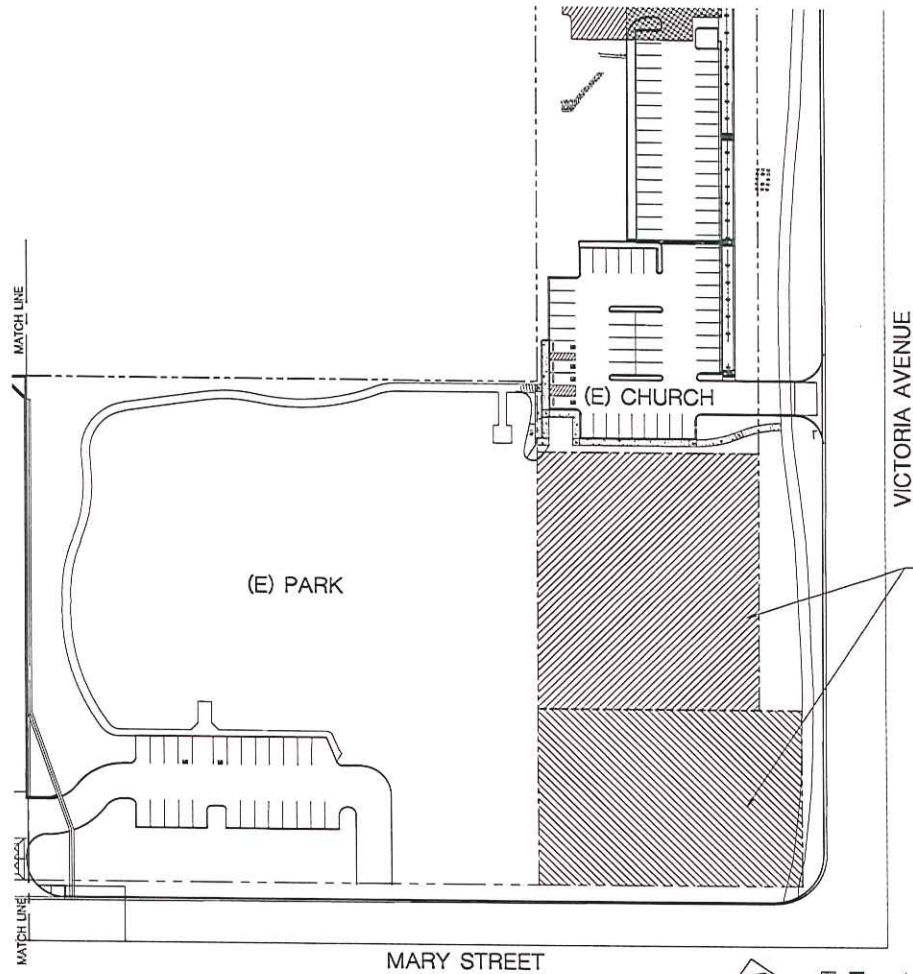
AREA SUBJECT TO ZONE CHANGE AND GENERAL PLAN AMENDMENT



ALUC REVIEW

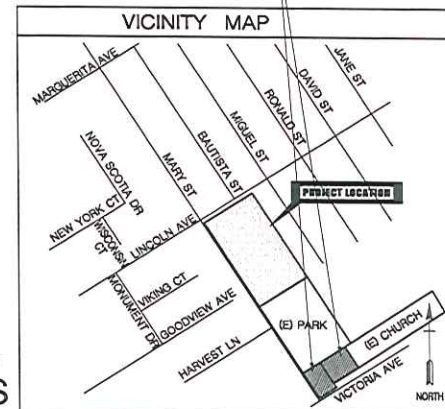
STATER BROS. RETAIL CENTER  
 SITE PLAN  
 SEC LINCOLN AVE. & MARY ST. RIVERSIDE, CALIFORNIA





AREAS SUBJECT TO ZONE CHANGE AND GENERAL PLAN AMENDMENT

APPLICANT:  
 Stater Bros. Markets  
 301 S. Tippence Avenue, San Bernardino, CA 92408  
 Phone: (909) 733-5005



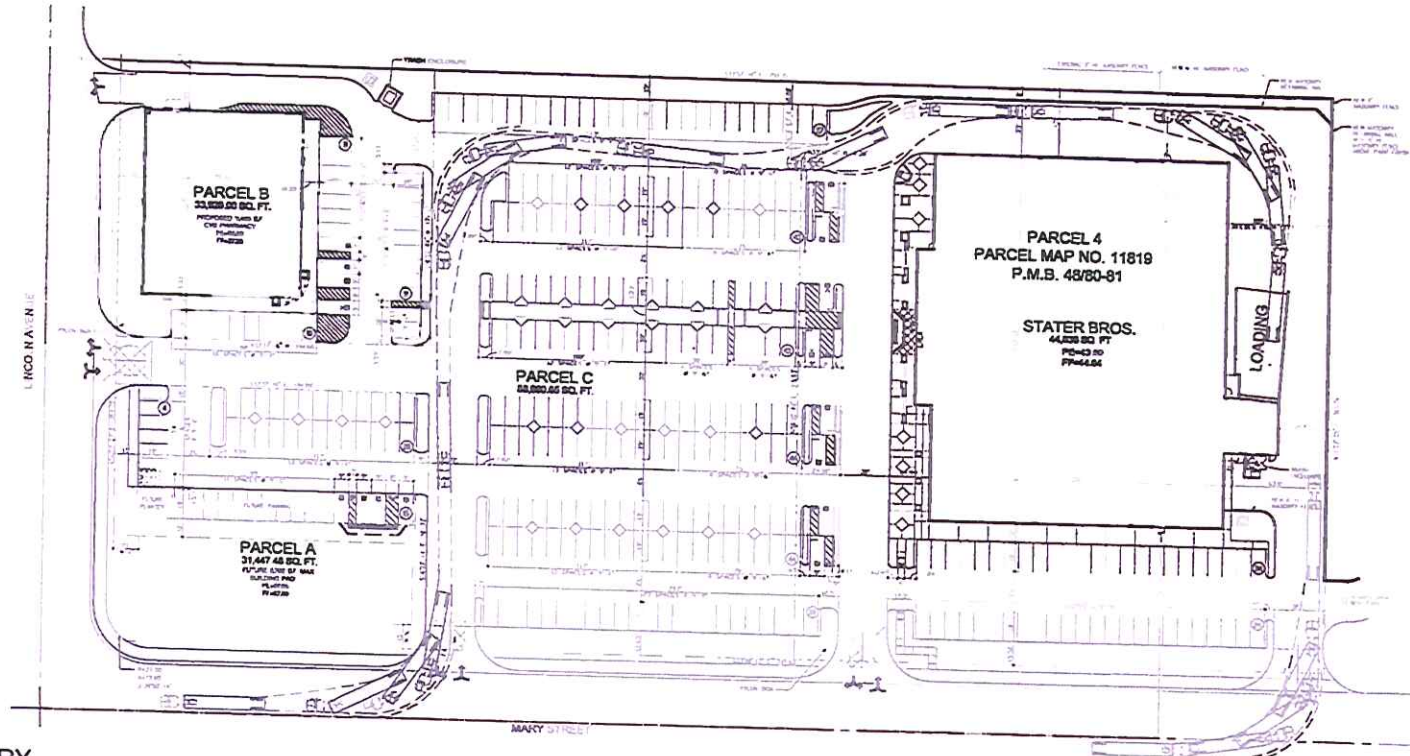
(GPA) P12-0042 (RZ) P12-0043 AFFECTED AREAS

STATER BROS. RETAIL CENTER

SITE PLAN  
 SEC LINCOLN AVE. & MARY ST. RIVERSIDE, CALIFORNIA







**PROJECT SUMMARY**

TOTAL LAND AREA 274,583 SF - 6.3 ACRES  
 TOTAL BUILDING AREA 64,861 SF  
 TOTAL PARKING PROVIDED 348 SPACES

**ZONING SUMMARY**

ZONING C-1 - COMMERCIAL

**BUILDING SUMMARY**

BUILDING CONSTRUCTION TYPE III B  
 TOTAL BLDG AREA 64,861 SF 1 PER 250 SF  
 PERCENT LOT COVERAGE (TOTAL GLA (± 64,861 SF.) / NET SITE AREA (± 274,583 SF.)) = 24%

**PARKING SUMMARY**

PARKING REQUIRED (CHAPTER 19.580.000) 260 STALLS  
 RETAIL SPACE REQD (1 PER 250 SF.)  
 TOTAL PARKING SPACES PROVIDED 348 STALLS  
 OVERALL PARKING RATIO 14 / 250  
 OVERALL PARKING RATIO 5.6 / 1000

TYP PARKING STALL VARIOUS - SEE PLAN  
 TYP DRIVE ISLE (MAIN) 30'W  
 TYP DRIVE ISLE (SECONDARY) 28'W  
 REQUIRED ADA STALLS 8 STALLS  
 PROVIDED ADA STALLS 16 STALLS  
 REQUIRED ADA VAN STALLS 2 STALLS  
 PROVIDED ADA VAN STALLS 4 STALLS

**ASSESSOR PARCEL NUMBER**

235-101-014, 15, 16, 17, 18 and 235 110-001

**ALLOWABLE FLOOR AREA**

BASIC ALLOWABLE FLOOR AREA FOR TYPE III B (ONE-STORY + MEZZ)  
 Table 503, 2007 CBC, 12,500 square feet

**ALLOWABLE AREA MODIFICATIONS**

Section 506.1, 2007 CBC  
 per section 506.2, (Equation 5-2)  
 $I = (915' / 915' - 0.25) 30 / 30 = 0.75$

**ALLOWABLE AREA INCREASE**

Automatic Sprinkler Systems Section 506.3, 2007 CBC  
 (Floor area may be doubled for a one-story building  
 if building is provided with an Automatic Sprinkler System throughout)

**TOTAL ALLOWABLE FLOOR AREA**

per section 506.1, (Equation 5-1)

$$A_s = [12,500 \text{ sq. ft.}] \times [12,500 \text{ sq. ft.} \times 0.75] + [12,500 \times 2] = 46,875 \text{ sq. ft.}$$

$$A_n = [12,500 \text{ sq. ft.}] + [9,375 \text{ sq. ft.}] + [125,000 \text{ sq. ft.}] = 146,875 \text{ sq. ft.}$$

**TOTAL ALLOWABLE FLOOR AREA**

$$44,636 \text{ sq.ft.} + 738 \text{ sq.ft.} = 45,374 \text{ sq.ft.} \text{ [Less Than 46,875 sq.ft.]}$$

**NOTE:**

Frontage Used For Allowable Area Increase Per CBC Section 506.2 Shall Be Permanently Maintained.  
 Automatic Fire Sprinkler System Installation shall Conform to Sections 903.3.11 CBC 2007

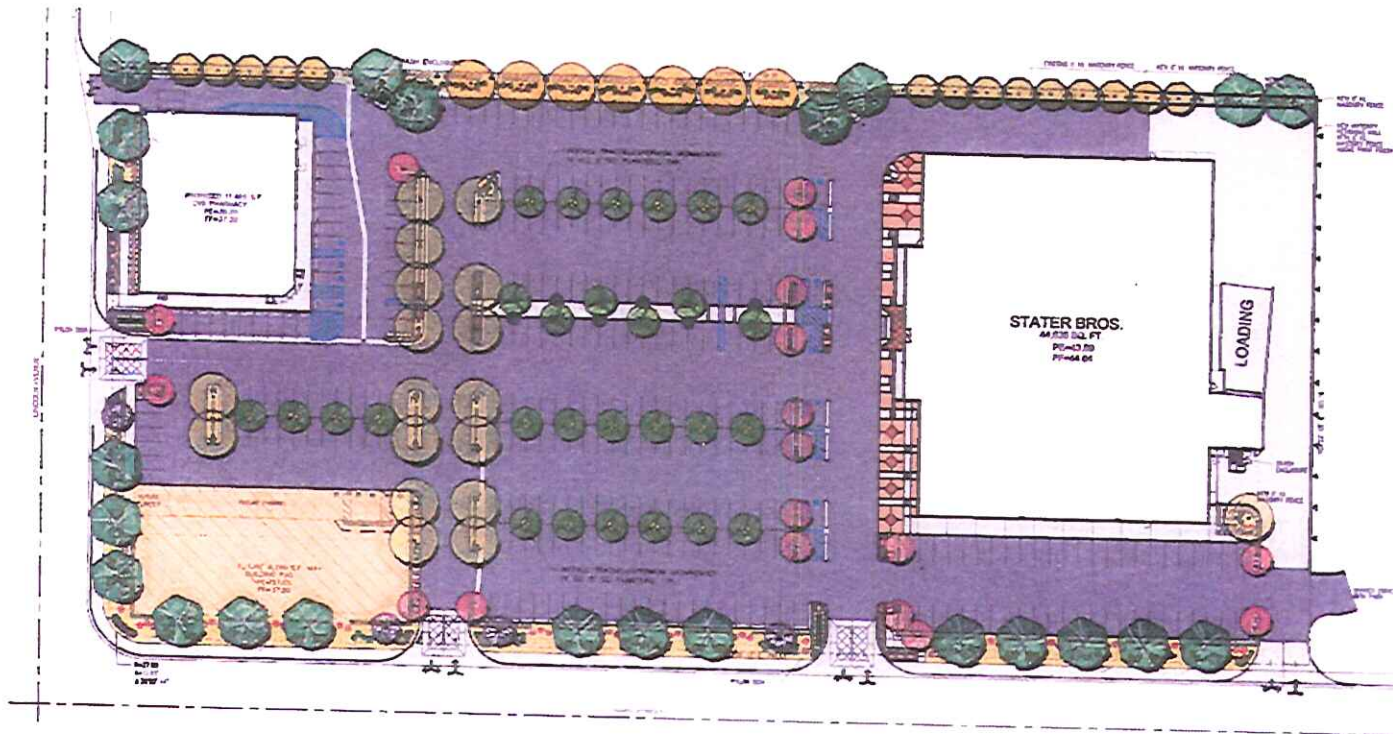
**VICINITY MAP**



**STATER BROS. RETAIL CENTER**  
 SITE PLAN

SEC LINCOLN AVE. & MARY ST. RIVERSIDE, CALIFORNIA





**PROJECT SUMMARY**

TOTAL LAND AREA 274,583 SF 6.3 ACRES  
 TOTAL BUILDING AREA 64,861 SF  
 TOTAL PARKING PROVIDED 349 SPACES

**ZONING SUMMARY**

ZONING C-1 - COMMERCIAL

**BUILDING SUMMARY**

BUILDING CONSTRUCTION TYPE III B

TOTAL BLDG AREA	SQUARE FOOTAGE	PARKING RATIO	PARKING PROVIDED
64,861 SF	1 PER 250 SF	200	24%

**PARKING SUMMARY**

PARKING REQUIRED (CHAPTER 19.500.060)  
 RETAIL SPACE REQD (1 PER 250 SF) 260 STALLS

TOTAL PARKING SPACES PROVIDED 349 STALLS  
 OVERALL PARKING RATIO 14 / 250  
 OVERALL PARKING RATIO 5.6 / 1000

TYP PARKING STALL VARIOUS - SEE PLAN  
 TYP DRIVE ISLE (MAIN) 30W  
 TYP DRIVE ISLE (SECONDARY) 26W

REQUIRED ADA STALLS 8 STALLS  
 PROVIDED ADA STALLS 16 STALLS  
 REQUIRED ADA VAN STALLS 2 STALLS  
 PROVIDED ADA VAN STALLS 4 STALLS

**ASSESSOR PARCEL NUMBER**

225-101-014,15,16,17,18 and 225-10-001

**ALLOWABLE FLOOR AREA**

BASIC ALLOWABLE FLOOR AREA FOR TYPE II B (ONE-STORY + MEZZ)  
 Table 503, 2007 CBC, 12,500 square feet

**ALLOWABLE AREA MODIFICATIONS**

Section 506.1, 2007 CBC  
 per section 506.2, (Equation 5-2)  
 $I_1 = (9'15" / 9'15" \cdot 0.25) 30 / 30 = 0.75$

**ALLOWABLE AREA INCREASE**

Automatic Sprinkler Systems Section 506.3, 2007 CBC  
 (Floor area may be doubled for a one-story building  
 if building is provided with an Automatic Sprinkler System throughout)

**TOTAL ALLOWABLE FLOOR AREA**  
 per section 506.1, (Equation 5-1)

$$A_s = \frac{(12,500 \text{ sq. ft.})}{(1 \text{ from CBC})} + \frac{(12,500 \text{ sq. ft.} \cdot 0.75)}{(1 \text{ from CBC})} + (12,500 \times 2) = 44,638 \text{ sq. ft.}$$

$$A_s = (12,500 \text{ sq. ft.}) + (9,375 \text{ sq. ft.}) + (25,000 \text{ sq. ft.}) = 46,875 \text{ sq. ft.}$$

**TOTAL ALLOWABLE FLOOR AREA**  
 44,638 sqft. + 788 sqft. = 45,426 sqft. [Less Than 46,875 sqft.]  
 (From Plan Area) (From Floor Area)

**NOTE:**

"Frontage Used For Allowable Area Increase Per CBC Section 506.2 Shall Be Permanently Maintained".  
 Automatic Fire Sprinkler System Installation shall Conform to Sections 903.3.11, CBC 2007



**STATER BROS. RETAIL CENTER**

**SITE PLAN**

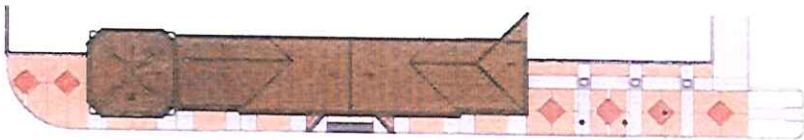
SEC LINCOLN AVE. & MARY ST. RIVERSIDE, CALIFORNIA







1 NORTH ELEVATION (FRONT WALL)



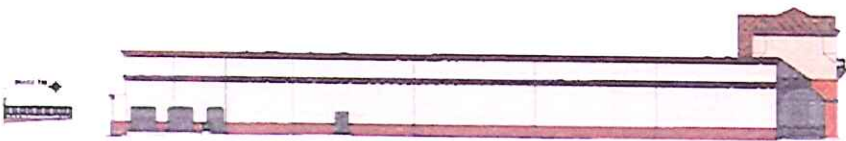
CONCRETE WALK PATTERN DESIGN

**FINISH SCHEDULE:**

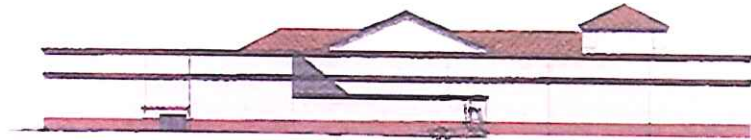
- |  |   |
|--|---|
| <p><b>A.</b> DUNN EDWARDS #DEC 763 -ALMOND-</p> <p><b>B.</b> DUNN EDWARDS #DEC 720 -CLIFFS VIEW-</p> <p><b>C.</b> DUNN EDWARDS #DEC 5216 -AUTUMN UMBER-</p> <p><b>D.</b> CONCRETE SCOFIELD -SOMBRERO BUFF-</p> | <p><b>E.</b> CONCRETE SCOFIELD -SUNBAKED CLAY-</p> <p><b>F.</b> CONCRETE -NATURAL GREY-</p> <p><b>G.</b> EAGLE TILE #3605 -SAN BENITO BLEND-</p> <p><b>H.</b> EL Dorado Stone VENEER -COUNTRY RUBBLE-</p> <p><b>J.</b> HIGGINS BRICK VENEER -HERMOSA-</p> <p><b>K.</b> ORCO FIELD PRECISION BLOCK -TAN-</p> <p><b>L.</b> ORCO PATTERN SPLITFACE BLOCK -TAN-</p> |
|--|---|



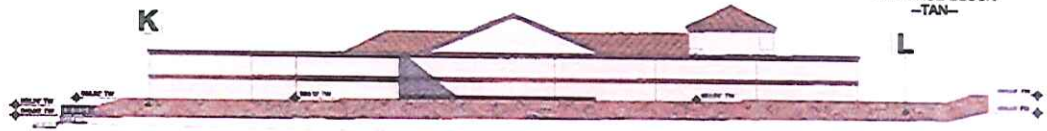
2 WEST ELEVATION (RIGHT WALL)



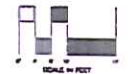
3 EAST ELEVATION (LEFT WALL)



4 SOUTH ELEVATION (REAR WALL)



5 SOUTH ELEVATION (REAR WALL) W/ WALL PROFILE



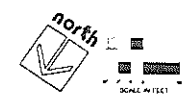
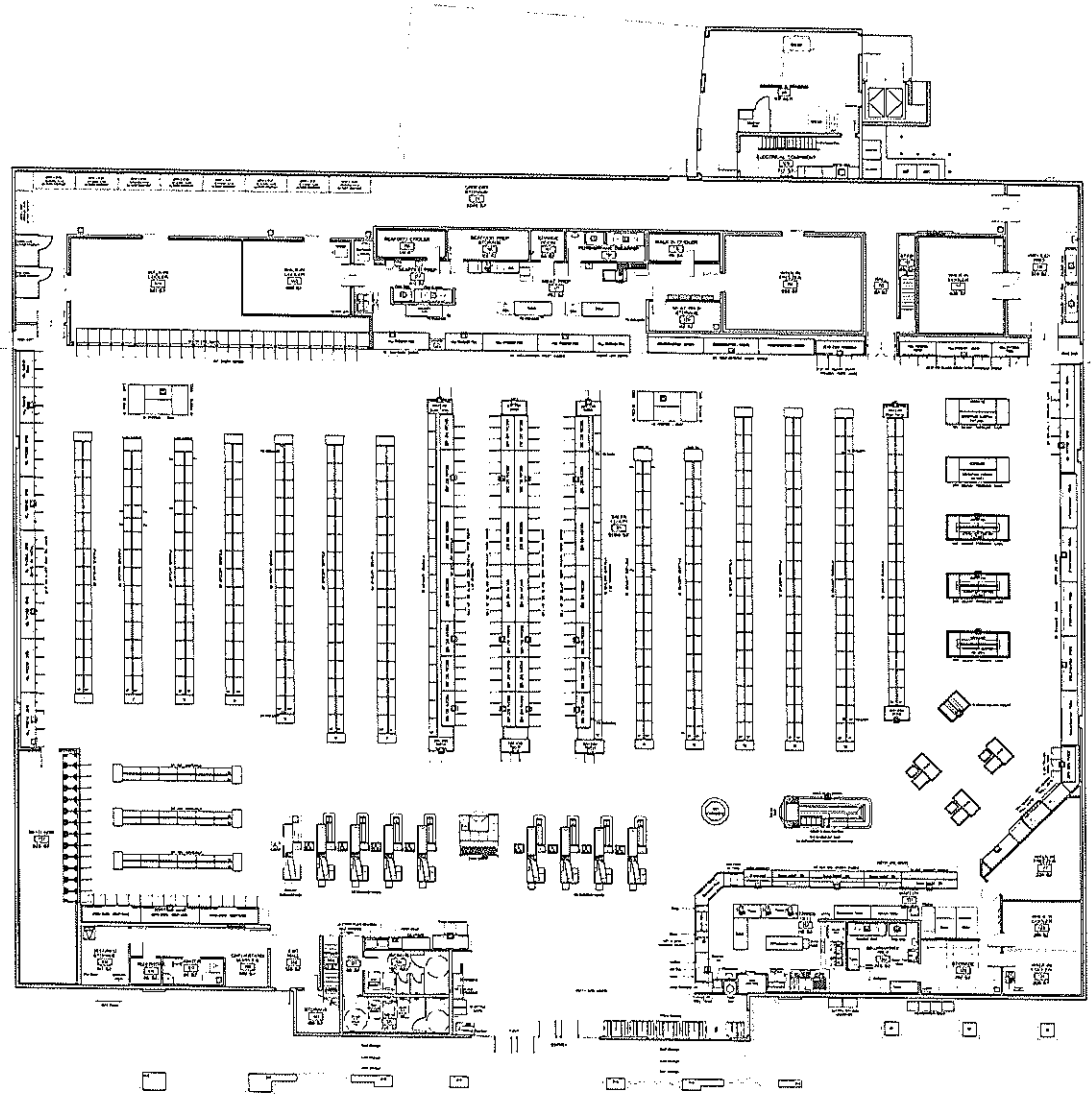
**STATER BROS. RETAIL CENTER**

EXTERIOR ELEVATIONS  
SEC LINCOLN AVE. & MARY ST. RIVERSIDE, CALIFORNIA









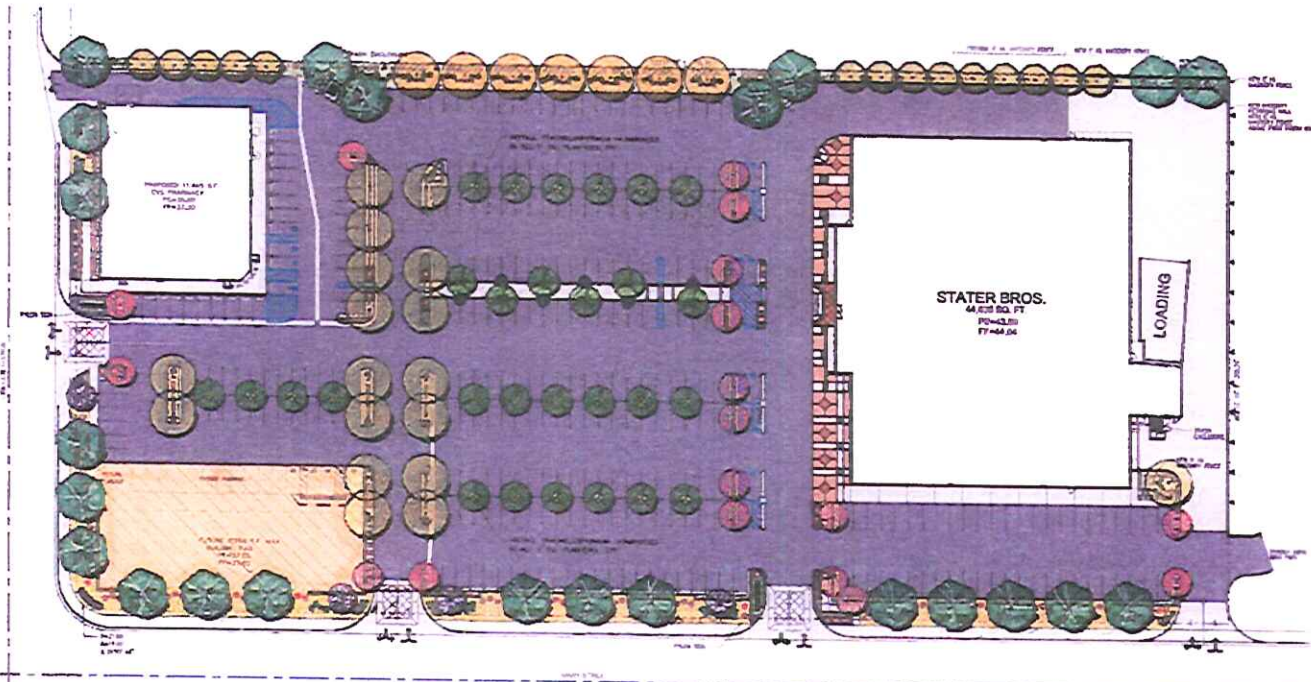
STATER BROS. RETAIL CENTER  
 FLOOR PLAN  
 SEC LINCOLN AVE. & MARY ST. RIVERSIDE, CALIFORNIA











**PROPOSED PLANTING LEGEND**

SYMBOL	BOTANICAL NAME COMMON NAME	QTY	CONTAINER SIZE W x H	WATER USE CATEGORY	SYMBOL	BOTANICAL NAME COMMON NAME	QTY	CONTAINER SIZE W x H	WATER USE CATEGORY
	PIZZI COPALMUS CARROT SEED	9	34" BGA 35" x 40"	MODERATE		DIKESIA VEGETA AFRICAN BOX	140	3 GALLON 7" x 7"	MODERATE
	DISCALYPTUS POLYANTHOS SLIMY DOLLAR BUSH	14	15 GALLON 30" x 40"	LOW		ELAEAGNUS PARVIFLORA SANDSPRINT	68	9 GALLON 4" x 6"	LOW
	JACARANDA JACARANDA	4	36" BGA 36" x 30"	MODERATE		SPRENGELIA SWEET GIBBERELLA	87	3 GALLON 7" x 6"	LOW
	NEOLARIX PANDULATA SANTALINI TRE	32	24" BGA 24" x 25"	LOW		MONOCASIA 'VELLA STONY' 'YELLOW DANGLY'	488	1 GALLON 5" x 3"	MODERATE
	LACINIA STRATA 'POPCORN LACE' CORNICE WREATH	24	30" BGA 30" x 30"	MODERATE		LANTANA 'NEW ZEAL' LANTANA	38	5 GALLON 7" x 7"	LOW
	FRAXINUS TORREYANA ORNAMENTAL PEAR	28	24" BGA 24" x 40"	MODERATE		HYDRANGEA 'DORIS SILVER GRASS	17	3 GALLON 4" x 6"	MODERATE
	SHAWNEE 'MAJESTIC BEAUTY' MAJESTIC BEAUTY HYDRANGEA	8	15 GALLON 8" x 20"	MODERATE		POLYANTHUS 'POLYANTH' POLYANTHUS	81	1 GALLON 4" x 4"	MODERATE
	FRAXINUS TORREYANA ORNAMENTAL PEAR	7	24" BGA 24" x 30"	LOW		FRAXINUS TORREYANA ORNAMENTAL PEAR	93	15 GALLON 8" x 8"	MODERATE
	FRAXINUS TORREYANA ORNAMENTAL PEAR	65	3 GALLON 7" x 7"	LOW		FRAXINUS TORREYANA ORNAMENTAL PEAR	49	3 GALLON 30" x 30"	MODERATE
	FRAXINUS TORREYANA ORNAMENTAL PEAR	53	5 GALLON 18" x 18"	MODERATE		FRAXINUS TORREYANA ORNAMENTAL PEAR	30	15 GALLON	LOW

**NOTES:**  
 ALL TREES OTHER THAN HYDRANGEA SHALL BE INSTALLED WITH A 200# ROOT BARRIER.  
 PLANTING AREAS SHALL BE MAINTAINED WITH A THREE INCH 1/2" MINIMUM LAYER OF ORGANIC MULD BELLOW AREAS OF CONTOURED PLANTED FROM PLANT SHALL BE MAINTAINED WITH A ONE AND ONE HALF INCH (1 1/2") MINIMUM LAYER OF ORGANIC MULD BELLOW.

**LANDSCAPE AREA AND WATER USE**  
 LOW WATER USE: 7,527 SQ. FT.  
 MODERATE WATER USE: 25,719 SQ. FT.  
 LOW WATER USE TEMPORARY: 14,200 SQ. FT.  
 TOTAL LANDSCAPE AREA: 47,446 SQ. FT.

**MAXIMUM ANNUAL WATER ALLOWANCE (GALLES)**  
 (16.25 GALLONS (0.7" x 1000') = 1,250 GALLONS PER YEAR)



**STATER BROS. RETAIL CENTER**  
 CONCEPTUAL LANDSCAPE PLAN  
 SEC LINCOLN AVE. & MARY ST. RIVERSIDE, CALIFORNIA



# **NOTICE OF PUBLIC HEARING**

## **RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION**

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the application described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. The proposed project application may be viewed at the Riverside County Administrative Center, 4080 Lemon Street, 14<sup>th</sup> Floor, Riverside, California 92501, Monday through Thursday from 8:00 a.m. to 5:00 p.m.

PLACE OF HEARING: Riverside County Administration Center  
4080 Lemon St., 1<sup>st</sup> Floor Hearing Room  
Riverside, California

DATE OF HEARING: April 11, 2013

TIME OF HEARING: 9:00 A.M.

### **CASE DESCRIPTION:**

ZAP1086MA12 – Stater Bros. Markets and CVS Drugs (Representative: Dan Schultz, Mile Stone Management) – City of Riverside Case Nos.: P12-0444 (Design Review) and P12-0761 (Conditional Use Permit). Reconsideration of height limit for drug store (request 36 foot height). These cases relate to the expansion and redevelopment of an existing 5.43-acre Stater Bros. commercial center located at the southeast corner of Mary Street and Lincoln Avenue. P12-0444 proposes to expand the center to 6.3 acres, with a new 44,636 square foot Stater Bros. grocery store, an 11,465 square foot CVS drug store with pharmacy, and an 8,760 square foot pad for future commercial development. P12-0761 is a request for a Conditional Use Permit to allow (1) the operation of a drive-thru pharmacy and (2) alcohol sales for off-site consumption. (Area III of the March Air Reserve Base Airport Influence Area).

**FURTHER INFORMATION:** Contact John Guerin at (951) 955-0982. The ALUC holds hearings for local discretionary permits within the Airport Influence Areas, reviewing for aeronautical safety, noise and obstructions. All other concerns should be addressed to Ms. Yvette Sennewald of the City of Riverside Planning Department, at (951) 826-5168.

235 030 003  
Jose Gutierrez  
6931 Lincoln Ave  
Riverside, CA 92506

235 030 011  
Benn Vavra  
2932 Mary St  
Riverside, CA 92506

235 030 056  
Pedro Aleman Jr.  
6905 Lincoln Ave  
Riverside, CA 92506

350 41 016  
Jose Hernandez  
2931 Miguel St  
Riverside, CA 92506

235 041 017  
Jose Holme Lovos Orellana  
2917 Miguel St  
Riverside, CA 92506

235 041 018  
Donna Dunn  
2905 Miguel St  
Riverside, CA 92506

235 042 016  
Laurie McCoy  
17130 Van Buren Blvd #157  
Riverside, CA 92504

235 042 017  
Francisco Javier Robles  
2918 Miguel St  
Riverside, CA 92506

235 042 018  
Rosella Hunt  
880 Eagle Crest Ct  
Riverside, CA 92506

235 042 019  
Victor Meinke Jr.  
5234 Clarewood Dr  
Oakland, CA 94618

235 042 020  
Lucy Brown  
2917 Bautista St  
Riverside, CA 92506

235 042 021  
Thomas Bums  
2931 Bautista St  
Riverside, CA 92506

235 043 016  
Faysal Bahhour  
2932 Bautista St  
Riverside, CA 92506

235043017  
George Londos  
2918 Bautista St  
Riverside, CA 92506

235043018  
Danny Dwayne Webb  
2906 Bautista St  
Riverside, CA 92506

235 043 019  
John Musarra  
2905 Mary St  
Riverside, CA 92506

235 043 020  
Ramon Duran & Eric Morgan  
2917 Mary St  
Riverside, CA 92506

235 043 021  
Peter King  
2931 Mary St  
Riverside, CA 92506

235 051 018  
Victor Rosales  
9215 Trovita Cir  
Riverside, CA 92508

235 091 001  
Paul & Gwendolyn Shook  
6906 Lincoln Ave  
Riverside, CA 92506

235 091 002  
Marilyn Burdett  
6916 Lincoln Ave  
Riverside, CA 92506

235 091 003  
Century Inland Empire LLC  
PO Box 480905  
Los Angeles, CA 90048

235 091 004  
Cecil Willey  
24600 S 630 Trl  
Wyandotte, OK 74370

235 091 011  
Steven & Cathryn Mendoza Jr.  
6939 Viking Ct  
Riverside, CA 92506

235 091 012  
Carlos Ronquillo  
6929 Viking Ct  
Riverside, CA 92506

235 091 014  
Michael James Grilli  
6909 Viking Ct  
Riverside, CA 92506

235 091 015  
Pathmarajan Appadorai  
1734 Healy Pl  
Riverside, CA 92506

235 091 016  
William Schubert  
1537 Jessamyn Ct  
Riverside, CA 92506

235 091 017  
Annette Dunn  
19428 Hebron Ct  
Riverside, CA 92508

235 091 018  
Raymond & Marcia Vasquez  
894 Sandcastle Dr  
Corona Del Mar, CA 92625



235 091 025  
Peter Huang  
10762 National Blvd  
Los Angeles, CA 90064

235 091 026  
Lee Family  
8223 Kinghurst Rd  
San Gabriel, CA 91775

235 091 027  
Lynda Kocis  
6917 Goodview Ave  
Riverside, CA 92506

235 091 028  
Morris Frank Mendoza  
7485 Santa Rosa Way  
Riverside, CA 92504

235 091 029  
Homaira Haider  
2380 Mary St  
Riverside, CA 92506

235 091 030  
Randell Carder  
6918 Goodview Ave  
Riverside, CA 92506

235 091 031  
Bruce Malone  
6928 Goodview Ave  
Riverside, CA 92506

235 091 032  
Rodolfo Zaragoza  
6938 Goodview Ave  
Riverside, CA 92506

235 091 052  
Anna Marie Viramontes  
6923 Harvest Ln  
Riverside, CA 92506

235 091 053  
Dale Phillips Jr. & Delann Hannan  
6913 Harvest Ln  
Riverside, CA 92506

235 091 054  
Tae Kwak  
6903 Harvest Ln  
Riverside, CA 92506

235 093 001  
Kevin Ferguson  
6902 Harvest Ln  
Riverside, CA 92506

235 093 002  
John Keen  
6746 Hawarden Dr  
Riverside, CA 92506

235 093 003  
Baltazar Reyes Jr. & Sandra Lee Reyes  
6922 Harvest Ln  
Riverside, CA 92506

235 093 004  
Anthony Andre  
6932 Harvest Ln  
Riverside, CA 92506

235 093 006  
Franklin Earley Sr. & Dorothy Earley  
2743 Autumn Ridge Pl  
Riverside, CA 92506

235 093 007  
Larry John Guidry  
2733 Autumn Ridge Pl  
Riverside, CA 92506

235 093 008  
Jerry Lee  
2723 Autumn Ridge Pl  
Riverside, CA 92506

235 093 019  
Jane Lamar  
2732 Mary St  
Riverside, CA 92506

235 101 004  
James Lee Gatten Sr.  
2894 Miguel St  
Riverside, CA 92506

235 101 005  
Liborio Leon  
2118 E Ward Ter  
Anaheim, CA 92806

235 101 006  
Jessica Ayala  
2870 Miguel St  
Riverside, CA 92506

235 101 007  
Roxane Young  
2101 Glenhaven Dr  
Corona, CA 92882

235 101 008  
Melinda Monk  
2844 Miguel St  
Riverside, CA 92506

235 101 009  
William Valdez  
7354 Diamond St  
Riverside, CA 92504

235 101 010  
Susan Reynolds  
2818 Miguel St  
Riverside, CA 92506

235 101 014  
Bros Markets Stater  
301 S Tippecanoe Ave  
San Bernardino, CA 92408

235 101 015  
Bros Markets Stater  
301 S Tippecanoe Ave  
San Bernardino, CA 92408

235 101 016  
Bros Markets Stater  
301 S Tippecanoe Ave  
San Bernardino, CA 92408

235 101 017  
Bros Markets Stater  
301 S Tippecanoe Ave  
San Bernardino, CA 92408

235 101 018  
Bros Markets Stater  
PO Box 150  
San Bernardino, CA 92402

235 102 001  
George Hayes Milliron  
2806 Ronald St  
Riverside, CA 92506

235 102002  
Reed Grundy  
2818 Ronald St  
Riverside, CA 92506

235 102 003  
Edward Peace  
2832 Ronald St  
Riverside, CA 92506

235 102 004  
Jean Okeefe  
2844 Ronald St  
Riverside, CA 92506

235 102 005  
Craig Thomson  
5939 Petite Ct  
Riverside, CA 92504

235 102 006  
Aldace & I Crow  
2870 Ronald St  
Riverside, CA 92506

235 102 007  
Simona Valero  
2882 Ronald St  
Riverside, CA 92506

235 102 008  
Ralph Winn  
11209 Saddle Ridge Rd  
Moreno Valley, CA 92557

235 102 010  
Robert McAndrew  
2881 Miguel St  
Riverside, CA 92506

235 102 011  
Jacinto Fierro  
2869 Miguel St  
Riverside, CA 92506

235 102 012  
Helen Nichols & Marianna Miklos  
2855 Miguel St  
Riverside, CA 92506

235 102 013  
Kenneth Crane  
2843 Miguel St  
Riverside, CA 92506

235 102 014  
Arlene Miller  
2831 Miguel St  
Riverside, CA 92506

235 102 015  
O Manuel Mundo & Mundo Lidoina Ju  
2817 Miguel St  
Riverside, CA 92506

235 110 001  
City of Riverside  
3900 Main St  
Riverside, CA 92522

235 110 004  
Pathway Christian Church of Riverside  
6755 Victoria Ave  
Riverside, CA 92506

235 110 009  
David Nelson  
2720 Jane St  
Riverside, CA 92506

235 110 010  
Lee Rivera  
6705 Victoria Ave  
Riverside, CA 92506

235 110 011  
Bros Markets Stater  
6893 Victoria Ave  
Riverside, CA 92506

235 121 032  
Stephen Youngerman  
7972 Peak Ct  
Riverside, CA 92506

235 121 033  
Nicholas KJute  
2725 Jane St  
Riverside, CA 92506

235 121 036  
Natalie Salazar Hernandez  
7032 Hawarden Dr  
Riverside, CA 92506

235 150 037  
Kenyard Carroll  
6909 Sundance Trl  
Riverside, CA 92506

235 150 038  
Miguel & Raquel Gloria  
6929 Sundance Trl  
Riverside, CA 92506

235 190 065  
Jane Gadzinski  
2605 Doubletree Dr  
Riverside, CA 92506

235 351 001  
Anthony Blakely  
6879 Sundance Trl  
Riverside, CA 92506

235 351 002  
Richard Evans  
6871 Sundance Trl  
Riverside, CA 92506

235 351 003  
Gerald Dunn  
6865 Sundance Trl  
Riverside, CA 92506

235 351 004  
Kevin Reilly  
6859 Sundance Trl  
Riverside, CA 92506

235 351 005  
William Ginwright  
6853 Sundance Trl  
Riverside, CA 92506

235 351 006  
John Falter Jr. & Cheryl Falter  
6847 Sundance Trl  
Riverside, CA 92506

235 351 007  
Saundra Ann Dickerson  
6841 Sundance Trl  
Riverside, CA 92506

235 351 008  
James Perod  
6835 Sundance Trl  
Riverside, CA 92506

235 400 001  
Loraine Hoffinan  
6717 Hunt Ct  
Riverside, CA 92506

235 400 002  
Norris Family  
6391 Magnolia Ave #C  
Riverside, CA 92506

235 400 003  
Claudia Chase  
6735 Foxhall Ct  
Riverside, CA 92506

235 400 004  
Linda Kay Townsend  
6727 Foxhall Ct  
Riverside, CA 92506

235 400 041  
Stonegate On Victoria Homeowners Ass  
18801 Ventura Blvd  
Tarzana, CA 91356

235 400 042  
Stonegate On Victoria Homeowners Ass  
18801 Ventura Blvd  
Tarzana, CA 91356

235 400 044  
Bradley Alan Sackett  
6700 Victoria Ave  
Riverside, CA 92506



Margaret Albanese Attn: Fire Prevention  
City of Riverside, Fire Department  
3900 Main Street, 3rd Floor  
Riverside, CA 92522

Deisy Ruiz, Mayor's Commission on Aging  
City of Riverside, Mayor's Office  
3900 Main Street, 7th Floor  
Riverside, CA 92522

Monique Gordon, Community on Disability Issues  
City of Riverside, Mayor's Office  
3900 Main Street, 7th Floor  
Riverside, CA 92522

Randy McDaniel, Project Manager  
City of Riverside, Park and Recreation  
6927 Magnolia Avenue, 2nd Floor  
Riverside, CA 92506

Matt Bates, Water Engineer  
City of Riverside, Public Utilities  
Mission Square Building  
3750 University Avenue, 4th Flr  
Riverside, CA 92501

Summer Delgado, Electric Eng., Sys. Planning  
City of Riverside, Public Utilities  
Mission Square Building  
3750 University Avenue, 4th Flr  
Riverside, CA 92501

Rob VanZanten  
City of Riverside, Public Works  
3900 Main Street, 4th Floor  
Riverside, CA 92522

Toni Redman tredmnn@riversideca.gov  
Planning  
City of Riverside, Public Utilities  
Mission Square Building  
3750 University Avenue, 4th Flr  
Riverside, CA 92501

Robert Filiar  
City of Riverside, Public Works Corp. Yard  
8095 Lincoln Avenue  
Riverside, CA 92504

Traci Dose, Administrative Analyst  
Riverside Police Department  
Magnolia Station - 10540 Magnolia Ave.  
Riverside, CA 92505

Nathan Freeman, Casu Blanca Project Area  
City of Riverside, Redevelopment Division  
3900 Main Street, 5th Floor  
Riverside, CA 92522

Tim Clark  
ABC District Administrator  
3737 Main Street, Ste 900  
Riverside, CA 92501

AT&T California  
Ilene Kutzle, AT&T Substructure Desk  
1265 Van Buren Blvd., Rm 180  
Anaheim, CA 92807

Dennis Garcia  
Casu Blanca Community Action Group  
7339 Peters St.  
Riverside, CA 92504

Center on Deafness-Inland Empire  
3576 Arlington Ave., Ste 211  
Riverside, CA 92506

John Guerin  
County of Riverside ALHC  
4080 Lemon Street, 141b Floor  
Riverside, CA 92501

Scott Walter Wheaton, Project Manager  
The Gas Company  
4495 Howard Avenue  
Riverside, CA 92507

Cindy Roth  
Greater Riverside Chambers of Commerce  
3985 University Avenue  
Riverside, CA 92501

Donald Chase MARB  
Global Environmental Consulting  
1736 Franklin Street, Suite 500  
Oakland, CA 94612

Tom Franklin,  
Riverside Transit Agency  
1825 Third Street  
Riverside, CA 92507-3416

Ken Mueller  
Riverside Unified School District  
3070 Washington Street  
Riverside, CA 92504

Janet Dixon  
Riverside Unified School District  
3070 Washington Street  
Riverside, CA 92504

Hal Snyder  
Victoria Avenue Forever  
5475 Victoria Avenue  
Riverside, CA 92506

Daniel M. Hays  
Victoria Avenue Restoration Project  
2640 Anna Street  
Riverside, CA 92506

Victoria Avenue Historic Restoration Group  
Mrs. Gerry Marr  
6816 Hawarden Drive  
Riverside, CA 92506

STATER BROS. DEVELOPMENT, INC  
ATTN: SCOTT LIMBACHER  
301 S. TIPPECANOE AVENUE  
SAN BERNARDINO, CA 92408

STATER BROS. DEVELOPMENT, INC  
ATTN: SCOTT LIMBACHER  
301 S. TIPPECANOE AVENUE  
SAN BERNARDINO, CA 92408

MILESTONE MANAGEMENT  
ATTN: DAN SCHULTZ  
1422 S. ALLEC STREET  
SUITE A  
ANAHEIM, CA 92805

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ANAHEIM, CA 92805

CITY OF RIVERSIDE COMMUNITY  
DEVELOPMENT DEPT PLANNING DIV  
ATTN: YVETTE SENNEWALD  
3900 MAIN STREET, 3RD FLOOR  
RIVERSIDE, CA 92522

CITY OF RIVERSIDE COMMUNITY  
DEVELOPMENT DEPT PLANNING DIV  
ATTN: YVETTE SENNEWALD  
3900 MAIN STREET, 3RD FLOOR  
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MILESTONE MANAGEMENT  
ATTN: DAN SCHULTZ  
1422 S. ALLEC STREET  
SUITE A  
ANAHEIM, CA 92805

KZ DEVELOPMENT COMPANY  
ATTN: ANTHONY LOPEZ  
18818 TELLER AVENUE  
SUITE 100  
IRVINE, CA 92612

KZ DEVELOPMENT COMPANY  
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SUITE 100  
IRVINE, CA 92612

# APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC Identification No.

## RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

ZAP 1086MA12

### PROJECT PROPONENT (TO BE COMPLETED BY APPLICANT)

Date of Application NOV 2, 2012  
Property Owner STATER BROS MARKETS Phone Number 909 733-5005  
Mailing Address 301 S. TIPPECANOE AVENUE  
SAN BERNARDINO, CA 92408

Agent (if any) SCOTT LIMBACHER, V.P. CONST. Phone Number 909 733-5005  
Mailing Address SAME

### PROJECT LOCATION (TO BE COMPLETED BY APPLICANT)

Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways

Street Address 2861 MARY STREET (all in use) 235-110-011 011000  
RIVERSIDE, CA 92506 235-110-0011 235-110-001000  
Assessor's Parcel No. 235-101-014, 15, 16, 17, 18 (CUP/DC)\* Parcel Size 6.3 ACRES  
Subdivision Name PPA relates to 235-110-0011 & 011000 Zoning  
Lot Number Rezone relates to 235-110-0011 Classification CR-NC

### PROJECT DESCRIPTION (TO BE COMPLETED BY APPLICANT)

If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed

Existing Land Use NEIGHBORHOOD SHOPPING CENTER  
(describe)

Proposed Land Use NEIGHBORHOOD SHOPPING CENTER  
(describe)

For Residential Uses Number of Parcels or Units on Site (exclude secondary units) \_\_\_\_\_

For Other Land Uses Hours of Use \_\_\_\_\_

(See Appendix C) Number of People on Site Maximum Number \_\_\_\_\_

Method of Calculation \_\_\_\_\_

Height Data Height above Ground or Tallest Object (including antennas and trees) 45' ft.  
Highest Elevation (above sea level) of Any Object or Terrain on Site 989.64' ft.

Flight Hazards Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?  Yes  No

If yes, describe \_\_\_\_\_



**REFERRING AGENCY (TO BE COMPLETED BY AGENCY STAFF)**

Date Received NOV 2 2012 Type of Project  
 Agency Name CITY OF RIVERSIDE  General Plan Amendment  
 Staff Contact YVETTE SENNEWALD  Zoning Amendment or Variance  
 Phone Number 951 826 5168  Subdivision Approval  
 Agency's Project No. P12-0442-0444  Use Permit  
CUP - P12-0761 / Design Rev. P12-0444  Public Facility  
 Other \_\_\_\_\_

**ALUC REVIEW (TO BE COMPLETED BY ALUC EXECUTIVE DIRECTOR)**

Application Receipt Date Received \_\_\_\_\_ By \_\_\_\_\_  
 Is Application Complete?  Yes  No  
 If No, cite reasons \_\_\_\_\_

Airport(s) Nearby \_\_\_\_\_  
 Primary Criteria Review Compatibility Zone(s)  A  B1  B2  C  D  E  HI.  
 Allowable (not prohibited) Use?  Yes  No  
 Density/Intensity Acceptable?  Yes  No  
 Open Land Requirement Met?  Yes  No  
 Height Acceptable?  Yes  No  
 Easement/Deed Notice Provided?  Yes  No

Special Conditions Describe: \_\_\_\_\_  
 \_\_\_\_\_

Supplemental Criteria Review  
 Noise \_\_\_\_\_  
 Safety \_\_\_\_\_  
 Airspace Protection \_\_\_\_\_  
 Overflight \_\_\_\_\_

**ACTIONS TAKEN (TO BE COMPLETED BY ALUC EXECUTIVE DIRECTOR)**

ALUC Executive Director's Action  Approve Date \_\_\_\_\_  
 Refer to ALUC  
 ALUC Action  Consistent Date \_\_\_\_\_  
 Consistent with Conditions (list conditions/attach additional pages if needed)  
 \_\_\_\_\_  
 Inconsistent (list reasons/attach additional pages if needed)  
 \_\_\_\_\_  
 \_\_\_\_\_



City of Arts & Innovation

# COMMUNITY DEVELOPMENT DEPARTMENT Planning Division

## Transmittal Sheet

### TRANSMITTAL OF MATERIALS

January 8, 2013

TO: Bob Jerz, Fire Department  
Rob Van Zanten, Public Works

RUSD, Ken Mueller

RUSD, Janet Dixon

AUSD, Sheryl Calhoun

Other: See Attached List

### TRANSMITTED HERewith ARE THE FOLLOWING ITEMS:

1. Plot Plan, Plat Map, or other graphic(s) for reference purposes

We have received the following application for the redevelopment of an existing Stater Bros. commercial center that includes a General Plan Amendment, a Rezone, Design Review and a Conditional Use Permit for CVS pharmacy to be reviewed by City Planning Commission in January, 2013. The following is a brief description of the project:

**PLANNING CASES P12-0042 (GPA), P12-0043 (RZ), P12-0044 (DR), and P12-0761 (CUP):** Proposal by Scott Limbacher, on behalf of Stater Brothers Markets, for consideration of a General Plan Amendment to amend the Land Use Designation of a portion of the project area from P - Park to C - Commercial and from LDR - Low Density Residential to P - Park, to Rezone a portion of the project area from R-1-13,000 - Single Family Residential to CR-NC - Commercial Retail - Neighborhood Center, the Design Review of plot plan and building elevations, as well as a Conditional Use Permit for a drive-thru pharmacy and off-sale alcohol sales (Type 21 - general alcohol) to facilitate the redevelopment of a commercial center including a new 44,636 square foot Stater Brothers grocery store and an 11,465 square foot CVS drug store with drive thru-pharmacy, as well as a commercial pad for future development on approximately 6.3 acres, currently developed with a grocery store, drug store and vacant retail shops, situated on the northeast corner of Mary Street and Lincoln Avenue, in the CR - NC - Commercial Retail - Neighborhood Center and R-1-13,000 - Single Family Residential Zones, and in Ward 4. **Contact Planner:** Yvette Sennewald, Senior Planner (951) 826-5168 [YSennewald@RiversideCa.gov](mailto:YSennewald@RiversideCa.gov)

Please return your comments to: Yvette Sennewald  
City of Riverside  
Community Development Department  
Planning Division  
3900 Main Street, 3rd Floor  
Riverside, CA 92522

All comments should be received by January 22, 2013.

Should you have any questions regarding this case, please do not hesitate to contact me at (951) 826-5168.

Yvette Sennewald, Senior Planner

**COUNTY OF RIVERSIDE  
AIRPORT LAND USE COMMISSION**

**STAFF REPORT**

**AGENDA ITEM:** 3.1

**HEARING DATE:** April 11, 2013

**CASE NUMBER:** ZAP1057RI13 – Exclusive Towing, Inc. (Representative: Michael Carpenter)

**APPROVING JURISDICTION:** City of Riverside

**JURISDICTION CASE NO:** P11-0767 (Rezoning), P11-0768 (Conditional Use Permit), P11-0769 (Design Review), P12-0138 (General Plan Amendment)

**MAJOR ISSUES:** This project was submitted to ALUC as a Conditional Use Permit/Design Review and Rezone. Subsequently, staff learned that the project also involves a general plan amendment. At this time, staff is still awaiting submittal of the review fee for the general plan amendment.

**RECOMMENDATION:**

- (1) Staff recommends that consideration of the General Plan Amendment be CONTINUED to ALUC's May 9 hearing, pending submittal of the review fee for the general plan amendment.
- (2) Staff recommends a finding of CONSISTENCY for the Rezoning, provided that the new zoning incorporates the Airport Protection Overlay Zone suffix (-AP-D), which refers to the site's location within Compatibility Zone D of the Riverside Municipal Airport Influence Area.
- (3) Staff recommends a finding of CONSISTENCY for the Conditional Use Permit and Design Review, subject to the conditions included herein.

**PROJECT DESCRIPTION:** The Conditional Use Permit and Design Review is a proposal to construct and establish a towing and impoundment business on a 2.07-acre property. The Rezone is a proposal to change the zoning of the project site from Business and Manufacturing Park (BMP) to General Industrial (I). The General Plan Amendment is a proposal to amend the land use designation on the project site from Business Office Park (B/OP) to Industrial (I).

**PROJECT LOCATION:** The project site is located at 9920 Arlington Avenue, at the southeasterly corner of Arlington Avenue and Crest Avenue within the City of Riverside, approximately 6,600 feet southwesterly of the westerly terminus of Runway 9-27 and 7,100 feet westerly of the southerly terminus of Runway 16-34 at Riverside Municipal Airport.



**LAND USE PLAN:** 2005 Riverside Municipal Airport Land Use Compatibility Plan

- a. Airport Influence Area: Riverside Municipal Airport
- b. Land Use Policy: Airport Compatibility Zone D
- c. Noise Levels: 55-60 CNEL noise contour

**BACKGROUND:**

Non-Residential Intensity: The Countywide Land Use Compatibility criteria for Airport Compatibility Zone D for non-residential land uses limit intensity to an average of 100 persons per acre and a maximum single-acre intensity of 300 persons in any given acre. The intensity proposed by the project based on the building floor plans and the building code method would total 38 people. Across the entire 2.0 acres, this would equate to an average of 19 people per acre, which would be consistent with the average acre criteria for Compatibility Zone D. The building area would be located within its own single-acre and its 38 total people would be consistent with the single-acre criteria for Compatibility Zone D. Beyond the proposed development, the proposed change of zone and general plan amendment would not substantially alter the type or intensity of uses that could be allowed on the project site that could exceed intensity criteria.

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited or discouraged in Compatibility Zone D (highly noise-sensitive outdoor non-residential uses and hazards to flight) within the project. The proposed change of zone and general plan amendment would allow for similar uses as are currently allowed by the existing zone and land use designation, which are industrial in nature and would not typically include highly noise-sensitive outdoor non-residential uses.

Noise: The site is located within the area subject to average aircraft noise levels between 55 and 60 CNEL. As a non-residential use not including any noise sensitive uses, no special measures to mitigate aircraft-generated noise are required.

PART 77: The elevation of Runway 9-27 at its westerly terminus is approximately 757.6 feet above mean sea level (AMSL). At an approximate distance of 6,600 feet to the property line and relevant slope of 100:1, any structure above 823.6 feet AMSL would require FAA Obstruction Evaluation Service notice. The elevation of Runway 16-34 at its southerly terminus is 747.5 feet AMSL. At an approximate distance of 7,100 feet, any structure above 818.5 feet AMSL would require FAA notice. Site elevation is approximately 765 feet AMSL and a maximum building height of 16.5 feet for a total height of 781.5 AMSL. Therefore, the 100:1 notice surface would not be breached, and ALUC may take action without need to wait for an obstruction evaluation review by the Federal Aviation Administration (FAA).

Open Area: Compatibility Zone D requires that 10% of area within major projects (10 acres or larger) be set aside as open land that could potentially serve as emergency landing areas. Since the project is less than 10 acres in size, this requirement does not apply.

Airport Overlay: In conjunction with adoption of its new General Plan, the City of Riverside amended its zoning ordinance to provide for Airport Protection Overlay Zones within the Airport Influence Areas of airports for which Compatibility Zones based on the “A through E” system utilized in the 2004 Riverside County Airport Land Use Compatibility Plan have been established. While the land uses permitted by the General Industrial (I) zone are consistent with a location in Compatibility Zone D, application of the Airport Protection Overlay Zone suffix is recommended in order to assure that the ultimate development will comply with Compatibility Zone D intensity limitations. This would further the objectives of promoting the continued operations of Riverside Municipal Airport and maintaining public awareness of its proximity. The City Zoning Ordinance provisions refer readers to the Compatibility Plan.

**CONDITIONS:**

1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses shall be prohibited:
  - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
  - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
  - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
  - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
  - (e) Noise sensitive outdoor nonresidential uses and hazards to flight.
3. The attached notice shall be given to all prospective purchasers and/or tenants of the property, and shall be recorded as a deed notice.

4. Any new retention or infiltration basins or ponds on the site shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around such basin(s) or pond(s) that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.
5. The City of Riverside shall either prohibit the following uses, or shall require additional review by the Airport Land Use Commission prior to the establishment of any of the following uses on the property or in any of the structures proposed through this conditional use permit and design review:

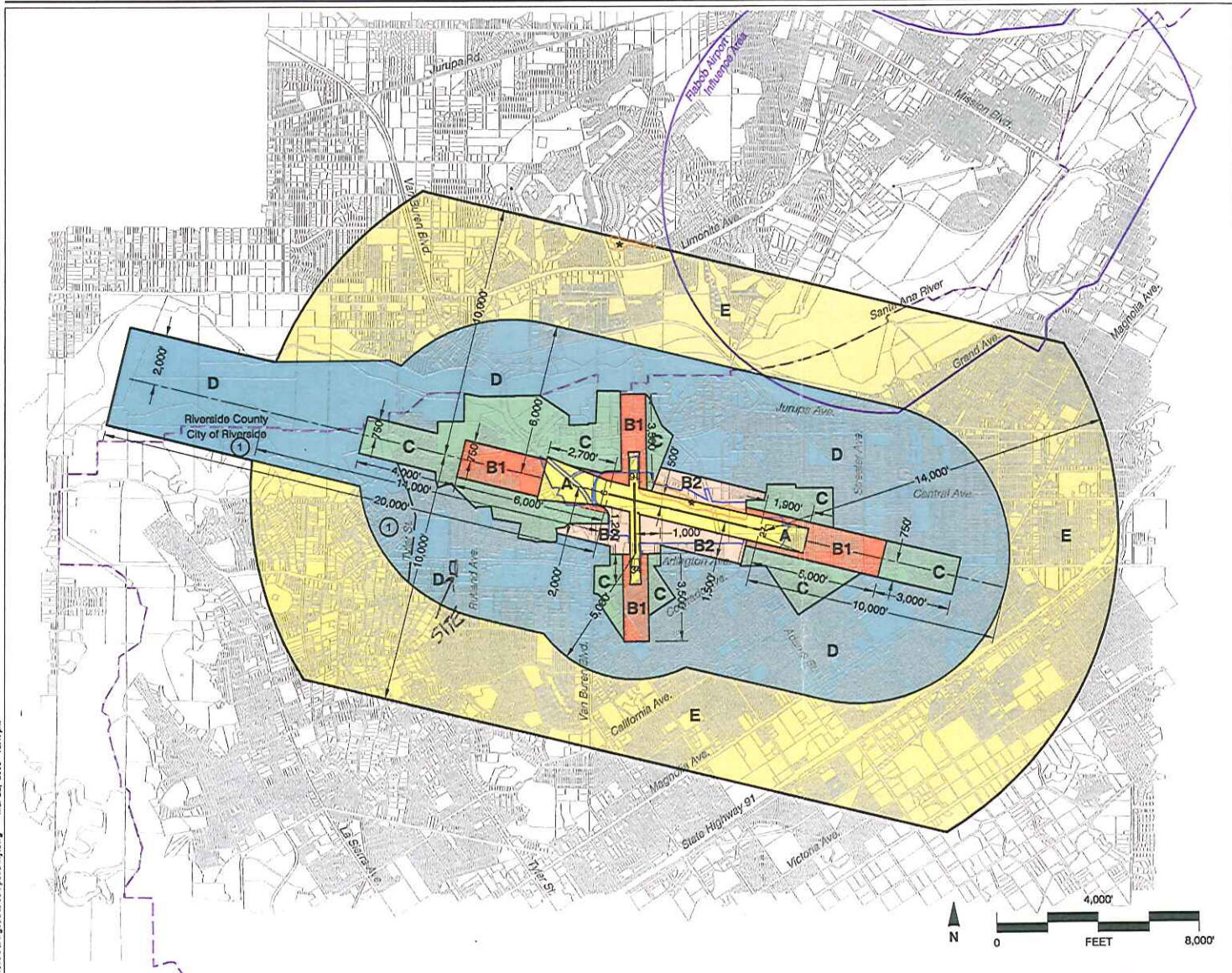
Auction rooms, assemblies of people (including places of worship, fraternal lodges, etc.), auditoriums, classrooms, conference rooms with a capacity of 200 or more persons, dance halls, exhibit rooms, gaming (including bingo), gymnasiums, homeless shelters with a capacity of 200 or more persons, lounges, reviewing stands, skating rinks, stages, swimming pools, vocational and technical schools, and other uses that would be considered to have an occupancy level greater than one person per 30 square feet (minimum square feet per occupant less than 30) pursuant to California Building Code (1998) Table 10-A.

The following temporary uses: circuses, fairs, concerts, and other events that could attract more than 200 persons to this location.



# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



**Legend**

- Compatibility Zones**
- Airport Influence Area Boundary
  - Zone A
  - Zone B1
  - Zone B2
  - Zone C
  - Zone D
  - Zone E
  - Height Review Overlay Zone

- Boundary Lines**
- Airport Property Line
  - City Limits

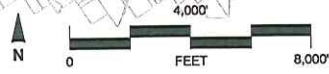
**Note**  
 Airport influence boundary measured from a point 200 feet beyond runway ends in accordance with FAA airspace protection criteria (FAR Part 77). All other dimensions measured from runway ends and centerlines.

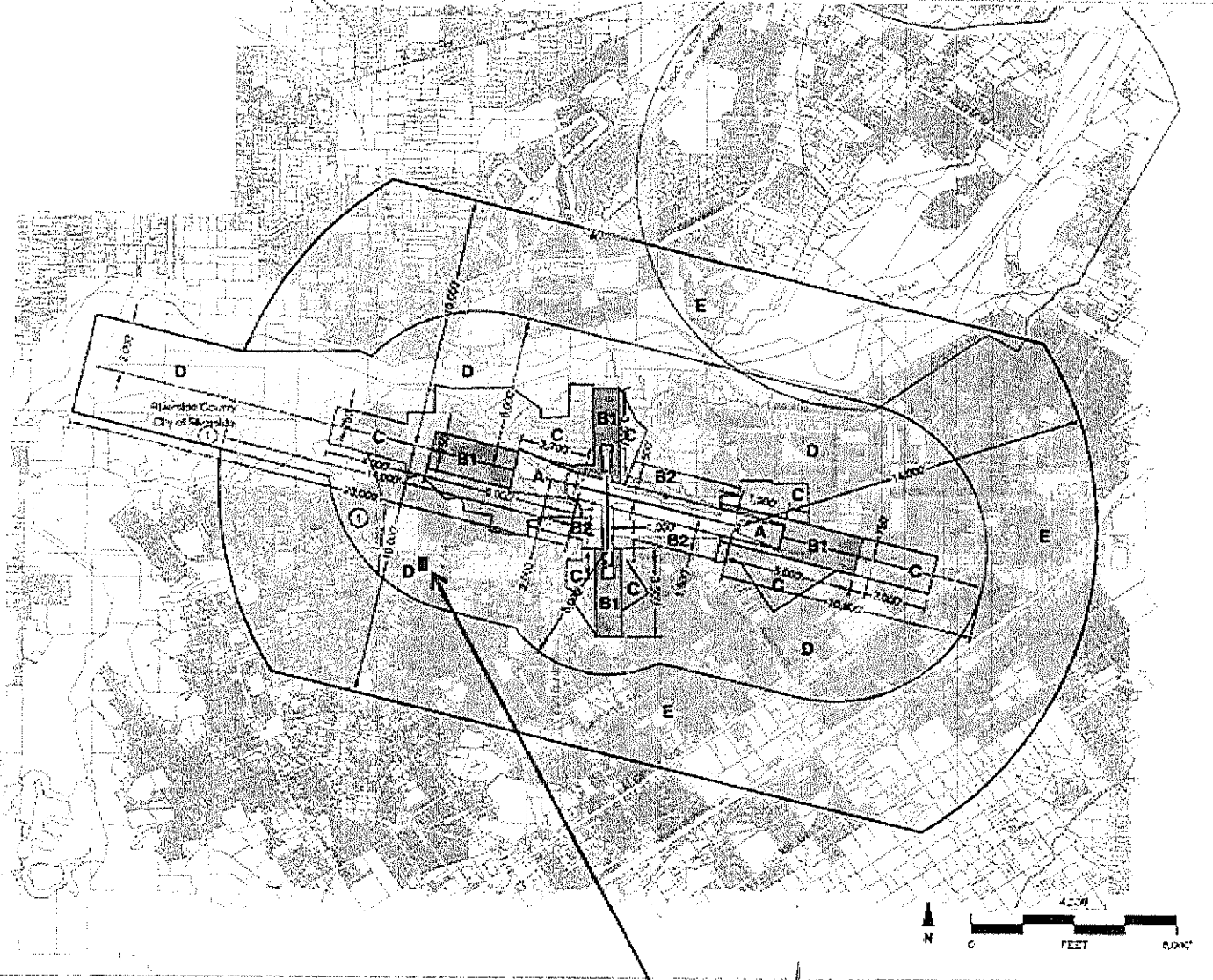
See Chapter 2, Table 2A for compatibility criteria associated with this map. See Section RI.2 for special exceptions to the Table 2A criteria.

**Riverside County  
 Airport Land Use Commission  
 Riverside County  
 Airport Land Use Compatibility Plan  
 Policy Document  
 (Adopted March 2005)**

Map RI-1

**Compatibility Map  
 Riverside Municipal Airport**





**Legend**

**Compatibility Zones**

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E
- Historic Review Overlay Zone

**Boundary Lines**

- Airport Property Line
- City Limits

**Note**

Airport Influence boundary measured from a center 200 feet beyond runway ends in accordance with FAA airspace protection criteria (14 CFR Part 77). All other dimensions measured from runway ends and centerlines.

See Chapter 2, Table 2A for compatibility criteria associated with this map. See Section 91.2 for special exceptions to the Table 2A criteria.

Riverside County  
 Airport Land Use Commission  
**Riverside County**  
**Airport Land Use Compatibility Plan**  
**Policy Document**  
 (Adopted March 2005)

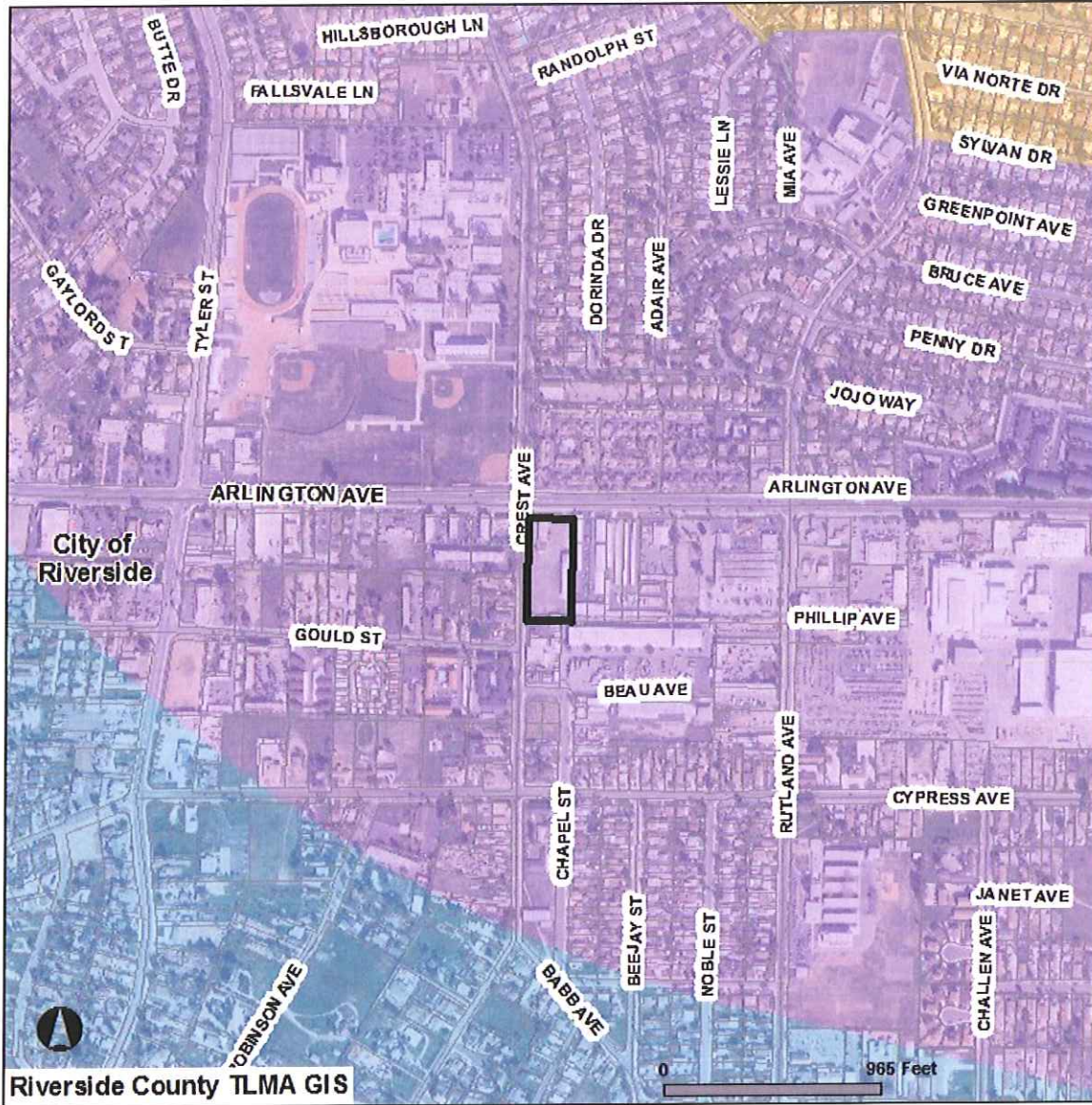
Map RW1

**Compatibility Map**  
 Riverside Municipal Airport

**SITE, EXCLUSIVE TOWING INC.**  
 9920 ARLINGTON AVE. RIVERSIDE, CA 92503



RIVERSIDE COUNTY GIS



Selected parcel(s):  
151-020-001

**AIRPORTS**

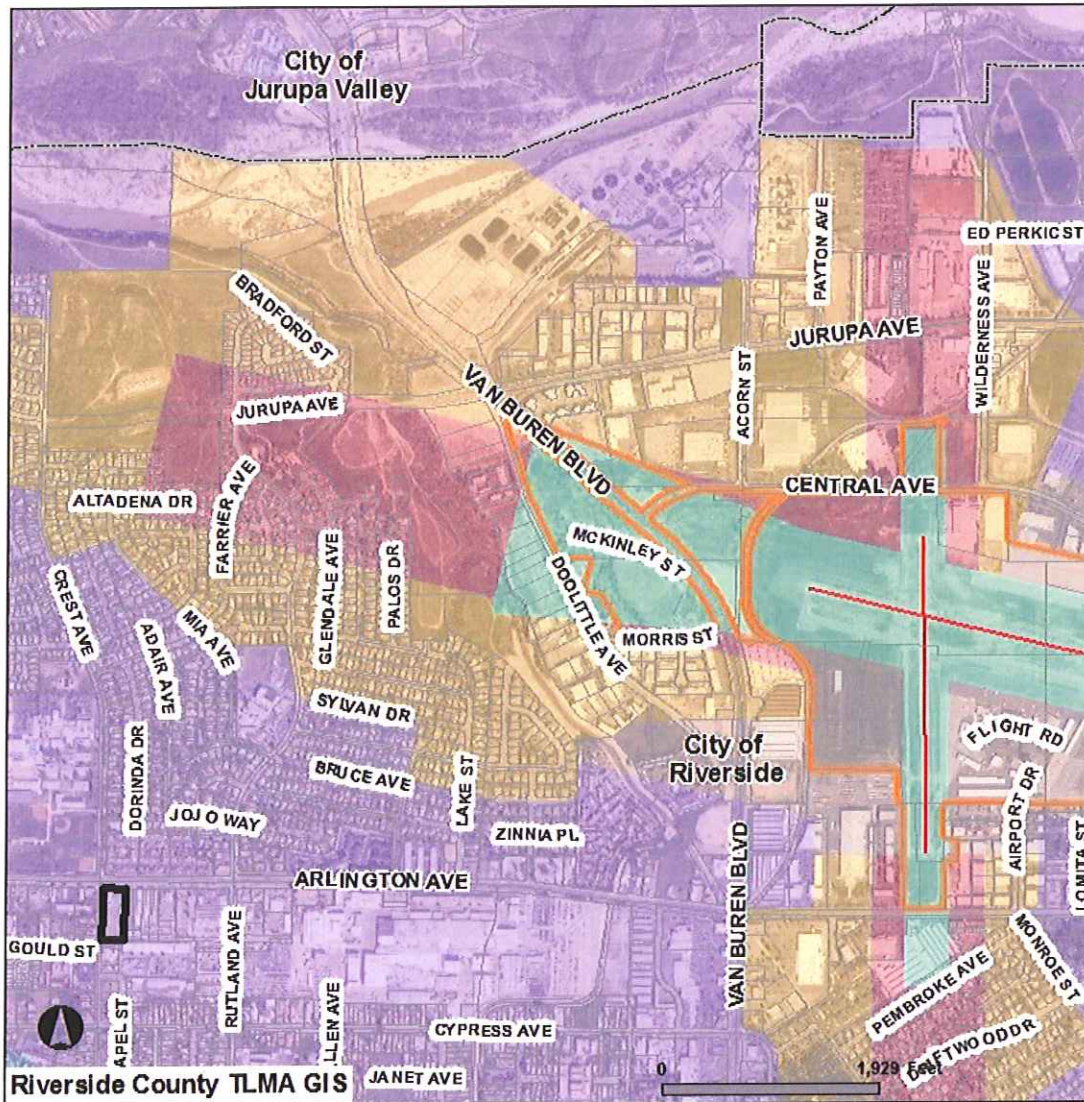
- SELECTED PARCEL
- AIRPORT INFLUENCE AREAS
- INTERSTATES
- COMPATIBLTY ZONE C
- HIGHWAYS
- COMPATIBLTY ZONE D
- PARCELS
- COMPATIBLTY ZONE E

**\*IMPORTANT\***  
 Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON...Wed Mar 13 09:46:01 2013  
 Version 121101



RIVERSIDE COUNTY GIS



Selected parcel(s):  
151-020-001

**AIRPORTS**

- |                       |                         |                      |                      |
|-----------------------|-------------------------|----------------------|----------------------|
| SELECTED PARCEL       | INTERSTATES             | HIGHWAYS             | PARCELS              |
| AIRPORT RUNWAYS       | AIRPORT INFLUENCE AREAS | AIRPORT BOUNDARIES   | COMPATIBILITY ZONE A |
| COMPATIBILITY ZONE B1 | COMPATIBILITY ZONE B2   | COMPATIBILITY ZONE C | COMPATIBILITY ZONE D |
| COMPATIBILITY ZONE E  | WATER BODIES            |                      |                      |

**\*IMPORTANT\***  
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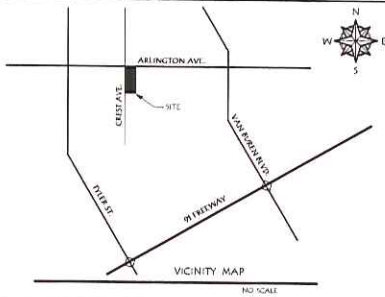
REPORT PRINTED ON...Wed Mar 13 09:46:40 2013

Version 121101









NOTE:  
 1. ALL CONSTRUCTION SHALL CONFORM TO THE CURRENT EDITION OF THE CALIFORNIA BUILDING CODE AND ALL APPLICABLE CODES, ORDINANCES AND REGULATIONS.  
 2. GENERAL CONTRACTOR TO VERIFY EXISTING CONDITIONS PRIOR TO CONSTRUCTION.  
 3. MICHAEL D. CARPENTER SHALL BE INFORMED IN WRITING OF ANY DISCREPANCY PRIOR TO CONSTRUCTION.

THE DISCHARGE OF SOLIDWASTES TO ANY 1-STORY SEWAGE SYSTEM (FROM WHITE SOLUBLE WASTE, PARTICULARLY FROM DISHES, SINKS, PARTICULATE, CONDENSATION WASTE, MATERIALS OR WASTE WATER) GENERATION ON CONSTRUCTION SITES OR BY CONSTRUCTION ACTIVITIES SHALL BE PLACED, CONVEYED OR DISCHARGED IN TO THE STREET, CURBS OR STORM DRAIN SYSTEM.

DEFERRED SUBMITTALS:  
 FIRE PROTECTION SYSTEM:  
 A SEPARATE PERMIT IS REQUIRED PRIOR TO WORK ON ANY FIRE PROTECTION SYSTEM OR COMPONENT, SUBMIT SHOP DRAWINGS, CALCULATIONS AND SPECIFICATIONS FOR REVIEW AND PERMIT.

REQUIRED SPECIAL INSPECTIONS:  
 IN ADDITION TO REGULAR INSPECTIONS, THE FOLLOWING CHECKED ITEM WILL ALSO REQUIRE SPECIAL INSPECTION IN ACCORDANCE WITH SEC. 5039 OF THE PREVIOUS BUILDING CODE:

ITEM	REQUIRED	REMARKS
STEEL ANCHORS	NO	PER APPROVING REPORT
FIELD WELDING	NO	PER APPROVING REPORT
HIGH STRENGTH CONCRETE	NO	PER APPROVING REPORT

PROJECT: EXCLUSIVE TOWING, INC. - TENANT IMPROVEMENT TOW YARD WITH OFFICE & GARAGE  
 JOB LOCATION: 9920 ARLINGTON AVE. RIVERSIDE, CA 92503

OWNER: EXCLUSIVE TOWING, INC.  
 9920 ARLINGTON AVE. RIVERSIDE, CA 92503

LEGAL DESCRIPTION:  
 LOT: TRACT A.P.N. 151-020-001

PROJECT DATA:  
 ZONE: RMP  
 OCCUPANCY: 9 - OFFICE / PROFESSIONAL  
 TYPE OF CONSTRUCTION: 99  
 FIRE SPRINKLES: NO  
 FIRE PROTECTION SYSTEM: YES (EXISTING WET STANDPIPE SYSTEM)

SITE DATA:  
 LOT SIZE: 2.07 ACRES  
 BUILDING SIZE: 5086 SF  
 NUMBER OF STORES: ONE  
 BUILDING HEIGHT: 10'  
 PARKING: 20 SPACES  
 PARKING LOT LANDSCAPING: LANDSCAPING  
 LANDSCAPING: 22,000 SF - 24,999

AREA TABULATION: TENANT IMPROVEMENT SEE TABLE AT LEFT

JURISDICTION: CITY OF RIVERSIDE, CA

GOVERNING CODES	CODE	2019 CALIFORNIA BUILDING CODE
BUILDING	C.B.C.	2019 CALIFORNIA BUILDING CODE
ELECTRICAL	C.E.C.	2019 CALIFORNIA ELECTRICAL CODE
PLUMBING	C.P.C.	2019 CALIFORNIA PLUMBING CODE
MECHANICAL	C.M.C.	2019 CALIFORNIA MECHANICAL CODE
ACCESSIBILITY	C.A.C.	2019 CALIFORNIA BUILDING CODE
ENERGY	C.E.C.	2019 CALIFORNIA ENERGY CODE
FIRE	C.B.C.	2019 CALIFORNIA FIRE CODE
ORDINANCES	C.B.C.	ALL APPLICABLE CITY, COUNTY AND STATE

DRAWINGS ISSUED FOR:  
 PRELIMINARY 9-20-17  
 PRELIMINARY 10-24-17  
 SUBMITTAL 11-10-17

MICHAEL D. CARPENTER



BUILDING DESIGN SERVICES

8925 IRONWOOD STREET  
 RANCHO CUCAMONGA, CA 91750  
 FAX: (909) 980-9483

(909) 724-9572

CARPENTERDESIGNS@PAOL.COM

OCCUPANT LOAD LEGEND - OFFICE			
FLOOR / ROOM	TABLE 1003.1	OCCUPANCY	CREDIT F
ROOM	AREA / OCCUPANT LOAD FACTOR	OCCUPANT LOAD	
OFFICE & STORE ROOM	285 SQUARE FEET / 500	31	
GARAGE	195 SQUARE FEET / 500	4	
TOTAL		35	

PER TABLE 1003.1.1 ALL OCCUPANT LOAD MORE THAN AS REQUIRED TWO DITS.  
 PER TABLE 1003.1.1 REQUIRED BUT WITHIN OCCUPANT LOADS: 28X100 = TOTAL OF 86% OF REQUIRED OUTDOOR WIDTH (1.0 X 100 = 100 FEET) X 86% = 86 FEET

PARKING ANALYSIS	
PER RIVERSIDE MUNICIPAL CODE, ZONING CHAPTER 19.350	
OFFICE	599 SF @ 1520 = 778 REQUIRED
GARAGE	985 SF @ 1020 = 112 REQUIRED
ELECT./MECH.	250 SF @ 1520 = 163 REQUIRED
DRIVE ROOM	902 SF @ 1520 = 593 REQUIRED
OUTDOOR STORAGE	= 1000 REQUIRED
TOTAL	= 2634 REQUIRED
HANDICAP STANDARD	2 EXISTING
REQUIRED PROVIDED	20 EXISTING

AREA TABULATION	
NEW PORCH	251 SQUARE FEET
EXISTING OFFICE	797 SQUARE FEET
EXISTING GARAGE 1 AND 2	795 SQUARE FEET
EXISTING ELECTRICAL & MECH. RM.	250 SQUARE FEET
EXISTING STORE ROOM	902 SQUARE FEET
EXISTING CARPORT	778 SQUARE FEET
EXISTING EXTERIOR STORAGE LOCKERS	229 SQUARE FEET

SCOPE OF WORK

- SITE LANDSCAPE / STREETSCAPE IMPROVEMENTS**
- REMOVE EXISTING 4" CONCRETE SITE WALL FROM FRONT AND SIDE YARD PROPERTY LINES.
  - REMOVE ASPHALT PAVING FROM FRONT AND SIDE YARDS.
  - INSTALL PERMANENT, BRICK CLADDING ON FRONT AND SIDE YARD WITH TREES, TURF, SHRUBS AND ORNAMENTALS.
  - INSTALL NEW 6" CONCRETE / MASONRY WALL BETWEEN FRONT PARKING AREA AND REAR YARD OUTDOOR STORAGE AREA.
  - INSTALL GRASS AND COLORADO BLUE SPRUCE ON NEW WALL AND ON EXISTING SITE WALL ON THE CHERTATE AVE. SIDE.
- SITE ACCESSIBILITY IMPROVEMENTS**
- UPGRADE EXISTING ACCESSIBLE PARKING SPACES WITH NEW PAVING, STEERING AND SIGNAGE.
  - INSTALL NEW CONCRETE WALKWAY (ACCESSIBLE PATH OF TRAVEL) FROM BUILDING ENTRANCE TO EXISTING CITY SIDEWALK.
  - INSTALL NEW LEVEL CONCRETE LANDSCAPE AT EXISTING BUILDING ENTRANCE.
  - INSTALL NEW ACCESSIBLE SIGNAGE AT EXISTING SITE AND BUILDING ENTRANCES.
- BUILDING IMPROVEMENTS**
- INSTALL NEW ROOF PARAPET (ON V.A.C. EQUIPMENT SCREENING) AT FULL PERIMETER OF BUILDING.
  - INSTALL NEW REDUCED GLAZED, FRAMED COLUMNS AND POP-OUTS TO CORREL EXISTING STEEL COLUMNS AND ROOF BEAM DOWN SPOUTS.
  - REPLACE ALL EXISTING EXTERIOR DOORS AND WINDOWS WITH ENERGY EFFICIENT EQUAL GLAZE, LOWEY CLEAR GLASS.
  - REMOVE PAINT FROM EXISTING MASONRY FRONT OF BUILDING TO RESTORE ORIGINAL APPEARANCE.
  - PATCH AND PAINT REMAINDER OF BUILDING.

**EXCLUSIVE TOWING INC. PROJECT MISSION STATEMENT**  
 OUR GOAL IS TO CREATE A SAFE, WELL ORGANIZED AND ACCESSIBLE FACILITY FROM WHICH WE CAN PROVIDE SUPERIOR SERVICE TO OUR CUSTOMERS AND TO THE COMMUNITY THAT WE SERVE.  
 WITH THESE MODERN ENHANCEMENTS TO THE BUILDING WE HOPE TO FOSTER A SENSE OF CONNECTION TO THE CITY'S PAST, FOSTER NEIGHBORHOOD PRIDE AND PAY HOMAGE TO THE CAMP AREA / ARLANDA COMMUNITY WITH WHICH WE OPERATE.

SHEET INDEX

- C COVER SHEET
- C1 3/16 PLAN
- C11 CONCEPTUAL STREET IMPROVEMENT PLAN
- C2 SITE SECTIONS
- C3 SITE SECTION
- C4 3/16 WALL PLAN
- C5 DR SITE TRACK MANEUVERING PLAN
- L1 CONCEPTUAL LANDSCAPE PLAN
- AE EXISTING FLOOR PLAN, ROOF PLAN AND ELEVATION
- FLOOR PLAN
- A11 SCHEDULES AND NOTES
- A2 EXTERIOR ELEVATIONS
- A4 ROOF PLAN
- A8 BUILDING SECTIONS
- A7 EXISTING COVERED PARKING CANOPY
- B1 DETAILS
- B4 DETAILS

NOTE:  
 THIS PROJECT SHALL COMPLY WITH THE 2019 EDITION OF THE CALIFORNIA BUILDING CODE.

TENANT IMPROVEMENT FOR:

EXCLUSIVE TOWING INC.  
 OFFICE WITH TOW YARD & GARAGE

9920 ARLINGTON AVE.  
 RIVERSIDE, CA 92503

COVER

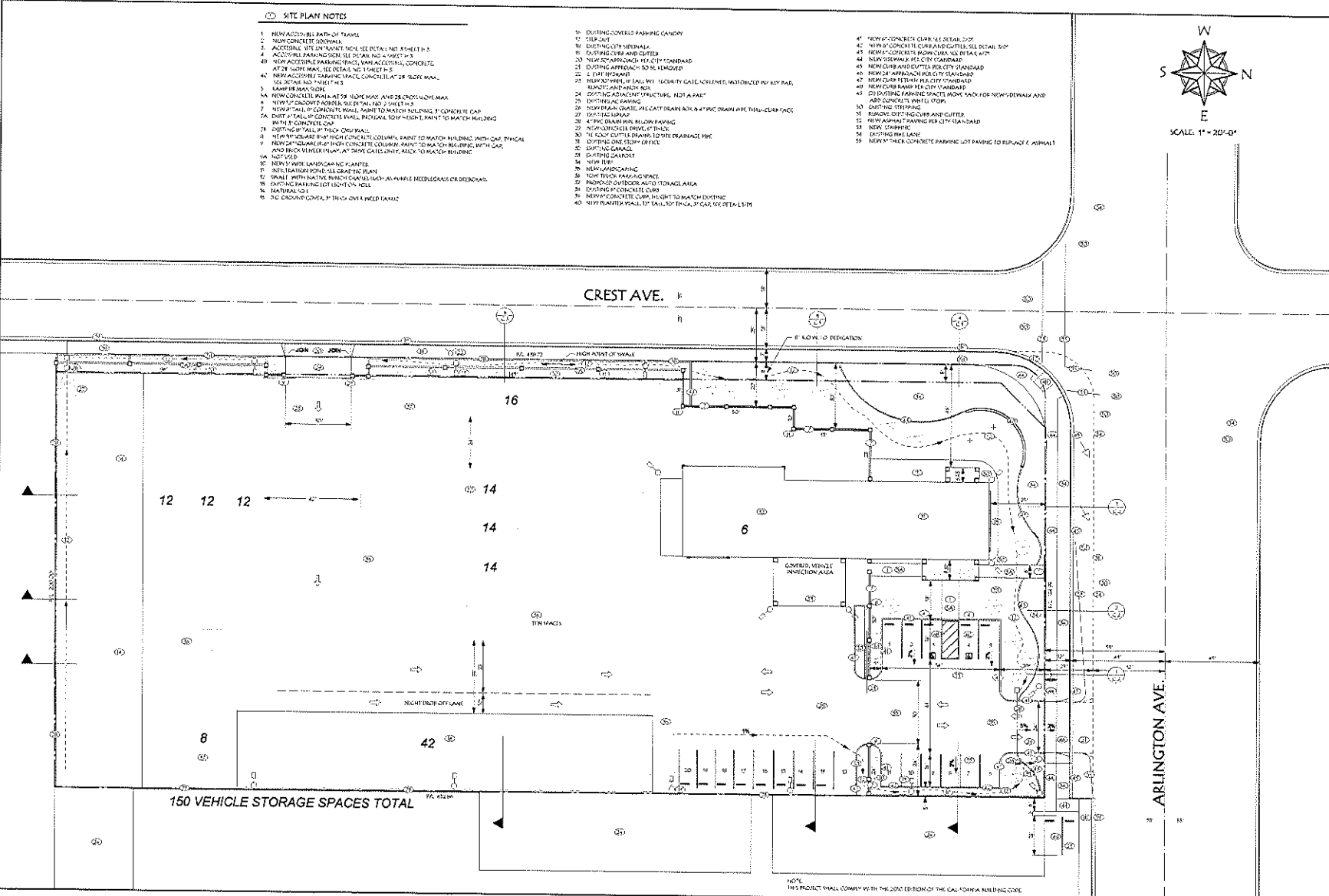
SCALE: AS NOTED

C



**SITE PLAN NOTES**

1. NEW ACCESSIBLE PATH OF TRAVEL
2. NEW CONCRETE SIDEWALK
3. ACCESSIBLE SITE ENTRY NOTE SEE DETAIL 100
4. ACCESSIBLE PARKING SPACE SEE DETAIL 100
5. NEW ACCESSIBLE PARKING SPACE, VAN ACCESSIBLE, CONCRETE
6. NEW ACCESSIBLE PARKING SPACE, NO VEHICLE
7. NEW ACCESSIBLE PARKING SPACE, CONCRETE, 12' X 18' MIN. VAN ACCESSIBLE
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59. NEW ACCESSIBLE PARKING SPACE, CONCRETE, 12' X 18' MIN. VAN ACCESSIBLE
60. NEW ACCESSIBLE PARKING SPACE, CONCRETE, 12' X 18' MIN. VAN ACCESSIBLE



DRAWINGS ISSUED FOR:  
 PRELIMINARY 1-20-12  
 REVISED 2-6-12  
 REVISED 2-28-12

MICHAEL D. CARPENTER

**CARPENTER**  
  
**DESIGNS**

BUILDING DESIGN SERVICES

1922 HICKWOOD STREET  
 CARPENTERSVILLE, CA 95757  
 (909) 916-2446

(909) 724-9572  
 CARPENTERDESIGNSPAUL.COM

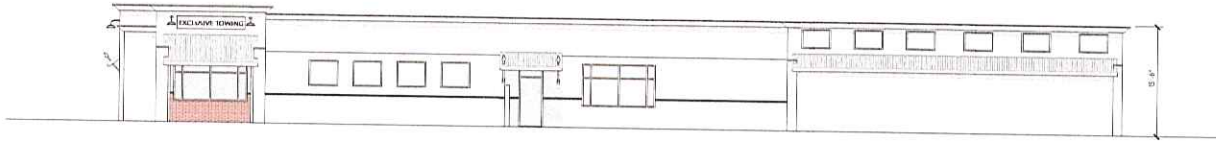
TENANT IMPROVEMENT FOR:  
**EXCLUSIVE TOWING INC.**  
 OFFICE WITH TOW YARD & GARAGE

1920 ARLINGTON AVE.  
 RIVERSIDE, CA 92503

SITE PLAN  
 SCALE: 1" = 20'-0"

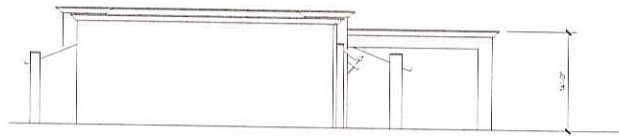
C 1

NOTE: THIS PROJECT SHALL COMPLY WITH THE 2008 EDITION OF THE CALIFORNIA BUILDING CODE.



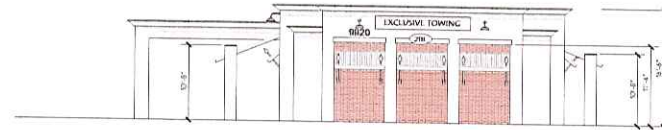
**WEST ELEVATION**

SCALE: 1/8" = 1'-0"



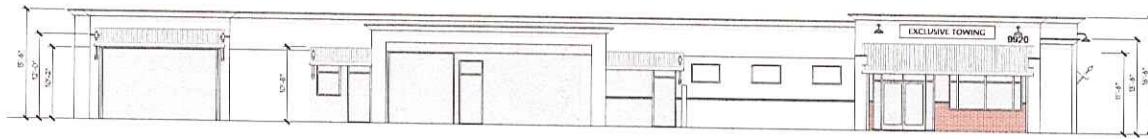
**SOUTH ELEVATION**

SCALE: 1/8" = 1'-0"



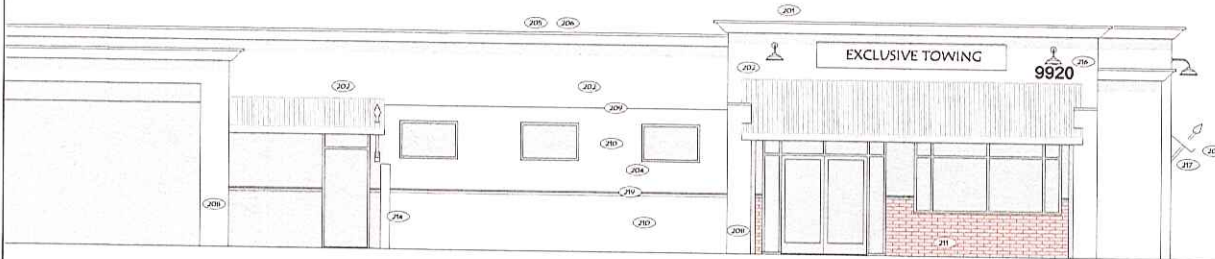
**NORTH ELEVATION**

SCALE: 1/8" = 1'-0"



**EAST ELEVATION**

SCALE: 1/8" = 1'-0"



**EAST ELEVATION AT ENTRY**

SCALE: 1/4" = 1'-0"

**EXTERIOR FINISH SCHEDULE**

PITCHED ROOFING:	GUM METAL GRAY COLOR
FLAT ROOFING:	COOL ROOF COATING, LIGHT GRAY COLOR
PLASTER:	LA HABRA STUCCO, ICC, EX-425L, LIGHT SAND TEXTURE, COLOR: 105 PACIFIC SAND
MASONRY PAINTED:	OVER EXISTING PAINT FINISH, NEW COLOR, BENE PREMIUM PLUS, GATEAU # 16140-TL (MATCH PLASTER)
MASONRY EXPOSED:	REMOVE EXISTING PAINT FINISH, EXPOSE EXISTING BRICK, DARK RED COLOR WITH GREY MORTAR
CORNICES & ENTICES:	LIGHT SAND TEXTURE, CREAM COLOR, SUNN, EDWARDS, LA HABRA, BUCOTTI
SOFFIT:	PAINT, BENE PREMIUM PLUS, COLOR - SPICE CAKE (EIGHT SQUARES) # 16153 - 8M
SITE WALLS:	C.I.P. P.C.C. AND BRIT FACE BLOCK, DRCO, COLOR TO MATCH (ATM C-93-96)
DECORATIVE IRON:	BENE, CHOCOLATE SWIRL # 16140 - 3L OR LKE, CASPERAL ADVISOR COAT, FINOZE, TORO-BRIT (E BROWN)
WINDOW FRAME:	2" X 4" 10" CENTER SET, ALUMINUM, CLEAR ANODIZED
LIGHT FIXTURES:	GUM METAL GRAY COLOR

**(200) EXTERIOR ELEVATION KEY NOTES**

- 201 FLAT ROOFING TO BE CLASS 44 BUILT UP
- 202 PITCHED ROOFING TO BE STANDING SEAM
- 203 EXTERIOR WALL FINISH TO BE PLASTER, LIGHT SAND TEXTURE, 7/8" PLASTER OVER GRADE, 1/2" ACETABULAR SANDER, 1/2" (INCLUDES AT 90-DEGREE)
- 204 WINDOWS, CLEAR GLAZ, CLEAR LOW SE, FRAME, 2" X 4" 10" CENTER SET, ALUMINUM, CLEAR ANODIZED
- 205 CORNICE TO BE SAND FINISH, PLASTER OVER FOAM
- 206 SHEET METAL GUM WITH DRIP EDGE
- 207 STUCCO OVER FRAMED COLUMNS
- 208 EXPANDED JOINT
- 209 EXISTING PAINT FINISH MASONRY WALL
- 210 REMOVE EXISTING PAINT FINISH TO EXPOSE EXISTING BRICK
- 211 CONCRETE CURB
- 212
- 213
- 214 SITE FENCE / WALL
- 215 PLANTER
- 216 STREET SIGNAGE, 6" HIGH, 1" DEEP, HELMETICA FONT, CONTRAST COLOR WITH BACKGROUND
- 217 KYLE BRACE, 2" PIPE WITH GRADE ADJ
- 218 FIBER REINFORCED PLASTER EXPANDED JOINT AND CONTRASTING PAINT FINISH
- 219 CONTRASTING PAINT FINISH, COLOR TO MATCH CORNICE

DRAWINGS ISSUED FOR:  
PRELIMINARY 9-30-11  
SUBMITTAL 11-10-11

MICHAEL D. CARPENTER

**CARPENTER**



**DESIGNS**

BUILDING DESIGN SERVICES

8923 IRONWOOD STREET  
RANCHO CUCAMONGA, CA. 91730  
FAX (909) 980-9483

(909) 724-9572

CARPENTERDESIGNS@PAOL.COM

EXCLUSIVE TOWING

TOW YARD WITH  
OFFICE & GARAGE

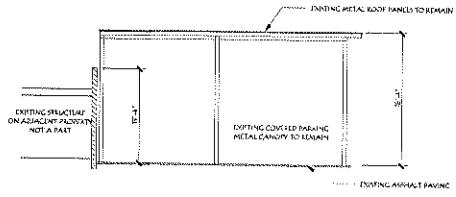
9920 ARLINGTON AVE.  
RIVERSIDE, CA

ELEVATIONS

SCALE: AS NOTED

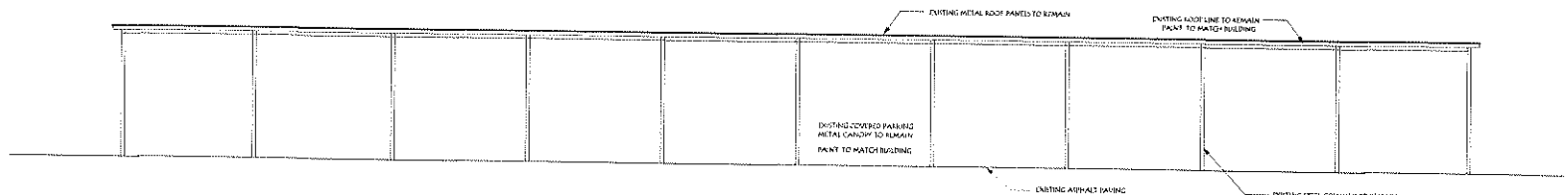
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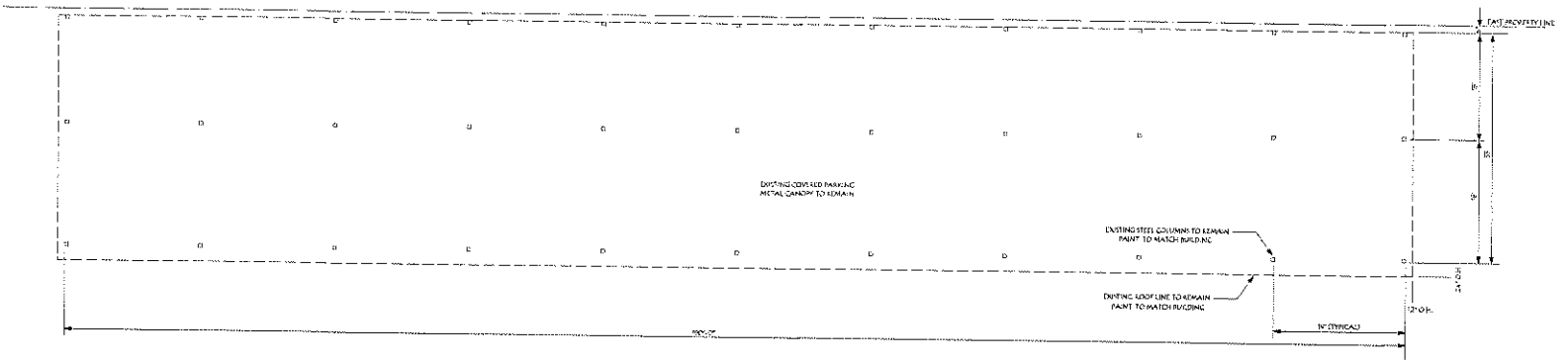


**NORTH ELEVATION**  
SCALE: 1/8" = 1'-0"

EXTERIOR FINISH SCHEDULE	
STEEL COLUMN:	PRIME COIL, BENE-PRIMER PLUS, PLATEAU # 16, MO-70 (MATCH BUILDING PANEL)
STEEL BEAMS:	PRIME COIL, BENE-PRIMER PLUS, PLATEAU # 16, MO-70 (MATCH BUILDING PANEL)
ROOF PAVI:	EXISTING, BURR BURNEPS, GARARTA, ELECT (MATCH BUILDING CORNER)
ROOF PANEL:	EXISTING OFF WHITE TO REMAIN



**WEST ELEVATION**  
SCALE: 1/8" = 1'-0"



**FLOOR PLAN**  
SCALE: 1/8" = 1'-0"

DRAWINGS ISSUED FOR:  
PRELIMINARY 3-7-11  
SUBMITTAL 11-10-11

MICHAEL D. CARPENTER



BUILDING DESIGN SERVICES

1422 HENRIEWOOD STREET  
SAN JOSE, CALIFORNIA, CA 95128  
FAX: (408) 980-1488

(909) 724-9572

CARPENTERDESIGNS@AOL.COM

TENANT IMPROVEMENT FOR:  
EXCLUSIVE TOWING INC.  
OFFICE WITH TOW YARD & GARAGE

9020 ARLINGTON AVE.  
RIVERSIDE, CA 92503

EXISTING COVERED  
PARKING CANOPY

SCALE: 1/8" = 1'-0"



9920 ARLINGTON AVE. PROPOSED IMPROVEMENTS



9920 ARLINGTON AVE. CURRENT CONDITION

<u>Use</u>	<u>Minimum Square Feet per Occupant</u>
1. Aircraft Hangars (no repair)	500
2. Auction Rooms	7
3. Assembly Areas, Concentrated Use (without fixed seats)	7
Auditoriums	
Churches and Chapels	
Dance Floors	
Lobby Accessory to Assembly Occupancy	
Lodge Rooms	
Reviewing Stands	
Stadiums	
Waiting Areas	3
4. Assembly Areas, Less Concentrated Use	15
Conference Rooms	
Dining Rooms	
Drinking Establishments	
Exhibit Rooms	
Gymnasiums	
Lounges	
Stages	
Gaming	11
5. Bowling Alley (assume no occupant load for bowling lanes)	4
6. Children's Homes and Homes for the Aged	80
7. Classrooms	20
8. Congregate Residences	200
9. Courtrooms	40
10. Dormitories	50
11. Dwellings	300
12. Exercising Rooms	50
13. Garage, Parking	200
14. Health-Care Facilities	80
Sleeping Rooms	120
Treatment Rooms	240
15. Hotels and Apartments	200
16. Kitchen – Commercial	200
17. Library Reading Room	50
Stack Areas	100
18. Locker Rooms	50
19. Malls	Varies
20. Manufacturing Areas	200
21. Mechanical Equipment Room	300
22. Nurseries for Children (Daycare)	35
23. Offices	100
24. School Shops and Vocational Rooms	50
25. Skating Rinks	50 on the skating area; 15 on the deck
26. Storage and Stock Rooms	300
27. Stores — Retail Sales Rooms	
Basements and Ground Floors	30
Upper Floors	60
28. Swimming Pools	50 for the pool area; 15 on the deck
29. Warehouses	500
30. All Others	100

Source: California Building Code (1998), Table 10-A

Table C1

## Occupancy Levels—California Building Code



# NOTICE OF PUBLIC HEARING

## RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the application described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. The proposed project application may be viewed at the Riverside County Administrative Center, 4080 Lemon Street, 14<sup>th</sup> Floor, Riverside, California 92501, Monday through Thursday from 8:00 a.m. to 5:00 p.m.

PLACE OF HEARING: Riverside County Administration Center  
4080 Lemon St., 1<sup>st</sup> Floor Hearing Room  
Riverside, California

DATE OF HEARING: April 11, 2013

TIME OF HEARING: 9:00 A.M.

### CASE DESCRIPTION:

ZAP1057RI13 – Exclusive Towing, Inc. (Representative: Michael Carpenter) - City Case Nos. P11-0767 (Rezoning), P11-0768 (Conditional Use Permit), P11-0769 (Design Review), and P12-0138 (General Plan Amendment). P12-0138 is a proposal to amend the City of Riverside General Plan land use designation on a 2.07-acre property with a site address of 9920 Arlington Avenue (located at the southeasterly corner of Arlington Avenue and Crest Avenue) from Business/Office Park (B/OP) to Industrial (I). P11-0767 is a proposal to rezone that same property from Business and Manufacturing Park (BMP) to General Industrial (I), or General Industrial-Airport Protection Overlay Zone D (I-AP-D). P11-0768 and P11-0769 propose to establish a towing and impoundment business on the site (Zone D of Riverside Municipal Airport Influence Area).

FURTHER INFORMATION: Contact Russell Brady at (951) 955-0549 or John Guerin at (951) 955-0982. The ALUC holds hearings for local discretionary permits within the Airport Influence Areas, reviewing for aeronautical safety, noise and obstructions. All other concerns should be addressed to Mr. Moises Lopez of the City of Riverside Planning Department, at (951) 826-5264.

DJANOGLY ASSOC  
10835 SANTA MONICA BLVD #100  
LOS ANGELES CA 90025

ALVORD UNIFIED SCHOOL DISTRICT  
10365 KELLR AVE.  
RIVERSIDE CA 92505

ALESSANDRO & ARLINGTON  
SELF STORAGE  
11693 SAN VICENTE BLVD #828  
LOS ANGELES CA 90049

BURCH LIVING TRUST  
718 LONG TIMBER LN  
MONUMENT CO. 80132

PALOMAREZ CARLOS /  
PALOMAREZ ELMA  
6298 CREST AVE  
RIVERSIDE CA 92503

ZOIS GORGE / ZOIS DOLORES  
15911 RANCHO VIEJO DR  
RIVERSIDE CA 92506+5824

ARLINGTON AUTO CENTER  
9301 WILSHIRE BLVD #315  
BEVERLY HILLS CA 90210

SEPULVEDA MARIA  
6270 CREST AVE  
RIVERSIDE CA 92503

PASILLAS JESUS  
6238 CREST AVE  
RIVERSIDE CA 92503

RAMOS ELVIRA R  
509 CORONA AVE  
CORONA CA 92879

GOULD STREET TRUST  
3410 LA SIERRA AVE #F517  
RIVERSIDE CA 92503

CARDELUCCI SAMUEL D  
/ CARDELUCCI VIRGINIA  
PO BOX 4067  
RIVERSIDE CA 92514

*Owner*

GERALD KOHUTEK  
3777 PLACENTIA LANE  
RIVERSIDE, CA. 92501

MICHAEL CARPENTER  
8923 IRONWOOD ST.  
RANCHO CUCAMONGA, CA.  
91730

CITY OF RIVERSIDE,  
PLANNING DEPARTMENT  
ATTN: M LOPEZ  
3900 MAIN ST.  
RIVERSIDE, CA. 92501

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91730

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PLANNING DEPARTMENT  
ATTN: M LOPEZ  
3900 MAIN ST.  
RIVERSIDE, CA. 92501



**APPLICATION FOR MAJOR LAND USE ACTION REVIEW**  
**RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION**

ALUC Identification No.

ZAP1057RII3

**PROJECT PROPONENT (TO BE COMPLETED BY APPLICANT)**

Date of Application

Property Owner

GERALD KOHUTEK

Phone Number 951 682 2003

Mailing Address

3777 PLACENTIA LANE  
 RIVERSIDE, CA. 92501

Agent (if any)

MICHAEL CARPENTER

Phone Number 909 724 9572

Mailing Address

CARPENTER DESIGNS  
 8923 IRONWOOD ST.  
 RANCHO CUCAMONGA, CA. 91730

**PROJECT LOCATION (TO BE COMPLETED BY APPLICANT)**

Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways

Street Address

9920 ARLINGTON AVE.  
 RIVERSIDE, CA. 92503

Assessor's Parcel No.

151-020-001

Parcel Size

2 ACRE

Subdivision Name

Lot Number

Zoning

Classification

BMP to I

**PROJECT DESCRIPTION (TO BE COMPLETED BY APPLICANT)**

If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed

Existing Land Use  
 (describe)

R.V. REPAIR AND SALES

Proposed Land Use  
 (describe)

TOWING COMPANY, VEHICLE IMPOUND YARD  
 AND RELATED OFFICES

For Residential Uses

Number of Parcels or Units on Site (exclude secondary units)

For Other Land Uses

Hours of Use

OFFICE 8 AM - 5 PM YARD 24 HRS

(See Appendix C)

Number of People on Site

Maximum Number

9 PER ACRE

Method of Calculation

CBC OCG. LOAD OF 33/2 = 17  
 17 / 2.07 ACRE = 9

Height Data

Height above Ground or Tallest Object (including antennas and trees)

BLDG. 20' 780.00 ft.

Highest Elevation (above sea level) of Any Object or Terrain on Site

GROUND 760.00 ft.

Flight Hazards

Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?

Yes

No

If yes, describe

RivM  
D

**REFERRING AGENCY (TO BE COMPLETED BY AGENCY STAFF)**

Date Received _____	Type of Project
Agency Name <u>City of Riverside</u>	<input type="checkbox"/> General Plan Amendment
Staff Contact <u>Moises Lopez</u>	<input checked="" type="checkbox"/> Zoning Amendment or Variance
Phone Number <u>(951) 826-5264</u>	<input type="checkbox"/> Subdivision Approval
Agency's Project No. <u>PII-0767, II-0768, II-0769</u>	<input checked="" type="checkbox"/> Use Permit
	<input type="checkbox"/> Public Facility
	<input type="checkbox"/> Other _____

**ALUC REVIEW (TO BE COMPLETED BY ALUC EXECUTIVE DIRECTOR)**

Application Receipt	Date Received _____	By _____
	Is Application Complete? <input type="checkbox"/> Yes <input type="checkbox"/> No	
	If No, cite reasons _____	

Primary Criteria Review	Airport(s) Nearby _____	Compatibility Zone(s)	<input type="checkbox"/> A	<input type="checkbox"/> B1	<input type="checkbox"/> B2	<input type="checkbox"/> C	<input type="checkbox"/> D	<input type="checkbox"/> E	<input type="checkbox"/> Ht.
		Allowable (not prohibited) Use?	<input type="checkbox"/> Yes	<input type="checkbox"/> No					
		Density/Intensity Acceptable?	<input type="checkbox"/> Yes	<input type="checkbox"/> No					
		Open Land Requirement Met?	<input type="checkbox"/> Yes	<input type="checkbox"/> No					
		Height Acceptable?	<input type="checkbox"/> Yes	<input type="checkbox"/> No					
		Easement/Deed Notice Provided?	<input type="checkbox"/> Yes	<input type="checkbox"/> No					

Special Conditions	Describe: _____
	_____

Supplemental Criteria Review	Noise _____
	Safety _____
	Airspace Protection _____
	Overflight _____

**ACTIONS TAKEN (TO BE COMPLETED BY ALUC EXECUTIVE DIRECTOR)**

ALUC Executive Director's Action	<input type="checkbox"/> Approve	Date _____
	<input type="checkbox"/> Refer to ALUC	

ALUC Action	<input type="checkbox"/> Consistent	Date _____
	<input type="checkbox"/> Consistent with Conditions (list conditions/attach additional pages if needed)	
	_____	
	_____	
	<input type="checkbox"/> Inconsistent (list reasons/attach additional pages if needed)	
	_____	
	_____	

# RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

## STAFF REPORT

### ADMINISTRATIVE ITEMS

- 4.1 Director's Approvals. As authorized pursuant to Section 1.5.2(d) of the 2004 Riverside County Airport Land Use Compatibility Plan, ALUC Director Ed Cooper has approved one non-legislative case determined to be consistent with an Airport Land Use Compatibility Plan, a proposal for a 48-foot monotree telecommunications facility in Compatibility Zone E of the Palm Springs Airport Influence Area. Staff is attaching a copy of the approval letter and background information.

Additionally, as authorized pursuant to ALUC Resolution No. 2011-02, ALUC Director Ed Cooper has reviewed one legislative case and issued a consistency letter based on the project having no possibility of being inconsistent with the land use planning guidelines contained in the applicable Airport Land Use Compatibility Plan. The specific project is an amendment to the City of Banning General Plan Circulation Element changing the required roadway levels of service from "C" to "D" and deleting the Highland Home Road interchange. Staff is attaching a copy of the approval letter and background information.

- 4.2 Resolution No. 2013-01 Establishing a Fee for Review of Non-Impact Legislative Proposals. Pursuant to the schedule of development review fees adopted by the Commission on August 13, 2007, a fee of \$1,188 is charged for each proposal for a general plan amendment or change of zone/ordinance amendment with airport impacts. However, there are some circumstances where a general plan amendment or ordinance amendment, while applicable to lands within an Airport Influence Area as well as other areas within the jurisdiction of a local agency, has no possibility of having an impact on airport land use compatibility and, therefore, does not require extensive review. Establishment of this new fee would support jurisdictions seeking to follow the letter of the law (including CEQA protections). Staff has reviewed the reasonable cost of reviewing such projects and believes that establishing a new fee of \$375 is sufficient to cover the estimated cost of providing this service (reviewing the document, with a redline copy available, preparing and sending out the letter).

As expressed at the March 14 meeting, the process would involve the jurisdiction providing ALUC with the existing text and/or maps, the proposed text and/or maps, and a redline copy showing the changes. ALUC staff would review the materials to determine whether the proposed project qualifies as a non-impact proposal. If no issue is identified indicating a possible impact on airport land use compatibility, the jurisdiction would be advised that the proposal would qualify for review pursuant to the new fee. Staff would then prepare a letter for the Director's signature indicating that the proposed project has been reviewed and determined to be consistent. In accordance with our existing procedures, the Commission would receive a copy of the approval letter and background information at its next scheduled meeting.





# AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY

**CHAIR**  
Simon Housman  
Rancho Mirage

March 27, 2013

**VICE CHAIRMAN**  
Rod Ballance  
Riverside

Mr. David A. Newell  
City of Palm Springs Planning Services Department  
3200 E Tahquitz Canyon Way  
Palm Springs, CA 92262

**COMMISSIONERS**

Arthur Butler  
Riverside

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW**

Glen Holmes  
Hemet

File No.: ZAP1012PS13  
Related File No.: 5.1295 CUP, 6.528 VAR  
APN: 681-170-067

John Lyon  
Riverside

Dear Mr. Newell:

Greg Pettis  
Cathedral City

Richard Stewart  
Moreno Valley

Under the delegation of the Riverside County Airport Land Use Commission (ALUC), staff reviewed the above-referenced proposal for the establishment of an unmanned telecommunications facility consisting of antennas on a 48-foot high monotree tower, with associated equipment shelter, on an 800 square foot lease area within a 2.28-acre parcel, located southerly of East Palm Canyon Drive, easterly of Cherokee Way, northerly of Seminole Road, and westerly of Linden Way, in the City of Palm Springs.

**STAFF**

Director  
Ed Cooper

John Guerin  
Russell Brady  
Barbara Santos

County Administrative Center  
4080 Lemon St., 14th Floor.  
Riverside, CA 92501  
(951) 955-5132

The site is located within Airport Compatibility Zone E of the Palm Springs International Airport Influence Area (AIA). The maximum height of the new permanent structures will not exceed approximately 48 feet above ground level. The site elevation (352.5 feet above mean sea level) is lower than the elevation at the nearest point on the airport's runways (395.5 feet above mean sea level at the southeasterly terminus of the primary runway). Given the site's distance from the runway (over 8,700 feet), there are no height-related obstruction issues requiring FAA notice or review. (Therefore, ALUC did not charge an additional fee for review of the variance in this case.)

[www.rcaluc.org](http://www.rcaluc.org)

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2005 Palm Springs International Airport Land Use Compatibility Plan, subject to the following conditions:

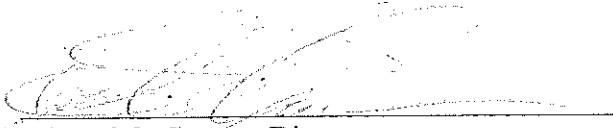
**CONDITIONS:**

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky.
2. The following uses shall be prohibited:

- (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
  - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
  - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area, including but not limited to, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, artificial marshes, incinerators, and fly ash disposal.
  - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
3. The attached notice shall be provided to all potential purchasers of the property and tenants of the buildings.

If you have any questions, please contact John Guerin, Principal Planner, at (951) 955-0982 or Russell Brady, Contract Planner, at (951) 955-0549.

Sincerely,  
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



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Edward C. Cooper, Director

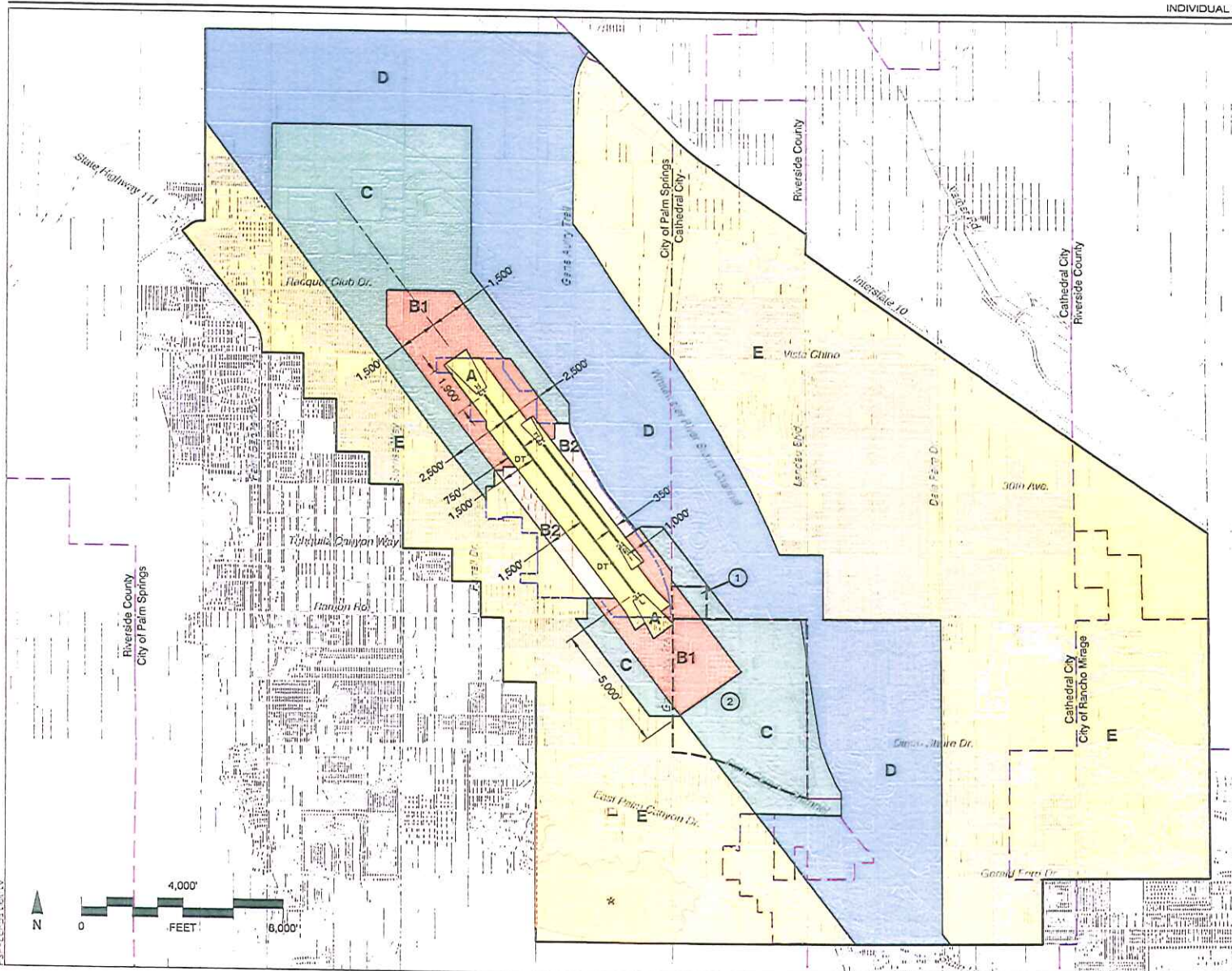
RB:bks

cc: Thomas Nolan, Manager, Palm Springs International Airport  
Korina Arvizu, Cable Engineering Services  
Sharon Dean, property owner  
ALUC Staff

# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)





**Legend**

- Compatibility Zones**
- Airport Influence Area Boundary
  - Zone A
  - Zone B1
  - Zone B2
  - Zone C
  - Zone D
  - Zone E
  - Height Review Overlay Zone

- Boundary Lines**
- Airport Property Line
  - City Limits

**Notes**

- All dimensions measured from runway ends and centerlines.
- DT = Displaced Threshold
- See Chapter 2, Table 2A for compatibility criteria associated with this map.
- See Policy PS.2.1.

Riverside County  
 Airport Land Use Commission  
**Riverside County  
 Airport Land Use Compatibility Plan  
 Policy Document**  
 (Adopted March 2005)

**ENGINEERING**

DATE: 01/15/93  
 DRAWN BY: J. J. JONES  
 CHECKED BY: J. J. JONES  
 PROJECT: NSB  
 SITE: PALM CANYON DR.  
 CITY: PALM SPRINGS, CALIFORNIA  
 COUNTY: RIVERSIDE  
 STATE: CALIFORNIA  
 TITLE: CONSTRUCTION DRAWING

**GENERAL NOTES**

1. ALL DIMENSIONS ARE IN FEET AND INCHES.  
 2. ALL DIMENSIONS ARE TO FACE UNLESS OTHERWISE NOTED.  
 3. ALL DIMENSIONS ARE TO CENTERLINE UNLESS OTHERWISE NOTED.  
 4. ALL DIMENSIONS ARE TO THE CENTERLINE OF THE ROAD UNLESS OTHERWISE NOTED.  
 5. ALL DIMENSIONS ARE TO THE CENTERLINE OF THE RAILROAD UNLESS OTHERWISE NOTED.  
 6. ALL DIMENSIONS ARE TO THE CENTERLINE OF THE AIRWAY UNLESS OTHERWISE NOTED.  
 7. ALL DIMENSIONS ARE TO THE CENTERLINE OF THE CANAL UNLESS OTHERWISE NOTED.  
 8. ALL DIMENSIONS ARE TO THE CENTERLINE OF THE DITCH UNLESS OTHERWISE NOTED.  
 9. ALL DIMENSIONS ARE TO THE CENTERLINE OF THE TRENCH UNLESS OTHERWISE NOTED.  
 10. ALL DIMENSIONS ARE TO THE CENTERLINE OF THE CUT UNLESS OTHERWISE NOTED.

**UNDERGROUND SERVICE ALERT OF SOUTHERN CALIFORNIA**  
 800-227-2600

**SITE INFORMATION**

OWNER: SHARON DEAN  
 77930 LAUD DR  
 LA JOLLA, CA 92053

PROJECT: NSB  
 4185 E. PALM CANYON DR.  
 PALM SPRINGS, CALIFORNIA 92264

LOT: 37  
 37' 47" 36.00" W  
 33.79405

SECTION: 116  
 116' 30" 01.00" W  
 +116.00084

SECTION: 63  
 352.20' ANSL  
 081-476-057

CITY: CITY OF PALM SPRINGS  
 C-2  
 U (TELECOM)

STATE: CALIFORNIA  
 COUNTY: RIVERSIDE  
 CITY: PALM SPRINGS  
 ZIP: 92264

**PROJECT TEAM**

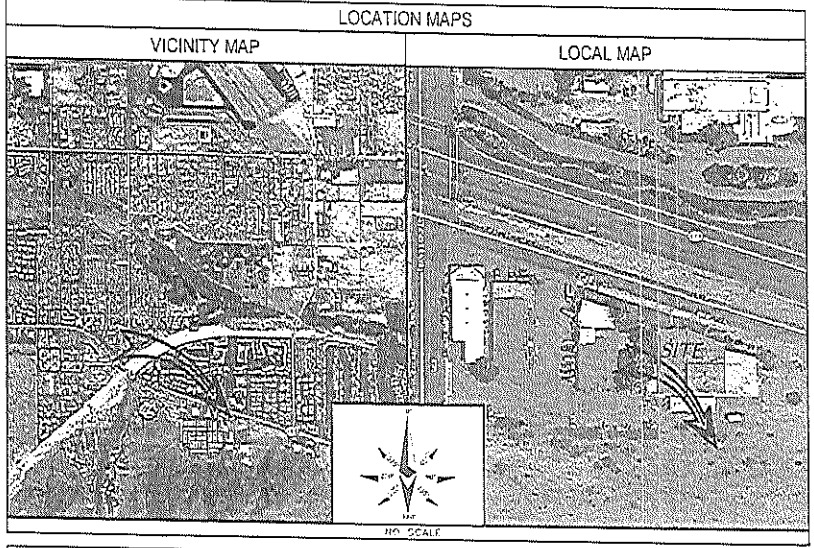
**PROJECT MANAGER:** J. J. JONES  
**ARCHITECTURE/ENGINEERING:** W. E. JACKSON CORPORATION  
 3900 WILSHIRE BOULEVARD, SUITE 2100  
 LOS ANGELES, CALIFORNIA 90010  
 (213) 350-0929

**DATE:** 01/15/93

**SITE NUMBER: RS0397**  
**SITE NAME: PALM CANYON Dr.**



**PROJECT: NSB**  
**SITE TYPE: MONOTREE**  
**SITE ADDRESS: 4185 E. PALM CANYON DR.**  
**PALM SPRINGS, CA 92264**



**DRIVING DIRECTIONS**

**DIRECTIONS FROM AT&T OFFICE: FROM PARK PLAZA DRIVE.**

1. START OUT GOING EAST ON PARK PLAZA DR. TOWARD SHOGIMAKER AND (1.0 MILES)
2. TAKE THE 1ST RIGHT ONTO SHOGIMAKER AVE. (.1 MILES)
3. TURN LEFT ONTO HIGHWAY 111 (1.4 MILES)
4. TURN RIGHT ONTO CARMENITA RD. (.3 MILES)
5. TURN LEFT ONTO CARMENITA AVE. (1.0 MILES)
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20. MERGE ONTO CARMENITA AVE. (1.0 MILES)

**CONSTRUCTION DRAWING**  
 IF USING 11"x17" PLOT, DRAWINGS WILL BE HALF SCALE

**APPROVALS**

THE FOLLOWING ENGINEERS HAVE REVIEWED THESE DRAWINGS AND APPROVE THE INFORMATION CONTAINED HEREIN FOR THE PURPOSES INTENDED. THE ENGINEER'S REVIEW IS LIMITED TO THE TECHNICAL ASPECTS OF THE DRAWINGS AND DOES NOT CONSTITUTE A GUARANTEE OF THE ACCURACY OF THE INFORMATION CONTAINED HEREIN.

DATE: 01/15/93

BY: J. J. JONES

**PROJECT DESCRIPTION**

THIS PROJECT IS A MONOTREE ANTENNA SITE FOR THE AT&T NETWORK. THE ANTENNA STRUCTURE IS TO BE CONSTRUCTED ON THE SITE AT 4185 E. PALM CANYON DR., PALM SPRINGS, CALIFORNIA. THE ANTENNA STRUCTURE IS TO BE CONSTRUCTED ON THE SITE AT 4185 E. PALM CANYON DR., PALM SPRINGS, CALIFORNIA.

**DRAWING INDEX**

SHEET NO.	SHEET TITLE
T-1	TITLE SHEET
C-1	TOPOGRAPHIC SURVEY
C-2	PHOTOGRAPHIC SURVEY
A-1	SITE PLAN
A-2	EQUIPMENT LAYOUT PLAN & ANTENNA LAYOUT PLANS
A-3	EXTERIOR ELEVATIONS
A-4	EXTERIOR ELEVATIONS
A-5	CUT CROSS SECTION

**DO NOT SCALE DRAWINGS**

UNLESS OTHERWISE SPECIFIED, ALL DIMENSIONS ARE TO BE TAKEN FROM THE CENTERLINE OF THE ROAD UNLESS OTHERWISE SPECIFIED. UNLESS OTHERWISE SPECIFIED, ALL DIMENSIONS ARE TO BE TAKEN FROM THE CENTERLINE OF THE ROAD UNLESS OTHERWISE SPECIFIED.



**PLANS PREPARED BY:** cable engineering services  
 PROJECT: COMMERCIAL

**W. E. JACKSON CORPORATION**  
 architecture & engineering

3900 WILSHIRE BOULEVARD, SUITE 2100  
 LOS ANGELES, CALIFORNIA 90010  
 (213) 350-0929

REV	DATE	DESCRIPTION

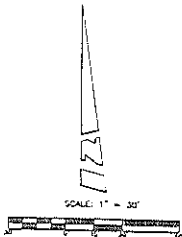


IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE SUPERVISION OF A LICENSED PROFESSIONAL ENGINEER, TO ALTER THIS DOCUMENT.

**RS0397**  
**4185 E. Palm Canyon Dr.**  
**Palm Springs, CA 92264**  
**Cell Site**

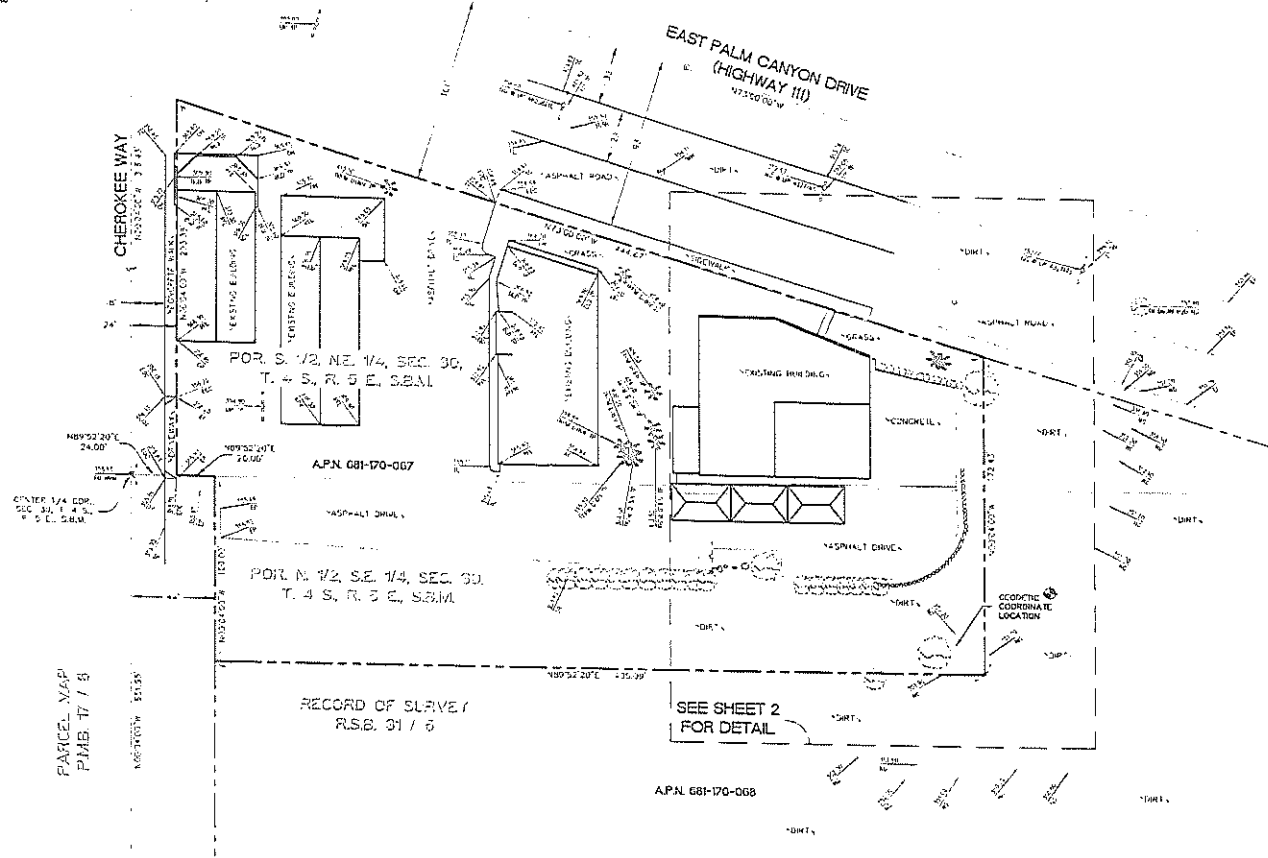
**SHEET TITLE**  
**TITLE SHEET**

**SHEET NUMBER**  
**T-1**



**LEGEND:**

- AP ASPHALT CONCRETE
- AS ASPHALT
- BC BRICK
- CA CONCRETE
- CL CLAY
- CO COARSE SAND
- CS CONCRETE
- CU CURB
- DC DRAINAGE
- DS DRAINAGE
- EA EXPOSED SAND
- EB EXPOSED SAND
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- PA PALM
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- SG SAND
- SH SAND
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**COORDINATES:**  
 LATITUDE 374732.837 N  
 LONGITUDE 4162006.477 W  
 AND 1983 GEODETIC COORDINATES AND ELEVATIONS WERE ESTABLISHED USING SURVEY GRADE "ASHTECH" GPS RECEIVERS AND KAMENICH SURVEY GRADE PRECISION SOFTWARE FOR POST-PROCESSING.

**BASIS OF BEARINGS:**  
 THE DEFINITION OF CHEROKEE WAY BEING NORTH 40°34'00" WEST PER PARCEL MAP, P.M.B. 1276, RECORDS OF EVERGOLD COUNTY.

**ASSESSOR'S IDENTIFICATION:**  
 INDEPENDENT COUNTY A.P.N. 681-170-067

**AREA:**  
 2.284 ACRES PER INDEPENDENT COUNTY ASSESSOR

**BENCH MARK REFERENCE:**  
 U.S.G.S. BENCH MARK "TM 343"  
 UNITED STATES GEOLOGICAL SURVEY BENCH MARK "TM 343" IS SHOWN ON THE "CENTRAL CITY" 7.5 MINUTE QUADRANGLE MAP.  
 ELEVATION 345.0 FEET AMSL (MAGNETIC DATUM VERIFIED IN FIELD TO BE WITHIN 1-A AGENCY STANDARDS)

**TITLE REPORT IDENTIFICATION:**  
 (REFER TO RECEIPT OF TITLE REPORT)

**EASEMENT NOTES:**  
 (REFER TO RECEIPT OF TITLE REPORT)

**LEGAL DESCRIPTION:**  
 (REFER TO RECEIPT OF TITLE REPORT)  
 THOSE PORTIONS OF THE NORTH HALF (1/2) OF THE SOUTHWEST QUARTER (1/4) AND THE SOUTH HALF (1/2) OF THE NORTHWEST QUARTER (1/4) OF SECTION 30, TOWNSHIP 4 SOUTH, RANGE 5 EAST, SAN BERNARDINO MERIDIAN, IN THE CITY OF PALM SPRINGS, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, ACCORDING TO THE OFFICIAL GOVERNMENT PLAT THEREOF.

**DATE OF SURVEY:**  
 OCTOBER 25, 2012

**SURVEYORS NOTE:**  
 THE BOUNDARY LINES AND THEIR INTERSECTIONS SHOWN HEREON ARE PER RECORD INFORMATION AND THEIR LOCATION ARE APPROXIMATE, HAVING REFERENCE TO THE RECORDS.

**LIVING PLANTS STATEMENT:**  
 THE HEIGHTS AND ELEVATIONS FOR THE TREES, BUSHES AND OTHER LIVING PLANTS SHOWN HEREON, SHOULD BE CONSIDERED APPROXIMATE (±10%) AND ONLY VALID FOR THE DATE OF THIS SURVEY. THEY ARE PRESENTED AS A GENERAL REFERENCE AND SHOULD NOT BE USED FOR DESIGN PURPOSES.

REVISION	DATE	BY
1	10/25/12	CW
2	10/25/12	CW
3	10/25/12	CW
4	10/25/12	CW

PREPARED BY: **at&t**

REGISTERED PROFESSIONAL SURVEYOR  
 STATE OF CALIFORNIA  
 LICENSE NO. 10000

**TOPOGRAPHIC SURVEY**

RS0397

4185 EAST PALM CANYON DRIVE  
 PALM SPRINGS, CA 92264

SCALE	DATE
1" = 30'	10/25/12
DRAWN BY	REVISION BY
BY	CW
CHECKED BY	BY
DATE	DATE
1	2

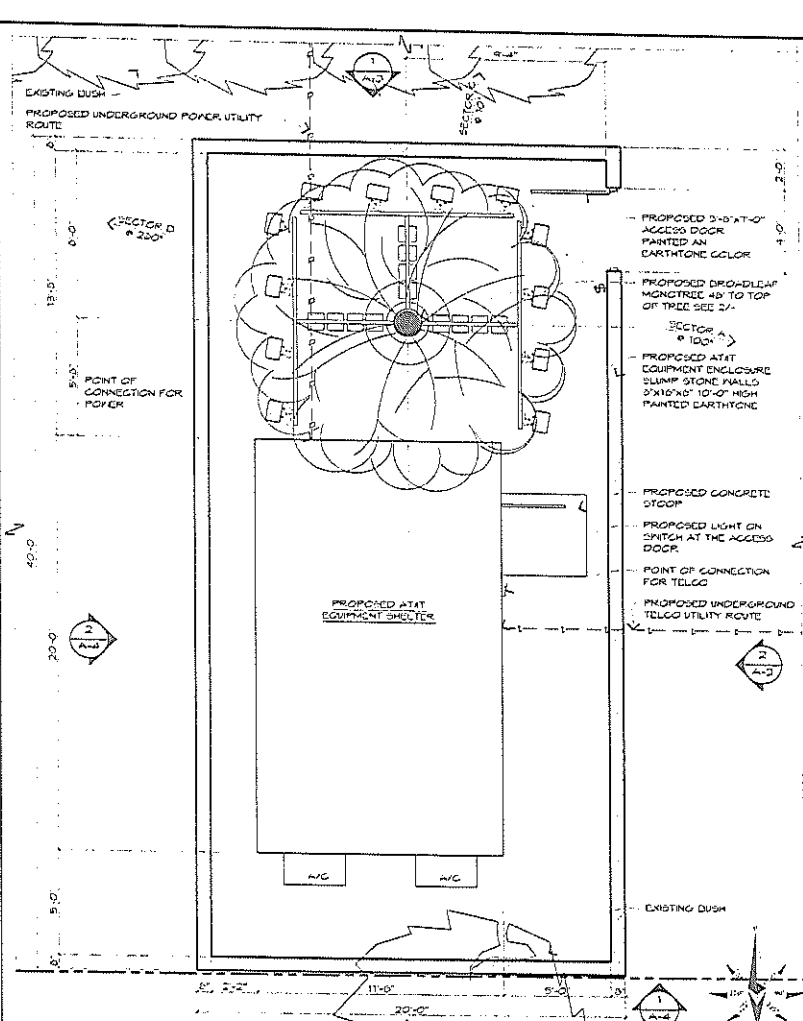
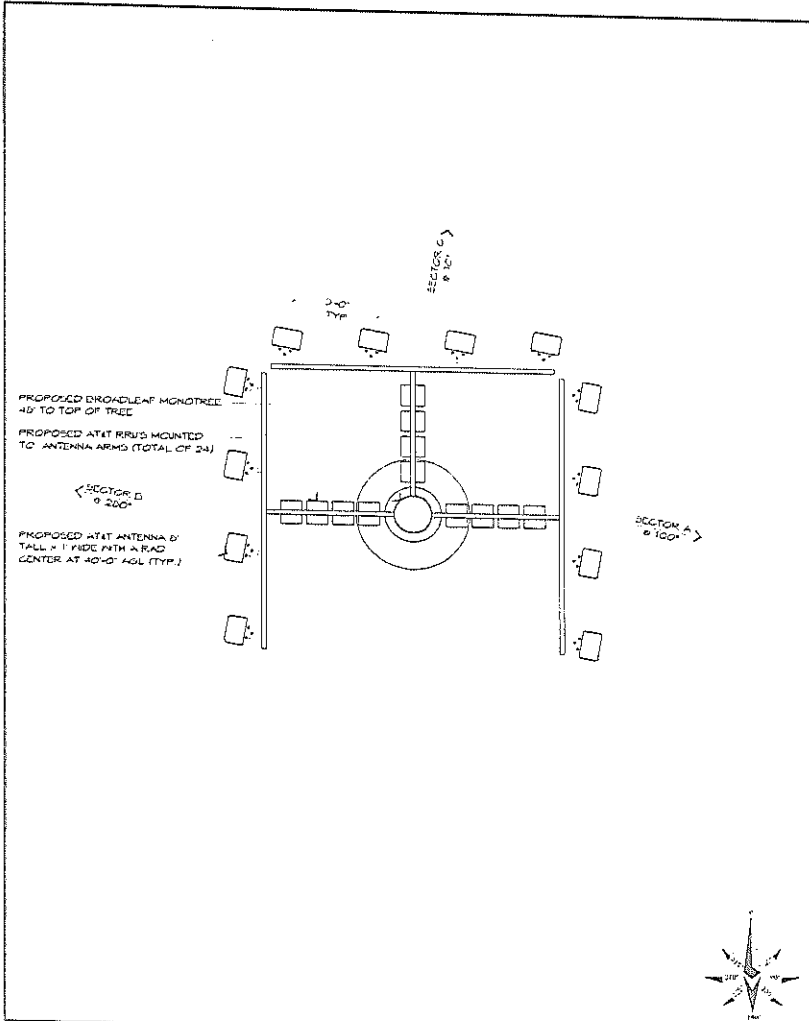


SEE SHEET 2 FOR SITE DETAILS









12000 PARK PLAZA (3RD FL)  
CERRITOS, CA 90710

PLAN REFERENCE

*at&t* **engineering services**

TELECOM COMMUNICATIONS, INC.  
17425 BUCKLEUP WAY, SUITE 100, LAKELAND, CA 92549  
PHONE: (951) 942-1000 FAX: (951) 942-1001

W. E. Jackson Corporation  
**architecture planning & engineering**

1900 WILSHIRE BOULEVARD, SUITE 2000  
LOS ANGELES, CALIFORNIA 90024  
(323) 330-0529

W. E. Jackson & H. B. Jackson, Architects - P. 1947

REV	DATE	DESCRIPTION



IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE SUPERVISION OF A LICENSED PROFESSIONAL LEGAL COUNSEL, TO ALTER THIS DOCUMENT.

RS0397  
4185 E. Palm Canyon Dr.  
Palm Springs, CA 92264  
Cell Site

SHEET TITLE  
**EQUIPMENT LAYOUT PLAN & ANTENNA LAYOUT PLAN**

SHEET NUMBER  
**A-2**

ANTENNA LAYOUT PLAN SCALE 1/2" = 1'-0" 2

EQUIPMENT LAYOUT PLAN SCALE 3/8" = 1'-0" 1

SYMBOL	DESCRIPTION
---	EXISTING DUSH
---	PROPOSED UNDERGROUND POWER UTILITY ROUTE
---	PROPOSED UNDERGROUND TELCO UTILITY ROUTE
---	PROPOSED CONCRETE STOOP
---	PROPOSED AT&T EQUIPMENT ENCLOSURE
---	PROPOSED AT&T EQUIPMENT SHELTER
---	PROPOSED BROADLEAF MONOTREE
---	PROPOSED AT&T RISERS
---	PROPOSED AT&T ANTENNA

SYMBOL	DESCRIPTION
---	EXISTING DUSH
---	PROPOSED UNDERGROUND POWER UTILITY ROUTE
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---	PROPOSED AT&T EQUIPMENT SHELTER
---	PROPOSED BROADLEAF MONOTREE
---	PROPOSED AT&T RISERS
---	PROPOSED AT&T ANTENNA

GRAPHICS LEGEND

ACRONYM LEGEND





LAND PREPARED BY: **CBM**  
 CONSULTING SERVICES  
 PROJECT COMMUNICATIONS INC.  
 10700 Rockledge Dr. Suite 1000, San Diego, CA 92121  
 PHONE: (619) 451-2222 FAX: (619) 451-2222

W. E. Jackson Corporation  
 architecture  
 planning & engineering **WEJ**  
 5900 WILSHIRE BOULEVARD, SUITE 2100H  
 LOS ANGELES, CALIFORNIA 90048  
 (323) 549-4029

REV	DATE	DESCRIPTION

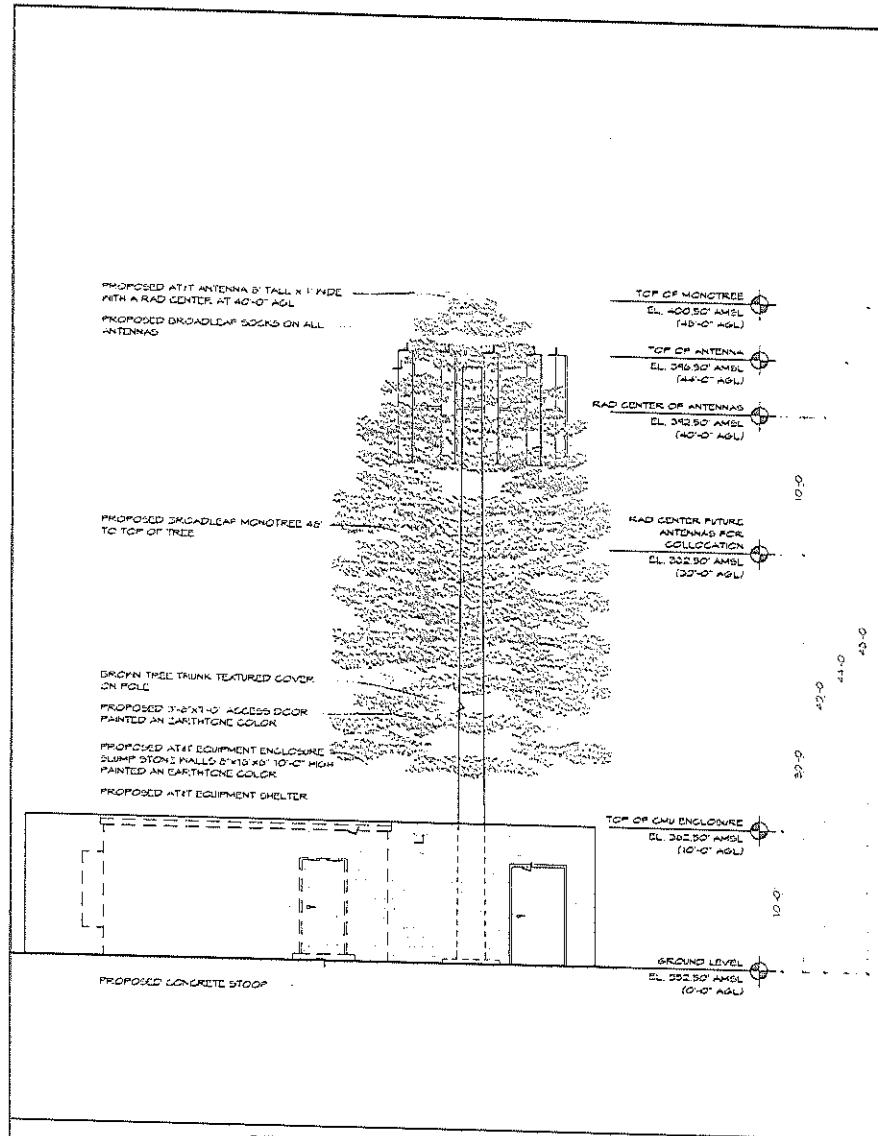


IF IT IS A VIOLATION OF LAW FOR ANY REASON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, TO ALTER THIS DOCUMENT.

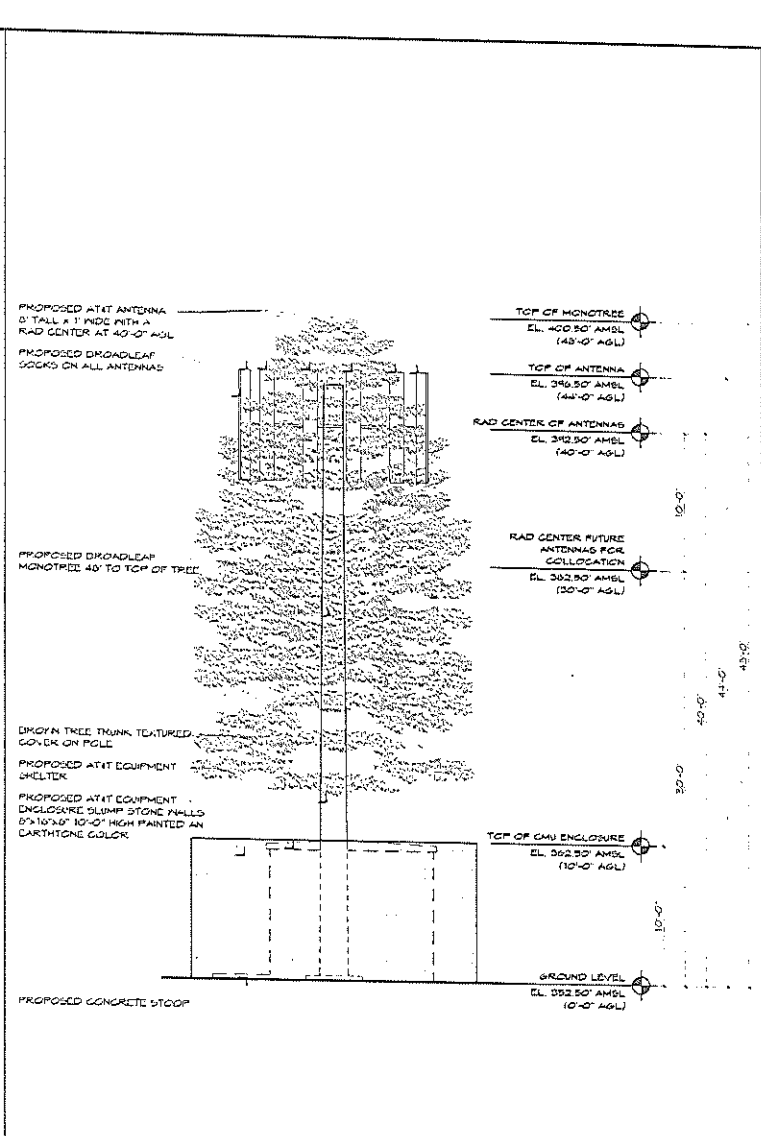
RS0397  
 4185 E. Palm Canyon Dr.  
 Palm Springs, CA 92264  
 Cell Site

SHEET TITLE  
 EXTERIOR ELEVATIONS

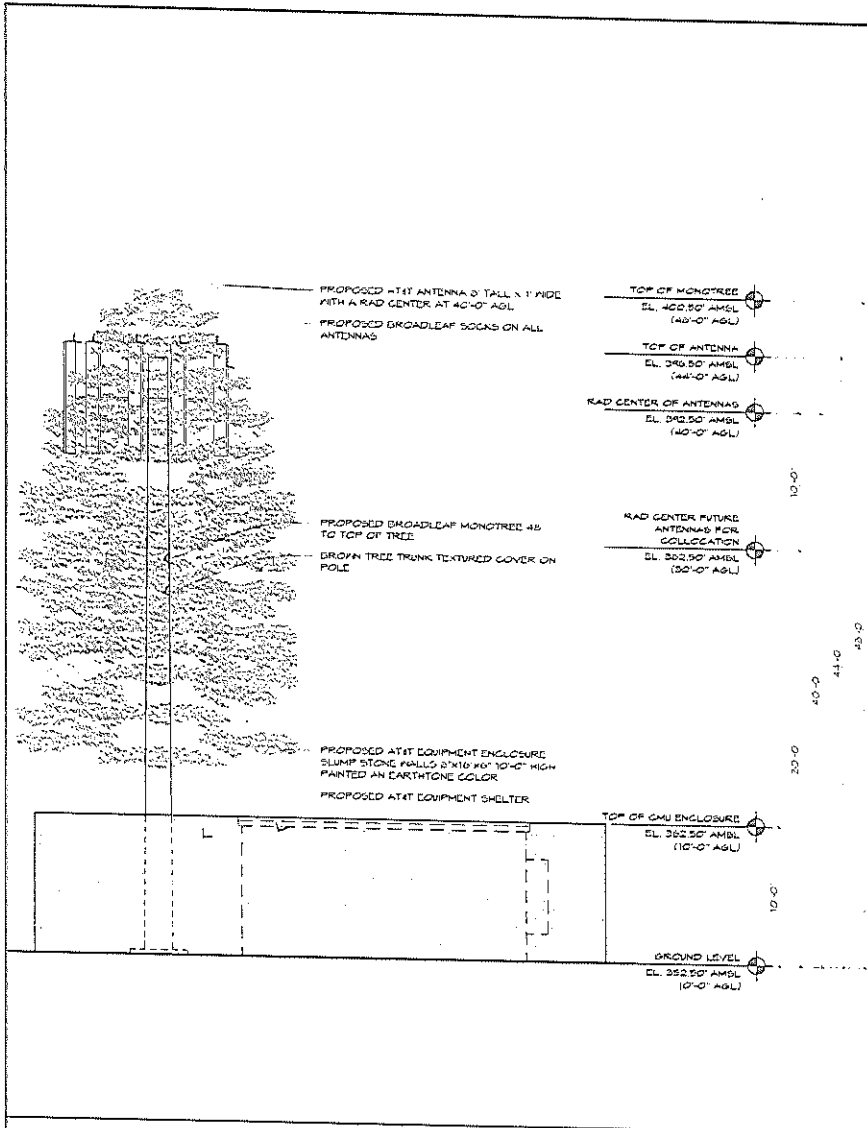
SHEET NUMBER  
 A-3



SCALE: 1/4" = 1'-0" 2

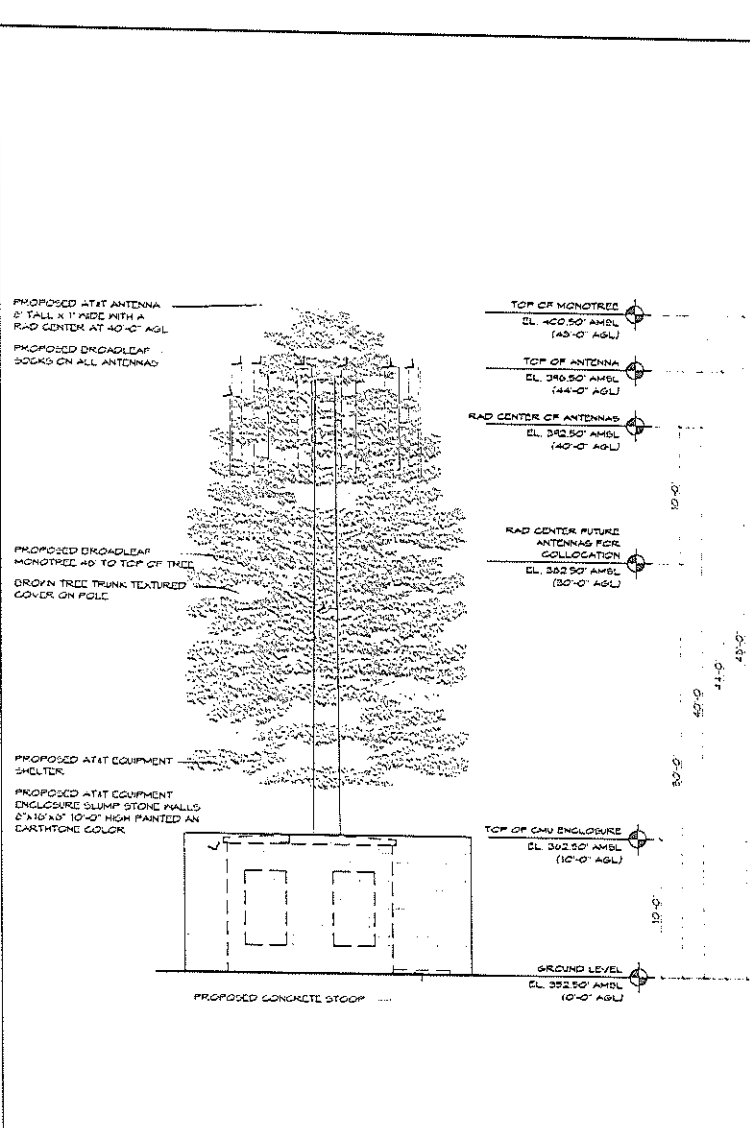


SCALE: 1/4" = 1'-0" 1




EXTERIOR ELEVATION - WEST

SCALE 1/4" = 1'-0" 2



EXTERIOR ELEVATION - SOUTH

SCALE 1/4" = 1'-0" 1



12900 PARK PLAZA DRIVE,  
CENTRO, CA 90703

PLANNED BY: **at&t**

DESIGNED BY: **at&t**

PROJECT COMMENCING DATE: **1988**

1488 SHERMAN ROAD, SUITE 200, PALM SPRINGS, CALIFORNIA 92264  
PHONE: (714) 333-1111 FAX: (714) 333-1111

**W. E. Jackson Corporation**  
architects  
planning & engineering

3300 WILSHIRE BOULEVARD, SUITE 2000  
LOS ANGELES, CALIFORNIA 90020  
(323) 550-2220

PHOTO: E. J. JONES, A. J. JONES, R. J. JONES

REV	DATE	DESCRIPTION

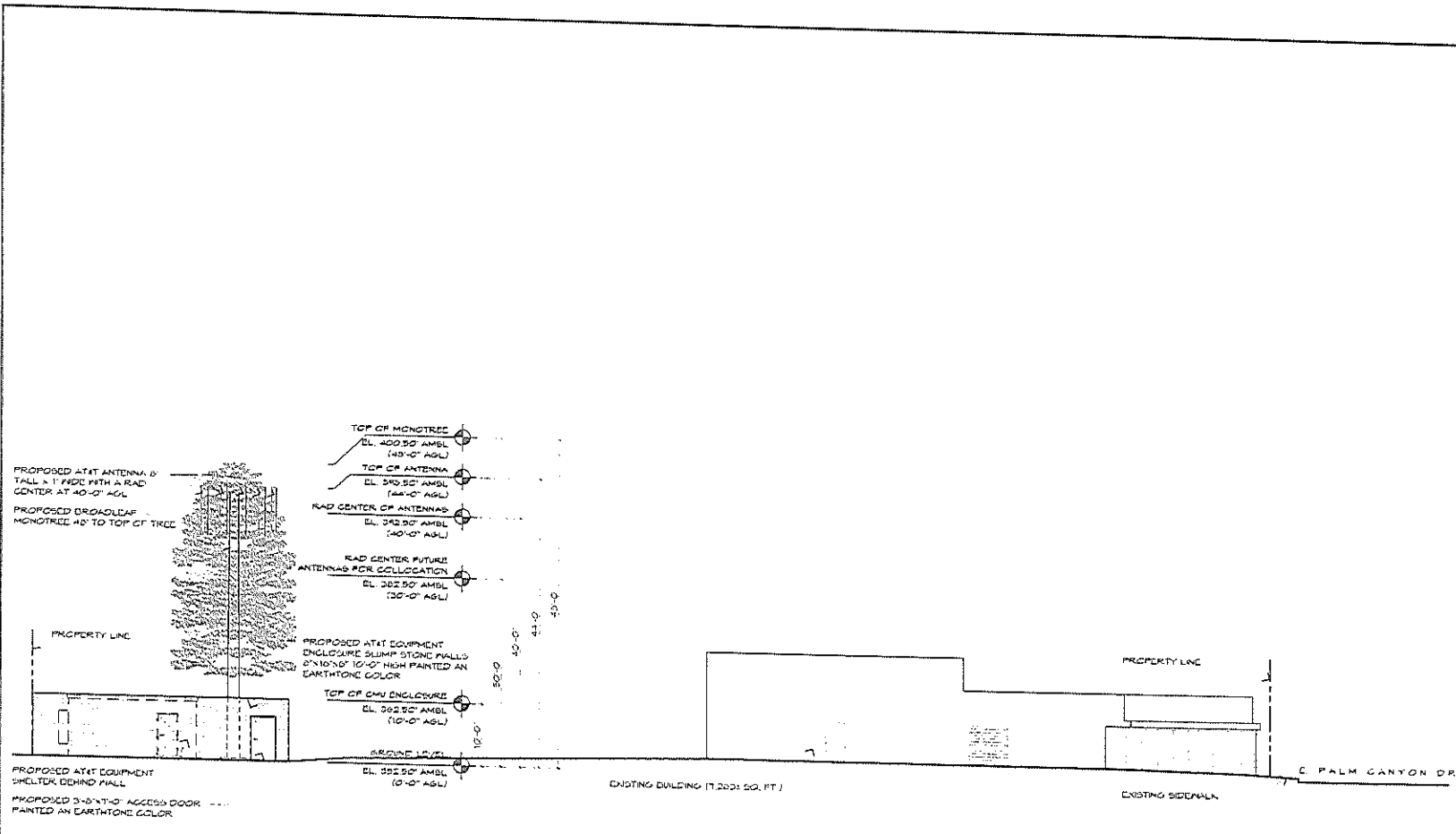
LICENSED ARCHITECT  
W. E. JACKSON  
No. 21000  
STATE OF CALIFORNIA  
11-30-80

IF IN A VIOLATION OF LAW FOR ANY REASON,  
UNLESS THEY ARE AGING UNDER THE SUPERVISION  
OF A LICENSED PROFESSIONAL, NO PERSON  
SHALL ALTER THIS DOCUMENT.

RS0397  
4185 E. Palm Canyon Dr.  
Palm Springs, CA 92264  
Call Site

SHEET TITLE  
**EXTERIOR ELEVATIONS**

SHEET NUMBER  
**A-4**



SITE CROSS SECTION

SCALE: 1/8" = 1'-0" 1



PLANS PREPARED BY  
**Cable Engineering Systems**  
 PROJECT COMMUNICATIONS INC.  
 17425 Sepulveda Blvd. Suite 200 • Encinitas, CA 92024  
 (619) 441-2222 Fax: (619) 441-2222

W. E. Jackson Corporation  
 architecture  
 planning & engineering **WEJ**  
 5900 Wilshire Boulevard, Suite 7400  
 Los Angeles, California 90028  
 (213) 438-0222  
 JOHN C. JACKSON, P.E., P.L.C., ARCHITECT - 01-1984

REV.	DATE	DESCRIPTION



IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE LICENSED UNDER THE SUPERVISION OF A LICENSED PROFESSIONAL ENGINEER, TO REPRODUCE THIS DOCUMENT.

RS0397  
 4185 E. Palm Canyon Dr.  
 Palm Springs, CA 92264  
 Cull Silo

SHEET TITLE  
 SITE CROSS SECTION

SHEET NUMBER  
 A-5



**APPLICATION FOR MAJOR LAND USE ACTION REVIEW**  
**RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION**

ALUC Identification No.

2AP100135

**PROJECT PROPONENT (TO BE COMPLETED BY APPLICANT)**

Date of Application 2/20/2013  
 Property Owner Sharon Dean Phone Number (760) 321-7629  
 Mailing Address 1843 N. Leonard Road  
Palm Springs, CA 92262

Agent (if any) CES as Agent for New Cingular Wireless dba AT&T Mobility Phone Number (818) 898 - 2352  
 Mailing Address 10640 Sepulveda Boulevard, Suite 1  
Mission Hills, CA 91345

**PROJECT LOCATION (TO BE COMPLETED BY APPLICANT)**

*Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways*

Street Address 4185 E. Palm Canyon  
Palm Springs, CA 92264  
 Assessor's Parcel No. 681-170-067 Parcel Size 2.28 AC  
 Subdivision Name See "Exhibit A" attached Zoning C-2  
 Lot Number See "Exhibit A" attached Classification C-2

**PROJECT DESCRIPTION (TO BE COMPLETED BY APPLICANT)**

*If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed*

Existing Land Use (describe) Retail-Consignment store

Proposed Land Use (describe) Unmanned Wireless Facility

For Residential Uses Number of Parcels or Units on Site (exclude secondary units) N/A

For Other Land Uses Hours of Use 24/7

(See Appendix C) Number of People on Site Maximum Number 1-2 monthly

Method of Calculation N/A

Height Data Height above Ground or Tallest Object (including antennas and trees) 48' ft.  
 Highest Elevation (above sea level) of Any Object or Terrain on Site 400 ft.

Flight Hazards Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?  Yes  
 No

If yes, describe \_\_\_\_\_  
 \_\_\_\_\_  
 \_\_\_\_\_





# AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY

March 18, 2013

**CHAIR**

Simon Housman  
Rancho Mirage

**VICE CHAIRMAN**

Rod Ballance  
Riverside

**COMMISSIONERS**

Arthur Butler  
Riverside

John Lyon  
Riverside

Glen Holmes  
Hemet

Greg Pettis  
Cathedral City

Richard Stewart  
Moreno Valley

**STAFF**

Director  
Ed Cooper

John Guerin  
Russell Brady  
Barbara Santos

County Administrative Center  
4080 Lemon St., 14<sup>th</sup> Floor.  
Riverside, CA 92501  
(951) 955-5132

[www.rcaluc.org](http://www.rcaluc.org)

Ms. Zai Abu Bakar, Community Development Director  
City of Banning  
P. O. Box 998  
Banning, CA 92220-0998

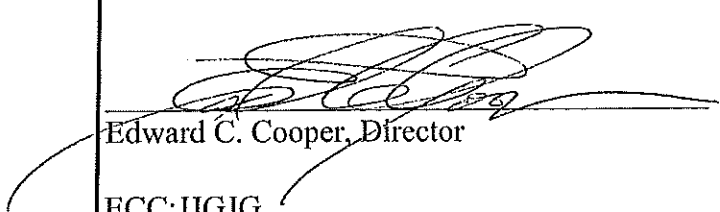
**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW**  
File No.: ZAP1011BA13  
Related File No.: City of Banning General Plan Amendment No. 13-2501  
APN: Citywide

Dear Ms. Abu Bakar:

As authorized by the Riverside County Airport Land Use Commission (ALUC) pursuant to its Resolution No. 2011-02, as ALUC Director, I have reviewed City of Banning General Plan Amendment No. 13-2501, an amendment to the text of the City's Circulation Element, and have determined that the amendment: (1) has no possibility for having an impact on airport land use compatibility within the Banning Municipal Airport Influence Area (AIA); (2) has no potential for being inconsistent with the compatibility criteria and policies of the Banning Municipal Airport Land Use Compatibility Plan; and (3) has no possibility for having an impact on the safety of air navigation within the AIA or on the long-term viability of Banning Municipal Airport operations. Therefore, I hereby find the above-referenced project, a proposal to amend required roadway levels of service from "C" to "D" and to remove the Highland Home Road interchange from the City's General Plan, **CONSISTENT** with the 2004 Banning Municipal Airport Land Use Compatibility Plan.

If you have any questions, please contact me at (951) 955-5132.

Sincerely,  
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

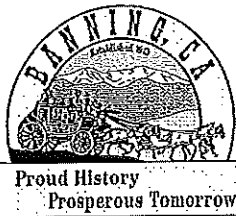
  
Edward C. Cooper, Director

ECC:JGJG

cc: Andrew J. Takata, City Manager  
Duane Burk, Director of Public Works  
ALUC Staff

Y:\AIRPORT CASE FILES\Banning\ZAP1011BA13\ZAP1011BA13.LTR.doc





# City of Banning

99 E. Ramsey Street • P.O. Box 998 • Banning, CA 92220-0998 • (951) 922-3125 • Fax (951) 922-3128

## COMMUNITY DEVELOPMENT

February 26, 2013

John Guerin  
Principal Planner  
Riverside County Airport Land Use Commission  
4080 Lemon Street, 9<sup>th</sup> Floor  
Riverside, CA 92501

**Subject:** General Plan Amendment No. 13-2501 – Amendment to the City of Banning General Plan Circulation Element

Dear Mr. Guerin:

Attached for your review in Exhibit "A" is the proposed amendment to the City of Banning General Plan Circulation Element. Attached in Exhibit "B" is the existing Circulation Element that was adopted in January 2006. The proposed amendment is to change the road performance from level of service C to D and remove Highland Home Road interchange from the City's General Plan. The amendment does not change any land uses, their locations, density and intensity. The application and the fee in the amount of \$1,188.00 for review by the Airport Land Use Commission were sent to you previously (attached).

If you have any questions regarding the proposed amendment and this letter, please contact me at (951) 922-3131.

Sincerely,

Zai Abu Bakar  
Community Development Director

cc: Andrew J. Takata, City Manager  
Duane Burk, Director of Public Works

### Attachments:

1. Exhibit "A" - Proposed amendment to the City of Banning General Plan Circulation Element
2. Exhibit "B" – Existing Circulation Element that was adopted in January 2006.
3. Application and Copy of Check #138586 in the amount of \$1,188.00 for review.

**Exhibit "A"**  
**Proposed Amendment to General Plan Circulation Element**





---

## **CIRCULATION ELEMENT**

---

### **PURPOSE**

The purpose of the Circulation Element is to provide goals, policies, programs and standards that correlate the City's transportation system with the types, intensities and locations of land uses within the City. It addresses those segments of the local transportation system that interface with and serve as extensions of the regional system connecting the City of Banning with the broader Pass Region and other communities in Southern California. The Circulation Element also serves as the blueprint for future land use policy decisions and social and economic development efforts.

### **BACKGROUND**

Due to its close interrelatedness, the Circulation Element is an outgrowth of City and regional land use planning. In addition to its effects on the physical, social and economic environment of the City, the Circulation Element also has a direct relationship with the Housing, Open Space, Noise and Air Quality elements. Being integrally tied to the Land Use Element, the Circulation Element is predictably influenced by the types, intensities and distribution of land uses within the community and surrounding area.

Local and regional air quality issues are closely related to the efficiency of the local and regional transportation system. As the City of Banning and the Pass Region continue to grow, vehicle miles will increase, and travel speeds will be reduced, resulting in higher emissions per mile traveled. The policies and programs established by the Circulation Element can play an important role in maintaining and enhancing the flow of traffic and preserving air quality in the community.

The Circulation Element has been developed to serve as a comprehensive transportation management strategy for vehicular traffic and other modes of transportation important to the City such as: air traffic and the Banning Municipal Airport; rail traffic; public transportation; and alternative transportation.

California Government Code sets forth the information and data analysis requirements of the Circulation Element. Government Code Section 65302(b) requires that the element describe major thoroughfares and that their planned development be closely coordinated with the Land Use Element of the General Plan.

It is also required that the Circulation Element include development or improvement standards that are responsive to changes in demand for capacity created by implementation of the Plan. Government Code Sections 65103 (f) and 65080, et seq., require that the City coordinate Circulation Element provisions with applicable regional and state transportation plans. In the Banning General Plan Planning Area, the following agencies are responsible for preparing these transportation plans: County of Riverside, Morongo Band of Mission Indians, Southern

California Association of Governments (SCAG) and California Department of Transportation (CalTrans). The state is also required to coordinate its planning efforts with those of local jurisdictions (§65080(a)), with the federal government being under a similar mandate (§134, Title 23 of the U.S. Code).

**Average Daily Traffic Volumes**

Average Daily Trips (ADT) is the total number of vehicles that travel a defined segment of roadway over a twenty-four hour period. ADT is a useful benchmark number for determining various roadway configurations and design aspects. The peak hour ADT, which is the highest volume of traffic to pass over a segment of roadway during an hour period, is also a useful means of determining a roadway's capacity and level-of-service. Traffic counts at intersections can provide an even more detailed picture of existing and future operating conditions at intersections.

Roadways are generally classified in a hierarchical manner, according to the number of vehicle lanes provided. Table III-15, below, lists the various roadway types/cross-sections found in the planning area and the maximum daily traffic volumes each type of roadway can accommodate at various levels-of-service. For example, for a Major Highway to operate at LOS D, it should accommodate no more than 34,200 vehicle trips per day, with a design capacity of 27,000 vehicle trips per day. These roadway capacities are "rule-of-thumb" estimates, which may vary depending upon site-specific factors, such as the number and configurations of intersections, roadway grades, sight distance, percentage of truck and bus traffic, and degree of access control.

**Table III-15  
Level-of-Service Volumes/Capacity Values  
For Various Roadway Classifications**

Classification	Roadway Width (Ft.)	Number of Lanes	Maximum Two-Way Average Daily Traffic Volume Level of Service		
			C	D	E
Collector	60	2	12,800	14,400	16,000
Secondary Highway	80	4	24,000	27,000	30,000
Major Highway	100	4	30,400	34,200	38,000
Major Highway	110	4	30,400	34,200	38,000
Major Highway	134	6	47,200	53,100	59,000

<sup>c</sup> Source: City of Banning General Plan Update Traffic Study 2005

## GENERAL PLAN CIRCULATION ELEMENT STREET CLASSIFICATION AND CROSS SECTIONS

The City of Banning and other communities in the pass region form a continuous and interconnected suburban development pattern, tied together by U.S. Interstate-10 and a network of arterial roadways. A variety of physical influences and constraints, including the geography of the city and the region, and the existing roadway network, affect traffic flows and the impacts of development on local roadways. The City has facilitated the construction and maintenance of a variety of major roadways of local importance. These roadways have been built along a north-south grid that interconnects with major arterials, some of which also pass through adjacent jurisdictions, primarily to the west.

The roadway system in Banning is defined using a classification system that describes a hierarchy of roadway types. The categories of roadways included in this classification system differentiate the size, function, and capacity of each type of roadway. The General Plan Circulation Element street classifications are listed below and the street cross-sections are shown in Exhibit III-4.

Collector: 66 feet of right of way, with a 44-foot street section from curb to curb. This represents two lanes of traffic, with parking lanes on each side, and a parkway on each side.

Divided Collector: 78 feet of right of way, with a 66-foot street section from curb to curb. This represents two travel lanes, with parking lanes on each side, a center median, and a parkway on each side.

Secondary Highway: 88 feet of right of way, with a 64-foot street section from curb to curb. This represents four travel lanes, with parking lanes on each side, and a parkway on each side.

Major Highway: 100 feet of right of way, with a 76-foot street section from curb to curb. This represents four travel lanes, with parking lanes on each side, a center median, and a parkway on each side.

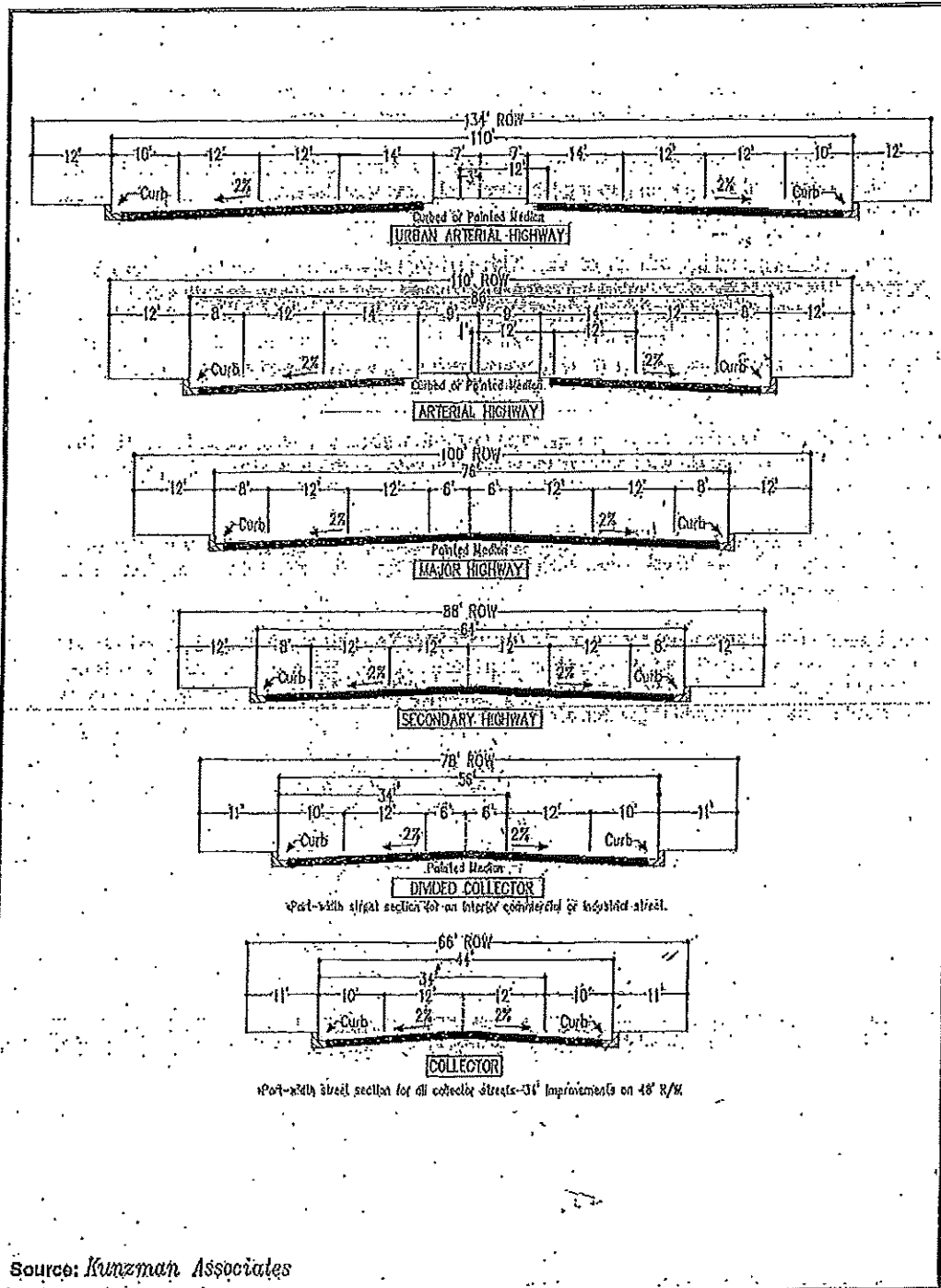
Arterial Highway: 110 feet of right of way, with an 86-foot street section from curb to curb. This represents four travel lanes, with parking lanes on each side, a center median, and a parkway on each side.

Urban Arterial Highway: 134 feet of right of way, with a 110-foot street section from curb to curb. This represents six travel lanes, with parking lanes on each side, a center median, and a parkway on each side.

Local Streets (those streets not shown on the General Plan Recommended Street System): 60 feet of right of way, with a 40-foot street section from curb to curb. This represents two travel lanes, with parking lanes on each side, and a parkway on each side. Local street standards may vary as described in the Policies and Programs of this Element.

The roadway classifications are also depicted on exhibit III-5, General Plan Circulation Element street system.





Source: *Kunzman Associates*



**Banning General Plan  
Street Cross Sections**

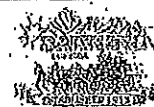
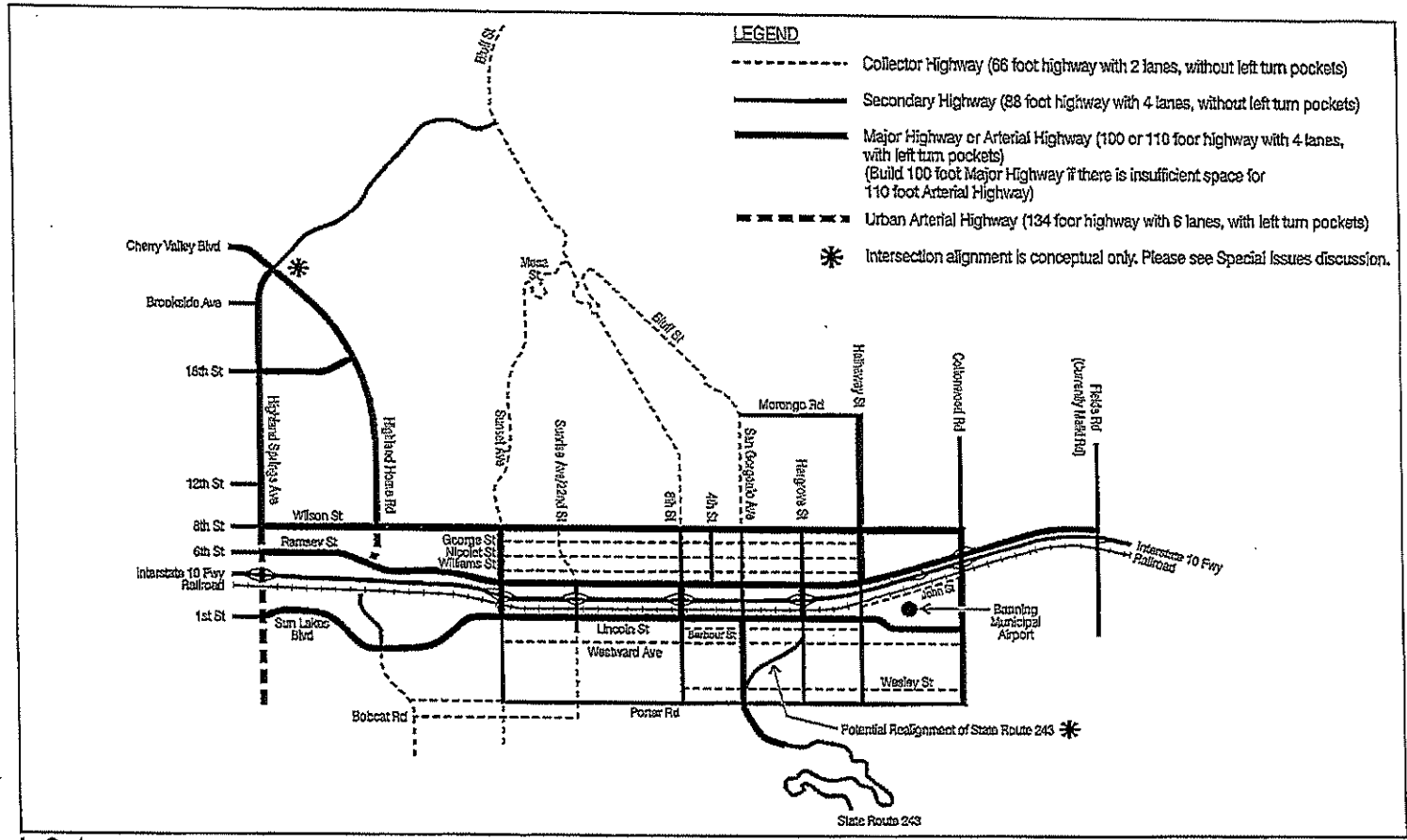


Exhibit  
**III-4**



**LEGEND**

- - - - - Collector Highway (66 foot highway with 2 lanes, without left turn pockets)
- Secondary Highway (88 foot highway with 4 lanes, without left turn pockets)
- Major Highway or Arterial Highway (100 or 110 foot highway with 4 lanes, with left turn pockets)  
(Build 100 foot Major Highway if there is insufficient space for 110 foot Arterial Highway)
- - - - - Urban Arterial Highway (134 foot highway with 6 lanes, with left turn pockets)
- \* Intersection alignment is conceptual only. Please see Special Issues discussion.

EXHIBIT III-5

Was III-6

SOURCE: Kutzman Associates  
SCOB1101VGGP Street System.cdr (2/25/13)

**Table III-16  
Roadway Classifications**

<b>Roadway Classification</b>	<b>Roadway</b>
<u>Urban Arterial Highway</u> (6 lanes with left turn pockets)	Highland Springs from Cherry Valley to Potrero Blvd.
<u>Major or Arterial Highway</u> (4 lanes with left turn pockets)	Highland Home from Cherry Valley Blvd. To Sun Lakes Blvd.  Highland Springs from Cherry Valley Blvd. to Wilson Street  Hathaway from Morongo St. to I-10  Sunset from Wilson to Lincoln  8 <sup>th</sup> Street from Ramsey to Lincoln  San Gorgonio from Lincoln to south City Limit  Hargrave from Ramsey to Lincoln  Cottonwood (North – South) from Ramsey to Porter  Wilson from Highland Springs to Cottonwood (North – South)  Ramsey from Highland Springs to Malki Rd.  Sun Lakes/Lincoln from Highland Springs to Cottonwood (North – South)
<u>Secondary Highway</u> (4 lanes without left turn pockets)	Porter from Sunset to Cottonwood (North – South)  Sunset from Wilson to Mesa  Sunset from Lincoln to Porter  22 <sup>nd</sup> Street from Ramsey to south of Lincoln  8 <sup>th</sup> Street from Wilson to Ramsey  8 <sup>th</sup> Street from Lincoln to Porter  4 <sup>th</sup> Street from Wilson to Ramsey  San Gorgonio from Wilson to Lincoln  Hargrave from Wilson to Porter  Hathaway from Ramsey to Porter  Cottonwood (North – South) from Ramsey northward  Cottonwood (North – South) from Porter southward  Malki Road from end to end



## **INDICATORS OF ROADWAY EFFICIENCY**

The efficient movement of vehicular and non-vehicular traffic on local and regional roadways is critical to the normal day-to-day functioning of a community. Consequences resulting from obstructions in traffic flow may include economic loss due to delays in transporting goods, increased psychological stress for the traveling public, and increased risk for motor vehicle accidents. The efficiency of a particular roadway can be determined by assessing the roadway's capacity, level-of-service, and average daily traffic volume, each of which is described below.

### Level-of-Service

Roadway capacity is defined as the number of vehicles that may pass over a section of roadway in a given time period under prevailing conditions. Roadway capacity is most restricted by intersection design and operation. The capacity of a roadway and the degree to which that capacity is being utilized is typically described as the roadway's "Level-of-Service" (LOS). Level-of-Service is a qualitative measure of the efficiency of traffic flow and is defined by alphabetical connotations, ranging from "A" through "F," that characterize roadway operating conditions.

LOS A represents an optimum or free-flowing condition, and LOS F indicates extremely slow speeds and system failure. Levels-of-Service are represented as volume-to-capacity (V/C) ratios, or vehicle demand divided by roadway capacity. V/C ratios smaller than 1.00 imply better operational characteristics and levels-of-service. V/C ratios that exceed 1.00 imply worse operating conditions and LOS F, where traffic demand exceeds roadway capacity. The table below defines the various LOS classifications.

Table III-14

Roadway Level Of Service Description	
Level of Service	Quality of Traffic Flow
A	Primarily free-flow operations at average travel speeds usually about 90 percent of the free-flow speed for the arterial classification. Vehicles are completely unimpeded in their ability to maneuver within the traffic stream. Stopped delay at signalised intersections is minimal.
B	Reasonably unimpeded operations at average travel speeds usually about 70% of the free-flow speed of the arterial classification. Ability to maneuver within the traffic stream is only slightly restricted. Stopped delays are not bothersome, and drivers generally are not subject to appreciable tension.
C	Traffic operations are stable. However, mid-block maneuverability may be more restricted than in LOS B. Longer queues, adverse signal coordination, or both may contribute to lower average travel speeds of about 50% of the average free-flow speed for the arterial classification. Motorists will experience some appreciable tension while driving.
D	Borders on a range where small increases in flow may cause substantial increases in approach delay and decreases in arterial speed. LOS D may be due to adverse signal progression, inappropriate signal timing, high volumes, or some combination of these factors. Average travel speeds are about 40% of the free-flow speed. For planning purposes, this level-of-service is the lowest that is considered acceptable.
E	Characterized by significant approach delays and average travel speeds of one-third or less of the free-flow speed. Typically caused by some combination of adverse progression, high signal density (more than two signalised intersections per mile), high volumes, extensive queuing, delays at critical intersections, and/or inappropriate signal timing.
F	Arterial flow at extremely slow speeds, below one-third to one-fourth of the free-flow speed. Intersection congestion is likely at critical signalised intersections, with high approach delays and extensive queuing. Adverse progression is frequently a contributor to this condition.

Source: p. 11-4, Highway Capacity Manual, Special Report 209, Transportation Research Board, 1994.

Traffic engineers and transportation planners are involved in on-going efforts to strike a balance between providing ideal roadway operating conditions and controlling the costs of infrastructure and right-of-way needed to assure those conditions. For General Plan purposes, LOS D is assumed to be the “acceptable” level-of-service for all General Plan roadways and intersections within the City and at Freeway interchanges.

**ALL WEATHER CROSSINGS**

All weather crossings include bridges and culverted streets that allow natural drainages to flow under the roadway during major storms. All weather crossings in the City are described in the following Table.

**Table III-18  
Buildout Bridges**

<b>Location</b>	<b>Needed Improvement</b>
Sunset Ave. at Pershing Channel	1 Bridge
8 <sup>th</sup> Street at Montgomery Creek	1 Bridge
Wilson Street at Smith Creek	1 Bridge
Sun Lakes Boulevard at Smith Creek	1 Bridge
Westward Avenue at Pershing Channel	1 Bridge
Wesley Street at Smith Creek	1 Bridge
Porter Road at Smith Creek	1 Bridge
Porter Road at Pershing Channel, and in two locations at Montgomery Creek	3 Bridges
Ramsey Street at Smith Creek and Pershing Channel	2 Bridges
Lincoln Avenue at east fork of Smith Creek, at west fork of Smith Creek, at Pershing Channel and at Montgomery Creek	4 Bridges

These facilities are needed both to allow the smooth flow of traffic in these areas, and to assure public safety in emergency situations. Construction of these facilities is particularly critical to the development of the south side of the City.

**Transportation Demand Management**

The continued urbanization and growth of the Pass Region will necessitate transportation demand and systems management to preserve and increase available roadway capacity. Transportation Demand Management (TDM) requires the development and implementation of policies, plans and programs that result in the use of a wider range of transportation alternatives, including public transit and bicycles.

TDM can include alternative travel modes, such as carpooling, van pooling and mass transit. In addition, employee flex-time work schedules that reduce peak hour travel and associated traffic congestion may also be considered. The Riverside County Transportation Commission (RCTC), in response to state mandates, has prepared a regional Congestion Management Program. This program required the City of Banning and other cities to prepare TDM ordinances or risk the loss of federal transportation funds. The City has adopted a TDM ordinance.



### **Traffic Calming**

Traffic calming is a useful tool available to the City to slow traffic in residential neighborhoods, and discourage shortcuts through these neighborhoods. The advantages of traffic calming must always be balanced, however, with the requirements of emergency vehicles.

Traffic calming can include such design features as curvilinear streets, speed humps and raised intersections, traffic circles or roundabouts, stop signs on through streets, and narrowed intersections. Using these design features improves safety by slowing traffic to 30 miles per hour or less. The restriction or blocking of turning movements can also be used effectively in traffic calming, particularly to reduce cut-through, or short cut traffic in neighborhoods.

## **ALTERNATIVE TRANSPORTATION**

### **Pedestrian, Equestrian, Bicycle and Golf Cart Circulation**

The General Plan and associated documents support planning that allows and enhances access to commercial services and places of employment and recreation without the essential use of motorized vehicles. In this regard, master planning sidewalks, bike lanes, off-street trails and golf cart routes is especially important along major roadways in the community.

#### Pedestrian and Equestrian Trails and Bike Paths

The City's adopted Parks and Recreation Master Plan includes a proposed trails system that would provide access from parks, city streets, and the surrounding community via urban, foothill and wash/hill trails. Two short, minimally developed urban trail segments currently exist in the City and are associated with the proposed trail system. The City has also approved, or is currently reviewing, several equestrian trails within and adjacent to private subdivisions.

There are currently no bikeways within the planning area. Several Class II and III bikeways have been proposed along City streets. However, development of a network of bikeways is constrained by the existing condition of street right-of-ways. Future bike routes should be planned to provide safe routes for intra-City bicycle traffic and should be clearly marked and striped. Where possible, they should be designed as one-way bike routes, which flow in the same direction as adjacent automobile traffic. Combination sidewalks/bikeways require an eight-foot width. These multi-use lanes will also serve as links to recreational facilities throughout the community. Policies and programs specifically addressing the need for trails and bicycle lanes are included in the Parks and Recreation Element.

#### Sidewalks

Within some portions of the City, sidewalks are non-existent or discontinuous, limiting their usefulness as safe alternatives to vehicle travel. As previously noted, rights-of-way along City roadways are inconsistent, limiting the potential for bikeway development. When considering future development, pedestrian and bicycle accommodation and safety should be given emphasis equal to that currently given to automobile access. This is particular true of collector and arterial roadways, where more traffic increases the hazards to pedestrians.

Another area where sidewalks are an important asset is in the historic Downtown of the City (between the I-10 right of way and Williams, and between 8<sup>th</sup> Street and Alessandro). Pedestrian

access in this area is a function of economic development for this area, and should be considered in all future development and redevelopment plans.

Conversely, sidewalks on local, neighborhood roadways may not be necessary, particularly in established neighborhoods where roadways have been developed to their ultimate right-of-way, and the addition of sidewalks would impact front yards and affect the character of the neighborhood.

Finally, sidewalks in proximity to schools provide children with a safe route to school. They are not currently available at all City schools.

#### Golf Cart Facilities

Golf cart use on public rights of way is controlled by state regulations, and provides a convenient, low impact alternative to access convenient commercial development, as well as parks and public services. Golf carts to be used on the public golf cart routes must meet specific physical requirements set forth in the City golf cart plan, must be certified as "road ready" by the City and carry an appropriate permit sticker. Golf cart operators must carry a valid California Driver's license, have proof of insurance, be equipped with seatbelts and appropriate child safety equipment, and be properly maintained. In order to implement a golf cart route system, the City is required to prepare and adopt a Golf Cart Plan and associated implementing ordinances.

Currently there are no existing golf cart facilities in the planning area, outside of private golf development in the Sun Lakes Country Club. Issues related to development of golf cart facilities are similar to those for trail and bikeway development, in that existing facilities that would accommodate development of cart paths do not exist or are discontinuous in some areas.

#### **Public Transportation**

##### Banning Transit Services

In 2004 the Banning Municipal Transit System provides fixed route bus service along three routes, two of which are in Banning and one east to Cabazon. There are five City-owned vehicles, three of which are in revenue service and two of which are in reserve. All are powered by compressed natural gas (CNG), and are equipped with bicycle racks. All are ADA compliant, with wheelchair lifts and tie-down stations.

The transit system also operates a Dial-A-Ride service that provides elderly and disabled persons in Banning with curb-to-curb transit services. In addition, Dial-A-Ride is the ADA complementary para-transit provider for the City's fixed route bus service. The Dial-A-Ride program operates two vehicles in revenue service, and one in reserve, all gasoline powered. All Dial-A-Ride vehicles are ADA compliant, with wheelchair lifts and tie-down stations.

In May 2002, the Banning City Council approved the final Pass Area Transit Plan. The Transit Plan establishes the Pass Transit System, which consists of two independent transit systems, the Banning Municipal Transit System and the Beaumont Municipal Transit System. The Plan provides for a coordinated service area including the cities of Banning and Beaumont, the

unincorporated areas of Cabazon and Cherry Valley, and the commercial area of the Morongo Band of Mission Indians Reservation. The Plan provides fixed route and dial-a-ride services.

The transit-needs study conducted for the Plan identified service related issues associated with the existing transit systems. It also identified areas not addressed in the Transit Plan requiring additional study or resources. These needs will be addressed in future transit plans, and include service for non-traditional work shifts in the region, students, elimination of one-way loops to reduce ride time on local routes, adjusting route schedules to provide timed transfers between routes, relocation of transfer points and development of a transit center.

Regional bus service is provided by the Riverside Transit Agency (RTA), which provides services to Hemet/San Jacinto (Route 31), Moreno Valley (Route 35), and Calimesa/Redlands (Route 36), from the bus stop at Kmart on Highland Springs.

#### Railroad

Rail service facilities through the Pass area, and the City of Banning, can carry approximately 60 trains per day, most of which are freight. Regional freight transfer facilities are located in West Colton. Local facilities carry approximately 60 trains per day, most of which are freight. Trains average approximately 7,000 feet in length and generally travel up to 60 miles per hour (mph) in the planning area. UPRR is expanding rail facilities between Los Angeles and Houston, Texas to facilitate increases in rail traffic. Rail lines in the planning area are designated Centralized Track Control (CTC).

There are currently no passenger services available in Banning, although a passenger rail station did operate in the past.

There are four at-grade railroad crossings in the City. These occur at Hargrave Street, San Gorgonio Avenue, 22<sup>nd</sup> Street, and Sunset Avenue. Grade separated crossings occur at 8th Street and Highland Springs Avenue. The City should explore federal, state, City and UPRR cost sharing arrangements for grade separation of at-grade crossings in conjunction with the California Public Utilities Commission.

Construction of drill spurs is possible to provide rail access to adjoining passenger or industrial uses. Sidings, switches, and additional track linkage would also be required to provide this access. Additional costs would also be associated with any special engineering requirements and potential engineering constraints.

#### Banning Municipal Airport

The Banning Municipal Airport is classified by the National Plan of Integrated Airport Systems (NPIAS) as a General Aviation airport. The airport includes 65 hangars and 32 tie downs, with a 5,200 foot runway.

The airport is capable of accommodating most private single-engine and corporate jet aircraft, as well as helicopters. It averages approximately 10 to 15 takeoffs and landings daily and about 12,000 operations per year. Air traffic is primarily comprised of private, two-engine fixed-wing aircraft. There is no control tower at the airport, so all operations operate under Visual Flight



Rules (VFR). Unicom service is provided from 8 a.m. to 5 p.m., every day. Fueling service is also available.

An approximately \$750,000 improvement asphalt overlay project is planned, and will be funded from Federal Aviation Administration grant monies.

### **Major Utility Corridors**

Major corridors and easements for the transport of natural gas, electricity, communications, domestic water and sewage, and storm drainage are also important components of the Circulation Element. Generally, the need for utility corridors is met through the provision of easements in or adjacent to City streets and along common lot lines.

A major electric corridor occurs immediately south of the Banning Bench. The right of way for the two 220 kV transmission lines in this area is approximately 500 feet in width. Additional 115 kV transmission lines occur in the southern end of the City, and along the San Bernardino Mountains.

Two major high-pressure natural gas pipelines traverse the City, and transport natural gas far beyond the City's boundaries. A 30 inch line occurs under Lincoln Street through the City. Another 30-inch pipeline transects the City in a northwesterly direction from Hargrave and Wilson to Highland Springs Avenue. Two crude oil (16") or petroleum (12") lines occur on the south side of the City, from the airport southwesterly to Wesley, then westerly to the western City limits. Please see Exhibit V-11, Natural Gas and Fuel Lines.

Future land use planning, including the development of subdivisions and the processing of development applications, will require coordination between the City, developers, utility companies, and other service providers to assure the availability and provision of easements and rights of way for the extension of roads, utility lines, and public services.

## **FUTURE DIRECTIONS**

### **Special Issues**

In the development of the 2005 traffic study for the General Plan, six issues were identified which required particular attention. These are individually described below.

#### State Route 243

The City's 1994 Circulation Element included a connection from 8<sup>th</sup> Street to State Route (SR) 243. This connection does not currently (2004) exist, but was proposed as a future roadway. 8<sup>th</sup> Street from Interstate 10 to SR 243 was required to be a Major Highway. The current connection between I-10 and SR 243 is south on 8<sup>th</sup> Street from I-10 to Lincoln, east on Lincoln to San Geronio, and south on San Geronio to SR 243. Current volumes on SR 243 are approximately 4,000 vehicles per day. At buildout, it is expected that SR 243 will carry approximately 8,000 trips per day. The buildout trips, although double the current trips generated by SR 243, will not impact the City's street system, and can be absorbed on the streets currently used to make this connection, as described above. It was therefore determined that 8<sup>th</sup> Street south of I-10 was

required to be a Secondary Highway, not a Major Highway, at buildout of the General Plan, and that the connection to SR 243 was not required for traffic flow.

A direct connection from a State Highway to an Interstate Highway is always preferred by CalTrans. In consultation with CalTrans (California Department of Transportation), the City has considered alternative alignments for SR 243, which are included in the General Plan traffic study. Should CalTrans or the City wish to change the current on-street connection to a direct connection, a potential alignment has been depicted on Exhibit III-6, Proposed General Plan Street System. However, since the impacts to the City's streets from SR 243 traffic is not considered to significantly impact City roadways, realignment should be considered a low priority.

#### Additional Freeway Interchange Capacity

The traffic analysis for this General Plan showed unacceptable levels of service during the peak hour at several I-10 ramp intersections, as discussed above. The study considered the potential of expanding existing interchange capacity, however, right of way is severely constrained, the under-crossings existing at 8<sup>th</sup> Street and Hargrave are undersized and would be extremely costly to widen.

As a result, the City will require an additional freeway interchange at Cottonwood Road (North - South) on the east end of the City. The added interchange at Cottonwood will alleviate congestion at 8<sup>th</sup> Street and Hargrave interchanges.

CalTrans plans to widen I-10 in the future, by adding one lane in each direction. Given the costs associated with this widening, and the cost of the interchanges, a coordinated construction program of widening and interchange additions would likely be most cost effective.

#### Highland Home Road/Cherry Valley Boulevard/Brookside/18<sup>th</sup> Street/Highland Springs

The General Plan roadway system has shown Highland Home connecting to Cherry Valley Boulevard in the City of Beaumont. The extension of Highland Home however, could connect to the west at Brookside Avenue in Beaumont. Further, Highland Springs is planned to extend to the northeast to Bluff Street, to provide access to the Black Bench area, and a second connection from the Banning Bench to the City. This connection could also be made through the extension of 18<sup>th</sup> Street to the northeast.

In all cases, traffic flow will not be significantly affected, insofar as traffic volumes on these streets in this area are not expected to be high. The Recommended General Plan Street System shows these streets in their currently envisioned configuration. However, as development occurs and the feasibility of the extensions is considered, flexibility is included in this General Plan to allow changes to the street system in the future.

#### At Grade Railroad Crossings

The City has two grade separated railroad crossings at streets with I-10 interchanges. The other four existing or planned interchanges must also be improved to include grade separations, in order to maintain acceptable levels of service. A grade separation is included in the

Transportation Uniform Mitigation Fee (TUMF) program for the Sunset Avenue interchange. The others are not in the TUMF program.

The City will need to aggressively pursue grade separations for the railroad tracks at all interchanges. This should include the preparation of feasibility studies, the securing of all available funding, and the cooperation of the development community. Although construction of these facilities may not occur in the near term, the planning must be initiated immediately, in order for the City to be able to implement the construction in the future.

#### Lincoln Street and Westward avenue west of Sunset Avenue

The 1994 Circulation Element included the extension of both Lincoln and Westward from Highland Home to Sunset. Both these roadways occur currently east of Sunset. West of Highland Home, only one roadway, Sun Lakes Boulevard, currently occurs. The traffic study for this General Plan considered the traffic volumes generated south of I-10 on Sunset, and the potential volumes for Lincoln and Westward east of Sunset. The study found that Westward will have sufficiently low volumes so as to require a 2 lane collector east of Sunset. Lincoln is projected as a Major Highway from Highland Home easterly, as is Sun Lakes Boulevard. Traffic volumes will result in LOS C or better for both Lincoln and Westward in this area. Therefore, the elimination of Westward west of Sunset will not have a negative effect on east-west traffic south of I-10.

#### Level of Service Policy

The traffic study for the 2013 General Plan Amendment to revise the city-wide LOS standards, Amendment found that the City will be able to maintain LOS D on City streets. LOS D does not represent a significant degradation in traffic flow. When balancing the need for an efficient traffic system and the widening of streets to accommodate peak hour traffic, it appears that changing the City's requirement from LOS C to LOS D will not result in a significant negative effect.

#### Alternative Transportation

As cited above, existing rights-of-way on City streets are not adequate to allow for development of non-motorized transportation. The City has generally been able to secure right-of-way from new development as it occurs to provide full-width mid-block roadway improvements, but the process can be time consuming and costly.

New development should be required to provide separate paths for bicycles and/or equestrians, pedestrians and golf carts to assure safety and avoid conflicts. Equestrian trails should be included in projects developed south of the railroad right-of-way, as well as a connection to the San Bernardino and San Jacinto mountains.

Bicycle and golf cart parking facilities should be integrated into the design of commercial office and public land uses. Connectivity should also be a primary goal of residential design and should emphasize easy accessibility within and between neighborhood and commercial services to maximize the opportunities for pedestrian, bicycle, equestrian and golf cart access by short and direct trips. This planning focus will also help to shorten vehicle trips for residents who must use their automobiles.



Public transport out of the City is limited, particularly into the Inland Empire communities to the west. As development occurs in the City, and increased pressure is brought to provide service and lower vehicular trips on a regional level, additional public transportation will become necessary.

## **GOALS, POLICIES, AND PROGRAMS**

### **Goal**

A safe and efficient transportation system.

### **Policy 1**

The City's Recommended General Plan Street System shall be strictly implemented.

#### **Program 1.A**

Street rights of way shall be 134 feet for Urban Arterial Highways, 110 feet for Arterial Highways, 100 feet for Major Highways, 88 feet for Secondary Highways, 78 feet for Divided Collectors, 66 feet for Collectors, and 60 feet for Local Streets. Local street standards can be amended as described in Policy 2.

**Responsible Agency:** Community Development Department, Public Works Department, Planning Commission, City Council

**Schedule:** Ongoing

#### **Program 1.B**

The City's Public Works roadway standards shall be amended to match the standards contained in this General Plan.

**Responsible Agency:** Public Works Department

**Schedule:** Ongoing

#### **Program 1.C**

Minimum lane width for all City streets shall be designed at 12 feet.

### **Policy 2**

Local streets shall be scaled to encourage neighborhood interaction, pedestrian safety and reduced speeds.

#### **Program 2.A**

The design of new local streets can vary from the City's standard of 60 foot right-of-way, 40 foot paved width, under the following conditions:

1. The minimum travel lane width shall be 12 feet.
2. Parking shall be provided on at least one side of any public street. Parking lanes shall be a minimum of 8 feet in width.
3. Parking may be eliminated on private streets, if provisions are made in Conditions, Covenants and Restrictions (CC&R's) for enforcement by the Homeowners' Association.
4. Landscaped traffic circles, chokers, and center islands are encouraged, but must meet the requirements of the Fire Department.

5. The minimum parkway width shall be 10 feet.
6. Linear sidewalks are discouraged. Meandering sidewalks, which provide landscaping and street trees adjacent to the curb, shall be included in local street design.

The design of local streets varying from the City's standard, shall be included in the Tentative Tract Map application, and shall be reviewed by the Planning Commission and approved by the City Council.

**Responsible Agency:** Community Development Department, Public Works Department, Planning Commission, City Council

**Schedule:** Ongoing

#### **Program 2.B**

Existing local streets will be inventoried, and a master plan of potential improvements designed to improve their aesthetic and safety, including landscaped medians, sidewalks and traffic calming devices, shall be developed, cost engineered, and implemented.

**Responsible Agencies:** Public Works Department, Planning Commission, City Council

**Schedule:** Ongoing

#### **Policy 3**

The City shall establish and maintain a 5-Year Capital Improvement Program for streets.

#### **Program 3.A**

The Public Works Department shall establish a Capital Improvement Program for 5 years, and update it annually.

**Responsible Agency:** Public Works Department

**Schedule:** Ongoing

#### **Policy 4**

Proactively participate in regional transportation planning.

#### **Program 4.A**

Maintain active relationships with the City of Beaumont, the County of Riverside, the Western Riverside County Council of Governments, the California Department of Transportation and the Morongo Band of Mission Indians to share information and promote comprehensive transportation planning in the region.

**Responsible Agency:** Public Works Department, City Manager's Office, City Council, City of Beaumont, County of Riverside, WRCOG, CalTrans, Tribe

**Schedule:** Ongoing

#### **Program 4.B**

Aggressively pursue Banning projects in the Transportation Uniform Mitigation Fee (TUMF) program, particularly the addition of projects to the TUMF project list, including grade separated road crossings.

**Responsible Agency:** Public Works Department

**Schedule:** Ongoing

**Program 4.C**

Aggressively pursue the design and development of interchange at Cottonwood Road (North - South), including all sources of funding, and the coordination of I-10 widening with the installation.

**Responsible Agency:** Public Works Department, City Manager's Office, City Council, CalTrans, Railroad

**Schedule:** Ongoing

**Policy 5**

Consider amendments to the Highland Home/Highland Springs/18<sup>th</sup> Street/Brookside street configurations based on public safety, design feasibility and area needs.

**Policy 6**

The City shall maintain peak hour Level of Service D or better on all local roadways and intersections.

**Program 6.A**

Periodically review current traffic volumes and the actual pattern of development to coordinate, program and, as necessary, revise road improvements.

**Policy 7**

New development proposals shall pay their fair share for the improvement of street within and surrounding their projects on which they have an impact, including roadways, bridges, grade separations and traffic signals.

**Policy 8**

Traffic calming devices shall be integrated into all City streets to the greatest extent possible and all new streets shall be designed to achieve desired speeds.

**Policy 9**

Street trees within the City right of way shall be preserved, unless a danger to the public health and safety or if the tree is diseased.

**Program 9.A**

Sidewalks in areas with street trees shall be designed to "wrap around" the tree if they are added to an existing neighborhood.

**Responsible Agency:** Public Works Department

**Schedule:** Ongoing

**Policy 10**

Sidewalks shall be provided on all roadways 66 feet wide or wider. In Rural Residential land use designation pathways shall be provided.



**Program 10.A**

The Public Works Department shall prepare an inventory of discontinuous sidewalks on all qualifying roadways, and fund individual projects through the Capital Improvement Program annually.

**Responsible Agency:** Public Works Department, City Council

**Schedule:** Ongoing

**Program 10.B**

All new development proposals located adjacent to qualifying roadways shall be required to install curb, gutter and sidewalk concurrent with construction.

**Responsible Agency:** Public Works Department, Planning Department

**Schedule:** Ongoing

**Program 10.C**

The City shall develop procedures to address neighborhood sidewalk needs as they are requested by that neighborhood.

**Responsible Agency:** Public Works Department

**Schedule:** Ongoing

**Program 10.D**

Work with the School District to develop safe routes to school.

**Responsible Agency:** Public Works Department

**Schedule:** Ongoing

**Policy 11**

Sidewalks or other pedestrian walkways shall be required on all streets within all new subdivisions.

**Policy 12**

In the absence of a vehicular grade separation, the City shall aggressively pursue a grade separated pedestrian access across San Geronio, to assure that high school students do not have to cross the railroad tracks on their way to and from school.

**Policy 13**

Pedestrian access in the Downtown Commercial designation shall be preserved and enhanced.

**Program 13.A**

All development and redevelopment proposals for the Downtown area shall include enhanced sidewalk, pedestrian walkway, lighting and landscaping designs and assure connections to existing and planned sidewalks.

**Responsible Agency:** Public Works Department, Planning Department

**Schedule:** As development proposals are presented

**Policy 14**

The City shall aggressively pursue the construction of all-weather crossings over General Plan roadways.

**Program 14.A**

The Public Works Department shall prioritize the need for bridges listed in this Element, develop preliminary cost estimates, identify and pursue sources of funding, including developer funding, for each facility.

**Responsible Agency:** Public Works Department, City Council

**Schedule:** Ongoing

**Program 14.B**

All new development proposals shall pay their fair share of bridge construction needed to serve their project.

**Responsible Agency:** Public Works Department, Planning Department

**Schedule:** Ongoing

**Policy 15**

The City shall develop a Golf Cart Plan compliant with state requirements.

**Program 15.A**

The City shall develop a golf cart plan and associated ordinances and other required implementation programs.

**Responsible Agency:** Public Works Department, City Council

**Schedule:** As budget allows

**Policy 16**

Golf cart paths and facilities shall be funded, to the greatest extent possible, by new development.

**Program 16.A**

The routing and facilities required in the Golf Cart Plan shall be incorporated into the Development Impact Fee when the Plan is adopted.

**Responsible Agency:** Public Works Department

**Schedule:** As opportunity arise

**Program 16.B**

Golf cart facilities shall be incorporated into new project plans located on golf cart routes.

**Responsible Agency:** Planning Department, Public Works Department, Planning Commission, City Council

**Schedule:** Ongoing

**Policy 17**

Encourage the expansion of an integrated Pass transit system.

**Program 17.A**

The City will explore the potential for either bus or rail connection to the Metrolink transit system.

**Responsible Agency:** City Manager's Office, Community Services Department

**Schedule:** Ongoing

**Policy 18**

The City shall review its transit service to major regional attractions, and intra-City recreational locations in future planning efforts, based on need.

**Policy 19**

Bus pullouts shall be designed into all new projects on arterial roadways, to allow buses to leave the flow of traffic and reduce congestion.

**Program 19.A**

Bus pullouts will be retrofitted on built-out streets, wherever possible.

**Responsible Agency:** Public Works Department, City Council

**Schedule:** Ongoing

**Policy 20**

Promote the location of a passenger rail station for long distance and commuter rail service.

**Policy 21**

Update the Airport Master Plan every five years to meet the needs of the general aviation, business and tourism segments of the community.

**Program 21.A**

Land use designation decisions within the area of influence of the airport shall be specifically reviewed to assure compatibility.

**Responsible Agency:** Planning Commission, City Council

**Schedule:** Ongoing

**Program 21.B**

Work with the Chamber of Commerce, the Morongo Band of Mission Indians, and other interested parties to provide services which meet the needs of passenger and freight transport.

**Responsible Agency:** Airport Management, Economic Development staff, Chamber of Commerce, Morongo Band of Mission Indians, City Council

**Schedule:** Ongoing

**Policy 22**

Maintain an accurate mapping of all utility corridors.

**Program 22.A**

The Building Department shall inventory and map transmission utility easements on the Land Use Map (including electric, fiber optics, natural gas and petroleum).

**Responsible Agency:** Building Department, Planning Department

**Schedule:** As budget allows

**Policy 23**

The City shall purchase and/or replace its fleet of vehicles with alternate fuel vehicles when available to the greatest extent possible, and shall encourage other agencies to do the same.

**Policy 24**

Public alleys throughout the City shall be maintained to be useful and safe at all times.

**Program 24.A**

The City shall create a downtown alley master plan and where appropriate pave, light and otherwise improve alleys.

**Responsible Agency:** Public Works Department

**Schedule:** Ongoing

**Program 24.B**

The Public Works Department shall inventory all public alleys, determine which are necessary, and vacate those that are not.

**Responsible Agency:** Public Works Department, City Council

**Schedule:** As budget allows

**Policy 25**

The City shall develop and implement plans for a coordinated and connected bicycle lane network in the community that allows for safe use of bicycles on City streets.

**Program 25.A**

The City shall inventory all streets for potential Class I, Class II and Class III bikeways, and shall program their installation in its Capital Improvement Program.

**Responsible Agency:** Planning Department; Engineering Division; Public Works Department; Planning Commission; City Council

**Schedule:** As budget allows

**Program 25.B**

Class I bikeways and sidewalks should be installed on both sides of Wilson Street, Ramsey Street, and Lincoln Street, and other major streets where sufficient right-of-way is available.

**Responsible Agency:** Engineering Division; Public Works Department

**Schedule:** Ongoing

**Program 25.C**

Class II bikeways and sidewalks should be designated on all existing arterial streets that have sufficient width to safely accommodate bicycle travel lanes.

**Responsible Agency:** Planning Department; Engineering Division; Public Works Department

**Schedule:** As budget allows

**Program 25.D**

The City should designate Class III bikeways only where Class I and Class II facilities are not feasible.

**Responsible Agency:** Planning Department; Public Works Department

**Schedule:** As budget allows



**Policy 26**

The City should continue to work with the Morongo Band of Mission Indians and neighboring cities and communities to create a regional bicycle and trail network.

**Policy 27**

The City shall provide for a comprehensive, interconnected recreational trails system suitable for bicycles, equestrians and/or pedestrians.

**Program 27.A**

Evaluate the practicality of utilizing flood control channels for multi-use trails, where flooding and safety issues can be accommodated, and negotiate inter-agency agreements for this purpose.

**Responsible Agency:** Planning Department

**Schedule:** As opportunity arise

**Program 27.B**

Evaluate the practicality of developing a multi-use trails system along the Banning Bench adjacent to and extending into San Bernardino National Forest lands, where environmental and safety issues can be accommodated, and negotiate inter-agency agreements with the U.S. Forest Service for this purpose.

**Responsible Agency:** Planning Department, U.S. Forest Service/San Bernardino National Forest

**Schedule:** As opportunity arise

**Program 27.C**

Establish a multi-purpose trail between Dysart Park and Smith Creek Park, suitable for equestrian, bicycle and pedestrian use.

**Responsible Agency:** Community Services Department; Public Works Department; Parks and Recreation Advisory Committee

**Schedule:** Ongoing as development occurs

**Policy 28**

Motorized vehicles shall be prohibited on City trails.

**Program 28.A**

The City shall develop a non-motorized trail system and associated ordinances and other required implementation programs.

**Responsible Agency:** Public Works Department, Planning Commission, City Council

**Schedule:** As budget allows

**Program 28.B**

The non-motorized trail system shall be funded, to the greatest extent possible, by new development.

**Responsible Agency:** Public Works Department

**Schedule:** As development opportunity arises

**Program 28.C**

The routing and facilities required in the non-motorized trail system Plan shall be incorporated into the Development Impact Fee when the Plan is adopted.

**Responsible Agency:** Public Works Department

**Schedule:** As opportunity arises

**Exhibit "B"**  
**Existing General Plan Circulation Element**

*Changes marked.*





## CIRCULATION ELEMENT

### PURPOSE

The purpose of the Circulation Element is to provide goals, policies, programs and standards that correlate the City's transportation system with the types, intensities and locations of land uses within the City. It addresses those segments of the local transportation system that interface with and serve as extensions of the regional system connecting the City of Banning with the broader Pass Region and other communities in Southern California. The Circulation Element also serves as the blueprint for future land use policy decisions and social and economic development efforts.

### BACKGROUND

Due to its close interrelatedness, the Circulation Element is an outgrowth of City and regional land use planning. In addition to its effects on the physical, social and economic environment of the City, the Circulation Element also has a direct relationship with the Housing, Open Space, Noise and Air Quality elements. Being integrally tied to the Land Use Element, the Circulation Element is predictably influenced by the types, intensities and distribution of land uses within the community and surrounding area.

Local and regional air quality issues are closely related to the efficiency of the local and regional transportation system. As the City of Banning and the Pass Region continue to grow, vehicle miles will increase, and travel speeds will be reduced, resulting in higher emissions per mile traveled. The policies and programs established by the Circulation Element can play an important role in maintaining and enhancing the flow of traffic and preserving air quality in the community:

~~The Circulation Element has been developed to serve as a comprehensive transportation management strategy, incorporating analysis of existing conditions within the City, as well as projected future development based on the buildout of the General Plan Land Use Map (see Land Use Element). It sets forth specific goals, policies and programs, which are based upon an engineering and computer modeling analysis of existing and projected future traffic conditions. Future traffic conditions have been forecasted utilizing the Pass Area Model (PAM), anticipated buildout land use patterns and intensities, projected regional growth expected to impact City streets and roadways, and a wide range of socioeconomic data and assumptions.~~

~~In addition to vehicular traffic~~ <sup>for</sup> ~~and other modes of transportation important to the City~~ <sup>and</sup> ~~are included in the Circulation Element:~~ <sup>such as</sup> air traffic and the Banning Municipal Airport; rail traffic; public transportation; and alternative transportation.

California Government Code sets forth the information and data analysis requirements of the Circulation Element. Government Code Section 65302(b) requires that the element describe

major thoroughfares and that their planned development be closely coordinated with the Land Use Element of the General Plan.

It is also required that the Circulation Element include development or improvement standards that are responsive to changes in demand for capacity created by implementation of the Plan. Government Code Sections 65103 (f) and 65080, et seq., require that the City coordinate Circulation Element provisions with applicable regional and state transportation plans. In the Banning General Plan Planning Area, the following agencies are responsible for preparing these transportation plans: County of Riverside, Morongo Band of Mission Indians, Southern California Association of Governments (SCAG) and California Department of Transportation (CalTrans). The state is also required to coordinate its planning efforts with those of local jurisdictions (§65080(a)), with the federal government being under a similar mandate (§134, Title 23 of the U.S. Code).

#### Indicators Of Roadway Efficiency

*Moved to Page 74f.*

The efficient movement of vehicular and non-vehicular traffic on local and regional roadways is critical to the normal day-to-day functioning of a community. Consequences resulting from obstructions in traffic flow may include economic loss due to delays in transporting goods, increased psychological stress for the traveling public, and increased risk for motor vehicle accidents. The efficiency of a particular roadway can be determined by assessing the roadway's capacity, level-of-service, and average daily traffic volume, each of which is described below.

#### Level-of-Service

Roadway capacity is defined as the number of vehicles that may pass over a section of roadway in a given time period under prevailing conditions. Roadway capacity is most restricted by intersection design and operation. The capacity of a roadway and the degree to which that capacity is being utilized is typically described as the roadway's "Level-of-Service" (LOS). Level-of-Service is a qualitative measure of the efficiency of traffic flow and is defined by alphabetical connotations, ranging from "A" through "F," that characterize roadway operating conditions.

LOS A represents an optimum or free-flowing condition, and LOS F indicates extremely slow speeds and system failure. Levels-of-Service are represented as volume-to-capacity (V/C) ratios, or vehicle demand divided by roadway capacity. V/C ratios smaller than 1.00 imply better operational characteristics and levels-of-service. V/C ratios that exceed 1.00 imply worse operating conditions and LOS F, where traffic demand exceeds roadway capacity. The table below defines the various LOS classifications.

Table III-14

Roadway Level Of Service Description	
Level of Service	Quality of Traffic Flow
A	Primarily free-flow operations at average travel speeds usually about 90 percent of the free-flow speed for the arterial classification. Vehicles are completely unimpeded in their ability to maneuver within the traffic stream. Stopped delay at signalised intersections is minimal.
B	Reasonably unimpeded operations at average travel speeds usually about 70% of the free-flow speed of the arterial classification. Ability to maneuver within the traffic stream is only slightly restricted. Stopped delays are not bothersome, and drivers generally are not subject to appreciable tension.
C	Traffic operations are stable. However, mid-block maneuverability may be more restricted than in LOS B. Longer queues, adverse signal coordination, or both may contribute to lower average travel speeds of about 50% of the average free-flow speed for the arterial classification. Motorists will experience some appreciable tension while driving.
D	Borders on a range where small increases in flow may cause substantial increases in approach delay and decreases in arterial speed. LOS D may be due to adverse signal progression, inappropriate signal timing, high volumes, or some combination of these factors. Average travel speeds are about 40% of the free-flow speed. For planning purposes, this level-of-service is the lowest that is considered acceptable.
E	Characterized by significant approach delays and average travel speeds of one-third or less of the free-flow speed. Typically caused by some combination of adverse progression, high signal density (more than two signalised intersections per mile), high volumes, extensive queuing, delays at critical intersections, and/or inappropriate signal timing.
F	Arterial flow at extremely slow speeds, below one-third to one-fourth of the free-flow speed. Intersection congestion is likely at critical signalised intersections, with high approach delays and extensive queuing. Adverse progression is frequently a contributor to this condition.

Source: p. 11-4, Highway Capacity Manual, Special Report 209, Transportation Research Board, 1994.

and intersections

Traffic engineers and transportation planners are involved in on-going efforts to strike a balance between providing ideal roadway operating conditions and controlling the costs of infrastructure and right-of-way needed to assure those conditions. For General Plan purposes, LOS ~~D~~ is assumed to be the "acceptable" level-of-service for all General Plan roadways within the City, and LOS ~~D~~ at Freeway interchanges. ~~CalTrans' policy for State Highway 243 and Interstate 10 is that LOS D will be maintained.~~

~~Typically, capacity can be increased by adding travel or turning lanes, constructing raised medians, alternative means of transportation, and/or restricting vehicle access to a roadway. By reducing the number of vehicle conflict points, traffic flow on a roadway can be substantially improved, avoiding the loss of capacity caused by disruptions to traffic flow resulting from vehicles entering or leaving the roadway (see Section III-B-3, Draft General Plan EIR for more information about mitigating traffic impacts).~~

#### Average Daily Traffic Volumes

Average Daily Trips (ADT) is the total number of vehicles that travel a defined segment of roadway over a twenty-four hour period. ADT is a useful benchmark number for determining various roadway configurations and design aspects. The peak hour ADT, which is the highest volume of traffic to pass over a segment of roadway during an hour period, is also a useful means of determining a roadway's capacity and level-of-service. Traffic counts at intersections can provide an even more detailed picture of existing and future operating conditions at intersections.

Roadways are generally classified in a hierarchical manner, according to the number of vehicle lanes provided. Table III-15, below, lists the various roadway types/cross-sections found in the planning area and the maximum daily traffic volumes each type of roadway can accommodate at various levels-of-service. For example, for a Major Highway to operate at LOS ~~D~~, it should accommodate no more than ~~30,400~~ vehicle trips per day, with a design capacity of 24,000 vehicle trips per day. These roadway capacities are "rule-of-thumb" estimates, which may vary depending upon site-specific factors, such as the number and configurations of intersections, roadway grades, sight distance, percentage of truck and bus traffic, and degree of access control.

34,200

27,000



**Table III-15  
Level-of-Service Volumes/Capacity Values  
For Various Roadway Classifications**

Classification	Roadway Width (Ft.)	Number of Lanes	Maximum Two-Way Average Daily Traffic Volume Level of Service		
			C	D	B
Collector	60	2	12,800	14,400	16,000
Secondary Highway	80	4	24,000	27,000	30,000
Major Highway	100	4	30,400	34,200	38,000
Major Highway	110	4	30,400	34,200	38,000
Major Highway	134	6	47,200	53,100	59,000

<sup>o</sup> Source: City of Banning General Plan Update Traffic Study 2004

~~CURRENT CONDITIONS~~ **GENERAL PLAN CIRCULATION ELEMENT STREET CLASSIFICATION AND CROSS SECTIONS**

The City of Banning and other communities in the Pass Region form a continuous and interconnected suburban development pattern, tied together by U.S. Interstate-10 and a network of arterial roadways. A variety of physical influences and constraints, including the geography of the City and the region, and the existing roadway network, affect traffic flows and the impacts of development on local roadways. ~~The existing roadway network is further described in the General Plan EIR and its traffic study.~~

~~Major Regional Roadways~~

~~Two major regional roadways provide primary access to the City: State Highway 243 and U.S. Interstate-10. U.S. Interstate-10 connects the Los Angeles region with Arizona and other cities and states to the east. State Highway 243 connects Banning and U.S. I-10 to the mountain community of Idyllwild, State Route 79, and the San Bernardino National Forest.~~

~~U.S. Interstate-10~~

~~U.S. Interstate 10 is the major transportation route through the City of Banning. It is a critical part of the local road network, moving people and goods into and through the Pass region. Where it passes through the City, it is an eight lane divided freeway. U.S. I-10 disrupts the internal circulation of the City as it bisects the City into distinct north and south communities. Currently, there are six access points to the City from I-10, via interchanges at the following streets (from east to west):~~

→ ~~The City has facilitated~~

The roadway system in Banning is defined using a classification system that describes a hierarchy of roadway types. The categories of roadways included in this classification system

City of Banning  
General Plan

- ~~• Fields Road~~
- ~~• Ramsey Street~~
- ~~• Hargrave Street~~
- ~~• 8<sup>th</sup> Street~~
- ~~• 22<sup>nd</sup> Street~~
- ~~• Sunset Avenue~~
- ~~• Highland Springs Avenue~~

differentiate the size, function, and capacity of each type of roadway. The General Plan Circulation Element street classifications are listed below and the street cross-sections are shown in Exhibit III-4.

~~The eastern portion of the Highland Springs Avenue interchange is in the City limits; the western portion is in the City of Beaumont.~~

~~Although CalTrans is responsible for development and maintenance of this facility, I-10 has been designated as a component of the Riverside County Congestion Management Plan (CMP) System.~~

~~State Highway 243~~

~~The designated State Highway begins on 8th Street south of I-10 and runs south to Lincoln Street. It continues east on Lincoln Street to San Geronimo Avenue to the City limits, where it becomes the Banning Idyllwild Panoramic Highway. State Highway 243 is designated a State Scenic Highway from the Banning City limits to State Route 74, 28.2 miles south, near the community of Idyllwild.~~

~~Major Local Roadways~~

~~The City has facilitated the construction and maintenance of a variety of major roadways of local importance. These roadways have been built along a north-south grid that interconnects with major arterials, some of which also pass through adjacent jurisdictions, primarily to the west.~~

~~A variety of traffic data was collected to evaluate existing traffic conditions in the planning area, including traffic counts and estimations from studies conducted for the cities of Banning and Beaumont, CalTrans, and special site-specific traffic studies conducted for development projects in the planning area.~~

~~As defined in the City's existing (1994) Circulation Element, the street system is projected to include the following roadways. It is important to note that the 1994 Circulation Element did not use the street classification system used in much of Riverside County today.~~

Major Highways

The General Plan defines Major Highways, or arterial streets, as those primarily for through traffic with limited access. These roadways are planned for 4 to 6 lanes in width at buildout. Arterials should connect residential, shopping, employment and recreational activities, but should not encroach upon neighborhoods. Roadways designated as arterial streets in the adopted General Plan include:

- Highland Springs Avenue - North and South of U.S. Interstate 10
- Highland Home Road - Cherry Valley Boulevard to southerly City limits
- Sunset Avenue - Gilman Avenue to Interstate 10

*Replaced by Chart*

*Major Arterial (encl. V to Wilson)*

*Urb Art*

*It's 5000 - Cherry V to Botero BI*

*Wilson to Lincoln*

*Son Lakes BI*

- 8<sup>th</sup> Street – Ramsey Street to State Route 243 *Lincoln*
- San Gorgonio – Lincoln to State Route 243 *→ south city limit*
- Hathaway Street – Ramsey Street to Morongo Road
- ? → ◦ 18<sup>th</sup> Street – Highland Springs Avenue to Highland Home Road *Hargrave-Ramsey to Lincoln*
- Wilson Street – Highland Springs Avenue to Hathaway *Cottonwood N-S*
- Ramsey Street – Highland Springs Avenue to Hathaway *Mulki*
- Sun lakes Boulevard – Highland Springs Avenue to Highland Home Road
- Lincoln Street – Highland Home Road to Hathaway *→ Cottonwood*

Secondary Highways

Secondary Highways are those that primarily receive traffic from arterials and distribute the movement within residential, commercial and industrial land use designations. These streets are planned for 4 lanes. The 1994 General Plan designates the following roadways as Secondary Highways:

- Sunset Avenue – Interstate 10 to Porter
- 8<sup>th</sup> Street - Wilson Street to Ramsey Street ✓ *+ Lincoln to Porter*
- 4<sup>th</sup> Street – Wilson Street to Ramsey Street ✓
- San Gorgonio Avenue - Wilson Street to Lincoln Street ✓
- Hargrave Street – Wilson Street to Porter Road ✓
- Cottonwood Road (North - South) – North of Ramsey to Porter Road
- Fields Road Mulki, end to end
- Porter Road – Sunset Avenue to Cottonwood Road (North - South) ✓

*Hathaway - Ramsey to Porter*  
*Sunset - Wilson to Mesa*  
*Lincoln to Porter*  
*22<sup>nd</sup> - Ramsey to S. of Lincoln*

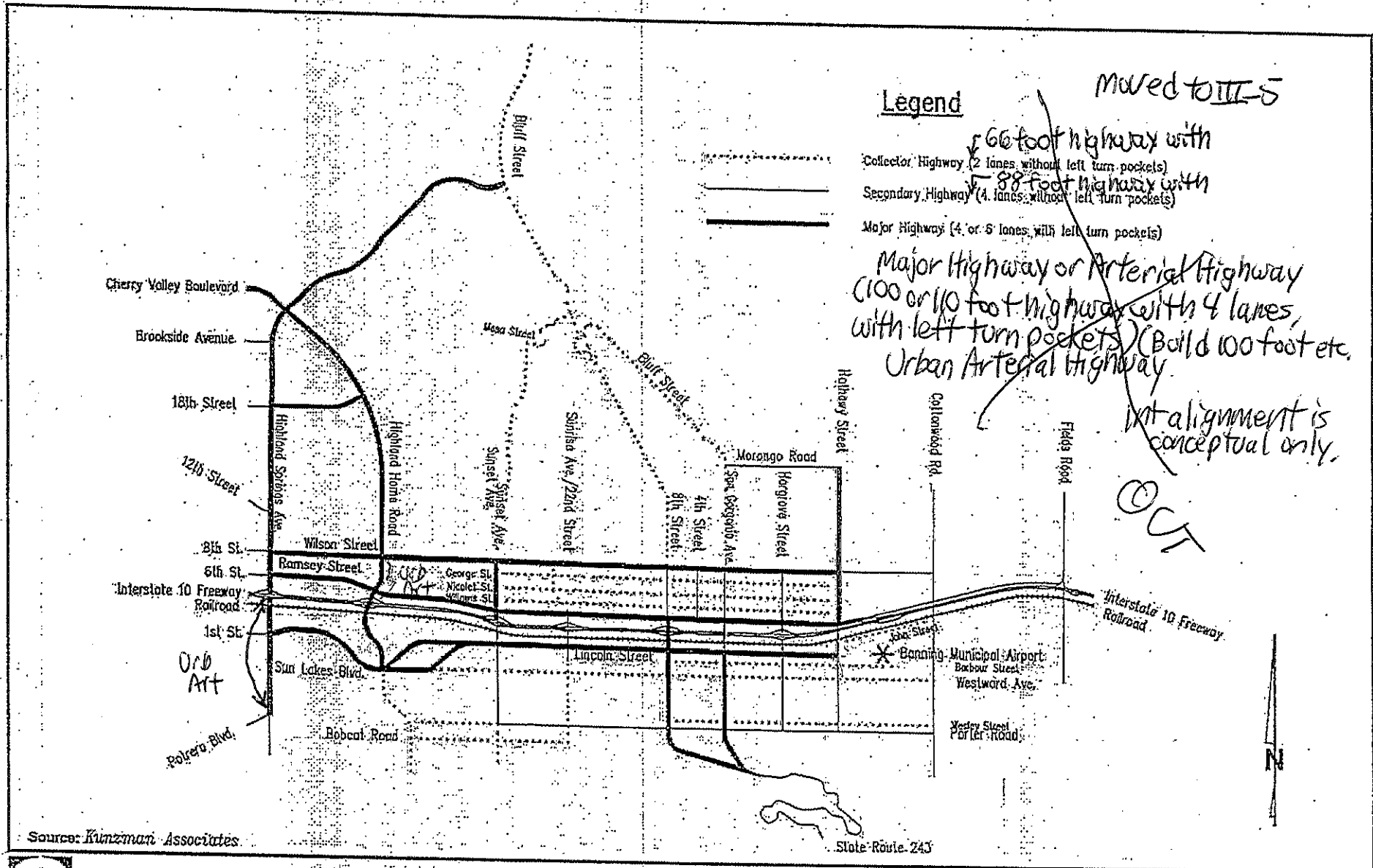
*Ramsey to N  
Porter to S.*

Collector Streets

Finally, the General Plan identifies Collector streets. Collectors are planned as 2 lane roads. The 1994 General Plan Collector streets are:

- Highland Home Road – Westward Avenue to southern City limits
- Sunset Avenue – Wilson Street to Bluff Street
- Bluff Street – Northern City limits to San Gorgonio Avenue
- 22<sup>nd</sup> Street – Lincoln Street to Bobcat Road
- 8<sup>th</sup> Street – Bluff Street to Wilson Street
- San Gorgonio Avenue – Morongo Road to Wilson Street
- George Street – Sunset Avenue to Hathaway Street
- Nicolet Street - Sunset Avenue to Hathaway Street
- Williams Street - Sunset Avenue to Hathaway Street
- Westward Avenue - Sunset Avenue to Cottonwood Road (North - South)
- Barbour Street – 8<sup>th</sup> Street to Airport
- Wesley Street – 8<sup>th</sup> Street to Cottonwood Road (North - South)
- Porter Road – Highland Home Road to Sunset Avenue
- Bobcat Road – Highland Home Road to 22<sup>nd</sup> Street

The existing General Plan arterials are shown on Exhibit III-4, Existing General Plan Street System.



**Legend**

- Collector Highway (2 lanes without left turn pockets)
- Secondary Highway (4 lanes without left turn pockets)
- Major Highway (4 or 6 lanes with left turn pockets)
- Major Highway or Arterial Highway (100 or 110 foot highway with 4 lanes, with left turn pockets) (Build 100 foot etc. Urban Arterial Highway)

Int alignment is conceptual only.  
 OCT

Source: Krinzman Associates



**Banning General Plan  
 Existing General Plan Street System**

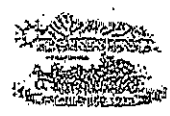


Exhibit  
 III-4

III-5



## TRAFFIC AND CIRCULATION PLANNING FOR GENERAL PLAN BUILDOUT

In integral part of the General Plan is the City's roadway system. In order to assure that traffic is efficiently transported through the City in the future, a traffic study and associated model were prepared to analyze the impacts of the land use map on the City's street system. This traffic model identified deficiencies in the City's network of roadways, and led the City's traffic consultant to a series of recommendations for future improvements to assure the smooth flow of traffic. The traffic analysis summarized below is described fully in the General Plan EIR, and included in its entirety as an appendix to that document.

The traffic model utilized for this General Plan was a TRANPLAN model, which is utilized throughout the County for transportation planning. Since Banning is part of the Pass region, it was important to standardize the traffic analysis to conform to regional standards.

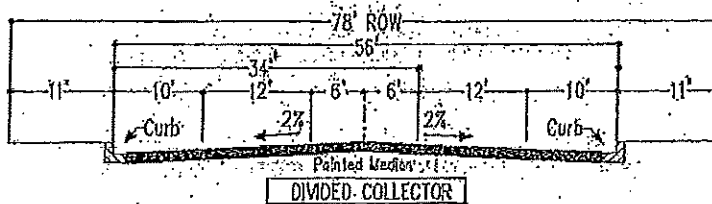
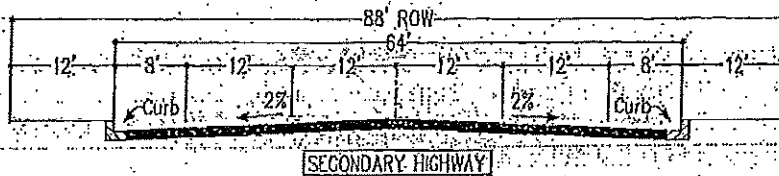
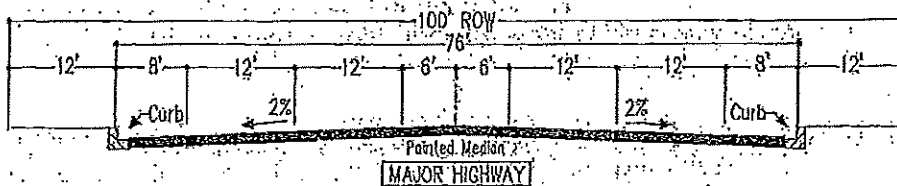
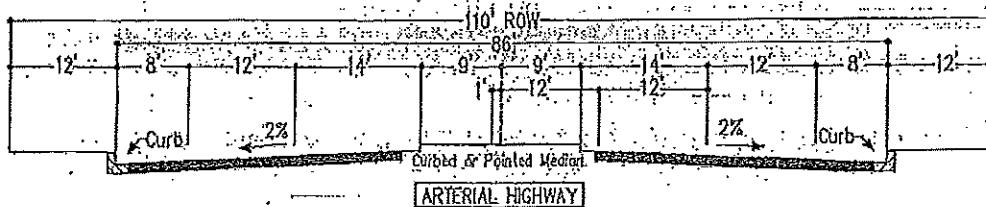
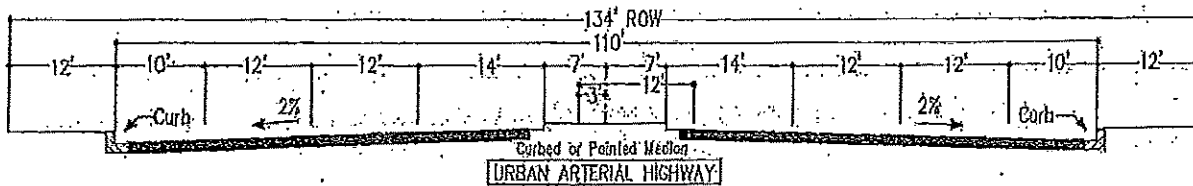
The traffic model divided the City into Traffic Analysis Zones (TAZs), which contained data on the land use in that zone, and the traffic which that land use would generate. It is important to note that some land uses are producers of traffic, while others are attractors for traffic. For example, residential land uses *produce* traffic, while retail commercial land uses *attract* traffic. Once the traffic model TAZs were assigned and their land use trip generation calculated, the trips were distributed on the roadway system.

The traffic modeling considered two issues: street segment impacts, and intersection impacts. Street segments were analyzed for average daily trips (ADT), while intersection impacts were analyzed for morning and evening peak hours (the hour in the morning or evening when the highest concentration of trips occurs, primarily controlled by commuter trips). A total of 23 intersections were analyzed.

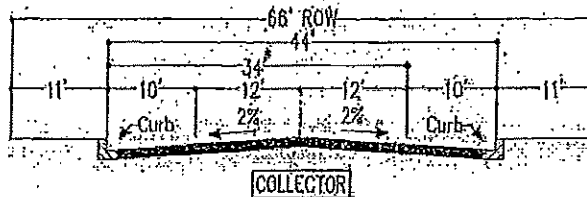
This led to conclusions on the needed size of streets, the areas where the street system would not function properly, and the types of improvements that would be necessary to make the street system work properly.

### Street Cross Sections

As previously stated, the 1994 Circulation Element street classification system is not consistent with the street classification system used in Riverside County. Because of the interconnectedness of roadways, and the City's location as one of a chain of cities in the County, the street cross sections described in this Circulation Element have been made consistent with County standards (see Exhibit III-5). In this way, the City can better integrate into regional transportation programs and plans, including the Transportation Uniform Mitigation Fee program and state and federal programs in the future. The new street classifications result in the following standards:



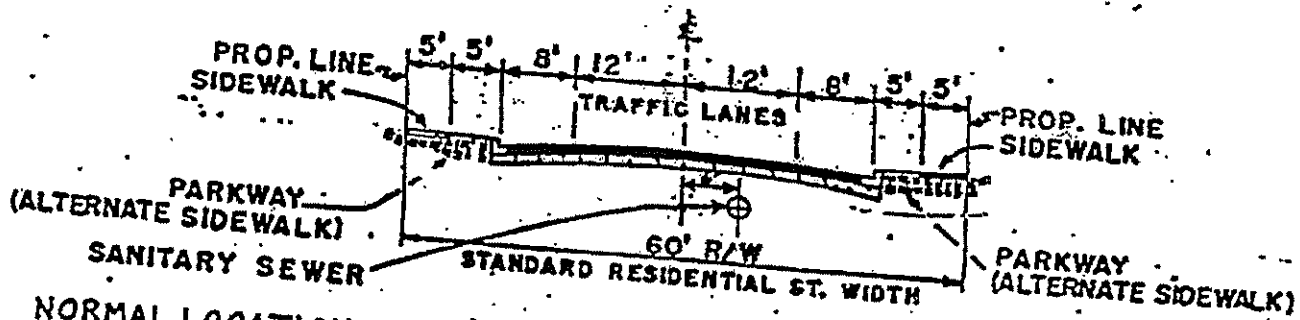
\*Part-width street section for an interior commercial or industrial street.



\*Part-width street section for all collector streets—34' improvements on 48' R/W.

Source: *Kunzman Associates*

Revised 1	Drawn 1	Date 1
10-12-87	Approved by <i>[Signature]</i>	12-17-87
<i>Sperry &amp; Sperry</i> Civil Engineers 12-20-85		
<b>City of Banning</b> TYPICAL STREET SECTION LOCAL STREET		
ST-103	STD. NO.	



NORMAL LOCATION OF UNDERGROUND UTILITIES STD. NO. W 730

COMBINED THICKNESS OF BASE AND SURFACE TO BE DETERMINED BY SOIL TEST.

MINIMUM PAVING THICKNESS TO BE 3" ASPHALT CONCRETE OVER 4" CLASS I BASE

TRANSVERSE SLOPE:

- MINIMUM - 1 1/2 %
- PREFERRED - 2 %
- MAXIMUM - 3 %

CROWN OF STREET TO BE AT CENTER LINE.





Collector: 66 feet of right of way, with a 44-foot street section from curb to curb. This represents two lanes of traffic, with parking lanes on each side, and a parkway on each side.

Divided Collector: 78 feet of right of way, with a 66-foot street section from curb to curb. This represents two travel lanes, with parking lanes on each side, a center median, and a parkway on each side.

Secondary Highway: 88 feet of right of way, with a 64-foot street section from curb to curb. This represents four travel lanes, with parking lanes on each side, and a parkway on each side.

Major Highway: 100 feet of right of way, with a 76-foot street section from curb to curb. This represents four travel lanes, with parking lanes on each side, a center median, and a parkway on each side.

Arterial Highway: 110 feet of right of way, with an 86-foot street section from curb to curb. This represents four travel lanes, with parking lanes on each side, a center median, and a parkway on each side.

Urban Arterial Highway: 134 feet of right of way, with a 110-foot street section from curb to curb. This represents six travel lanes, with parking lanes on each side, a center median, and a parkway on each side.

Local Streets (those streets not shown on the General Plan Recommended Street System): 60 feet of right of way, with a 40-foot street section from curb to curb. This represents two travel lanes, with parking lanes on each side, and a parkway on each side. Local street standards may vary as described in the Policies and Programs of this Element.

*The roadway classifications are also depicted on exhibit III-5,  
General Plan Circulation Element street system.*

**Future Traffic Volumes**

The traffic model was used to estimate the number of trips which would be generated on the City's principal roadways. These estimates, and the current (2004) trips on these roadways are shown in Table III-16, below.

**Table III-16  
Existing Conditions Summary  
for Major Roadways in the Planning Area**

<b>Roadway Link</b>	<b>Existing ADT</b>	<b>General Plan Buildout (ADT)</b>
<b>8<sup>th</sup> Street</b>		
N of Wilson St.	7,494	7,494
S of Wilson St.	10,513	10,513
N of Ramsey	16,320	16,320
S of Ramsey	23,321	34,566
N of I-10 WB Ramps	21,232	34,566
S of I-10 WB Ramps	16,561	28,612
N of I-10 EB Ramps	17,261	28,612
S of I-10 EB Ramps	12,097	24,740
N of Lincoln	12,016	23,042
S of Lincoln	4,315	8,452
<b>Hargrave St.</b>		
N of Ramsey	10,823	19,930
S of Ramsey	15,046	43,562
N of I-10 WB Ramps	14,564	43,562
S of I-10 WB Ramps	10,433	34,038
N of I-10 EB Ramps	10,214	34,038
S of I-10 EB Ramps	5,325	39,082
N of Lincoln	5,325	36,487
S of Lincoln	3,214	36,487
<b>Lincoln St.</b>		
W of Hargrave	2,797	25,914
E of Hargrave	2,513	25,478
E. of 22 <sup>nd</sup> St.	2,300	
W. of 22 <sup>nd</sup> St.	1,700	
W of Sunset	--	25,290
E of Sunset	3,018	29,416
W of 8 <sup>th</sup>	3,730	30,458
E of 8 <sup>th</sup>	5,516	35,531
<b>Ramsey St.</b>		
W of Hargrave	9,009	9,009
E of Hargrave	9,423	22,418
W of 8 <sup>th</sup>	23,011	23,011
E of 8 <sup>th</sup>	22,460	26,294
W of Sunset Avenue	16,378	25,650
E of Sunset Avenue	16,435	24,430
W of Highland Home	12,544	32,083
E of Highland Home	12,303	25,238

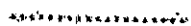




W of Highland Springs Ave.	22,082	23,862
E of Highland Springs Ave.	19,201	28,398
<b>Wilson St.</b>		
W. of Sunset Ave.	12,303	28,211
E. of Sunset Ave.	10,915	22,350
E of 8 <sup>th</sup>	8,631	20,298
W. of Highland Springs Ave.	6,427	12,489
E. of Highland Springs Ave.	12,544	25,318
<b>Sunset Ave.</b>		
N of Wilson	677	22,452
S of Wilson	7,345	19,637
N of Ramsey	14,782	29,603
S of Ramsey	16,171	34,204
N of I-10 WB Ramps	16,022	34,204
S of I-10 WB Ramps	11,190	29,346
N of I-10 EB Ramps	11,454	29,346
S of I-10 EB Ramps	3,896	36,375
N of Lincoln	3,896	29,284
S of Lincoln	2,984	10,696
<b>Highland Springs Ave.</b>		
N of Wilson	8,633	40,378
S of Wilson	15,804	36,691
N of Ramsey	19,740	43,593
S of Ramsey	31,930	50,565
N of I-10 WB Ramps	31,930	50,565
S of I-10 WB Ramps	28,600	49,384
N of I-10 EB Ramps	28,635	45,062
S of I-10 EB Ramps	24,503	42,362
N of Sun Lakes	24,503	25,210
S of Sun Lakes	3,600	25,648
<b>Sun Lakes Blvd./First St.</b>		
W of Highland Springs	--	20,214
<b>Highland Home Rd.</b>		
N of Wilson	--	31,332
S of Wilson	--	29,575
N of I-10 WB Ramps	--	39,045
S of I-10 WB Ramps	--	24,283
N of I-10 EB Ramps	--	24,283
S of I-10 EB Ramps	--	12,242
<b>Fields Road</b>		
N of I-10 WB Ramps	3,994	15,736
S of I-10 WB Ramps	10,490	15,736
<b>Interstate 10 WB Ramps</b>		
W of Highland Springs	10,983	13,372
E of Highland Springs	5,715	14,710
W of Highland Home	--	11,475
W of Sunset	3,982	15,832
W of 8 <sup>th</sup>	8,734	16,982
E of Sunset	2,869	7,158

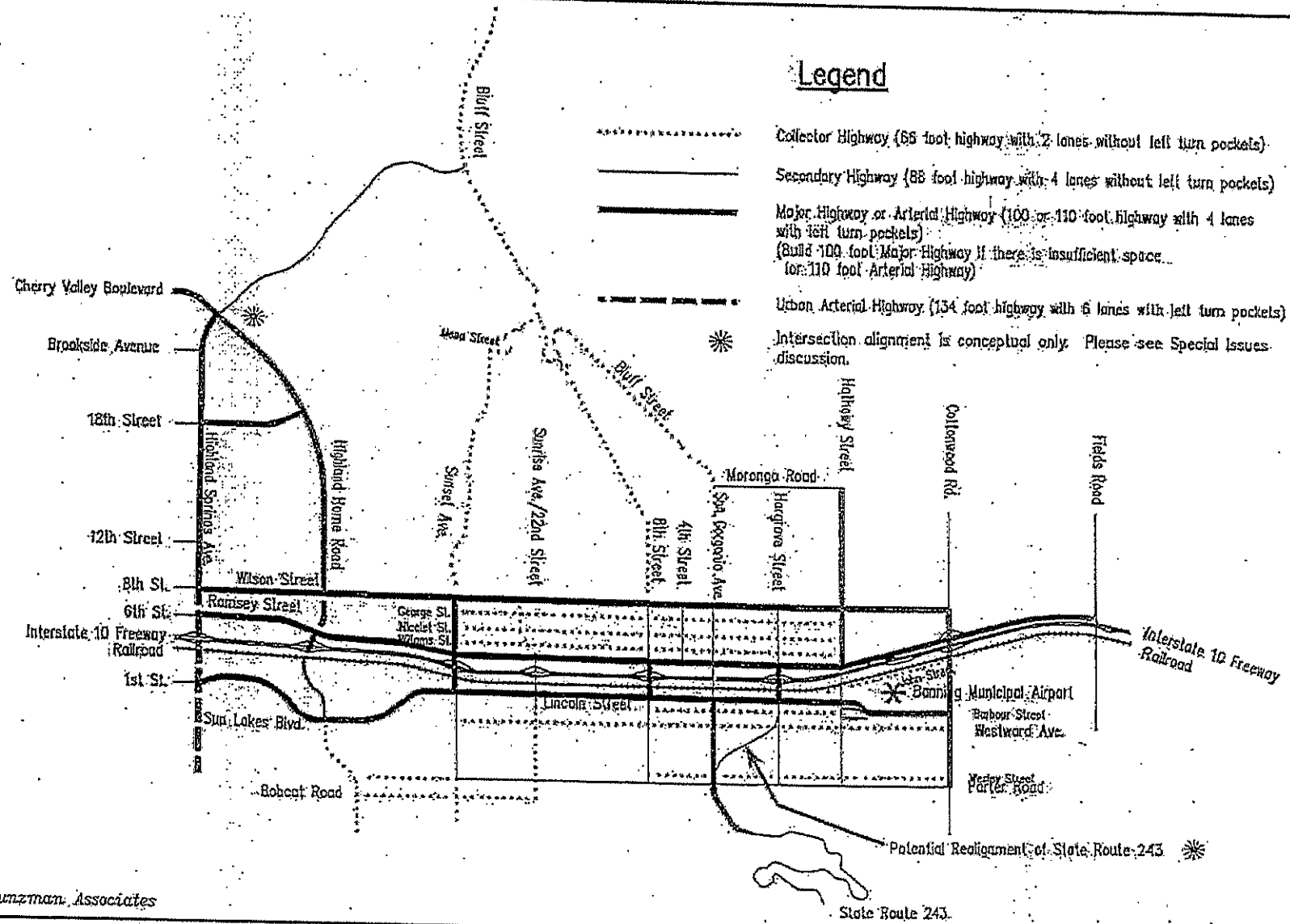
E of 8 <sup>th</sup>	2,709	10,511
W of Hargrave	6,450	24,614
E of Hargrave	2,066	10,847
W of Fields	57	13,138
E of Fields	11,397	11,397
<b>Interstate 10 EB Ramps</b>		
W of Highland Springs	10,972	12,951
E of Highland Springs	--	8,304
W of Highland Home	---	9,622
W of Sunset	4,832	20,859
E of Sunset	2,995	6,034
W of 8 <sup>th</sup>	7,758	15,873
E of 8 <sup>th</sup>	2,525	9,006
W of Hargrave	5,153	24,451
E of Hargrave	2,582	14,725

Based on the trips generated on the roadway, recommendations have been made on the street classifications for these streets, as shown in the following Table. The roadway classifications are also depicted on Exhibit III-6, Proposed General Plan Street System.

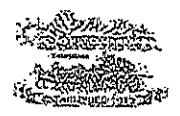


### Legend

-  Collector Highway (66 foot highway with 2 lanes without left turn pockets)
-  Secondary Highway (88 foot highway with 4 lanes without left turn pockets)
-  Major Highway or Arterial Highway (100 or 110 foot highway with 4 lanes with left turn pockets)  
(Build 100 foot Major Highway if there is insufficient space for 110 foot Arterial Highway)
-  Urban Arterial Highway (134 foot highway with 6 lanes with left turn pockets)
-  Intersection alignment is conceptual only. Please see Special Issues discussion.



Source: Kunzman Associates



**Table III-17  
Buildout Roadway Classifications**

Roadway Classification	Roadway
<u>Urban Arterial Highway</u> (6 lanes with left turn pockets)	Highland Springs from Cherry Valley to Potrero Blvd.
<u>Major or Arterial Highway</u> (4 lanes with left turn pockets)	Highland Home from Cherry Valley Blvd. To Sun Lakes Blvd.  Highland Springs from Cherry Valley Blvd. to Wilson Street  Hathaway from Morongo St. to I-10  Sunset from Wilson to Lincoln  8 <sup>th</sup> Street from Ramsey to Lincoln  San Gorgonio from Lincoln to south City Limit  Hargrave from Ramsey to Lincoln  Cottonwood (North – South) from Ramsey to Porter  Wilson from Highland Springs to Cottonwood (North – South)  Ramsey from Highland Springs to <sup>Maj/Ki</sup> Fields Rd.  Sun Lakes/Lincoln from Highland Springs to Cottonwood (North – South)
<u>Secondary Highway</u> (4 lanes without left turn pockets)	Porter from Sunset to Cottonwood (North – South)  Sunset from Wilson to Mesa  Sunset from Lincoln to Porter  22 <sup>nd</sup> Street from Ramsey to south of Lincoln  8 <sup>th</sup> Street from Wilson to Ramsey  8 <sup>th</sup> Street from Lincoln to Porter  4 <sup>th</sup> Street from Wilson to Ramsey  San Gorgonio from Wilson to Lincoln  Hargrave from Wilson to Porter  Hathaway from Ramsey to Porter  Cottonwood (North – South) from Ramsey northward  Cottonwood (North – South) from Porter southward  <sup>Maj/Ki</sup> Fields Road from end to end

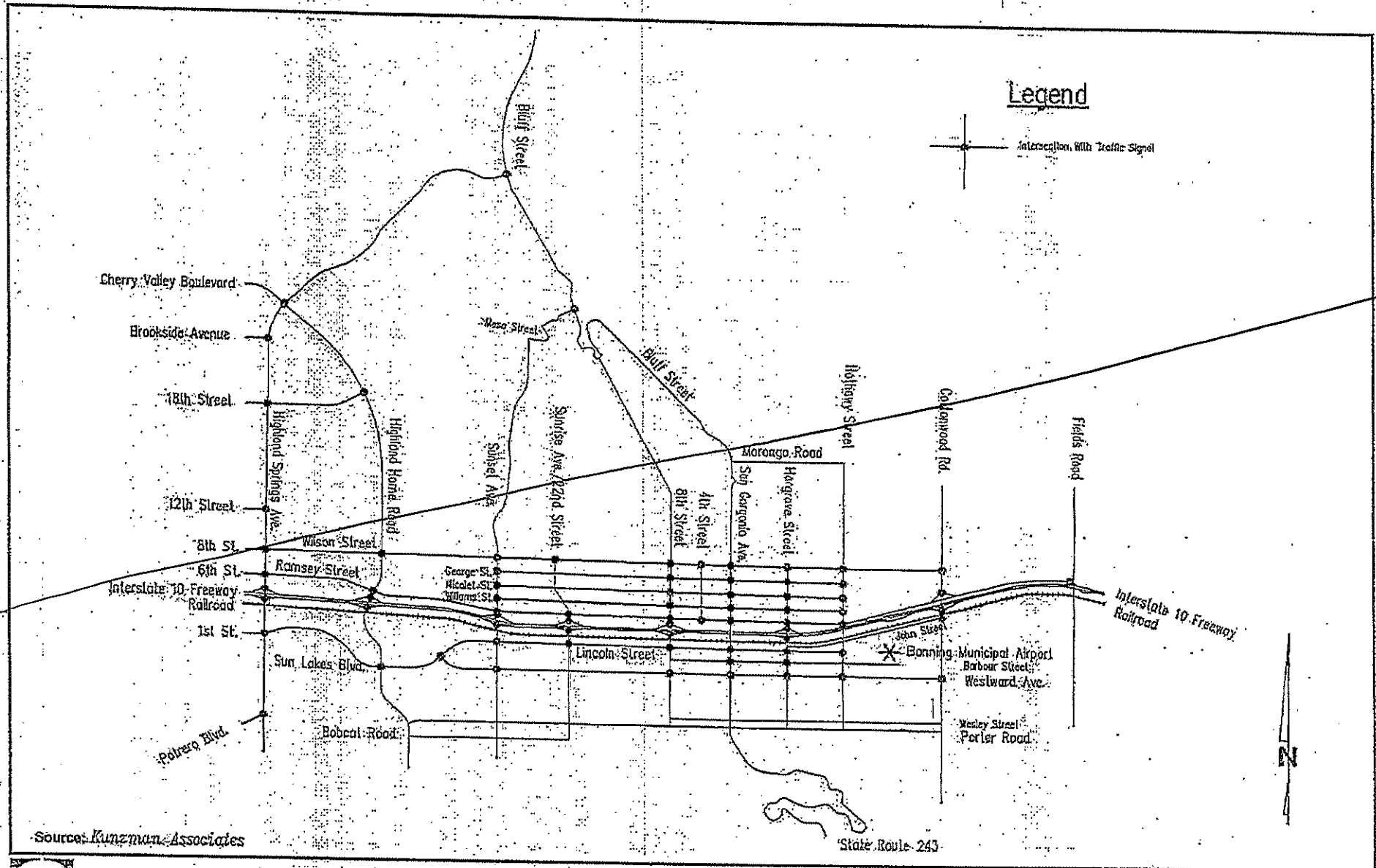
### **Intersection Analysis**

Intersections are the most impacted component of the traffic system. If there are delays, they will occur at intersections, rather than in roadway segments, simply because traffic signals and stop signs increase delays, whereas roadway segments are always "green lights." The City has strived for a level of service (LOS) of C at intersections. This standard, although desirable, is generally unattainable in most of southern California, especially during peak hours.

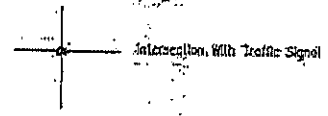
A total of 23 intersections in the City were analyzed to determine whether they would operate at LOS C during peak hours. The analysis found that all street intersections would operate at LOS C, except Highland Springs at Ramsey, Sunset at Ramsey and 8<sup>th</sup> Street at Ramsey, which would each operate at LOS D. At freeway ramp intersections, a LOS D condition would occur at General Plan buildout, except at 8<sup>th</sup> Street at I-10 westbound, which would operate at LOS E; and 8<sup>th</sup> Street at I-10 eastbound, Hargrave at I-10 westbound, and Hargrave at I-10 eastbound, which will operate at LOS F. In order to improve the level of service at these locations, additional interchanges with Interstate 10 are required, as discussed under "Special Issues," below.

### **Traffic Signals**

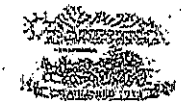
An analysis of the needed traffic signals for General Plan buildout was also conducted. The locations of traffic signal warrants at buildout are depicted on Exhibit III-7.



**Legend**



Source: Kunzman Associates





**All Weather Crossings**

*all caps*

All weather crossings include bridges and culverted streets that allow natural drainages to flow under the roadway during major storms. All weather crossings in the City are described in the following Table.

**Table III-18  
Buildout Bridges**

Location	Needed Improvement
Sunset Ave. at Pershing Channel	1 Bridge
8 <sup>th</sup> Street at Montgomery Creek	1 Bridge
Wilson Street at Smith Creek	1 Bridge
Sun Lakes Boulevard at Smith Creek	1 Bridge
Westward Avenue at Pershing Channel	1 Bridge
Wesley Street at Smith Creek	1 Bridge
Porter Road at Smith Creek	1 Bridge
Porter Road at Pershing Channel, and in two locations at Montgomery Creek	3 Bridges
Ramsey Street at Smith Creek and Pershing Channel	2 Bridges
Lincoln Avenue at east fork of Smith Creek, at west fork of Smith Creek, at Pershing Channel and at Montgomery Creek	4 Bridges

These facilities are needed both to allow the smooth flow of traffic in these areas, and to assure public safety in emergency situations. Construction of these facilities is particularly critical to the development of the south side of the City.

**Transportation Demand Management**

The continued urbanization and growth of the Pass Region will necessitate transportation demand and systems management to preserve and increase available roadway capacity. Transportation Demand Management (TDM) requires the development and implementation of policies, plans and programs that result in the use of a wider range of transportation alternatives, including public transit and bicycles.

TDM can include alternative travel modes, such as carpooling, van pooling and mass transit. In addition, employee flex-time work schedules that reduce peak hour travel and associated traffic congestion may also be considered. The Riverside County Transportation Commission (RCTC), in response to state mandates, has prepared a regional Congestion Management Program. This program required the City of Banning and other cities to prepare TDM ordinances or risk the loss of federal transportation funds. The City has adopted a TDM ordinance.

**Traffic Calming**

Traffic calming is a useful tool available to the City to slow traffic in residential neighborhoods, and discourage shortcuts through these neighborhoods. The advantages of traffic calming must always be balanced, however, with the requirements of emergency vehicles.

Traffic calming can include such design features as curvilinear streets, speed humps and raised intersections, traffic circles or roundabouts, stop signs on through streets, and narrowed intersections. Using these design features improves safety by slowing traffic to 30 miles per hour or less. The restriction or blocking of turning movements can also be used effectively in traffic calming, particularly to reduce cut-through, or short cut traffic in neighborhoods.

## **ALTERNATIVE TRANSPORTATION**

### **Pedestrian, Equestrian, Bicycle and Golf Cart Circulation**

The General Plan and associated documents support planning that allows and enhances access to commercial services and places of employment and recreation without the essential use of motorized vehicles. In this regard, master planning sidewalks, bike lanes, off-street trails and golf cart routes is especially important along major roadways in the community.

#### Pedestrian and Equestrian Trails and Bike Paths

The City's adopted Parks and Recreation Master Plan includes a proposed trails system that would provide access from parks, city streets, and the surrounding community via urban, foothill and wash/hill trails. Two short, minimally developed urban trail segments currently exist in the City and are associated with the proposed trail system. The City has also approved, or is currently reviewing, several equestrian trails within and adjacent to private subdivisions.

There are currently no bikeways within the planning area. Several Class II and III bikeways have been proposed along City streets. However, development of a network of bikeways is constrained by the existing condition of street right-of-ways. Future bike routes should be planned to provide safe routes for intra-City bicycle traffic and should be clearly marked and striped. Where possible, they should be designed as one-way bike routes, which flow in the same direction as adjacent automobile traffic. Combination sidewalks/bikeways require an eight-foot width. These multi-use lanes will also serve as links to recreational facilities throughout the community. Policies and programs specifically addressing the need for trails and bicycle lanes are included in the Parks and Recreation Element.

#### Sidewalks

Within some portions of the City, sidewalks are non-existent or discontinuous, limiting their usefulness as safe alternatives to vehicle travel. As previously noted, rights-of-way along City roadways are inconsistent, limiting the potential for bikeway development. When considering future development, pedestrian and bicycle accommodation and safety should be given emphasis equal to that currently given to automobile access. This is particular true of collector and arterial roadways, where more traffic increases the hazards to pedestrians.

Another area where sidewalks are an important asset is in the historic Downtown of the City (between the I-10 right of way and Williams, and between 8<sup>th</sup> Street and Alessandro). Pedestrian access in this area is a function of economic development for this area, and should be considered in all future development and redevelopment plans.

Conversely, sidewalks on local, neighborhood roadways may not be necessary, particularly in established neighborhoods where roadways have been developed to their ultimate right-of-way,

and the addition of sidewalks would impact front yards and affect the character of the neighborhood.

Finally, sidewalks in proximity to schools provide children with a safe route to school. They are not currently available at all City schools.

#### Golf Cart Facilities

Golf cart use on public rights of way is controlled by state regulations, and provides a convenient, low impact alternative to access convenient commercial development, as well as parks and public services. Golf carts to be used on the public golf cart routes must meet specific physical requirements set forth in the City golf cart plan, must be certified as "road ready" by the City and carry an appropriate permit sticker. Golf cart operators must carry a valid California Driver's license, have proof of insurance, be equipped with seatbelts and appropriate child safety equipment, and be properly maintained. In order to implement a golf cart route system, the City is required to prepare and adopt a Golf Cart Plan and associated implementing ordinances.

Currently there are no existing golf cart facilities in the planning area, outside of private golf development in the Sun Lakes Country Club. Issues related to development of golf cart facilities are similar to those for trail and bikeway development, in that existing facilities that would accommodate development of cart paths do not exist or are discontinuous in some areas.

#### Public Transportation

##### Banning Transit Services

<sup>in 2004</sup>  
Currently (2004) the Banning Municipal Transit System provides fixed route bus service along three routes, two of which are in Banning and one east to Cabazon. There are five City-owned vehicles, three of which are in revenue service and two of which are in reserve. All are powered by compressed natural gas (CNG), and are equipped with bicycle racks. All are ADA compliant, with wheelchair lifts and tie-down stations.

The transit system also operates a Dial-A-Ride service that provides elderly and disabled persons in Banning with curb-to-curb transit services. In addition, Dial-A-Ride is the ADA complementary para-transit provider for the City's fixed route bus service. The Dial-A-Ride program operates two vehicles in revenue service, and one in reserve, all gasoline powered. All Dial-A-Ride vehicles are ADA compliant, with wheelchair lifts and tie-down stations.

In May 2002, the Banning City Council approved the final Pass Area Transit Plan. The Transit Plan establishes the Pass Transit System, which consists of two independent transit systems, the Banning Municipal Transit System and the Beaumont Municipal Transit System. The Plan provides for a coordinated service area including the cities of Banning and Beaumont, the unincorporated areas of Cabazon and Cherry Valley, and the commercial area of the Morongo Band of Mission Indians Reservation. The Plan provides fixed route and dial-a-ride services.

The transit-needs study conducted for the Plan identified service related issues associated with the existing transit systems. It also identified areas not addressed in the Transit Plan requiring additional study or resources. These needs will be addressed in future transit plans, and include

service for non-traditional work shifts in the region, students, elimination of one-way loops to reduce ride time on local routes, adjusting route schedules to provide timed transfers between routes, relocation of transfer points and development of a transit center.

Regional bus service is provided by the Riverside Transit Agency (RTA), which provides services to Hemet/San Jacinto (Route 31), Moreno Valley (Route 35), and Calimesa/Redlands (Route 36), from the bus stop at Kmart on Highland Springs.

#### Railroad

Rail service facilities through the Pass area, and the City of Banning, can carry approximately 60 trains per day, most of which are freight. Regional freight transfer facilities are located in West Colton. Local facilities carry approximately 60 trains per day, most of which are freight. Trains average approximately 7,000 feet in length and generally travel up to 60 miles per hour (mph) in the planning area. UPRR is expanding rail facilities between Los Angeles and Houston, Texas to facilitate increases in rail traffic. Rail lines in the planning area are designated Centralized Track Control (CTC).

There are currently no passenger services available in Banning, although a passenger rail station did operate in the past.

There are ~~currently (2004)~~ four at-grade railroad crossings in the City. These occur at Hargrave Street, San Geronio Avenue, 22<sup>nd</sup> Street, and Sunset Avenue. Grade separated crossings occur at 8th Street and Highland Springs Avenue. The City should explore federal, state, City and UPRR cost sharing arrangements for grade separation of at-grade crossings in conjunction with the California Public Utilities Commission.

Construction of drill spurs is possible to provide rail access to adjoining passenger or industrial uses. Sidings, switches, and additional track linkage would also be required to provide this access. Additional costs would also be associated with any special engineering requirements and potential engineering constraints.

#### Banning Municipal Airport

The Banning Municipal Airport is classified by the National Plan of Integrated Airport Systems (NPIAS) as a General Aviation airport. The airport includes 65 hangars and 32 tie downs, with a 5,200 foot runway.

The airport is capable of accommodating most private single-engine and corporate jet aircraft, as well as helicopters. It averages approximately 10 to 15 takeoffs and landings daily and about 12,000 operations per year. Air traffic is primarily comprised of private, two-engine fixed-wing aircraft. There is no control tower at the airport, so all operations operate under Visual Flight Rules (VFR). Unicom service is provided from 8 a.m. to 5 p.m., every day. Fueling service is also available.

An approximately \$750,000 improvement asphalt overlay project is planned, and will be funded from Federal Aviation Administration grant monies.



### Major Utility Corridors

Major corridors and easements for the transport of natural gas, electricity, communications, domestic water and sewage, and storm drainage are also important components of the Circulation Element. Generally, the need for utility corridors is met through the provision of easements in or adjacent to City streets and along common lot lines.

A major electric corridor occurs immediately south of the Banning Bench. The right of way for the two 220 kV transmission lines in this area is approximately 500 feet in width. Additional 115 kV transmission lines occur in the southern end of the City, and along the San Bernardino Mountains.

Two major high-pressure natural gas pipelines traverse the City, and transport natural gas far beyond the City's boundaries. A 30 inch line occurs under Lincoln Street through the City. Another 30-inch pipeline transects the City in a northwesterly direction from Hargrave and Wilson to Highland Springs Avenue. Two crude oil (16") or petroleum (12") lines occur on the south side of the City, from the airport southwesterly to Wesley, then westerly to the western City limits. Please see Exhibit V-11, Natural Gas and Fuel Lines.

Future land use planning, including the development of subdivisions and the processing of development applications, will require coordination between the City, developers, utility companies, and other service providers to assure the availability and provision of easements and rights of way for the extension of roads, utility lines, and public services.

### FUTURE DIRECTIONS

#### Special Issues

In the development of the <sup>2005</sup> traffic study for ~~this~~ <sup>the</sup> General Plan, six issues were identified which required particular attention. These are individually described below.

#### State Route 243

The City's 1994 Circulation Element included a connection from 8<sup>th</sup> Street to State Route (SR) 243. This connection does not currently (2004) exist, but was proposed as a future roadway. 8<sup>th</sup> Street from Interstate 10 to SR 243 was required to be a Major Highway. The current connection between I-10 and SR 243 is south on 8<sup>th</sup> Street from I-10 to Lincoln, east on Lincoln to San Gorgonio, and south on San Gorgonio to SR 243. Current volumes on SR 243 are approximately 4,000 vehicles per day. At buildout, it is expected that SR 243 will carry approximately 8,000 trips per day. The buildout trips, although double the current trips generated by SR 243, will not impact the City's street system, and can be absorbed on the streets currently used to make this connection, as described above. It was therefore determined that 8<sup>th</sup> Street south of I-10 was required to be a Secondary Highway, not a Major Highway, at buildout of the General Plan, and that the connection to SR 243 was not required for traffic flow.

A direct connection from a State Highway to an Interstate Highway is always preferred by CalTrans. In consultation with CalTrans (California Department of Transportation), the City has considered alternative alignments for SR 243, which are included in the General Plan traffic

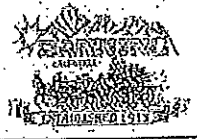
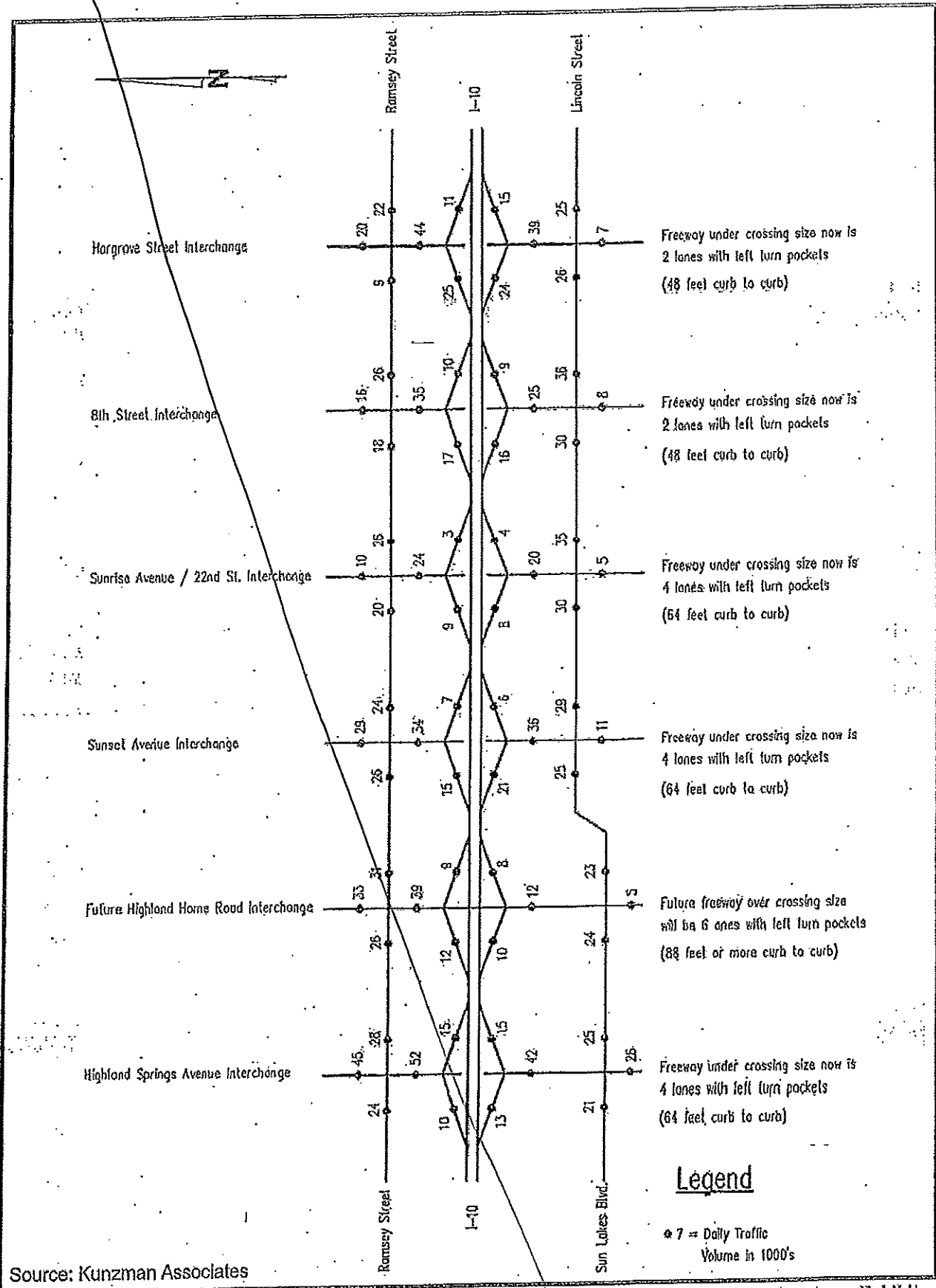
study. Should CalTrans or the City wish to change the current on-street connection to a direct connection, a potential alignment has been depicted on Exhibit III-6, Proposed General Plan Street System. However, since the impacts to the City's streets from SR. 243 traffic is not considered to significantly impact City roadways, realignment should be considered a low priority.

#### Additional Freeway Interchange Capacity

The traffic analysis for this General Plan showed unacceptable levels of service during the peak hour at several I-10 ramp intersections, as discussed above. ~~Exhibit III-8 shows the estimated volumes of traffic at interchanges at buildout of this General Plan.~~ The study considered the potential of expanding existing interchange capacity, however, right of way is severely constrained, the under-crossings existing at 8<sup>th</sup> Street and Hargrave are undersized and would be extremely costly to widen.

As a result, the City will require <sup>an</sup> ~~two~~ additional freeway interchanges, ~~one at Highland Home Road on the west end of the City, and one at Cottonwood Road (North - South) on the east end of the City. The Highland Home interchange, which has been shown as a future interchange in the 1994 Circulation Element, will alleviate congestion at Highland Springs and Sunset interchanges.~~ The added interchange at Cottonwood will alleviate congestion at 8<sup>th</sup> Street and Hargrave interchanges.

CalTrans plans to widen I-10 in the future, by adding one lane in each direction. Given the costs associated with this widening, and the cost of the interchanges, a coordinated construction program of widening and interchange additions would likely be most cost effective.



Highland Home Road/Cherry Valley Boulevard/Brookside/18<sup>th</sup> Street/Highland Springs

The General Plan roadway system has shown Highland Home connecting to Cherry Valley Boulevard in the City of Beaumont. The extension of Highland Home however, could connect to the west at Brookside Avenue in Beaumont. Further, Highland Springs is planned to extend to the northeast to Bluff Street, to provide access to the Black Bench area, and a second connection from the Banning Bench to the City. This connection could also be made through the extension of 18<sup>th</sup> Street to the northeast.

In all cases, traffic flow will not be significantly affected, insofar as traffic volumes on these streets in this area are not expected to be high. The Recommended General Plan Street System shows these streets in their currently envisioned configuration. However, as development occurs and the feasibility of the extensions is considered, flexibility is included in this General Plan to allow changes to the street system in the future.

At Grade Railroad Crossings

The City has two grade separated railroad crossings at streets with I-10 interchanges. The other four existing or planned interchanges must also be improved to include grade separations, in order to maintain acceptable levels of service. A grade separation is included in the Transportation Uniform Mitigation Fee (TUMF) program for the Sunset Avenue interchange. The others are not in the TUMF program.

The City will need to aggressively pursue grade separations for the railroad tracks at all interchanges. This should include the preparation of feasibility studies, the securing of all available funding, and the cooperation of the development community. Although construction of these facilities may not occur in the near term, the planning must be initiated immediately, in order for the City to be able to implement the construction in the future.

Lincoln Street and Westward avenue west of Sunset Avenue

The 1994 Circulation Element included the extension of both Lincoln and Westward from Highland Home to Sunset. Both these roadways occur currently east of Sunset. West of Highland Home, only one roadway, Sun Lakes Boulevard, currently occurs. The traffic study for this General Plan considered the traffic volumes generated south of I-10 on Sunset, and the potential volumes for Lincoln and Westward east of Sunset. The study found that Westward will have sufficiently low volumes so as to require a 2 lane collector east of Sunset. Lincoln is projected as a Major Highway from Highland Home easterly, as is Sun Lakes Boulevard. Traffic volumes will result in LOS C or better for both Lincoln and Westward in this area. Therefore, the elimination of Westward west of Sunset will not have a negative effect on east-west traffic south of I-10.

Level of Service Policy

~~The City has in the past enforced a LOS C policy for City streets, except at freeway interchanges, where a LOS D is considered acceptable. The traffic study for this General Plan found that the City will be able to maintain LOS C on most City streets, except Ramsey. In order to maintain LOS C on Ramsey Street, additional travel lanes would be required at its intersection with Highland Springs, Sunset, 8<sup>th</sup> Street and Hargrave. Some of these lanes would need to be on~~

the 2013

Amendment to revise the city wide LOS standards, Amendment



~~Ramsey, some on the cross street. Without the additional lanes, intersections along Ramsey will operate at LOS D at General Plan buildout. As described at the beginning of this Element, LOS D does not represent a significant degradation in traffic flow. When balancing the need for an efficient traffic system and the widening of streets to accommodate peak hour traffic, it appears that a lowering of the City's requirement for Ramsey Street from LOS C to LOS D will not result in a significant negative effect.~~

Alternative Transportation

As cited above, existing rights-of-way on City streets are not adequate to allow for development of non-motorized transportation. The City has generally been able to secure right-of-way from new development as it occurs to provide full-width mid-block roadway improvements, but the process can be time consuming and costly.

New development should be required to provide separate paths for bicycles and/or equestrians, pedestrians and golf carts to assure safety and avoid conflicts. Equestrian trails should be included in projects developed south of the railroad right-of-way, as well as a connection to the San Bernardino and San Jacinto mountains.

Bicycle and golf cart parking facilities should be integrated into the design of commercial office and public land uses. Connectivity should also be a primary goal of residential design and should emphasize easy accessibility within and between neighborhood and commercial services to maximize the opportunities for pedestrian, bicycle, equestrian and golf cart access by short and direct trips. This planning focus will also help to shorten vehicle trips for residents who must use their automobiles.

Public transport out of the City is limited, particularly into the Inland Empire communities to the west. As development occurs in the City, and increased pressure is brought to provide service and lower vehicular trips on a regional level, additional public transportation will become necessary.

## GOALS, POLICIES, AND PROGRAMS

### Goal

A safe and efficient transportation system.

### Policy 1

The City's Recommended General Plan Street System shall be strictly implemented.

### Program 1.A

Street rights of-way shall be 134 feet for Urban Arterial Highways, 110 feet for Arterial Highways, 100 feet for Major Highways, 88 feet for Secondary Highways, 78 feet for Divided Collectors, 66 feet for Collectors, and 60 feet for Local Streets. Local street standards can be amended as described in Policy 2.

Responsible Agency: Planning Department, Public Works Department, Planning Commission, City Council

Schedule: 2005-2006, Ongoing

### Program 1.B

The City's Public Works roadway standards shall be amended to match the standards contained in this General Plan.

Responsible Agency: Public Works Department

Schedule: 2005-2006, Ongoing

### Program 1.C

Minimum lane width for all City streets shall be designed at 12 feet.

### Policy 2

Local streets shall be scaled to encourage neighborhood interaction, pedestrian safety and reduced speeds.

### Program 2.A

The design of new local streets can vary from the City's standard of 60 foot right-of-way, 40 foot paved width, under the following conditions:

1. The minimum travel lane width shall be 12 feet.
2. Parking shall be provided on at least one side of any public street. Parking lanes shall be a minimum of 8 feet in width.
3. Parking may be eliminated on private streets, if provisions are made in Conditions, Covenants and Restrictions (CC&R's) for enforcement by the Homeowners' Association.
4. Landscaped traffic circles, chokers, and center islands are encouraged, but must meet the requirements of the Fire Department.
5. The minimum parkway width shall be 10 feet.
6. Linear sidewalks are discouraged. Meandering sidewalks, which provide landscaping and street trees adjacent to the curb, shall be included in local street design.

The design of local streets varying from the City's standard, shall be included in the Tentative Tract Map application, and shall be reviewed by the Planning Commission and approved by the City Council.

**Responsible Agency:** <sup>C.D.</sup> ~~Planning~~ Department, Public Works Department, Planning Commission, City Council

**Schedule:** ~~2005-2006~~ Ongoing

#### **Program 2.B**

Existing local streets will be inventoried, and a master plan of potential improvements designed to improve their aesthetic and safety, including landscaped medians, sidewalks and traffic calming devices, shall be developed; cost engineered, and implemented.

**Responsible Agencies:** Public Works Department, Planning Commission, City Council

**Schedule:** ~~2006-2007~~ Ongoing

#### **Policy 3**

The City shall establish and maintain a 5-Year Capital Improvement Program for streets.

#### **Program 3.A**

The Public Works Department shall establish a Capital Improvement Program for 5 years, and update it annually.

**Responsible Agency:** Public Works Department

**Schedule:** ~~2006-2007~~ Ongoing

#### **Policy 4**

Proactively participate in regional transportation planning.

#### **Program 4.A**

Maintain active relationships with the City of Beaumont, the County of Riverside, the Western Riverside County Council of Governments, the California Department of Transportation and the Morongo Band of Mission Indians to share information and promote comprehensive transportation planning in the region.

**Responsible Agency:** Public Works Department, City Manager's Office, City Council, City of Beaumont, County of Riverside, WRCOG, CalTrans, Tribe

**Schedule:** ~~2005-2006~~ Ongoing

#### **Program 4.B**

Aggressively pursue Banning projects in the Transportation Uniform Mitigation Fee (TUMF) program, particularly the addition of projects to the TUMF project list, including grade separated road crossings.

**Responsible Agency:** Public Works Department

**Schedule:** ~~2005-2006~~ Ongoing

**Program 4.C**

Aggressively pursue the design and development of interchanges at ~~Highland Home Road and Cottonwood Road (North - South)~~, including all sources of funding, and the coordination of I-10 widening with their installation.

**Responsible Agency:** Public Works Department, City Manager's Office, City Council, CalTrans, Railroad

**Schedule:** ~~2005-2006~~ Ongoing

**Policy 5**

Consider amendments to the Highland Home/Highland Springs/18<sup>th</sup> Street/Brookside street configurations based on public safety, design feasibility and area needs.

**Policy 6**

The City shall maintain peak hour Level of Service <sup>D</sup> or better on all local <sup>roadways and</sup> intersections, ~~except those on Ramsey Street and at I-10 interchanges, where Level of Service D or better shall be maintained.~~

**Program 6.A**

Periodically review current traffic volumes and the actual pattern of development to coordinate, program and, as necessary, revise road improvements.

**Policy 7**

New development proposals shall pay their fair share for the improvement of street within and surrounding their projects on which they have an impact, including roadways, bridges, grade separations and traffic signals.

**Policy 8**

Traffic calming devices shall be integrated into all City streets to the greatest extent possible and all new streets shall be designed to achieve desired speeds.

**Policy 9**

Street trees within the City right of way shall be preserved, unless a danger to the public health and safety or if the tree is diseased.

**Program 9.A**

Sidewalks in areas with street trees shall be designed to "wrap around" the tree if they are added to an existing neighborhood.

**Responsible Agency:** Public Works Department

**Schedule:** Ongoing

**Policy 10**

Sidewalks shall be provided on all roadways 66 feet wide or wider. In Rural Residential land use designation pathways shall be provided.



**Program 10.A**

The Public Works Department shall prepare an inventory of discontinuous sidewalks on all qualifying roadways, and fund individual projects through the Capital Improvement Program annually.

**Responsible Agency:** Public Works Department, City Council

**Schedule:** ~~Inventory in 2006-2007, Annually thereafter~~ Ongoing

**Program 10.B**

All new development proposals located adjacent to qualifying roadways shall be required to install curb, gutter and sidewalk concurrent with construction.

**Responsible Agency:** Public Works Department, Planning Department

**Schedule:** Ongoing

**Program 10.C**

The City shall develop procedures to address neighborhood sidewalk needs as they are requested by that neighborhood.

**Responsible Agency:** Public Works Department

**Schedule:** ~~2005-2006~~, Ongoing

**Program 10.D**

Work with the School District to develop safe routes to school.

**Responsible Agency:** Public Works Department

**Schedule:** ~~2005-2006~~, Ongoing

**Policy 11**

Sidewalks or other pedestrian walkways shall be required on all streets within all new subdivisions.

**Policy 12**

In the absence of a vehicular grade separation, the City shall aggressively pursue a grade separated pedestrian access across San Geronio, to assure that high school students do not have to cross the railroad tracks on their way to and from school.

**Policy 13**

Pedestrian access in the Downtown Commercial designation shall be preserved and enhanced.

**Program 13.A**

All development and redevelopment proposals for the Downtown area shall include enhanced sidewalk, pedestrian walkway, lighting and landscaping designs and assure connections to existing and planned sidewalks.

**Responsible Agency:** Public Works Department, Planning Department

**Schedule:** As development proposals are presented

**Policy 14**

The City shall aggressively pursue the construction of all weather crossings over General Plan roadways.

**Program 14.A**

The Public Works Department shall prioritize the need for bridges listed in this Element, develop preliminary cost estimates, identify and pursue sources of funding, including developer funding, for each facility.

**Responsible Agency:** Public Works Department, City Council

**Schedule:** ~~2005-2006, Annually thereafter~~ Ongoing

**Program 14.B**

All new development proposals shall pay their fair share of bridge construction needed to serve their project.

**Responsible Agency:** Public Works Department, Planning Department

**Schedule:** Ongoing

**Policy 15**

The City shall develop a Golf Cart Plan compliant with state requirements.

**Program 15.A**

The City shall develop a golf cart plan and associated ordinances and other required implementation programs.

**Responsible Agency:** Public Works Department, City Council

**Schedule:** ~~2006-2007~~ As budget allows

**Policy 16**

Golf cart paths and facilities shall be funded, to the greatest extent possible, by new development.

**Program 16.A**

The routing and facilities required in the Golf Cart Plan shall be incorporated into the Development Impact Fee when the Plan is adopted.

**Responsible Agency:** Public Works Department

**Schedule:** ~~2006-2007~~ As opportunity arise

**Program 16.B**

Golf cart facilities shall be incorporated into new project plans located on golf cart routes.

**Responsible Agency:** Planning Department, Public Works Department, Planning Commission, City Council

**Schedule:** ~~2005-2006~~, Ongoing

**Policy 17**

Encourage the expansion of an integrated Pass transit system.

**Program 17.A**

The City will explore the potential for either bus or rail connection to the Metrolink transit system.

**Responsible Agency:** City Manager's Office, Community Services Department

**Schedule:** ~~2006-2007~~ Ongoing

**Policy 18**

The City shall review its transit service to major regional attractions, and intra-City recreational locations in future planning efforts, based on need.

**Policy 19**

Bus pullouts shall be designed into all new projects on arterial roadways, to allow buses to leave the flow of traffic and reduce congestion.

**Program 19.A**

Bus pullouts will be retrofitted on built-out streets, wherever possible.

**Responsible Agency:** Public Works Department, City Council

**Schedule:** ~~2006-2007~~ Ongoing

**Policy 20**

Promote the location of a passenger rail station for long distance and commuter rail service.

**Policy 21**

Update the Airport Master Plan every five years to meet the needs of the general aviation, business and tourism segments of the community.

**Program 21.A**

Land use designation decisions within the area of influence of the airport shall be specifically reviewed to assure compatibility.

**Responsible Agency:** Planning Commission, City Council

**Schedule:** Ongoing

**Program 21.B**

Work with the Chamber of Commerce, the Morongo Band of Mission Indians, and other interested parties to provide services which meet the needs of passenger and freight transport.

**Responsible Agency:** Airport Management, Economic Development staff, Chamber of Commerce, Morongo Band of Mission Indians, City Council

**Schedule:** Ongoing

**Policy 22**

Maintain an accurate mapping of all utility corridors.

**Program 22.A**

The Building Department shall inventory and map transmission utility easements on the Land Use Map (including electric, fiber optics, natural gas and petroleum).

**Responsible Agency:** Building Department, Planning Department

**Schedule:** ~~Inventory in 2005-2006, Annually thereafter~~ As budget allows

**Policy 23**

The City shall purchase and/or replace its fleet of vehicles with alternate fuel vehicles when available to the greatest extent possible, and shall encourage other agencies to do the same.

**Policy 24**

Public alleys throughout the City shall be maintained to be useful and safe at all times.

**Program 24.A**

The City shall create a downtown alley master plan and where appropriate pave, light and otherwise improve alleys.

**Responsible Agency:** Public Works Department

**Schedule:** Ongoing

**Program 24.B**

The Public Works Department shall inventory all public alleys, determine which are necessary, and vacate those that are not.

**Responsible Agency:** Public Works Department, City Council

**Schedule:** ~~2006-2007~~ *As budget allows*

**Policy 25**

The City shall develop and implement plans for a coordinated and connected bicycle lane network in the community that allows for safe use of bicycles on City streets.

**Program 25.A**

The City shall inventory all streets for potential Class I, Class II and Class III bikeways, and shall program their installation in its Capital Improvement Program.

**Responsible Agency:** Planning Department; Engineering Division; Public Works Department; Planning Commission; City Council

**Schedule:** ~~2005-2006~~ *As budget allows*

**Program 25.B**

Class I bikeways and sidewalks should be installed on both sides of Wilson Street, Ramsey Street, and Lincoln Street, and other major streets where sufficient right-of-way is available.

**Responsible Agency:** Engineering Division; Public Works Department

**Schedule:** ~~2005-2006~~ Ongoing

**Program 25.C**

Class II bikeways and sidewalks should be designated on all existing arterial streets that have sufficient width to safely accommodate bicycle travel lanes.

**Responsible Agency:** Planning Department; Engineering Division; Public Works Department

**Schedule:** ~~2005-2006~~ *As budget allows*

**Program 25.D**

The City should designate Class III bikeways only where Class I and Class II facilities are not feasible.

**Responsible Agency:** Planning Department; Public Works Department

**Schedule:** ~~Continuous~~ *As budget allows*



**Policy 26**

The City should continue to work with the Morongo Band of Mission Indians and neighboring cities and communities to create a regional bicycle and trail network.

**Policy 27**

The City shall provide for a comprehensive, interconnected recreational trails system suitable for bicycles, equestrians and/or pedestrians.

**Program 27.A**

Evaluate the practicality of utilizing flood control channels for multi-use trails, where flooding and safety issues can be accommodated, and negotiate inter-agency agreements for this purpose.

Responsible Agency: Planning Department

Schedule: ~~2005-2006~~ As opportunity arise

**Program 27.B**

Evaluate the practicality of developing a multi-use trails system along the Banning Bench adjacent to and extending into San Bernardino National Forest lands, where environmental and safety issues can be accommodated, and negotiate inter-agency agreements with the U.S. Forest Service for this purpose.

Responsible Agency: Planning Department, U.S. Forest Service/San Bernardino National Forest

Schedule: ~~2005-2006~~ As opportunity arise

**Program 27.C**

Establish a multi-purpose trail between Dysart Park and Smith Creek Park, suitable for equestrian, bicycle and pedestrian use.

Responsible Agency: Community Services Department; Public Works Department; Parks and Recreation Advisory Committee

Schedule: ~~2005-2006~~ Ongoing as development occurs

**Policy 28**

Motorized vehicles shall be prohibited on City trails.

**Program 28.A**

The City shall develop a non-motorized trail system and associated ordinances and other required implementation programs.

Responsible Agency: Public Works Department, Planning Commission, City Council

Schedule: ~~2006-2007~~ As budget allows (bid)

**Program 28.B**

The non-motorized trail system shall be funded, to the greatest extent possible, by new development.

Responsible Agency: Public Works Department

Schedule: ~~2006-2007~~ As development opportunity arises

**Program 28.C**

The routing and facilities required in the non-motorized trail system Plan shall be incorporated into the Development Impact Fee when the Plan is adopted.

**Responsible Agency:** Public Works Department

**Schedule:** ~~2006-2007~~ *As opportunity arises*

**APPLICATION FOR MAJOR LAND USE ACTION REVIEW**  
**RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION**

ALUC Identification No.

ZAP1011BA13

**PROJECT PROPONENT (TO BE COMPLETED BY APPLICANT) City of Banning**

Date of Application January 31, 2013  
 Property Owner City of Banning Phone Number (951) 922-3131  
 Mailing Address 99 East Ramsey Street  
Banning, CA 92220

Agent (if any) Zai Abu Bakar Phone Number (951) 922-3131  
 Mailing Address P.O. Box 998  
Banning, CA 92220

**PROJECT LOCATION (TO BE COMPLETED BY APPLICANT)**

*Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways*

Street Address Citywide General Plan Circulation Element amendment. Policy change on level of service and replacement of Highland Home Interchange  
 Assessor's Parcel No. N/A Parcel Size N/A  
 Subdivision Name N/A Zoning Classification N/A  
 Lot Number N/A

**PROJECT DESCRIPTION (TO BE COMPLETED BY APPLICANT)**

*If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed*

Existing Land Use (describe) Amendment to General Plan Circulation Element to: (1) Change the existing LOS C to D; and (2) replace the designated interchange improvement at 1-10/Highland Home Road with an overcrossing

Proposed Land Use (describe) (1) Changing citywide road level of service from C to D  
(2) Replace the designated interchange improvement at 1-10/Highland Home Road with an overcrossing

For Residential Uses Number of Parcels or Units on Site (exclude secondary units) N/A  
 For Other Land Uses Hours of Use N/A  
 (See Appendix C) Number of People on Site Maximum Number N/A  
 Method of Calculation N/A  
N/A

Height Data Height above Ground or Tallest Object (including antennas and trees) N/A ft.  
 Highest Elevation (above sea level) of Any Object or Terrain on Site N/A ft.

Flight Hazards Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?  Yes  
 No  
 If yes, describe \_\_\_\_\_  
 \_\_\_\_\_  
 \_\_\_\_\_

**REFERRING AGENCY (TO BE COMPLETED BY AGENCY STAFF)**

Date Received	<u>2-6-13</u>	Type of Project
Agency Name	<u>City of Banning</u>	<input checked="" type="checkbox"/> General Plan Amendment
Staff Contact	<u>Zai Abu Bakar</u>	<input type="checkbox"/> Zoning Amendment or Variance
Phone Number		<input type="checkbox"/> Subdivision Approval
Agency's Project No.	<u>General Plan Circulation Element</u>	<input type="checkbox"/> Use Permit
		<input type="checkbox"/> Public Facility
		<input type="checkbox"/> Other

**ALUC REVIEW (TO BE COMPLETED BY ALUC EXECUTIVE DIRECTOR)**

Application Receipt	Date Received _____	By _____
	Is Application Complete? <input type="checkbox"/> Yes <input type="checkbox"/> No	
	If No, cite reasons _____	

Airport(s) Nearby	_____								
Primary Criteria Review	Compatibility Zone(s)	<input type="checkbox"/> A	<input type="checkbox"/> B1	<input type="checkbox"/> B2	<input type="checkbox"/> C	<input type="checkbox"/> D	<input type="checkbox"/> E	<input type="checkbox"/> Ht.	
	Allowable (not prohibited) Use?	<input type="checkbox"/> Yes	<input type="checkbox"/> No	_____					
	Density/Intensity Acceptable?	<input type="checkbox"/> Yes	<input type="checkbox"/> No	_____					
	Open Land Requirement Met?	<input type="checkbox"/> Yes	<input type="checkbox"/> No	_____					
	Height Acceptable?	<input type="checkbox"/> Yes	<input type="checkbox"/> No	_____					
	Easement/Deed Notice Provided?	<input type="checkbox"/> Yes	<input type="checkbox"/> No	_____					

Special Conditions	Describe: _____
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Supplemental Criteria Review	Noise	_____
	Safety	_____
	Airspace Protection	_____
	Overflight	_____

**ACTIONS TAKEN (TO BE COMPLETED BY ALUC EXECUTIVE DIRECTOR)**

ALUC Executive Director's Action	<input type="checkbox"/> Approve	Date _____
	<input type="checkbox"/> Refer to ALUC	
ALUC Action	<input type="checkbox"/> Consistent	Date _____
	<input type="checkbox"/> Consistent with Conditions (list conditions/attach additional pages if needed)	_____
	<input type="checkbox"/> Inconsistent (list reasons/attach additional pages if needed)	_____



**RESOLUTION NO. 2013-01**  
**CONCERNING AIRPORT LAND USE COMMISSION**  
**NON-IMPACT LEGISLATIVE REVIEW FEE**

WHEREAS the Airport Land Use Commission of the County of Riverside (ALUC) is empowered and required by Public Utilities Code section 21670, *et seq.*, to review and process certain local agency land use plans, actions, regulations and permits; and,

WHEREAS, the ALUC is authorized by Section 21671.5 of the Public Utilities Code (PUC) to establish a schedule of fees as necessary to defray the costs incurred for the provisions of such services; and,

WHEREAS, pursuant to Section 21671.5 of the PUC, the ALUC on August 13, 2007, adopted and implemented, a Schedule of Development Review Fees; and,

WHEREAS, the ALUC has found and determined it appropriate to establish a new fee to defray its cost for the review of Non-Impact Legislative projects; Now therefore,

BE IT RESOLVED, FOUND and DETERMINED by the Airport Land Use Commission of the County of Riverside, State of California, assembled on \_\_\_\_\_, 2013, that the following Non-Impact Legislative Review Fee shall be charged to the proponent of the project subject to review and processing:

<u>SERVICE</u>	<u>FEE</u>
<b>Non-Impact Legislative Review</b>	<b>\$375.00</b>

BE IT FURTHER FOUND and RESOLVED that the Non-Impact Legislative Review Fee established hereby shall be in addition to those fees set forth in the adopted Schedule of Development Review Fees.

BE IT FURTHER FOUND and RESOLVED that the Non-Impact Legislative Review Fee does not exceed the estimated reasonable cost of providing the service for which the fee is charged.

BE IT FURTHER FOUND and RESOLVED that the Non-Impact Legislative Review Fee shall replace any other such fee adopted by the ALUC for the review of non-impact legislative projects.

BE IT FURTHER FOUND and DETERMINED that the Non-Impact Legislative Review Fee was adopted pursuant to Section 66016 of the Government Code.

BE IT FURTHER FOUND and DETERMINED that the Non-Impact Review Fee shall be effective immediately on adoption of this Resolution.

The foregoing Resolution was adopted on a motion by Commissioner \_\_\_\_\_ and seconded by Commissioner \_\_\_\_\_ at a regularly scheduled meeting held on the \_\_\_\_ day of \_\_\_\_\_, 2013 by the following vote:

AYES:           Commissioners:  
NOES:           Commissioners:  
ABSENT:        Commissioners:

\_\_\_\_\_  
Chairman, Riverside County Airport Land Use Commission

WITNESS, my hand this \_\_\_\_ day of \_\_\_\_\_ 2013

\_\_\_\_\_  
Director, Riverside County Airport Land Use Commission